

PART F

AREA CHARACTER

OUTCOMES

PREFACE

This Part of the DCP contains a suite of Character Statements and associated objectives and development control provisions applying to specific neighbourhood planning and locality areas within the Local Government Area.

The Character Statements seek to set the desired future character of areas based on a “place management” approach. This Part focuses on tailored controls aiming to achieve a particular character for each unique place, supplementing other provisions applying to particular zones or land uses.

The Local Government Area has been divided into 9 Planning Areas, which are further subdivided into 61 Locality Areas.

Part F of the DCP includes the following sections containing character statements, objectives and development control provisions which apply specifically to each respective Planning Area and Locality Area:

Section 1:	Planning and Locality Areas
Section 2:	North Sydney Planning Area
Section 3:	St Leonards / Crows Nest Planning Area
Section 4:	Cammeray Planning Area
Section 5:	North Cremorne Planning Area
Section 6:	South Cremorne Planning Area
Section 7:	Neutral Bay Planning Area
Section 8:	Kirribilli Planning Area
Section 9:	Lavender Bay Planning Area
Section 10:	Waverton / Wollstonecraft Planning Area



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SECTION 1 PLANNING AND LOCALITY AREAS

1.1 APPLICATION OF THIS PART

The Part applies to all development applications.

Where there is an inconsistency between this Part and Parts B-E of the DCP, the provisions within this Part of the DCP prevail.

Applicants need to refer to both the Character Statement for the Planning Area and the outcomes, objectives and provisions for the Locality Area relevant to their property, as matters covered within a Planning Area Character Statement are not necessarily covered within the requirements for a Locality Area.

1.2 PLANNING AREAS

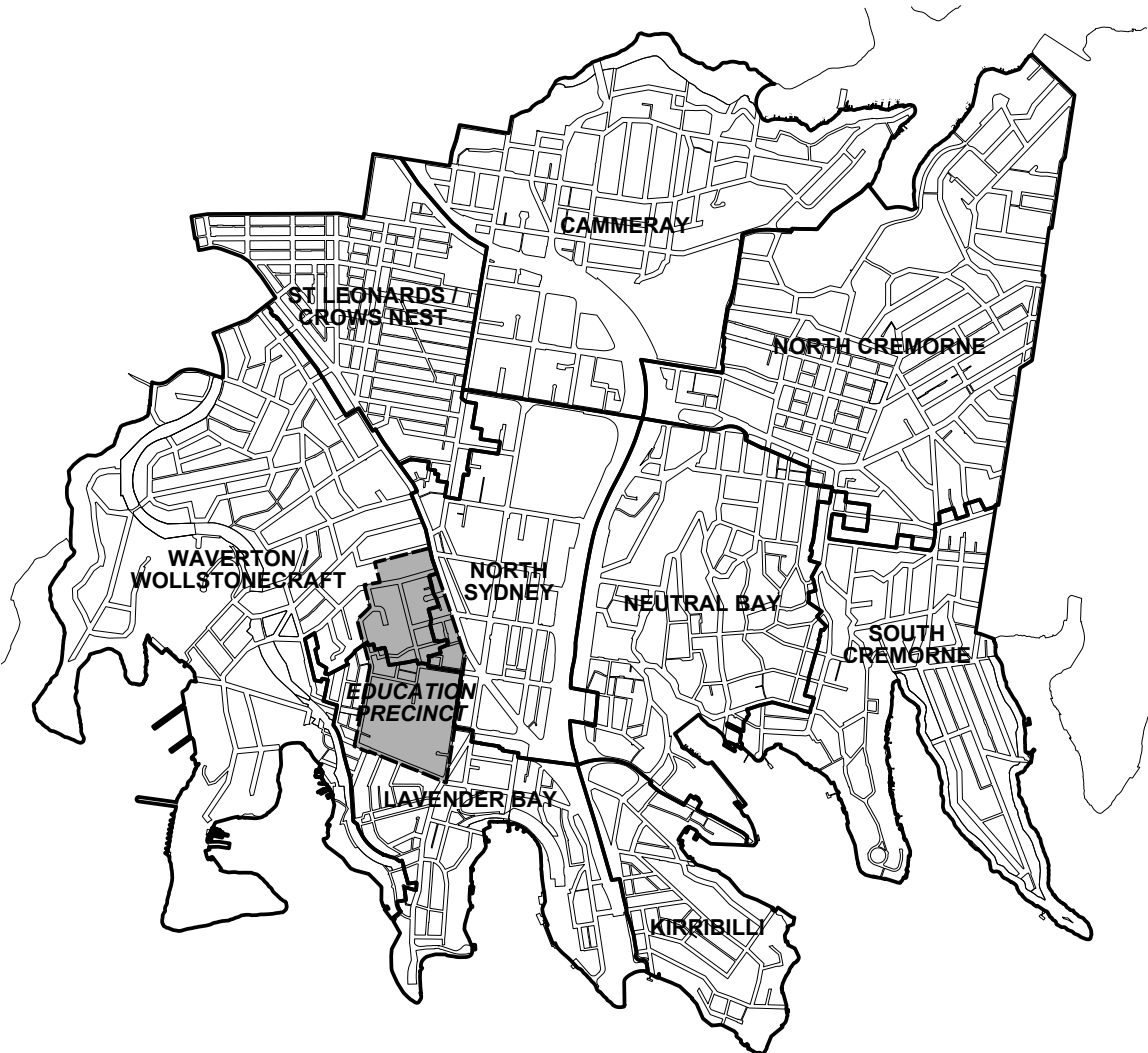


Figure F-1.1: Planning Areas



The Local Government Area is divided into 9 Planning Areas (refer to Figure F-1.1) and generally reflects the extent of each official suburb within the LGA. These Planning Areas are addressed in the following Sections of this Part of the DCP:

- Section 2: North Sydney Planning Area
- Section 3: St Leonards / Crows Nest Planning Area
- Section 4: Cammeray Planning Area
- Section 5: North Cremorne Planning Area
- Section 6: South Cremorne Planning Area
- Section 7: Neutral Bay Planning Area
- Section 8: Kirribilli Planning Area
- Section 9: Lavender Bay Planning Area
- Section 10: Waverton / Wollstonecraft Planning Area

Figures F-2.1, F-3.1, F-4.1, F-5.1, F-6.1, F-7.1, F-8.1, F-9.1 and F-10.1 located in the opening sub-section to each of the above Planning Areas identifies the physical extent of each Locality Area in more detail.

1.3 LOCALITY AREAS

The nine (9) Planning Areas across the North Sydney LGA are further subdivided into a total of 62 Localities. Each Planning Area includes a number of Locality Areas which generally reflect the following hierarchy where applicable:

- Central Business District
- Town Centres
- Village Centres
- Neighbourhoods
- Conservation Areas

The extent of these areas generally reflects one of the following:

- (b) a heritage conservation area,
- (c) common land uses under the LEP, or
- (d) an area exhibiting a generally consistent character.

Each Locality Area is subject to a specific character statement and provisions outlining how the desired future outcomes can be met. The Locality provisions do not necessarily describe the existing character or existing features of any of those neighbourhoods unless those features are to be retained.

In some instances, site specific controls are identified within the Locality Area provisions. These additional controls have been incorporated to ensure that developments on specific sites have a minimal impact on the wider locality. These controls are contained within a sub-section to each Locality Area. Where there is an inconsistency between the site specific controls and those within the relevant Locality Area, the site specific controls will prevail.

The Locality Areas in this Part of the DCP are shown in Table F-1.1.

Section 1 - Planning and Locality Areas

TABLE F-1.1: Identification of Locality Areas	
Planning Areas	Locality Areas
North Sydney	Central Business District
	Civic Neighbourhood
	Eden Neighbourhood
	Hampden Neighbourhood
	Walker/Ridge Street Conservation Area
	McLaren Street Conservation Area
St Leonards / Crows Nest	St Leonards Town Centre
	Crows Nest Town Centre
	Crows Nest Neighbourhood
	Holtermann Estate Conservation Area A
	Holtermann Estate Conservation Area B
	Holtermann Estate Conservation Area C
Camberay	Holtermann Estate Conservation Area D
	Cammeray Village
	Cammeray Neighbourhood
	Anzac Neighbourhood
	Palmer Neighbourhood
	Plateau Conservation Area
North Cremorne	Cammeray Conservation Area
	Neutral Bay and Cremorne Town Centres
	Military Road Island Neighbourhood
	Waters Neighbourhood
	Benelong and Northern Foreshore Neighbourhood
	Murdoch Neighbourhood
	Montague Road Conservation Area
South Cremorne	Oaks Avenue Conservation Area
	Kurraba Point South Neighbourhood
	Kurraba Point Conservation Area
	Cremorne Conservation Area
Neutral Bay	Cremorne Point Conservation Area
	Forthsyth Neighbourhood
	Neutral Neighbourhood
	Whaling Road Conservation Area



TABLE F-1.1: Identification of Locality Areas	
Planning Areas	Locality Areas
Kirribilli	Kirribilli Village Centre
	Kirribilli Neighbourhood
	Careening Cove Conservation Area
	Kirribilli Conservation Area
	Jeffreys Street Conservation Area
Lavender Bay	Milsons Point Town Centre
	McMahons Point Business Precinct
	Luna Park Neighbourhood
	Graythwaite, Shore & St Joseph Neighbourhood
	Lavender Bay Neighbourhood
	McMahons Point Neighbourhood
	McMahons Point North Conservation Area
	McMahons Point South Conservation Area
	Lavender Bay Conservation Area
	Union, Bank, Thomas Street Conservation Area
Waverton / Wollstonecraft	Waverton Village Centre
	Upper Slopes Neighbourhood
	Wollstonecraft Peninsula Neighbourhood
	Gasworks Neighbourhood
	Waverton Village Neighbourhood
	Waverton Neighbourhood
	Waverton Peninsula Neighbourhood
	John Street Waterfront Neighbourhood
	Sawmillers Neighbourhood
	Wollstonecraft Conservation Area
	Crows Nest Road Conservation Area
	Edward Street Conservation Area
	Priory Road Conservation Area
	Bay Road Conservation Area



SECTION 2 NORTH SYDNEY PLANNING AREA

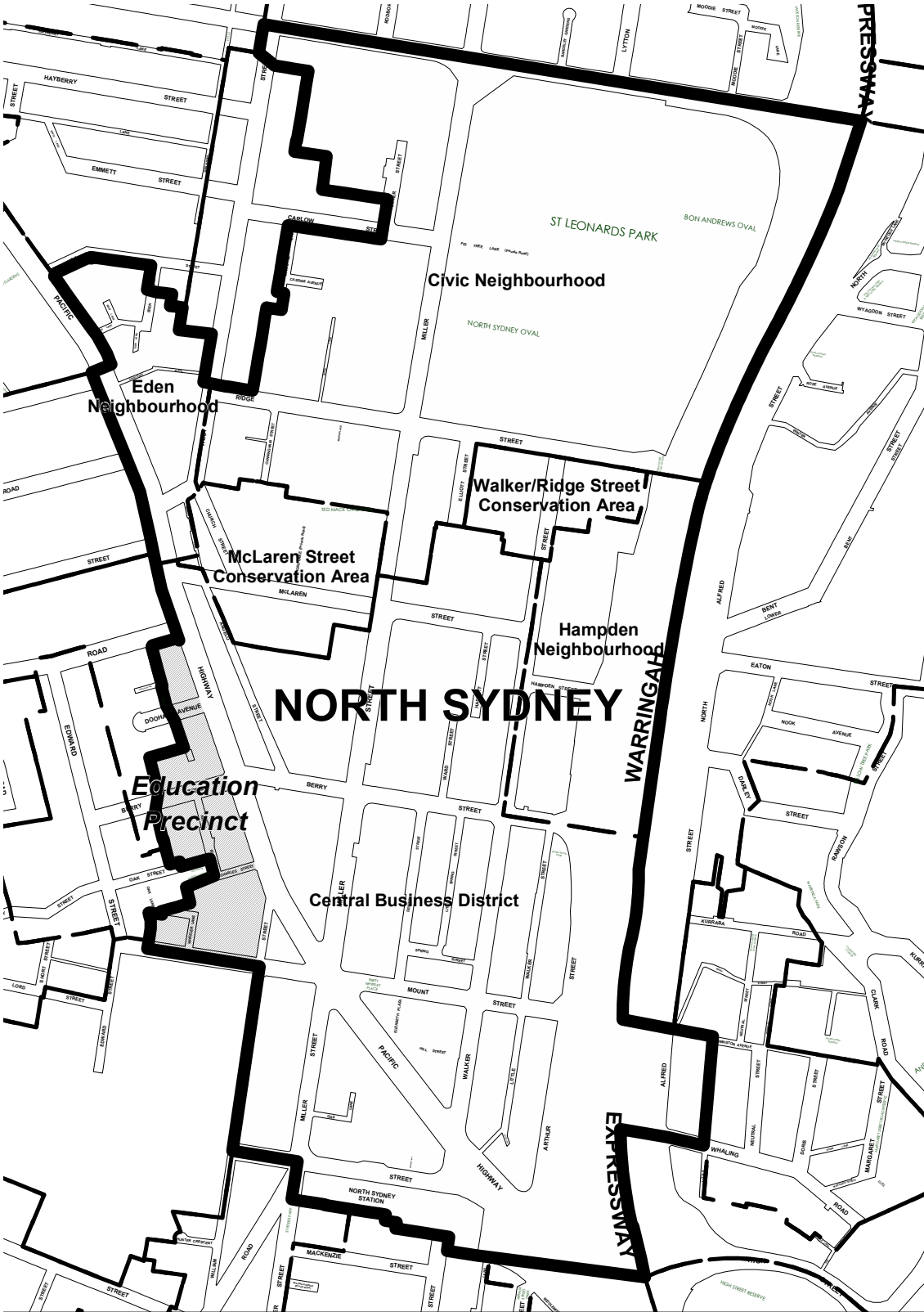
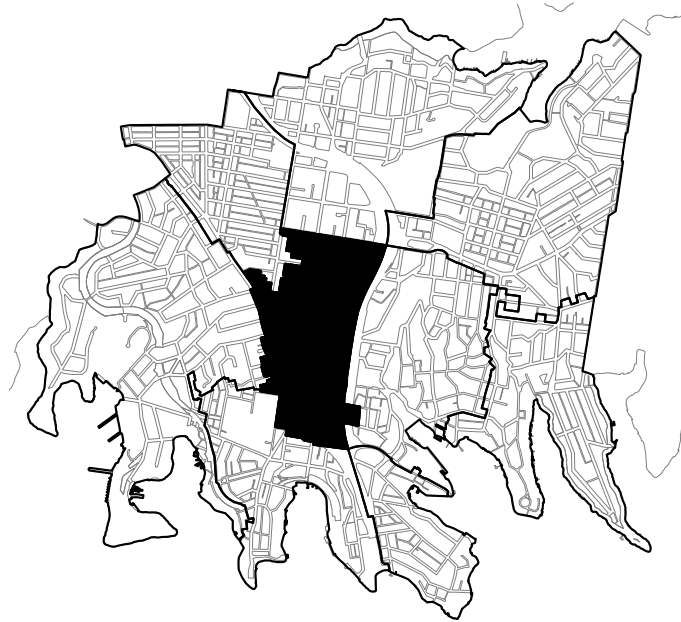


Figure F-2.1: North Sydney Planning Area and associated Locality Areas

2.0 NORTH SYDNEY PLANNING AREA CHARACTER STATEMENT



The North Sydney Planning Area is an iconic, attractive, and sustainable area, with the focus on the North Sydney CBD, which is identified under the Regional Strategy as a global commercial centre. A portion of the Education Precinct is located within the Planning Area on the western edge of the North Sydney CBD and the Ward Street Precinct is located within the northern portion of the Planning Area.

New development within the Planning Area should result in:

- a viable and attractive employment centre
- a diverse range of living, employment, recreation and social opportunities being provided that attract both local and regional populations which contribute to the vibrancy of the North Sydney CBD
- a vibrant and engaging Education Precinct with safe pedestrian networks and a range of formal and informal public spaces for students to study, socialise and engage with the local community
- the creation of a new vibrant public meeting place within the Ward Street Precinct for workers, residents and visitors which has high levels of amenity and is activated in the daytime, evenings and weekends
- a high level of amenity for residents, workers, students and visitors to enjoy
- a high quality built form
- a high level of public transport patronage which is easily accessible to residents, workers, students and visitors
- the area being linked to the Sydney CBD, other suburban centres and many parts of the Sydney Region by rail and bus as well as by road and is a place of interchange between the various modes

Section 2 - North Sydney Planning Area

and where:

Function

- The North Sydney CBD comprises one of Australia's largest commercial centres serving the local population and that of the Sydney region
- There is a mix of uses and activities to meet the demands of the residents, workforce, students and visitors
- There is a civic focus supported by community facilities such as North Sydney Oval, the Stanton Library and the Independent Theatre
- There is an education focus in the Education Precinct, supported by educational establishments such as the Australian Catholic University (ACU), the Sydney Church of England Grammar School (Shore) and North Sydney Demonstration School
- Creation of a new pedestrian focal point centrally within the Ward Street Precinct, through the provision of new public squares which integrate with a highly permeable pedestrian network and is activated by commercial and community frontages
- Community facilities meet the needs of the CBD's working and resident population, students, visitors, and residents of nearby neighbourhoods, in terms of wellbeing, culture and recreation, and add to the diversity and activity of the CBD
- Various grades and sizes of commercial floorspace accommodate a mix of small and larger business, services and retail
- New residential development will not occur in the commercial core, with further high density housing to be concentrated in the areas zoned mixed-use
- There is housing choice in the mix of dwelling sizes and in the range of affordability
- There are active uses outside of standard business hours
- Parks and public spaces are well used and provide for a range of social and recreational activities

Environmental Criteria

- The extremes of sun, wind and rain are mitigated by good building design
- Buildings, public places and streets all receive good access to natural light
- Mechanical noise and other commercial noise is controlled, to protect residential amenity
- Use of local flora extends habitats for native birds and other fauna
- Solar access to special areas and open spaces is maintained and contributes to the enjoyment of those spaces for use by the public
- There is an opportunity to enjoy the views from and within the area towards the Sydney CBD, Sydney Harbour, heritage items and surrounding areas
- Additional public open space is provided for increased worker and residential populations

Built Form

- There is a pleasant, well designed and well-lit series of easily accessible and connected urban plazas and gardens
- The setback on the eastern edge of Miller Street between McLaren Street and Mount Street is maintained and incorporates landscaped areas and actively utilised open space



- Physical breaks are provided between tall commercial towers to afford occupants of commercial buildings reasonable levels amenity in terms of ventilation, daylight access, solar access and views
- Physical breaks are provided between tall commercial towers to afford users of the public domain reasonable levels of amenity in terms of daylight access, solar access, sky views and reduction in adverse wind impacts
- Avoiding continuous, uninterrupted walls of towers to reinforce a human scale when read from the public domain
- The significance of heritage items is retained, and promotes the rich development history and provides interest in the physical fabric of the area
- Heritage items are protected, and significant streetscape elements are conserved in the Walker and McLaren Street Conservation Areas
- Predominant early 20th Century character of the McLaren Street Conservation Area is maintained and protected
- Victorian and Federation character of streets in the Walker Street Conservation Area is maintained and protected
- Pedestrian connections provide alternate east/west and north/south routes through the CBD to promote pedestrian movement

Quality Urban Environment

- There are links to the Sydney CBD, other suburban centres and many parts of the Sydney region by ferry, rail, bus and road and the CBD is a place of interchange between the various transport modes
- Public transport, including walking and cycling, is the main form of access to the North Sydney CBD and the Education Precinct
- Traffic is managed so that pedestrians can move within the area freely and safely, and amenity is maintained
- Parking is managed in a way that maintains pedestrian safety and the quality of the public realm and minimises traffic generation
- The cumulative desired future built form outcome within the Ward Street Precinct does not result in a nett increase in traffic generation for the Precinct
- Rear lanes allow for the primary vehicular access to properties
- Limited increase to the capacity of private parking
- Pedestrians are assisted to safely cross barriers such as the Pacific Highway
- Educational establishments are oriented to the public domain to provide increased surveillance and activation
- Loading and delivery facilities should be located away from the street and where possible be located underground

Efficient Use of Resources

- Energy efficient design and life cycle assessment of buildings enables the conservation of natural resources and minimisation of use of non-renewable energy resources
- Stormwater runoff is minimised, and recycled on-site where possible

Public Domain

- Additional open space is provided to service the increased residential, working and student population of the North Sydney CBD and the Education Precinct
- A new public square/s be provided centrally within the Ward Street Precinct which is integrated with a highly permeable pedestrian network



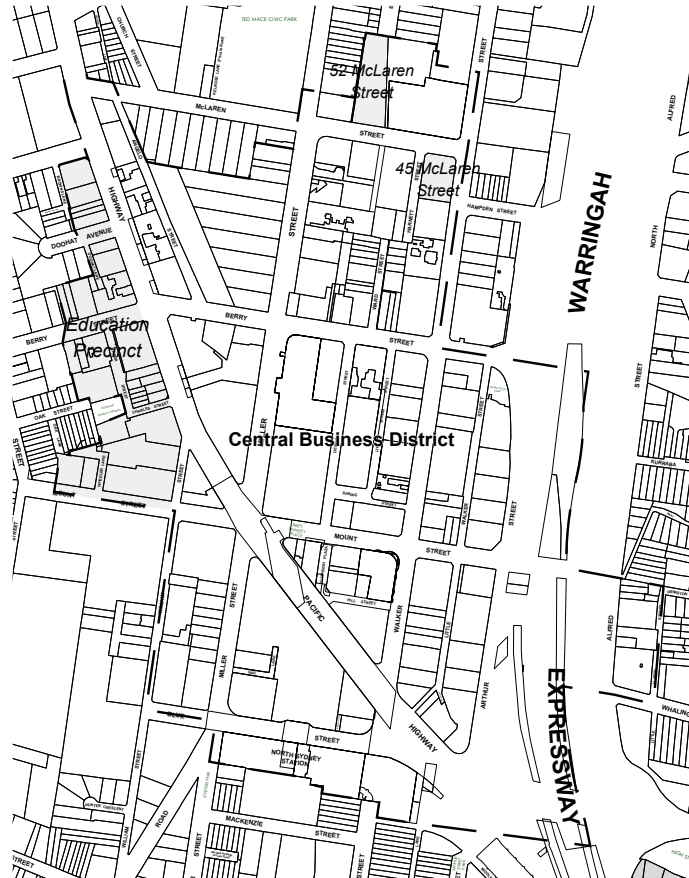
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- Streetscape improvements occur in accordance with the *North Sydney Centre Public Domain Strategy*, *Education Precinct Public Domain Masterplan* and *Ward Street Precinct Masterplan*

The North Sydney Planning Area includes the following Locality Areas in which development is also subject to the provisions below:

- Section 2.1: Central Business District
 - Section 2.1.4: 45 McLaren Street
 - Section 2.1.5: 52 McLaren Street
- Section 2.2: Civic Neighbourhood
- Section 2.3: Eden Neighbourhood
- Section 2.4: Hampden Neighbourhood
 - Section 2.4.4 East Walker Street Precinct
- Section 2.5: Walker/Ridge Street Conservation Area
- Section 2.6: McLaren Street Conservation Area

2.1 CENTRAL BUSINESS DISTRICT



2.1.1 Significant Elements

Land Use

- P1 Predominantly high-rise commercial development.
- P2 Medium to high rise mixed commercial and residential development at the fringes.
- P3 Educational facilities.
- P4 Regional rail and road infrastructure.

Topography

- P5 Typically falling from the north to the south towards Sydney Harbour.

Natural Features

- P6 Natural rock outcrops at 136 Walker Street.

Views

- P7 The following views and vistas are to be preserved and where possible enhanced:
 - (a) From the plaza at No.5 Blue Street and located over North Sydney Rail Station to the Sydney Harbour Bridge.
 - (b) From Doris Fitton Park (160-166 Arthur Street) to Sydney Harbour and Neutral Bay district.

Section 2 - North Sydney Planning Area

- (c) Views along the Pacific Highway to the Post Office on Mount Street from the south-east.
- (d) Views along the Pacific Highway to Sydney Harbour from the intersection with Mount Street.

Identity / Icons

- P8 Brett Whiteley Place.
- P9 Greenwood.
- P10 Post Office and Court House.
- P11 MLC Building.
- P12 Don Bank Museum.

Subdivision

- P13 Predominantly large consolidated allotments within a rigid grid pattern.
- P14 Smaller allotments generally along the Pacific Highway north of Berry Street.

Streetscape

- P15 Wide fully paved footpaths, promoting heavy pedestrian use.
- P16 Active street frontages provided with a variety of shops, cafes and other commercial uses.
- P17 Buildings generally built to the boundary, with entry at street level.
- P18 Continuous awnings provided on commercial buildings.
- P19 Street trees.

Public transport

- P20 Development is to take advantage of the accessibility provided by existing and planned public transport infrastructure.

2.1.2 Desired Future Character

Diversity of activities, facilities, opportunities and services

- P1 High rise and medium density, commercial and mixed use developments.
- P2 Provision of a variety of different sized office, retail, community and entertainment spaces.
- P3 Provision of a variety of outdoor and indoor community spaces (e.g. urban plazas, gymnasium; gardens; outdoor and indoor dining areas and food courts).
- P4 The commercial focus of the CBD is to be enhanced by preventing any further residential development from occurring in its core (i.e. the *E2 Commercial Centre* zone).
- P5 Development above the Victoria Cross metro station will provide significant commercial floorspace, as well as retail, dining and community uses that will contribute to the overall amenity and vitality of the CBD.
- P6 Council will pursue its vision for Miller Street as the civic heart of North Sydney. This will involve significant interventions and public domain improvements aimed at creating a vibrant place for people, with vehicle movements removed or minimised as much as practicable and both sides of Miller Street activated.
- P7 Creation of a substantial new public space within the middle of the street block bound by McLaren, Walker, Berry and Miller Streets, activated by surrounding buildings and new community facilities consistent with the *Ward Street Precinct Masterplan*. This space is to be designed to be highly adaptable for events and prominent activities and



form a spill over space for other community events associated with community facilities to be located in adjacent buildings.

- P8 Brett Whiteley Place is a key public space for the North Sydney CBD which will incorporate an expanded Elizabeth Plaza, as well as portions of Denison Street and Mount Street. This expanded plaza will provide dedicated space for outdoor dining, large and small events, and other activities.
- P9 Consideration be given to expanding Berrys Square to the northern side of Berry Street.
- P10 The Central Laneways Precinct will become a major focal point of pedestrian activity and amenity.
- P11 Active frontages to the Metro site, 1 Denison Street and the MLC building will contribute to the activation of the public domain in the Central Laneways Precinct.
- P12 A new laneway is provided across the redevelopment of 1 Denison Street to link the Metro site, Denison Street, Little Spring Street and Walker Street.
- P13 Alternatives to the current entry of the commercial car park entry at 100 Miller Street (Northpoint) will be pursued to reduce or remove traffic on Miller Street and improve pedestrian amenity.
- P14 Provide roof top gardens and/or public facilities that allow the public and/or residents to access district views.
- P15 Development should maximise opportunities to incorporate retail, restaurant, bar facilities and other non-residential floor space at ground level to promote street level activation, amenity, diversity and place making objectives.
- P16 Provide a diverse mix of higher density, non-residential land uses in the *MU1 Mixed Use* zone of the Education Precinct, including education, shops, cafes, gyms, entertainment and small businesses.
- P17 Provide continuous active uses such as shops and cafes at the ground level of all buildings along Pacific Highway, Berry Street and Napier Street, especially within the Education Precinct.
- P18 Consideration should be given to the inclusion of educational or community-related purposes in the redevelopment of 110, 112, 116 and 120 Pacific Highway and 9 Napier Street.
- P19 Community facilities that are of a type that support the creation of a cultural destination which is activated in the daytime, evenings and weekends are to be incorporated into the lower levels of the redevelopment of 56-66 Berry Street and the Ward Street Carpark site and these facilities are to front onto a new public square/s consistent with the *Ward Street Precinct Masterplan*.

Accessibility and permeability

- P20 Victoria Cross metro station is designed to enhance the North Sydney CBD as a major commercial centre and further encourage the use of public transport. Pedestrians are prioritised throughout the CBD with a number of interconnected pedestrian routes that facilitates all direction movement and encourages fine grain retail and dining uses.
- P21 Consideration be given to providing a direct vertical pedestrian connection between the new public open space located centrally within the Ward Street Precinct and the Victoria Cross Metro Station concourse.
- P22 Barriers to pedestrian movement, particularly within the Ward Street Precinct, Miller Street, Berry Street and the Pacific Highway, will be reimagined such that their function and treatment favour pedestrian movement and amenity.
- P23 New development focuses on the use of public transport, cycling and walking.
- P24 Pick up and drop off points for public transport and taxi ranks are located as close as possible to public spaces and activities, and main building entries.

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- P25 Loading and delivery facilities should be located away from the street and where possible be located underground.
- P26 The following through site links are to be provided, retained and enhanced:
- (a) A north - south pedestrian link from McLaren Street to Elliot Street across 52 and 54 McLaren Street.
 - (b) A north - south pedestrian link from McLaren Street to Ward Street across the vehicle access of 221 Miller Street.
 - (c) An east-west pedestrian link of at least 3m in width is provided from Miller Street to the future public open space off Ward Street along the northern boundary of 201 Miller Street.
 - (d) An east-west pedestrian link of at least 6m in width is provided from Miller Street to the future public open space off Ward Street along the southern boundary of 213-219 Miller Street.
 - (e) An east - west pedestrian link from Miller Street to Ward Street across 221 Miller Street.
 - (f) A north - south pedestrian link from Charles Street to Wheeler Lane across 100 Pacific Highway and 16 Mount Street.
 - (g) An east - west pedestrian link from Harnett Street to Walker Street across 142 Walker Street.
 - (h) An east - west pedestrian link from Harnett Street to Walker Street across 144-150 Walker Street.
 - (i) An east-west pedestrian link from Harnett Street to Walker Street across 45 McLaren Street.
 - (j) An east - west pedestrian link from Walker Street to Little Walker Street across 81-83 Walker Street.
 - (k) An east - west pedestrian link from Little Walker Street to Arthur Street across 100 Arthur Street.
 - (l) An east - west pedestrian link from Napier Street to Oak Street across the southern side of 8-20 Napier Street.
 - (m) A north - south pedestrian link from the green space of 100 Pacific Highway to Wheeler Lane across the western side of 1 Wheeler Lane. The pedestrian link may be made through the building envelope or an alternative architectural treatment that gradually 'reveals' the establishments on either side.
 - (n) An east - west pedestrian link through the Victoria Cross metro station linking Miller Street and Denison Street.
 - (o) A new laneway is provided across the redevelopment of 1 Denison Street to link the Metro site, Denison Street, Little Spring Street and Walker Street.
 - (p) A widened and improved pedestrian link from Little Spring Street and Walker Street across 102 Walker and 110 Walker Street.
- P27 Consideration should be given to the provision of an east - west pedestrian link from the Pacific Highway to Napier Street across either the northern side of 120 Pacific Highway or the southern side of 33 Berry Street.
- P28 Consideration should be given to the demolition of the single-storey structure at the northern end of 105 Miller Street (MLC Building) to improve accessibility and permeability to Miller Street and the Victoria Cross metro station.



2.1.3 Desired Built Form

Subdivision

- P1 Development sites should be of a size which enable:
- (a) the creation of large high quality floor plates which helps to reinforce the Centre's role as a Global City as identified within the Regional Strategy; and
 - (b) the incorporation of adequate setbacks and breaks between tall commercial towers above the podium level to maintain reasonable access to sky views and daylight to the public domain.
- P2 Development on small sites should not detrimentally impact on the long-term ability of the amalgamation of sites for significant commercial development.
- P3 Site amalgamation should occur generally in accordance with that identified within the Ward Street Precinct Masterplan to maximise the delivery of the desired outcomes of the Masterplan.

Form, massing and scale

- P4 Buildings should be carefully designed to minimise the impact of their height and bulk on surrounding residential areas.
- P5 Roof design contributes to building's appearance from a regional view catchment.
- P6 Buildings on 116 and 120 Pacific Highway and 9 Napier Street should be designed such that their bulk steps down from the Pacific Highway to Napier Street to protect sunlight access to the Don Bank Museum and enhance pedestrian amenity to Napier Street.
- P7 No part of a building located above the podium and which exceeds 45m in height may have a horizontal width of more than 60m.
- P8 Development is to be designed to maximise year round solar access to the new public squares to be created centrally within the Ward Street Precinct.
- P9 Despite any other provision in this DCP, development must not result in a nett increase in overshadowing to the main southern square to be created centrally within the Ward Street Precinct between 9am and 4pm from the March equinox to the September equinox (inclusive).

Setbacks

Note. Additional setbacks may be required for any part of the building located above the podium level.

- P10 Buildings are to provide a zero metre setback to all street frontages and adjacent to heritage items, with the following exceptions:
- (a) In accordance with cl.6.4 of the LEP for all properties on the eastern side of Miller Street, north of the Pacific Highway.
 - (b) 5m from the western side of Walker Street, north of 76 Berry Street.
 - (c) 5.4m from the southern side of Berry Street, between Pacific Highway and Miller Street.
 - (d) 14.5m from the western side of Miller Street, at No.60 Miller Street.
 - (e) 15m from the southern side of Mount Street, at No.60 Miller Street.
 - (f) Maintaining the setback of existing buildings to all properties comprising Monte St Angelo Convent and Girls School.
 - (g) Maintaining the setback of existing buildings to all properties on the northern side of McLaren Street.
 - (h) 1.5m whole of building setback from the eastern side of Napier Street, at No. 1-9 Napier Street and No.120 Pacific Highway.
 - (i) 2m setback to the ground level of buildings fronting Arthur Street.

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P11 Buildings must be setback to conserve views to, and the setbacks and settings of, heritage items at 86 and 144 - 150 Walker Street, 94 Pacific Highway (Post Office), 36 Blue Street (Greenwood), 153 Miller Street (MLC Building), 168 - 172 Pacific Highway and 1-7 Napier Street.

P12 The setback of new buildings or alterations and additions to existing buildings on land fronting McLaren Street between Miller and Walker Streets are to match that existing to protect the existing fig trees. Encroachments will only be permitted where the development does not cover the drip line of any of the existing trees.

Podium Heights

P13 Podiums are to be provided in accordance with the Podium Heights Map (refer to Figure F-2.2).

P14 Despite P13, a podium of between 3 and 5 storeys above the finished level of the new public squares to be created centrally within the Ward Street Precinct, where buildings front onto those squares, maintaining a human scale to the squares.

P15 The entirety of a podium must be setback from any street frontage if required under Provisions P10 – P12 to s.2.1.3 to this Part of the DCP.

P16 Podiums are to be built to all side and rear boundaries that do not have a street frontage, except where the site directly adjoins residentially zoned land (i.e. *R2 Low Density Residential*, *R3 Medium Density Residential*, *R4 High Density Residential*) or requires a through site link as required under P23 to s.2.1.2 to this Part of the DCP.

P17 Podium heights should match or provide a transition in height between immediately adjacent buildings.

P18 Podium heights should match the height of adjacent heritage items.

P19 Podium height may be reduced to that part of the building devoted to commercial use in mixed-use buildings.

Above Podium Setbacks

P20 All parts of a building located above the podium are to be setback from the podium's frontage to a street or laneway in accordance with the Above Podium Setbacks Map (refer to Figure F-2.3). Setbacks are to be measured from the outer wall of the podium. Where a weighted setback is required, it applies to the width of the building's façade located above the podium.

P21 Despite P20, no part of any building may be located within 3m of the outer wall of the podium level fronting a street or laneway.

P22 Despite P20 and P21, for buildings fronting Ward Street and the new public squares to be created within the Ward Street Precinct, that part of the building located above the podium is to be setback a sufficient distance to ensure the minimising of potential wind impacts and maintaining a human scale to the squares.

P23 Where a building is to be erected on land zoned *E2 Commercial Centre* or on any other land and the building will not contain residential accommodation, all parts of the building located above the podium must be setback a minimum of 6m from any side boundary, unless:

- (a) the area of the site is less than 1,000sqm; and
- (b) the building does not exceed 45m in height.



Figure F-2.2: Podium Heights Map

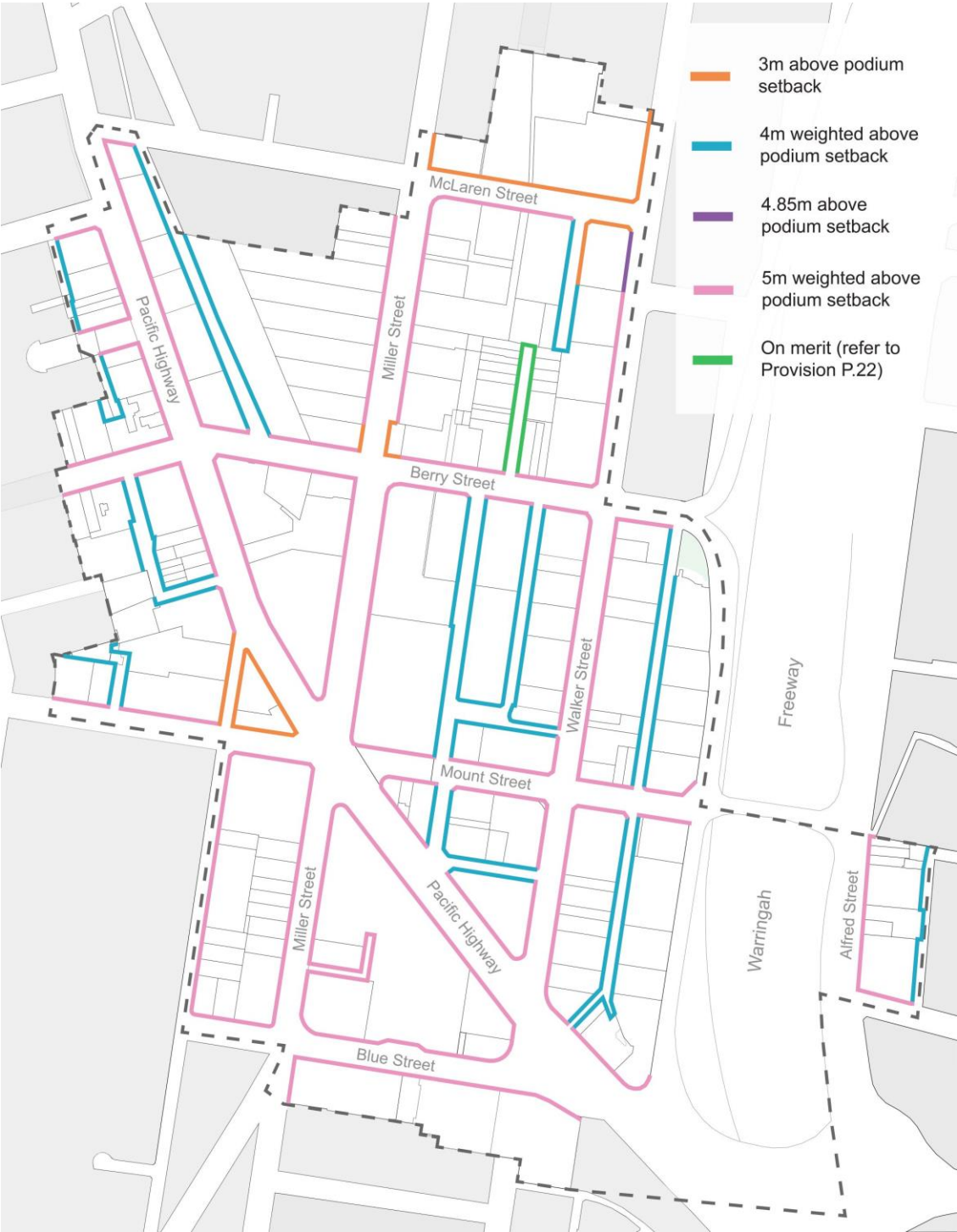


Figure F-2.3: Above Podium Setbacks Map



- P24 Despite P23, no setback is required above the podium level to:
- (a) the southern boundaries of 122 Arthur Street (SP 57439) or 100 Walker Street (Lots 1 and 2, DP 542915), if developed in isolation from sites to their north;
 - (b) the southern boundary of 90 Arthur Street (Lot 1, DP 738262), if developed in isolation from the site to the south;
 - (c) the northern boundary of 20 Berry Street (Lot 1, DP 550167) if developed in isolation from the site to the north;
 - (d) the eastern boundary of 80 Mount Street (Lot 1, DP 566189) if developed in isolation from the site to the east; and
 - (e) the southern boundary of 1 Wheeler Lane (Lot 101, DP 1076397) if developed in isolation from the site to the south.
- P25 Where a building does not contain residential accommodation, all parts of the building located above the podium must be setback a minimum of 6m from any rear boundary that does not have a street or laneway frontage.
- P26 Despite P25, no setback is required above the podium level to the western boundary of 65 Berry Street (Lot 2, DP 1230458) if developed in isolation from the site to the west.
- P27 Where there is more than one tower located above the podium level on land zoned *E2 Commercial Centre* or on any other land and the building will not contain residential accommodation, the towers must maintain a minimum 12m separation from one another.

Building design

- P28 Provide architectural detailing, high quality materials and ornamentation provide a rich visual texture and a symbolic/decorative reference to the history of the place, the building's use or occupant.
- P29 Provide a visually rich intimate pedestrian environment with active street frontages at ground level.
- P30 The natural rock outcrop at 136 Walker Street should be incorporated into the design of any redevelopment proposal for the site.

Awnings

- P31 Continuous awnings must be provided to all commercial buildings, except on the eastern side of Miller Street between the Pacific Highway and McLaren Street.
- P32 Consideration should be given to the provision of weather protection at the pedestrian entrances or over outdoor seating areas for buildings fronting Miller Street between the Pacific Highway and McLaren Street.

Streetscape

- P33 A 'sense of arrival' is established at North Sydney Station with strong linkage to the north to connect with the pedestrian bridge over Pacific Highway and Denison Street.
- P34 The Greenwood historic school building and large Moreton Bay Figs are retained and incorporated as the southern pedestrian gateway to the North Sydney CBD.
- P35 The intersection of Miller Street and Pacific Highway forms an important focal point of the North Sydney Centre with a distinctive character reinforced by the Post Office and the MLC building.
- P36 Improve amenity and safety by installing lighting, public art and/or landscape along the eastern facade of 12-14 Mount Street.
- P37 Provide a consolidated green space over the Don Bank Museum and 100 Pacific Highway. Consideration should be given to extending this green space over the western side of 1 Wheeler Lane to allow for a more direct north – south pedestrian link from Charles/Napier Street to Wheeler Lane.

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- P38 33 Berry Street should be designed such that Napier Street is activated by non-residential tenancies such as commercial or educational facilities (At least 50% of its frontage should comprise of non-residential tenancies at the ground level).

Public Domain

- P39 Have regard to Public Domain designed in accordance with the *North Sydney Centre Public Domain Strategy* and *North Sydney Council Infrastructure Manual*.
- P40 A new multi-purpose public square with a minimum dimension of 52m by 40m is to be created in the centre of the Ward Street Precinct and integrated with a secondary public square of minimum dimension 35m by 33m consistent with the *Ward Street Precinct Masterplan*. The squares are to be designed to maximise the flexibility of uses.
- P41 Consideration be given to activating the secondary northern square within the Ward Street Precinct with a café, public facilities or other active and community uses.
- P42 Consideration be given to expanding Berry Square across to the northern side of Berry Street.

Landscaping

- P43 Continued use of tree planting and use of native vegetation to enhance the urban environment and attract birdlife.
- P44 Choice of trees and vegetation in accordance with *North Sydney Centre Public Domain Strategy*, *Street Tree Strategy* and *North Sydney Council Infrastructure Manual*.

Car accommodation

- P45 Short stay parking spaces should be located within or as close as possible to meeting places.
- P46 Reduce the amount of long stay commuter parking on site.
- P47 Reduce the amount of non-residential parking on site.

Vehicle Servicing

- P48 Consideration to be given to providing all vehicular access to 213-219 Miller Street, 56-66 Berry Street and the Ward Street Carpark site from Harnett Lane and underneath the proposed new public squares, to maximise pedestrian amenity within the Ward Street Precinct.
- P49 Vehicular access to 76 Berry Street is to be provided from Walker Street.

2.1.4 45 McLaren Street





2.1.4.1 Desired Future Character, Design Objectives and Key Principles

- P1 Development is to respond to the scale and character of existing development and desired future character of the surrounding area.
- P2 Built form, scale and massing is to transition in scale across the site from north to south and east to west, whilst respecting the built form of adjacent and neighbouring heritage items.
- P3 Deliver a mixed use development with active frontages to McLaren and Harnett Streets.
- P4 Development is to maximise solar access to the proposed squares within the Ward Street Precinct.
- P5 Maintaining a landscaped setback to Walker Street.
- P6 An improved pedestrian through site link between Walker Street and Harnett Street.

2.1.4.2 Desired Built Form

Objectives

- O1 To provide for increased opportunity for height and density in the North Sydney CBD, in close proximity to public transport and services.
- O2 To allow an appropriate stepping down in built form between 168 and 150 Walker Street.
- O3 To ensure that solar access to the proposed public spaces within the Ward Street Precinct is maximised.
- O4 To achieve appropriate separation distances between existing and proposed buildings and ensure reasonable privacy and solar access is maintained to surrounding dwellings, mindful of the need for renewal at the site.
- O5 To positively relate to the heritage context surrounding the site.

Provisions

Building Height

- P1 Development must not exceed 14 storeys above the crown of McLaren Street as it presents to McLaren Street.
- P2 Development must not exceed 10 storeys above ground level existing fronting its southern boundary.

Side Setbacks

- P3 Buildings must be setback a minimum 1.5m setback to the southern boundary.

Podium Height

- P4 A podium must:
 - (a) comply with the requirements illustrated in Figure F-2.2 to this section of the DCP. and
 - (b) not exceed 3 storeys to the southern boundary.

Above Podium Setbacks

- P5 The following minimum setbacks must be provided above the podium level:
 - (a) the minimum setback illustrated in Figure F-2.3 to this section of the DCP, and
 - (b) 4.5m from the site's southern boundary, and
- P6 Despite Provision P5(a) above, the consent authority may permit a balcony to encroach into the setback area to McLaren street, but only if:
 - (a) The balcony is unenclosed on at least two sides, and

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- (b) The balcony maintains a minimum 1m setback to McLaren Street, and
- (c) The cumulation of balconies on any one level does not exceed 50% of the façade's length to McLaren Street.

2.1.4.3 Landscaped Area

Objectives

- O1 To ensure that landscaping is used to provide appropriate amenity for development and soften the appearance of buildings and their interface with the neighbouring dwellings and the public domain.
- O2 To provide a quantum of landscaping reflecting the transition between the North Sydney CBD and adjoining residential areas.
- O3 To incorporate appropriate materials conducive of its heritage context.

Provisions

- P1 Any basement structure should be setback at least 5m to Walker Street, to ensure the provision of deep soil zones to accommodate trees and vegetation of a scale reflective with the scale of the proposed future development on the site.
- P2 Despite P1, a basement may be permitted within the setback area to Walker Street, but only where it can be adequately demonstrated that the setback area is predominantly soft landscaped and is capable of accommodating trees and vegetation of a scale reflective of the proposed future development on the site.
- P3 Consideration to be given to using sandstone or natural rock features in the landscaping or lower parts of buildings, reflective of the site's heritage context.

2.1.4.4 Through-Site Link

Objectives

- O1 To enhance the existing through site link that runs across the northern boundary of 144-152 Walker Street between Walker Street and Harnett Street.

Provisions

- P1 All buildings are to be setback a minimum of 1.5m from the southern boundary to enable the construction of a through site pedestrian link between Walker Street and Harnett Street which is integrated with the existing through site link across the northern boundary of 144-152 Walker Street.
- P2 Consideration is to be given to the use of sandstone to line surface or any adjacent retaining walls or walls to the through-site link.
- P3 Appropriate lighting be provided to ensure this link provides adequate safety for users.

2.1.4.5 Vehicular Access

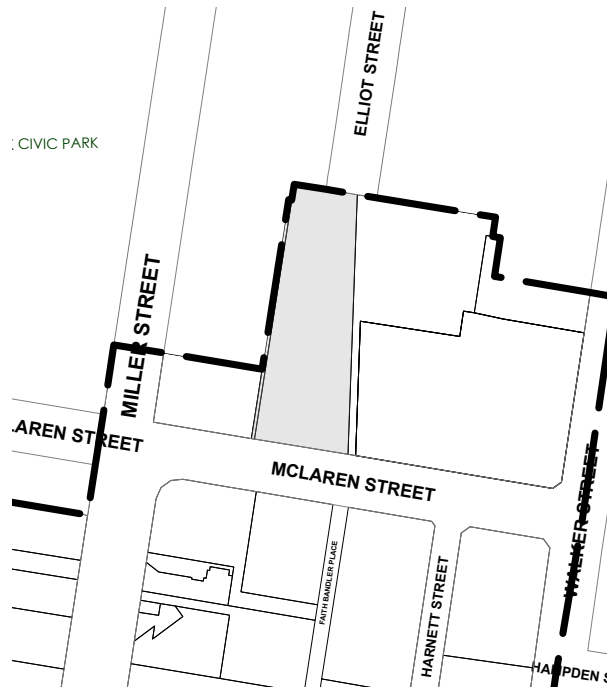
Objectives

- O1 To ensure vehicular and pedestrian conflicts are minimised.
- O2 To maximise activation of street frontages.

Provisions

- P1 No vehicular access is to be provided from McLaren Street.
- P2 Vehicular access should be provided from Walker Street, subject to ensuring that pedestrian safety is maximised.

2.1.5 52 McLaren Street



Note: to remove any doubt, the following controls apply to land known as Lot 2, DP 218407; Lots 1 and 2, DP 1150156; and former drainage reserve in DP 979505.

2.1.5.1 Desired Future Character, Design Objectives and Key Principles

- P1 To allow a scale and form which reflects its proximity to the Northern Portal of the Victoria Cross Metro Station, whilst enabling a suitable transition in scale from the north to the south, and respecting the built form of adjacent development, including neighbouring heritage items.
- P2 Development responds to the scale and character of existing development and desired future character of the surrounding area.
- P3 To ensure development is integrated with and provides a safe and accessible connection with the Northern Portal of the Victoria Cross Metro Station.
- P4 To deliver a mixture of retail, commercial, community and residential uses that positively contribute to the vitality and vibrancy of the North Sydney Centre.
- P5 To provide active frontages at the ground plane to McLaren Street and the future through-site link such that it contributes to an active and safe public domain.
- P6 Maximising solar access to the proposed squares within the Ward Street Precinct.
- P7 Maintaining a landscaped setback to the northern side of McLaren Street.
- P8 Deliver a safe and welcoming dedicated pedestrian through-site link between McLaren Street and Elliot Street.
- P9 Minimise impacts on adjacent sensitive uses.

2.1.5.2 Desired Built Form

Objectives

- O1 To provide for increased opportunity for height and density in the North Sydney CBD, in close proximity to public transport and services.
- O2 To allow an appropriate stepping down in built form between the Ward Street Precinct to the south and mid-rise development to the north.

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- O3 To deliver a slender built form that minimises overshadowing impacts to existing buildings, whilst ensuring building depths and lengths provide adequate light, cross ventilation, and amenity for occupants, visitors and/or workers.
- O4 To ensure that solar access to the proposed public spaces within the Ward Street Precinct is maximised.
- O5 To achieve appropriate separation distances between existing and proposed buildings and ensure reasonable privacy and solar access is maintained to surrounding dwellings, mindful of the need for renewal at the site.
- O6 To maintain a consistent setback to the northern side of McLaren Street.
- O7 To positively relate to the heritage context surrounding the site.
- O8 To ensure appropriate articulation of building facades and 'fine grain' response to the public domain.

Provisions

Solar access

- P1 Despite any other provision of this DCP, any development at 52 McLaren Street must not result in a nett increase in overshadowing to the main southern proposed square to be created within the Ward Street Precinct between 9am and 4pm from the March equinox to the September equinox (inclusive).

Building Height

- P2 Development must not exceed 24 storeys as it presents to McLaren Street.
- P3 Development must not exceed 8 storeys as it presents to its northern boundary at ground level.

Setbacks

- P4 Buildings must be setback:
 - (a) A minimum of 10.5m to McLaren Street, consistent with the general alignment of 168 Walker Street as it presents to McLaren Street, and
 - (b) A minimum of 6m to the eastern boundary, and
 - (c) A minimum of 9.5m setback to the northern boundary, and
 - (d) A minimum of 6m to the western side boundary.
- P5 Despite Provision P4(a) above, the consent authority may permit a setback of 9.5m at Levels 1 and 2 only, and those levels are used for non-residential purposes.
- P6 Despite Provision P4(d) above, the consent authority may permit a setback of 5m at Levels 1 and 2 only, but only along that part of the site that directly adjoins the Victoria Cross Metro Northern Portal and those levels are used for non-residential purposes.

Podium Height

- P7 A 3-storey podium must be provided.

Above Podium Setbacks

- P8 The following minimum setbacks must be provided above the podium level:
 - (a) 10.5m from the McLaren Street frontage, and
 - (b) 6m from the eastern boundary, and
 - (c) 12.5m from the northern boundary, and
 - (d) 6m from the western boundary, and
- P9 Despite P8(b) a 24m building separation must be provided to the building located at 168 Walker Street to the east.



Building Form

- P10 The podium level is to have a different architectural expression to the tower elements above to reflect the different uses of the building.
- P11 That part of the building located above the podium must present as two expressed forms to suitably break the scale and massing of the tower.
- P12 Tower elements located above the podium level must not exceed 45m in horizontal width or length, unless punctuated with a significant visual break or articulation element.
- P13 The building is to be designed to prevent the overlooking of sensitive areas of the adjacent school and proposed childcare centre within.
- P14 Consideration is given to the incorporation of 1.8m high solid balustrades to any communal rooftop terraces to mitigate any potential adverse wind impacts upon the users of the terrace.
- P15 The building is to incorporate façade articulation and fenestration to ensure an appropriate human scale is provided to the elevations.
- P16 A continuous awning must be provided along the full length of the McLaren Street elevation and continue past all main pedestrian entries to the building.

Heritage interface

- P17 Avoid providing blank walls directly facing heritage items or an abrupt imposing built form and presentation.
- P18 Provide a deep soil zone along the western boundary adjacent to No.243 Miller Street, to enable tree planting that will help to soften the transition between development on the site and the adjacent heritage item.

Communal Open Space

- P19 Communal open space may be provided on rooftop areas of any building on the site, but only where it can be adequately demonstrated that there are no resulting adverse visual privacy or acoustic impacts to the adjoining school and residential private open spaces and dwellings.

2.1.5.3 Child Care Centre

Objectives

- O1 To locate the proposed child care centre adjacent to compatible uses.
- O2 To provide sufficient screening of the child care centre from the public domain.
- O3 To ensure a direct physical connection between the internal and external spaces is provided, which facilitates provision for a high degree of child supervision.
- O4 To provide convenient and safe access to the child care centre.

Provisions

- P1 A child care centre is to be located at the ground level of the building at its northern end such that can maximise its access to natural daylight and opportunities for providing a consolidated open space area.
- P2 The child care centre must provide:
- (a) a minimum 450sqm of indoor space; and
 - (b) a minimum 450sqm of outdoor space,
- both at the same level, providing for a minimum of 900sqm in total area.
- P3 The outdoor play area is to be:
- (a) north facing, and

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- (b) predominantly open to the sky, whilst also providing for an outdoor covered area offering sun and rain protection as necessary, and
 - (c) a single consolidated space.
- P4 A dedicated lift must be provided from the child care centre to the basement car parking area.
- P5 The external facade of the child care facility shall incorporate full height screening, using louvres or other architectural treatments to restrict views into the child care centre, whilst also permitting daylight into the child care centre.
- P6 The development shall provide a total of six (6) parking spaces within the basement of the development comprising:
- (a) two (2) all-day designated car spaces for child care staff; and
 - (b) four (4) designated car spaces for child care drop-off and pick-up times.
- These spaces may be utilised outside the child care centre's normal hours of operation for other parking purposes (e.g. residential visitor spaces).

2.1.5.4 Landscaping

Objectives

- O1 To ensure that landscaping is used to provide appropriate amenity for the residents and tenants of the development, including weather protection.
- O2 To soften the appearance of buildings and their interface with neighbouring uses, heritage items and the public domain.
- O3 To maintain and protect existing street trees and canopy trees on-site.
- O4 To reduce the heat island effect and create a user-friendly green space.

Provisions

- P1 The existing fig trees adjacent to the McLaren Street frontage are to be retained.
- P2 Any basement structure/s must be setback a minimum of 6m to McLaren Street, to maximise the extent of the root zone to the existing fig trees. A greater setback may be required if the level of impact on the root zone would adversely affect the health and safety of the trees.
- P3 A landscaped area of at least 450sqm, must be provided along the western boundary, adjacent to No.243 Miller Street and the northern portal to the Victoria Cross Metro Station. The depth and horizontal dimensions of the landscaped area must be capable of accommodating trees and vegetation of a scale reflective of the scale of the proposed future development on the site.
- P4 Podium/terrace areas shall incorporate landscaped elements, including planter boxes or the like, with appropriate dimensions to facilitate mature vegetation to soften the appearance of the development and greening of the through-site link. Podium landscaping shall also wrap around the development and provide for a curtain effect to the car park entry.

2.1.5.5 Through-Site Link

Objectives

- O1 To provide a safe and accessible dedicated pedestrian through-site link connecting McLaren and Elliot Streets contributing to the overall desired pedestrian outcomes under the North Sydney Public Domain Strategy.
- O2 To ensure that development facilitates cross and through movements, enhancing precinct permeability and access to the transport interchange functions of the locality.
- O3 To provide opportunities for increased permeability through the site to adjoining properties.



- O4 To provide for areas within the through-site link that encourage people to rest, meet and socialise.

Provisions

- P1 A continuous and unobstructed 3m wide pedestrian pathway is to be provided along the entirety of the eastern boundary connecting McLaren Street to Elliot Street.
- P2 The ground level of all buildings facing the through-site link shall be designed to accommodate an active frontage.
- P3 The through-site link shall be designed to comply with the requirements of the Disability Discrimination Act.
- P4 The edges of the through-site link shall be softened using landscaped terracing up to the plaza level. Seating is to be provided at intervals along the length of the through-site link integrated into the various terraced levels.
- P5 Consideration is to be given to the integration of a potential future pedestrian through-site link between 168 Walker Street and 54 McLaren Street enabling access through to Walker Street.
- P6 The through-site link is to be appropriately lit to enable safe 24 hour use.
- P7 The through-site link is to be designed to ensure that vehicles are prevented from using the pathway.

2.1.5.6 Vehicular Access & Parking Provision

Objectives

- O1 To ensure vehicular and pedestrian conflicts are minimised.
- O2 To maximise activation of street frontages.
- O3 To minimise traffic congestion in close proximity to mass public transport.
- O4 To protect and retain the existing canopy trees adjacent to the McLaren Street frontage.

Provisions

- P1 Maximum of 1 vehicular access point, off McLaren Street.
- P2 The width of any vehicular access point should be minimised to ensure that pedestrian safety is maximised.
- P3 The location of the vehicular access point must be positioned to retain the existing trees to McLaren Street and delivery of a dedicated pedestrian through-site link along the site's eastern boundary.
- P4 Any structures located between the vehicular access point and the new through-site link must be designed to maximise visibility between drivers and pedestrians, especially at the link's southernmost end.

2.2 CIVIC NEIGHBOURHOOD



2.2.1 Significant elements

Land Use

- P1 Predominantly community based facilities such as educational establishments, places of public worship and public administration buildings.
- P2 Mixed commercial and residential development.
- P3 Passive and active recreational spaces.

Topography

- P4 Generally flat forming the top of the ridgeline, with a fall from the north to the south along Miller Street south of McLaren Street.

Views

- P5 The following views and vistas are to be preserved and where possible enhanced:
 - (a) To Kirribilli and Sydney Harbour from St Leonards Park.

Identity / Icons

- P6 Stanton Library.
- P7 St Leonards Park & North Sydney Oval.
- P8 North Sydney Council Chambers.
- P9 Independent Theatre.
- P10 Civic Centre and Park.
- P11 Warringah Freeway a major arterial thoroughfare.
- P12 Miller and Falcon Streets a major sub-arterial thoroughfare.

Subdivision

- P13 Regular grid pattern with rectangular lots of varying sizes.

Streetscape

- P14 Wide fully paved footpaths.



P15 Buildings are aligned to the street.

P16 Awnings generally provided for shops, cafes and other commercial uses on Miller Street.

P17 A variety of street trees and shrubs.

Public transport

P18 Development is to take advantage of high accessibility to high frequency public bus services along Miller and Falcon Streets.

P19 Public transport, cycling and walking are the main forms of public transport.

P20 Traffic calming measures along Miller Street, Falcon Street, and Ridge Street.

2.2.2 Desired Future Character

Diversity of activities, facilities, opportunities and services

P1 A variety of specialty uses including education, public services, community facilities and recreational parks.

P2 Active street frontages from such activities as cafes and shops at ground level on both Miller and Ridge Streets.

P3 Limited residential development in the form of dwelling houses, semi-detached dwellings, multi dwelling housing and residential flat buildings according to zone.

P4 Open space used by local residents, students and the wider regional population for social and recreational purposes.

Accessibility and permeability

P5 Pedestrian connections are to be provided to Stanton Library from Ridge and Miller Streets.

P6 Pedestrian connections through Civic Park and St Leonards Park.

P7 Pedestrian routes are as direct as possible with direct through views from adjacent streets or public domain.

Public spaces and facilities

P8 Cohesiveness throughout the area is achieved through landscaping and tree planting.

P9 Safe pedestrian links with improved lighting and passive surveillance.

Junction and termination of streets

P10 Junction at Miller and Falcon Streets provides a gateway to North Sydney Centre.

2.2.3 Desired Built Form

Form, scale and massing

P1 Generally 1-3 storeys in height with a strong relationship with adjacent building heights.

P2 Development is to be designed to maximise solar access year round to the new public squares to be created within the Ward Street Precinct.

P3 Development must not result in a nett increase in overshadowing to the main southern square to be created within the Ward Street Precinct between 9am and 4pm from the March equinox to the September equinox (inclusive).

Setbacks

P4 Minimum front setback of 3m for residentially zoned land with landscaping, including trees and shrubs provided at street frontage.



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Fences

P5 Front fences no higher than 800mm.

Gardens

P6 Substantial planting including trees in front gardens to complement street trees.

Car accommodation

P7 Short stay metered car parking for visitors on-street.

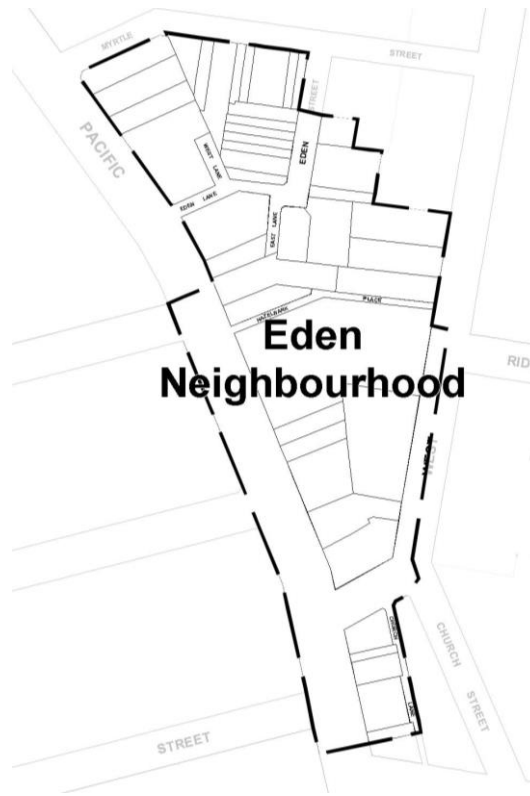
P8 Retention and enhancement of off-street public car parking at the Ridge Street car park.

Streetscape

P9 Avenue of trees to line roads provides attractive streetscape and provides borders to the road.

P10 Encourage open street frontages with low fences or no fences, landscaping to complement street planting.

2.3 EDEN NEIGHBOURHOOD



2.3.1 Significant elements

Land Use

P1 Predominantly commercial and mixed commercial and residential development.

Topography

P2 Slight falls to the south west from West and Myrtle Streets.

Views

P3 The following views and vistas are to be preserved and where possible enhanced:

(a) Views to the North Sydney CBD along the Pacific Highway.

Identity / Icons

P4 Union Hotel.

P5 Freemasons Hall (Wellbeing Centre).

P6 Pacific Highway, a major sub regional thoroughfare.

Subdivision

P7 Irregular grid pattern, due to the streets aligning with the topography.

Streetscape

P8 Wide fully paved footpaths.

P9 Buildings built to street along the Pacific Highway.

P10 Awnings provided along the Pacific Highway.

P11 Irregular planting of street trees and shrubs.

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Public transport

- P12 Development to take advantage of high levels of accessibility to high frequency bus services along the Pacific Highway.

2.3.2 Desired Future Character

Diversity

- P1 Diverse range of activities including residential, public services (including the postal service and police) commercial and retail.
- P2 A variety of different sized non-residential spaces (e.g. smaller showrooms, small offices).
- P3 Pick up and drop off points for public transport and taxi ranks are located close to public spaces and activities, and main building entrances.

Accessibility and permeability

- P4 The following through site links are to be provided, retained and enhanced:
- (a) An east-west pedestrian link along Hazelbank Place from West Street to Pacific Highway, with good lighting and paving.
 - (b) New pedestrian links which encourage access along Eden Lane to Myrtle Street, West Street and Pacific Highway.
 - (c) New pedestrian links which encourage access to and from Pacific Highway, West Street and Ridge Street.
- P5 Through site links are made safe through adequate lighting and passive surveillance.

2.3.3 Desired Built Form

Form, massing and scale

- P1 A variety of building heights in the mixed use area with the average height being 4 storeys.
- P2 Buildings adjacent to conservation areas transition in height to match building height in conservation areas.

Podiums

- P3 A podium of 4 storeys to all streets, with a weighted setback of 3m above the podium with the following exceptions:
- (a) A podium of 4 storeys to Eden Street, Eden Lane and Myrtle Street, with a setback of 1.5m above the podium.
 - (b) A podium of 3 storeys to Hazelbank Lane and Church Lane, with a setback of 1.5m above the podium.

Building Design

- P4 Building form differs throughout the area, with an emphasis on quality mixed use design to complement heritage items.
- P5 Form, scale and massing reflects surrounding development to provide visual interest through a range of detailing such as recesses, balconies and a variety of materials.
- P6 Ground floor commercial/retail uses to provide activity and interest along Pacific Highway, Eden Street and Eden Lane.
- P7 Active uses along Eden Street and Eden Lane such as outdoor dining to encourage activity outside standard business hours.
- P8 Mixed use development complements lower scale residential development in adjoining conservation areas.



Noise

- P9 Elevations of buildings fronting the Pacific Highway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

Awnings

- P10 Consistent awnings along Pacific Highway to provide shelter from weather conditions.

Car accommodation

- P11 Short stay metered on-street parking for visitors.
- P12 Access to underground car parking should be provided through a single combined entry and exit.

Public Spaces and facilities

- P13 Footpath paving along property frontages in accordance with Council's specifications.
- P14 Roof top gardens and public facilities that allow public access to district views from higher floors.
- P15 Encourage active uses outside standard business hours to encourage active streets and street surveillance.
- P16 Locate any outdoor dining within clearly defined areas located away from main roads; provide weather protection providing equal and unobstructed pedestrian movement.

Streetscape

- P17 Streetscape to provide consistency with surround areas with street tree planting and use of street furniture.
- P18 Tree lined streets encourage birdlife and lessen impact to traffic noise.



2.4 HAMPDEN NEIGHBOURHOOD



2.4.1 Significant elements

Land Use

- P1 Residential accommodation.
- P2 Educational establishments.

Topography

- P3 Moderate falls to the south from Ridge Street and steep falls to the east from Walker Street to the Warringah Expressway.

Views

- P4 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Maintain views of Kirribilli and the Harbour from Walker Street.
 - (b) Strong vista along Walker Street to southern part of CBD.

Identity / Icons

- P5 North Sydney Club.
- P6 Warringah Expressway, a major arterial thoroughfare.
- P7 Sandstone wall in the middle of Walker Street.



Streetscape

- P8 Tree lined streets with grassed verges and concrete footpaths.
- P9 Split level streets to Hampden Street.
- P10 Landscaped medians on Hampden Street.
- P11 Double rail timber fences on Hampden Street.
- P12 Low front fences of brick or masonry on Walker Street.
- P13 Residential flat buildings are setback from the boundary and aligned with the street frontage.

Public transport

- P14 Development to take advantage of reasonable proximity to high levels of public bus and train services.
- P15 Public transport, cycling and walking are the main forms of public transport.

2.4.2 Desired Future Character

Diversity

- P1 A mixture of modern multi-level residential flat buildings with older low rise residential flat buildings.
- P2 Retention of the 2-3 storey original attached dwelling houses on Hampden Street.

Accessibility and permeability

- P3 Pedestrian walk along the western side of Warringah Expressway accessed from Hampden Street.

2.4.3 Desired Built Form

Form, scale and massing

- P1 Early and original residential buildings complement the topography to maintain views and easy access.
- P2 Ground floors may not relate to street level due to topography of the area, with high sandstone retaining walls at ground level.
- P3 Generally a maximum of 2 storeys on Hampden Street.
- P4 Development is to be designed to maximise solar access year round to the new public squares to be created within the Ward Street Precinct.
- P5 Development must not result in a net increase in overshadowing to the main southern square to be created within the Ward Street Precinct between 9am and 4pm from the March equinox to the September equinox (inclusive).

Setbacks

- P6 Maintain existing setbacks from property boundary along the eastern side of Walker Street and the northern side of Berry Street.

Fences

- P7 Low fences (max 800mm).
- P8 Small picket fences above sandstone bases.

Car parking

- P9 Located off-street and below ground for all residential flat buildings and multi dwelling housing.
- P10 Located on-street parking for all heritage listed attached dwellings.

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P11 Short term on-street meter parking.

P12 Existing sandstone retaining walls on street frontages must not be breached to accommodate garages or car parking spaces.

Streetscape

P13 Heritage features such as Walker Street and Hampden Street sandstone walls.

P14 Substantial gardens within front setback area.

P15 Steps and pathways along Walker and Hampden Streets are maintained.

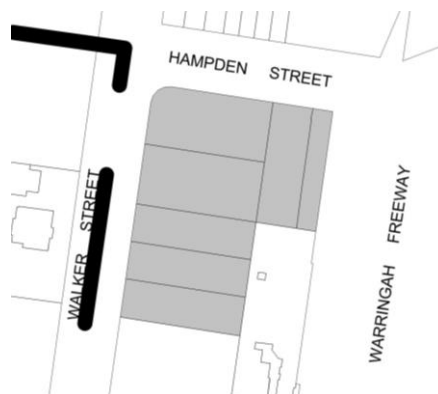
P16 Landscaping in front gardens/private open space.

P17 Tree lined streets and mature vegetation on median enhances area.

Noise

P18 Elevations of buildings fronting the Warringah Freeway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

2.4.4 East Walker Street Precinct



Where there is an inconsistency between the East Walker Street Precinct Controls and the Hampden Neighbourhood Controls, the East Walker Street Precinct Controls prevail.

Building Setbacks

Front setbacks

P1 2m landscaped setback to Walker Street.

P2 2m landscaped setback to Hampden Street.

Side setbacks

P3 Nil side setbacks to the eastern side property boundary of 17 Hampden Street.

Rear setbacks

P4 12m for Walker Street properties (173-179 Walker Street).

P5 12m for Hampden Street properties (15-17 Hampden Street).

Podium Height

P6 2 storeys to Walker Street.

P7 2-3 storeys to Hampden Street.

P8 The Hampden Street podium is to respond to the rhythm and grain of the adjacent heritage properties through the incorporation of vertical proportions.



Above Podium Setbacks

P9 3m above the podium level.

Building Separation

P10 12m separation distance above the podium, is to be provided between the two building forms.

P11 The building separation is to provide a view corridor through the site.

2.5 WALKER/RIDGE STREET CONSERVATION AREA



2.5.1 History

The Walker/Ridge Streets Conservation Area was subdivided as part of the Town Plan for North Sydney, known at the time as St Leonards, but remained largely undeveloped until the 1880s. Some of the first buildings were "Lamona", built by Dr Kelynack in 1883, "Park House" built by Francis Punch in 1886, followed by "St Helens" 1889.

Land on the western side of Walker Street was owned by Francis Lord as part of an estate surrounding his house "The Lodge". A smaller block was owned by William Tucker. Subdivision and development on the western side of Walker Street occurred in the late 19th century, mostly for private homes. The area was once comprised part of "the Macquarie Street of the north" due to the number of doctors surgeries and cottage hospitals. A portion of the area is now occupied by Wenona School.

The main building period is 1880-1900.

2.5.2 Description

The Walker/Ridge Streets Conservation Area includes the eastern end of Ridge Street and the sloping, northern portion of Walker Street. It is bounded St Leonards Park to the north, Elliot Street to the west and the Warringah Expressway to the east.

The landform falls to the south. Subdivision is determined by the grid pattern of the streets and lot sizes are irregular and many are developed for attached housing.

The area is characterised by intact groups of single and two storey detached and attached dwelling houses in a mix of Victorian Italianate and Federation styles with St. Helens being a remnant of the Victorian Georgian. The main buildings at the northern end of Walker Street are substantial residences in the Federation Arts and Crafts and Queen Anne styles. There is some modern residential infill and educational buildings associated with Wenona.

Street verges are 3.5 metres wide with concrete footpaths and lined with regularly spaced street trees.

The vista towards the War Memorial in St Leonards Park up Walker Street is a significant feature of the Conservation Area and there are views from the eastern edge to Sydney Harbour. Original fencing and retaining walls are important unifying elements in the streetscape.

Mature landscaping is a unifying and significant feature of the Conservation Area and the gardens to Walker Street contribute to this. There are street trees to Walker Street.

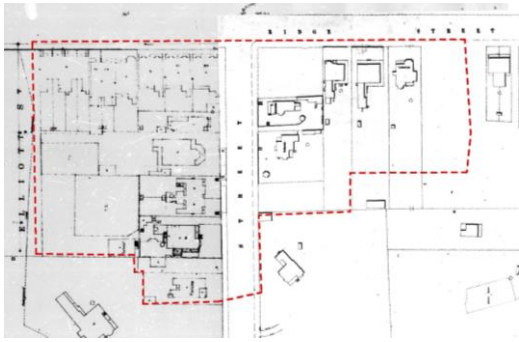

Uncharacteristic elements include contemporary multi-storey buildings; over-scaled, two storey additions; over-scaled and poorly detailed carports and garages; front and side

dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street.

2.5.3 Statement of Significance

The Walker/Ridge Streets Conservation Area is significant:

- (a) For its late 19th and early 20th century character defined by the number of intact heritage items in the area.

	<p>Figure F-2.5.1 (left): Circa 1890</p>
	<p>Figure F-2.5.2 (below left): Circa 1943</p>
	<p>Figure F-2.5.3 (below): Circa 2008</p> 

2.5.4 Significant elements

Topography

- P1 Steeply sloping to the south along Walker Street from Ridge Street.

Subdivision

- P2 Lot sizes – 700m² to 1250m².
- P3 Rectilinear with narrow boundary to street.

Streetscape

- P4 Continuity of fences and landscaping.

Views

- P5 Warringah Expressway Lookout at the end of Ridge Street. Towards St Leonards Park War Memorial along Walker Street.

Section 2 - North Sydney Planning Area

2.5.5 Characteristic buildings

- P1 Detached and attached Victorian Italianate dwelling houses.
- P2 Detached Federation Queen Anne and Arts and Crafts style dwelling houses.

2.5.6 Characteristic built elements

Siting

- P1 Dwellings are oriented to face the street, parallel to the street alignment.
- P2 Dwellings are sited forward and middle of lot.

Form, massing and height

- P3 Single-storey and two-storey dwelling houses with hipped and gabled roofs with skillion rear wings.
- P4 Reduced height and scale to rear.
- P5 Open verandahs to front.
- P6 Projecting front gables beside recessed verandahs.
- P7 Detached and semi-detached dwelling houses of identical design often have continuous front verandahs.
- P8 Strong skyline of simple pitched roofs and chimneys visible from street and St Leonards Park.
- P9 Front setbacks generally between 4-5m.
- P10 Side setbacks of 1.5m.

Roofs

- P11 Pitched between 30 and 45 degrees without dormers or openings that can be seen from the street.
- P12 Hipped roofs with some gabled elements.
- P13 Gabled ends for projecting bays to the street.
- P14 Skillion roofs to rear extensions.
- P15 Brick and rendered chimneys with terracotta chimney pots.

External Materials

- P16 Face brick on Federation buildings with sandstone foundations.
- P17 Original rendered walls on Victorian buildings.
- P18 Slate, corrugated metal and terracotta tiled roofs.
- P19 Timber windows, doors and joinery.
- P20 Original front garden landscaping.

Windows, doors and joinery

- P21 Consistent with building period and style. Timber.

Fences and kerbing

- P22 Original front fences less than 800 mm height with views to the garden.
- P23 Timber fences to rear and side.
- P24 Sandstone plinths, sandstone piers, metal palisade and gates, timber pickets, timber rails and mesh, pipe and mesh gates, original face brick with piers.
- P25 Sandstone kerbing.



Car accommodation

P26 No garages or carports located in front of building line.

2.5.7 Desired built form

P1 Views to the war memorial in St Leonards Park should not be obstructed and opportunities to improve the vistas to and from the park along Walker Street should be sought.

P2 Improve the interface of the Walker/Ridge Street conservation area boundary with St Leonards Park.

2.5.8 Uncharacteristic elements

P3 Modern infill development, loss of original detailing and materials on elevations visible from the public domain, dormers and skylights on front or side elevations, modified roof planes, glazed roofing, new balconies and decks above street level, infilled verandas and balconies, roof cut-outs for decks and terraces, full width roof additions, glazed balustrades, metal wall cladding, extensive glazing, infill of breezeways, paved gardens, lack of landscape setting including trees, high solid walls and fences to the street, parking except at the rear, concrete kerbing.

2.6 McLAREN STREET CONSERVATION AREA



2.6.1 History

The McLaren Street Conservation Area was subdivided as part of the Town Plan for North Sydney, known at the time as St Leonards.

The land for St Thomas' Church was allocated in 1842 and the first church erected in 1843. Further land was purchased by the Church and a schoolhouse was erected in 1848. The original Church was replaced by a larger Church erected around it between 1877 and 1884 to a design by Edmund Blacket.

A rectory, designed by E. Jeaffreson Jackson, was built to the east of the Church in 1900, and a memorial hall was added to the site in 1922. The earliest recorded occupant of adjacent land was James Husband, and plans from 1892 show "St Thomas' Terrace" fronting Miller Street.

During the 1890's a large residence and doctor's surgery, designed by E. Jeaffreson Jackson, was erected for Dr Capper at the corner of Miller and McLaren Streets. The building was acquired in 1926 and remodelled as Council Chambers for North Sydney Council.

At the turn of the 20th century Miller Street was known as the "Macquarie Street of the North Shore" because of the concentration of doctors, dentists and hospitals.

2.6.2 Description

The McLaren Street Conservation Area is made up of two areas either side of McLaren Street and Church Street including the park to the north of the North Sydney Council Chambers.

The topography slopes down from Ridge Street towards McLaren Street with Church Street following the slope. The park is modelled with small rises and a terraced area towards Miller Street. The subdivision pattern relates only to McLaren Street and Church Street and the Victorian pattern has been lost in the Civic precinct.

The characteristic buildings in the area are typically Federation and Edwardian Queen Anne with pockets of Victorian dwelling houses along Church Street. The area contains several public buildings including St Thomas's Church and Hall and the North Sydney Council Chambers, a Federation building with modern extension. The buildings are typically:

- (a) single and two-storey, freestanding buildings with materials relating to the age of construction,
- (b) Victorian rendered dwelling houses with verandahs and slate and corrugated metal roofs, and
- (c) Federation face brick dwelling houses with verandahs and terracotta tile roofs.

The church is a prominent, stone building with associated vestry building and halls.

There are street plantings along each of the streets and plantings in the park.

2.6.3 Statement of Significance

The McLaren Street Conservation Area is significant:

- (a) As an area that is close to the centre of North Sydney that retains representative details from its development from the late 19th and early 20th centuries, including street formation, buildings, gardens and fencing.
- (b) For its landmark qualities and associations with St Thomas' Church, North Sydney Council buildings, park and public court.

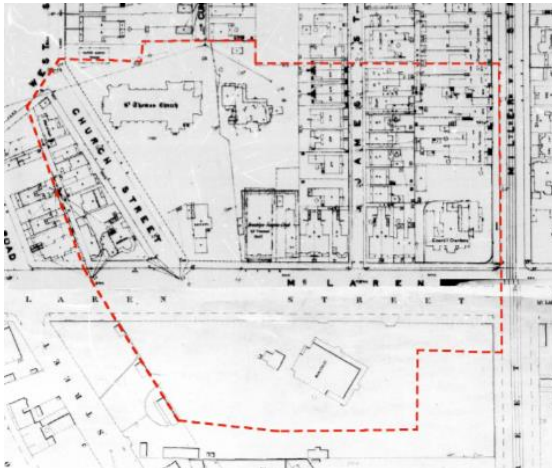


Figure F-2.6.1 (left):
Circa 1890

Figure F-2.6.2 (below left):
Circa 1943

Figure F-2.6.3 (below):
Circa 2008



2.6.4 Significant elements

Topography

P1 Sloping to the south from Ridge Street.

Section 2 - North Sydney Planning Area

Subdivision

- P2 Variety of lot sizes related to land use and building/dwelling type. Generally rectilinear with narrowest frontage to street for perimeter development.

Streetscape

- P3 Buildings at street level or raised above it.
 P4 Varying scale of housing facing the street in garden settings located forward on the lot.
 P5 Public buildings in landscaped setting.
 P6 Concrete and stone kerbs, sandstone walls, palisade fences, street gardens.
 P7 Street trees and Stanton Park.

Views

- P8 Views within area along Miller and McLaren Streets to St Thomas' Church and the Council buildings.

2.6.5 Characteristic buildings

- P1 Victorian and Federation.

2.6.6 Characteristic elements

Siting

- P1 Dwellings are oriented to face the street, parallel to the street alignment, forward on lots.
 P2 Front setback 6-8m and side setbacks of 1.5-2m.

Form, massing and height

- P3 Single and two-storey, detached dwelling houses.
 P4 Simple forms articulated with verandahs to front.
 P5 Reduced bulk and scale to the rear.
 P6 Multi-storey public buildings.
 P7 Mainly gabled and hipped roofs pitched between 30 and 45 degrees.

Materials

- P8 **Walls:** render, stone and face brick on sandstone foundations.
 P9 **Roofs:** slate, terracotta and corrugated metal on rear extensions.
 P10 **Windows and doors:** Timber.
 P11 Tall chimneys with chimney pots, stucco and face brick.
 P12 Tessellated tiles to verandahs.

Windows and doors

- P13 Consistent with building period and style. Timber.

Fences

- P14 Brick or sandstone piers and base with metal palisade and timber panels.
 P15 Stone boundary wall to Church.

Car accommodation

- P16 Locate garages and carports to rear of the property.
 P17 No garages or carports located in front of building line.



P18 Single driveways.

2.6.7 Uncharacteristic elements

- P1 Modern additions and buildings; painting and rendering of face brick; high fences to street; excessive paved areas for parking; buildings built to the front boundary.
- P2 Modern shopfront to 5-7 McLaren Street.



SECTION 3 ST LEONARDS / CROWS NEST PLANNING AREA

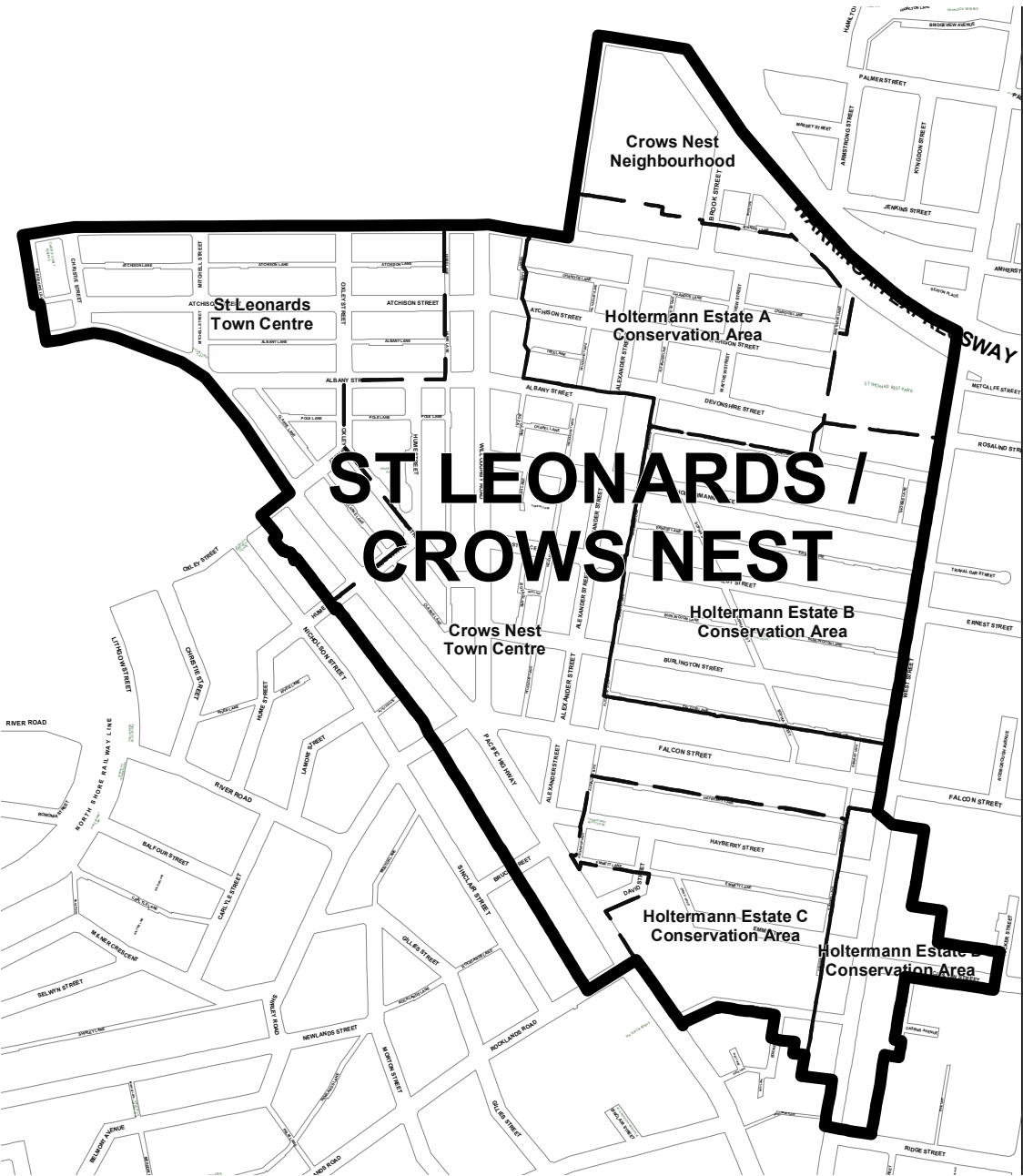
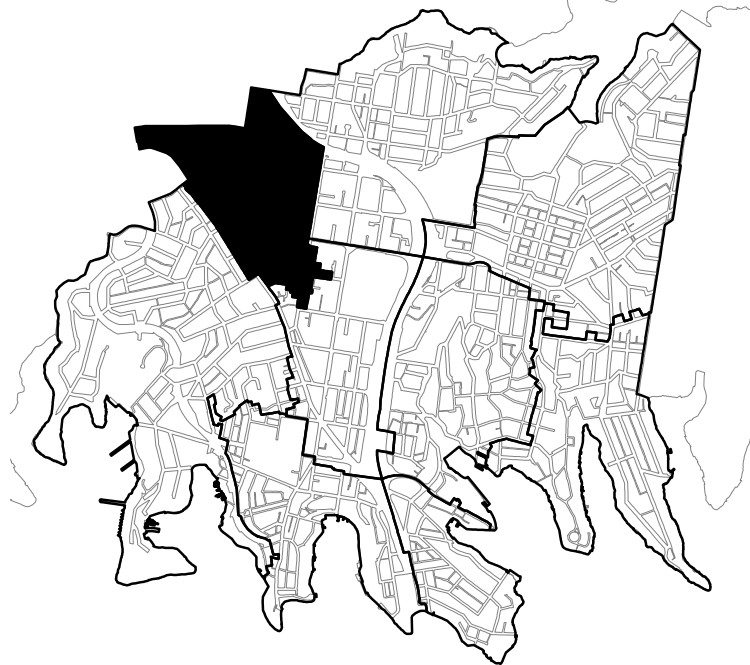


Figure F-3.0: St Leonards / Crows Nest Planning Area and associated Locality Areas

3.0 ST LEONARDS / CROWS NEST PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the St Leonards/Crows Nest Planning Area. This statement has been informed by aims and desired outcomes of Council's Planning Studies undertaken in the Planning Area and the State Government's *2036 Plan for St Leonards Crows Nest*. The Planning Area is located partially within the Crows Nest Transport Oriented Development Precinct and is subject to the requirements of *Crows Nest Transport Oriented Development Precinct Design Guide* (Design Guide). In the event of any inconsistency between the Design Guide and this DCP, the objectives and provisions of the Design Guide will prevail to the extent of that inconsistency. Where no provisions are indicated in the Design Guide, the relevant provisions of this DCP will apply.

The Planning Area is focussed around the town centres of St Leonards and Crows Nest in the north-west of the Local Government Area, both of which are situated on major vehicular traffic routes. The remainder of the Planning Area comprises a number of predominantly low-density residential neighbourhoods, much of which is characterised by retention of the historic subdivision pattern.

The St Leonards Town Centre is identified as a Strategic Centre within the heart of the Eastern Economic Corridor and forms part of an identified Health and Education Precinct under the Regional Plan. In particular, it is a dense, significant, sustainable and busy urban centre where:

- it will accommodate sustained growth in the health and technology sectors, whilst supporting creative industries, small to medium sized businesses, startups, galleries, entertainment and speciality retail to ensure it enhances the economic function of North Sydney
- a diverse range of living, employment, recreation and social opportunities are provided which serve both local and regional populations and contribute to the vibrancy of the Town Centre

Section 3 - St Leonards / Crows Nest Planning Area

- residents, workers and visitors enjoy a high level of amenity and quality of the natural and built environment
- residents, workers and visitors can easily access the Planning Area through excellent public transport links to the Sydney CBD, other suburban centres and many parts of the Sydney Region by rail and bus.

The Crows Nest Town Centre is smaller in scale in comparison to St Leonards, with 19th Century, two storey shopfront parapets along Willoughby Road and parts of the Pacific Highway. The Town Centre services the daily needs of residents and visitors, as well as having a lively dining district. Traffic is actively managed so pedestrians can move freely across Willoughby Road.

The quiet residential neighbourhoods in the east of the Planning Area are low in scale and characterised by wide roads with street tree plantings. Laneways facilitate movement and provide rear lane access to properties. Local shops, dispersed throughout the area, serve both local and regional needs. St Thomas Rest Park, located toward the north-eastern edge of the Planning Area, as well as Hume Street Park, provides much needed open space and complements pocket parks within the area, with access to St Leonards Park on its eastern edge.

and where:

Function

- various grades and sizes of business spaces are provided in the St Leonards and Crows Nest Town Centres to accommodate a mix of small to large business premises, retail premises and community services
- the lower levels of commercial and mixed use buildings in the St Leonards Town Centre are designed to provide flexible spaces to support the growth of creative industries, small to medium sized businesses, startups, galleries, entertainment and speciality retail
- active uses outside normal business hours are encouraged to be established to enhance the vitality of the Town Centres
- the village atmosphere of Crows Nest is maintained and enhanced, with Willoughby Road continuing to be an active high street.
- community facilities meet the needs of the Planning Area's working and resident population, visitors, and residents of nearby neighbourhoods, in terms of wellbeing, culture and recreation, and add to the diversity and activity of the Town Centres
- there is housing choice in the mix of dwelling types and in the range of affordability
- public transport, including walking and cycling, is the main form of access to the Town Centres
- the grid pattern of streets and lanes imposes order and allows freedom of movement
- mid-block pedestrian connections provide alternative routes through long street blocks at street level to assist pedestrian movement
- the area is highly permeable for pedestrians

Environmental Criteria

- the extremes of sun, wind and rain are mitigated by continuous awnings and other good building design elements
- natural light reaches buildings, public places and streets
- solar access to existing and proposed public open spaces is maximised and contributes to the enjoyment of those spaces used by the public



- mechanical and other noise is controlled to protect residential amenity
- there is opportunity for sharing of views within the area
- additional public open space is provided and existing spaces embellished to cater for increases in resident and worker populations
- increased opportunities for tree planting are provided at the ground level to minimise the urban heat island effect

Quality Built Form

- a safe, high quality urban environment is achieved through careful design of buildings and use of materials, and a well designed and maintained public domain
- the high ridge that underlies St Leonards is reflected in its built form and the skyline is an interesting and distinctive feature in the broader landscape
- buildings are scaled down from the railway and metro stations towards surrounding areas, to fit in with lower scale development and reducing adverse affects on lower scale areas
- high rise development is generally focused along the Pacific Highway and the area generally contained by the Pacific Highway to the west, Oxley Street to the east and south and Chandos Street to the north
- the character in the St Leonards Town Centre is highly urbanised, but softened through urban design and landscaping
- the visual characteristics of the Crows Nest neighbourhood's heritage conservation areas are reflected in new development, with low rise small scale dwellings predominating
- heritage items retain their heritage significance, and contribute to area's rich development history and provide interest in the physical fabric of the area

Quality Urban Environment

- tree planting in private and public spaces and small landscaped areas provides softening from the built form
- traffic is managed so that pedestrians can move within the area safely and freely and amenity is maintained
- parking is managed to maintain pedestrian safety, maximise public and active transport modes and minimise traffic generation
- pedestrians are assisted to safely cross barriers such as the Pacific Highway and the railway at grade
- the perceived extent of the public domain is increased through the incorporation of landscaped setbacks and reversed podiums
- rear lanes are used for vehicle access to properties

Efficient Use of Resources

- energy efficient design and life cycle assessment of buildings enables the conservation of natural resources and minimal use of non-renewable energy resources
- stormwater runoff is minimised, and reused on-site where possible

The St Leonards / Crows Nest Planning Area includes the following Locality Areas in which development is also subject to relevant provisions below:

Section 3.1: St Leonards Town Centre

Section 3.2: Crows Nest Town Centre

Section 3.2.4 Hume Street Park

	Section 3.2.5	27-57 Falcon Street, Crows Nest
Section 3.3:	Crows Nest Neighbourhood	
Section 3.4	Holtermann Estate Conservation Area A	
Section 3.5:	Holtermann Estate Conservation Area B	
Section 3.6:	Holtermann Estate Conservation Area C	
Section 3.7:	Holtermann Estate Conservation Area D	

3.1 ST LEONARDS TOWN CENTRE

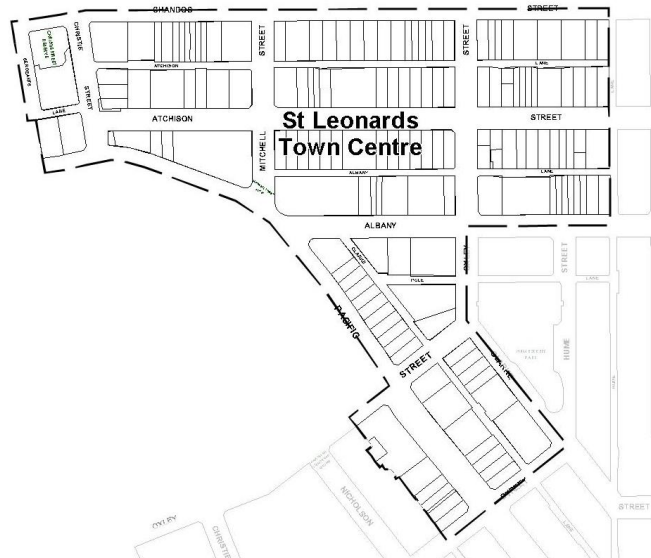


Figure F-3.1-1: Locality Area Map

3.1.1 Significant elements

Land Use

- P1 Predominantly retail and mixed commercial and residential development.
- P2 Commercial development.
- P3 High density residential development.
- P4 Community facilities.
- P5 Passive and active recreational spaces.

Topography

- P6 Slight falls to the east and north east from the Pacific Highway which generally follows the ridgeline.
- P7 Slight fall to the west along the Pacific Highway from Albany Street.

Identity / Icons

- P8 The Forum development and plaza.
- P9 St Leonards Railway Station and Crows Nest Metro Station, major transport interchange hubs.
- P10 Pacific Highway, a major sub-arterial thoroughfare.
- P11 Christie Street Reserve.
- P12 Hume Street Park.

Subdivision

- P13 Generally rectilinear grid pattern with dual frontages.

Streetscape

- P14 Wide fully paved footpaths along Pacific Highway and adjacent to other commercial and mixed use buildings.

Section 3 - St Leonards / Crows Nest Planning Area

- P15 Atchison Street between Christie Street and Mitchell Street is one way only, with wide paved footpaths, landscaping and other urban furniture.
- P16 Awnings provided along the Pacific Highway and for other commercial and mixed use buildings.
- P17 Irregular planting of street trees.

Public transport

- P18 Development is to take advantage of high levels of access to high frequency public train, Metro and bus services.
- P19 Public transport, cycling and walking are the main forms of transport to the Centre.

3.1.2 Desired Future Character

Diversity of activities, facilities, opportunities and services

- P1 Predominantly high to medium-high rise, commercial and mixed commercial and residential development.
- P2 Provision of a variety of different sized commercial office, business, retail, recreation, (indoor and outdoor) community, entertainment, food and drink and other active non-residential uses at the street level in the *E2 Commercial Centre* and *MU1 Mixed Use* zones.
- P3 Intensify the provision of commercial office and business premises in close proximity to the railway and metro stations with active uses such as food and drink premises and retail located at the ground level addressing the public domain.
- P4 Maximise ground level activation along Christie, Mitchell, Oxley, Atchison, Chandos and Clarke Streets by focusing food and drink premises and retail within a fine grain built form. This can be achieved through the emphasis of small to medium sized tenancies which directly address the public domain.
- P5 High density residential accommodation within mixed use buildings is concentrated closes to the railway and metro stations and along the Pacific Highway.
- P6 Retention of the medium density residential precinct in the north-eastern corner of the Locality Area.

Public spaces and facilities

- P7 Establish Atchison Street as the civic main street by:
 - (a) ensuring that the design of building exteriors at the lower levels incorporates high levels of architectural modulation (i.e. no blank walls) along with high quality materials and finishes;
 - (b) maximising active uses such as retail, food and drink and outdoor dining at the ground level; and
 - (c) improving the public domain in accordance with Council's Public Domain Style Manual and Design Codes.
- P8 Outdoor dining to be concentrated along widened footpaths to Atchison, Chandos, Mitchell, Oxley and Clarke Streets.
- P9 Maintain and enhance Christie Street Reserve as a sunny and inclusive landscaped space which is activated along its southern edge with ground floor retail premises to 100 Christie Street.
- P10 Maintain and enhance Mitchell Street Plaza at the intersection of Mitchell Street and Pacific Highway.
- P11 A linear landscaped park is provided along the western side of Mitchell and Oxley Streets, between Chandos and Pacific Highway.



- P12 A shared way is provided along Mitchell Street between Atchison Street and Albany Lane.
- P13 Consideration be given to integrating artworks, water features and wind breaks into the design of Mitchell Street Plaza.
- P14 Plazas incorporate space for public entertainment and expression of community identity, large enough to hold an open air performance or market.
- P15 Roof top gardens and public facilities that allow public access to district views from higher floors.

3.1.2.1 Accessibility and permeability

Objectives

- O1 Improve permeability and provide connections to main pedestrian and cycling networks, key destinations and residential areas.
- O2 Ensure that the design of through site links and mid-block connections is high quality, safe, well-lit, accessible and pedestrian friendly.
- O3 Encourage walking and cycling as part of the broader street network to promote community interaction, better health outcomes and reduce vehicle movements.
- O4 Encourage active frontages along through site link without compromising safe pedestrian access and use.
- O5 Ensure that any proposed privately owned lanes have a fully public nature equivalent to the public domain.

Provisions

- P1 The through site links identified in Figure F-3.1-2 are to be provided, retained and enhanced.
- P2 New through site links are to align as best as possible with existing through site links to maximise permeability.
- P3 Through site links that are proposed in addition to those identified under P1 must demonstrate that it meets the objectives and provisions of this subsection.
- P4 Through site links are to be provided in accordance with Section 2.4.7 to Part E of the DCP and the following criteria:
- (a) The design and finish must be in accordance with the relevant Public Domain Strategy.
 - (b) Include landscaping where practical to assist guiding people along the link while maintaining long sightlines.
 - (c) Be fully open to the sky. Internal through site links will only be considered where they are provided with double height spaces to convey a sense of publicness.
 - (d) Provide public access 24 hours a day 7 days a week.
 - (e) Be activated on both sides of the link.
 - (f) Be clearly distinguished from vehicular accessways.

Section 3 - St Leonards / Crows Nest Planning Area



Figure F-3.1-2: Through Site Link Map

3.1.3 Desired Built Form

3.1.3.1 Subdivision

Objectives

- O1 To enable the development of feasible built forms which also incorporate suitable side and rear setbacks at the upper levels to afford daylight and ventilation between buildings.

Provisions

- P1 Maintain a frontage of 20m - 40m, which equates approximately to the amalgamation of two or three original allotments.
- P2 Development on consolidated allotments with a frontage wider than 20m - 40m frontage is to be broken down by articulation, design and detailing, change in materials and colours.

3.1.3.2 Form, massing and scale

Objectives

- O1 To ensure that density and scale is concentrated close to the rail and metro stations and along the Pacific Highway.
- O2 To ensure a positive transition in height and scale down from tall towers to neighbouring and adjoining lower density areas.
- O3 To enable the lower levels of buildings to be converted between retail, commercial and community uses over time.



Section 3 - St Leonards / Crows Nest Planning Area

- O4 To increase amenity for the occupants of buildings by providing direct access to sunlight, daylight and natural ventilation.
- O5 To maximise year round solar access to existing and new public spaces to be created within the Planning Area.

Provisions

- P1 Buildings should generally transition in height from St Leonards Railway Station, Crows Nest Metro Station and Pacific Highway down to the surrounding areas and the lower scale development on Chandos Street, Willoughby Road, Crows Nest Town Centre, the Upper Slopes Neighbourhood and Crows Nest Neighbourhood.
- P2 Roof design presents a varied, composed and interesting skyline when viewed from a regional context.
- P3 Architectural detailing and ornamentation provides a rich visual texture and a symbolic reference to the history of the place, the building's use or occupant.
- P4 Developments on land greater than 1,000sqm should consider the incorporation of internal courtyards adjacent to laneways and through site links to broaden the range and form of open space in the locality.
- P5 Development is to be designed to maximise year round solar access to existing and the new public spaces to be created within the Planning Area.
- P6 Tower elements located above the podium level should not exceed 45m in width, unless it can be adequately demonstrated that sufficient physical vertical articulation can be provided to break the tower elevation into distinct elements.
- P7 Minimum floor to floor heights on land zoned *E2 Commercial Centre* or *MU1 Mixed Use* must be provided:
- (a) Ground floor level: 4.0 – 4.5m
 - (b) First floor level: 3.6m
 - (c) Levels above: 3.0m (residential uses) or 3.6m (non-residential uses)
- P8 Residential floor to floor heights should generally not exceed 3.2m, except on the first floor level in accordance with P7 above.
- P9 Rooftop plant and structures should be designed and positioned to comprise a minor element of the roofscape and minimise any increases to the building's overall overshadowing impacts.

3.1.3.3 Setbacks

Objectives

- O1 Reinforce the spatial definition of streets and public spaces.
- O2 Emphasise the street as a distinct spatial entity and design the street wall frontage with an appropriate human scale and sense of enclosure for the street.
- O3 Ensure consistent street frontages along the street alignment.
- O4 Recognise the variation in street frontage heights throughout the centre and allow flexibility to respond to context.
- O5 Ensure adequate transition in scale between employment / mixed use zones and residential zones.
- O6 Enable deep soil planting along the Pacific Highway and identified linear parks.

Provisions

Note. Additional setbacks may be required for any part of the building located above the podium level. Refer to Section 3.1.3.5 to this Part.

- P1 Buildings are to be setback from all street frontages in accordance with the Building Setbacks Map (refer to Figure F-3.1-3).

Section 3 - St Leonards / Crows Nest Planning Area

- P2 A nil setback is permitted to all side and rear boundaries without a street frontage on land zoned *E2 Commercial Core* or *MU1 Mixed Use*.
- P3 Despite P2, a minimum 6m setback is required where land zoned *E2 Commercial Centre* or *MU1 Mixed Use* directly adjoins land which is residentially zoned along the zone interface. This requirement does not apply if the land is separated by a public road.
- P4 A minimum 4.5m setback is required to all side and rear boundaries without a street frontage on land zoned *R4 High Density Residential*.
- P5 Despite P1 and P2, setbacks may be varied to be consistent with the built form envelope approved as part of the approval for the Crows Nest Metro Station and Over Station Development (OSD).
- P6 Despite P1-P4, where a site contains a heritage item and that item is to be retained, the heritage item may maintain its existing setback.
- P7 Despite P1-P4, increased setbacks may be required for podium levels that contain residential uses to achieve adequate building separation in accordance with [SEPP \(Housing\) 2021](#).
- P8 Building alignments and setbacks should also respond to important elements of the nearby context including public spaces and heritage buildings, monuments and landscape elements, in order to complement the streetscape. In some places, this may require greater building setbacks than those specified in Figure F-3.1-3).
- P9 Where a whole of building setback is required to the Pacific Highway, Chandos Street, Oxley Street or Mitchell Street, any basement is also required to be setback the same distance, unless it can be adequately demonstrated that sufficient soil depth can be provided to enable the planting of canopy trees capable of growing to at least the height of any adjacent podium.



Figure F-3.1-3: Building Setbacks Map

3.1.3.4 Podium Heights

Objectives

- O1 To strengthen the urban form of the Centre with consistent street wall heights.
- O2 To achieve comfortable street environments for pedestrians in terms of daylight, scale, sense of enclosure and wind mitigation as well as a healthy environment for street trees.
- O3 To enhance the distinctive character of streets within the Centre.
- O4 To ensure the context of heritage items are not adversely eroded.

Provisions

- P1 Podiums are to be provided in accordance with the Podium Heights Map (refer to Figure F-3.1-4).
- P2 Despite P1, corner sites are to maintain a consistent podium height to all street frontages to achieve consistent built form.
- P3 Despite P1, sites containing a heritage item and where that heritage item is to be retained the existing podium height may be retained.
- P4 Despite P1 podium heights may be varied to be consistent with the built form envelope approved as part of the approval for the Crows Nest Metro Station and Over Station Development (OSD). However, distinct architectural elements are to be incorporated to reflect the desired podium heights.
- P5 If required, podiums are to step down with the topography.



Figure F-3.1-4: Podium Heights Map

Section 3 - St Leonards / Crows Nest Planning Area

3.1.3.5 Above Podium Setbacks

Objectives

- O1 To protect daylight access to the street level to enable the successful growing of street trees.
- O2 To permit sky views from the street and neighbouring residential areas.
- O3 To ensure that built form achieves comfortable public domain conditions for pedestrians, with adequate daylight, appropriate scale, and mitigation of urban heat and wind effects of tower buildings.
- O4 To reinforce important elements of the local context including public spaces, heritage buildings, monuments and landscape elements.
- O5 To provide adequate privacy, access to light, air and outlook for the occupants of buildings, neighbouring properties and future buildings.
- O6 To ensure towers are sufficiently separated to be seen in the round and reduce the cumulative overshadowing impact of towers on neighbouring residential areas.
- O7 To ensure development does not prejudice the re-development of adjoining sites in the future.
- O8 To avoid awkward ziggurat style built forms.

Provisions

- P1 All buildings are to be designed to provide setbacks above the podium in accordance with the Above Podium Setbacks Map (refer to Figure F-3.1-5). Setbacks above the podium are to be measured from the outer wall of the podium.
- P2 A minimum above podium setback to all side and rear boundaries is required as follows:
 - (a) 4.5m for buildings up to 8 storeys in height; or
 - (b) 6m for buildings from 9 storeys up to 18 storeys in height; or
 - (c) 12m for buildings over 18 storeys in height.
- P3 Despite P1 and P2, above podium setbacks may be varied to be consistent with the built form envelope approved as part of the approval for the Crows Nest Metro Station and Over Station Development (OSD).
- P4 Despite P2, a minimum 15m above podium setback, measured from the property boundary, is required where land zoned *E2 Commercial Centre* or *MU1 Mixed Use* directly adjoins land which is residentially zoned along the zone interface. This requirement does not apply if the land is separated by a public road.
- P5 Despite P1-P4, increased setbacks above the podium may be required to achieve adequate building separation in accordance with [SEPP \(Housing\) 2021](#).
- P6 Only one step in the built form between the podium walls and tower is permissible.
- P7 An existing adjacent building, even if heritage listed, cannot be used to justify a reduced setback that would compromise the development potential of the adjacent site in the future.

Note. *Setbacks are measured to the outer face of any part of the building including balconies, architectural detailing and the like.*



Figure F-3.1-5: Above Podium Setbacks Map

3.1.3.6 Active Frontages

Objectives

- O1 To provide for the amenity, interest and liveliness of the street environment and public open spaces.
- O2 To ensure a positive experience for pedestrians with the necessary fine grain environment of the street.
- O3 To enable sensory engagement with the street and public spaces.
- O4 To provide an active ground floor frontage that is accessible and integrated with the design of the public domain.
- O5 To maximise the extent of active frontages in the public domain.
- O6 To promote activity, connectivity and variety in the public domain.
- O7 To increase passive surveillance of the street and other public spaces and enhance safety.

Provisions

- P1 Buildings must contain active frontages to all street frontages with the exceptions of public laneways.
- P2 Despite P1, an active frontage is to be provided to Sergeants Lane along the southern boundary to 100 Christie Street.
- P3 Active frontages to public laneways are encouraged where practical.

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- P4 Where a site has a direct frontage to an existing or proposed open space, an active frontage is to be provided to that interface. In particular, active frontages are to be provided to:
- (a) Christie Street Reserve;
 - (b) Mitchell Street, between Chandos Street and Pacific Highway;
 - (c) Oxley Street, between Chandos Street and Pacific Highway; and
 - (d) Clarke Street, between Oxley Street and Hume Street.
- P5 Active frontages are to be provided along both sides of the through site links.
- P6 Where a site has multiple street frontages, all service and vehicular access points must not be provided off the primary street frontage.
- P7 Fire escapes and service doors must be seamlessly incorporated into the facade with quality materials.
- P8 Avoid the use of colonnades along all street frontages.

3.1.3.7 Awnings

Objectives

- O1 To maximise pedestrian comfort and continuous protection.

Provisions

- P1 Awnings are to be provided along all street frontages in the *E2 Commercial Centre* and *MU1 Mixed Use* zones.

3.1.3.8 Solar access

Objectives

- O1 To ensure existing and future parks and public spaces in and around the Centre are afforded a high level of amenity through the maintaining and maximising of solar access during periods in the day when they are most used throughout the year.
- O2 To promote active and passive recreation to public spaces to service existing and planned population of the Centre and surrounds.
- O3 To ensure the successful growth and survival of trees and vegetation within the parks and public spaces.

Provisions

- P1 Development is to be designed to maximise year round solar access to existing and proposed new public spaces to be created within the Planning Area.
- P2 Development must not result in a net increase in overshadowing to the following existing and proposed public spaces between 10am and 3pm from the March Equinox to the September Equinox (inclusive):
- (a) Christie Street Reserve, comprising all land within Lots 2 and 3, DP 733528 and zoned *RE1 Public Recreation*; and
 - (b) Hume Street Park, comprising all land located between Pole Lane, Willoughby Road, Clarke Street and Oxley Street and zoned *RE1 Public Recreation*; and
 - (c) Ernest Place, comprising all land within the Ernest Street road reserve located between Willoughby Road and Willoughby Lane and zoned *RE1 Public Recreation*; and
 - (d) Holtermann Reserve (proposed), comprising that part of Lot 1, DP 785343 and zoned *SP2 Infrastructure – Car Park*.
- P3 Development must not result in a net increase in overshadowing to the following existing and proposed public spaces between 10am and 3pm from the March Equinox to the September Equinox (inclusive):



- (a) Willoughby Road road reserve, between Chandos Street and Falcon Street; and
- (b) Mitchell Street road reserve and the whole of building setback requirement to the western side of Mitchell Street as required under s.3.1.3.3, between Chandos Street and the Pacific Highway; and
- (c) Oxley Street road reserve, and the whole of building setback requirement to the western side of Oxley Street as required under s.3.1.3.3, between Chandos Street and Clarke Street

P4 Despite P2 and P3, development on land located directly adjacent to the identified public spaces may result in a net increase in overshadowing of that space, but only if that development strictly complies with key built form envelope controls that apply to that land under both NSLEP 2013 and this DCP.

3.1.3.9 Noise

Objectives

O1 Ensure adequate amenity to occupants of future development adjacent primary vehicular routes.

Provisions

P1 Elevations of buildings fronting the Pacific Highway and Chandos Street are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

3.1.3.10 R4 – High Density Residential Zone

Note: These provisions only apply to land within the R4 High Density Residential zone. Provisions to s.3.1.3.10 prevail over the provisions under s.3.1.1-s.3.1.3.9 to Part F of the DCP to the extent of any inconsistency that arises.

Objectives

- O1 Ensure that development complements the physical form of development in the adjoining mixed use areas.
- O2 Promotion of communal open space areas.
- O3 Promotion of casual visual surveillance over the public domain.

Provisions

- P1 Generally 5 storeys in height, with flat roofs and the topmost storey setback from the levels below.
- P2 Height of development responds to adjacent building height and form.
- P3 Landscaped areas should be accessible to all residents and not fenced off into separate courtyards.
- P4 Rear open spaces must be accessible from the street.
- P5 Laneway fences generally between 900 and 1200mm high.

3.1.3.11 Car accommodation

Objectives

- O1 To minimise the impact on pedestrian safety.
- O2 Ensure that traffic generation is minimised.
- O3 To minimise the visual impact of vehicles.
- O4 Ensure high levels of accessibility.

Provisions

P1 Where a property has a frontage to a laneway, vehicular access must be provided from the laneway.



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- P2 All off-street car parking must be provided underground.
- P3 Pick up and drop off points for public transport and taxi ranks should be located close to public spaces and activities, and main building entries.
- P4 Short stay (ten minute) parking spaces should be located close to meeting places.
- P5 The level of parking provided on sites in close proximity to the railway and metro stations should be minimised as far as practical.

3.2 CROWS NEST TOWN CENTRE



Figure F-3.2-1: Locality Area Map

3.2.1 Significant elements

Land Use

- P1 Predominantly retail and mixed commercial and residential development.
- P2 Commercial development.
- P3 Public parking facilities.
- P4 Community facilities.
- P5 Medium and high density residential accommodation.
- P6 Passive and active recreational spaces.

Topography

- P7 Slight falls to the east and north east from the Pacific Highway which generally follows the ridgeline.
- P8 Substantial fall down from Alexander Street to Alexander Lane south of Falcon Street.

Natural Features

- P9 Ridge line following the alignment of Pacific Highway.

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Views

P10 The following views and vistas are to be preserved and where possible enhanced:

- (a) Vista north along Willoughby Road (incorporating the St Leonards Catholic Church steeple) and Pacific Highway.
- (b) District views from the upper levels of taller buildings.

Identity / Icons

- P11 Crows Nest Fiveways intersection.
- P12 Formalised outdoor dining on Willoughby Road, Burlington, Ernest and Holtermann Streets.
- P13 Pacific Highway and Falcon Streets, major sub-arterial thoroughfares.
- P14 Crows Nest Metro Station.
- P15 Hume Street Park.
- P16 Ernest Place, Crows Nest Community Centre and the proposed Holtermann Street Park.

Subdivision

- P17 Regular grid pattern interrupted by diagonal streets.
- P18 Generally long narrow allotments with dual street frontages.

Streetscape

- P19 In mixed use areas, buildings are generally built to the street and aligned with the street frontage.
- P20 Continuous awnings provided for shops, cafes and other commercial uses.
- P21 Wide footpaths with designated outdoor dining areas on Willoughby Road, Burlington, Ernest, Holtermann and Clarke Streets.
- P22 Landscaping provided along Willoughby Road to improve amenity for pedestrians and outdoor diners.
- P23 Traffic calming and pedestrian crossings provided near shops and cafes on and around Willoughby Road.
- P24 Irregular planting of street trees and shrubs.

Public transport

- P25 Development is to take advantage of the Area's high levels of accessibility to public metro, train and bus services.

3.2.2 Desired Future Character

3.2.2.1 Diversity of activities, facilities, opportunities and services

- P1 Intensify commercial and mixed use development in close proximity to the Metro station and along the Pacific Highway with active uses at the ground floor levels, commercial within the podium levels and residential above.
- P2 Maintaining a low scale built form to Willoughby Road, between Falcon Street and Albany Street, with two storey parapet shopfronts with shops at ground level, non-residential or residential above, with additional height set back above a 2 storey parapet.
- P3 Predominantly medium rise mixed use development, built boundary to boundary, with setbacks to laneways, and above the podium, with shops at ground level, non-residential/residential on first floor and residential above.



- P4 Medium density residential development along Falcon Street, consistent with its residential zoning.
- P5 Expansion of Hume Street Park to provide a large connected piece of open space connecting Willoughby Road to Oxley Street.
- P6 Provision of a new public open space off Holtermann Street and backing onto the Crows Nest Community Centre.

3.2.2.2 Accessibility and permeability

Objectives

- O1 Improve permeability and provide connections to main pedestrian and cycling networks, key destinations and residential areas.
- O2 Ensure that the design of through site links and mid-block connections is high quality, safe, well-lit, accessible and pedestrian friendly.
- O3 Encourage walking and cycling as part of the broader street network to promote community interaction, better health outcomes and reduce vehicle movements.
- O4 Encourage active frontages along through site link without compromising safe pedestrian access and use.
- O5 Ensure that any proposed privately owned lanes have a fully public nature equivalent to the public domain.

Provisions

- P1 Provide, retain and enhance through site links for pedestrians identified on the Through Site Link Map (refer to Figure F-3.2-2).
- P2 New through site links are to align as best as possible with existing through site links to maximise permeability.
- P3 Through site links that are proposed in addition to those identified under P1 must demonstrate that it meets the objectives and provisions of this subsection.
- P4 Through site links are to be provided in accordance with Section 2.4.7 to Part E of the DCP and the following criteria:
- (a) The design and finish must be in accordance with the relevant Public Domain Strategy.
 - (b) Include landscaping where practical to assist guiding people along the link while maintaining long sightlines.
 - (c) Be fully open to the sky. Internal through site links will only be considered where they are provided with double height spaces to convey a sense of publicness.
 - (d) Provide public access 24 hours a day 7 days a week.
 - (e) Be activated on both sides of the link.
 - (f) Be clearly distinguished from vehicular accessways.

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Figure F-3.2-2: Through Site Link Map



3.2.2.3 Public spaces and facilities

Objectives

- O1 To increase the quantum of public open space in the locality.
- O2 To ensure public spaces are enhanced to maximise their amenity.

Provisions

- P1 Maintain Willoughby Road as the civic main street of the Town Centre by:
 - (a) ensuring that the design of building exteriors at the lower levels incorporate high levels of architectural modulation (i.e. no blank walls) along with high quality materials and finishes;
 - (b) maximising active uses such as retail, food and drink and outdoor dining at the ground level;
 - (c) seamlessly integrating with adjoining and neighbouring public open spaces; and
 - (d) improving the public domain in accordance with Council's Public Domain Style Manual and Design Codes.
- P2 Hume Street Park is expanded to provide the principle public open space area within the St Leonards / Crows Nest locality, generally located on land bound by Pole Lane, Oxley Street, Clarke Street and Hume Streets, with an integrated connection through to Willoughby Road.
- P3 Ernest Place provides a strong community focus for the Town Centre.
- P4 A new park off Holtermann Street adjacent to the Crows Nest Community Centre will provide additional public open space with excellent year-round sunlight.
- P5 Ensure that community facilities are integrated with public open spaces to maximise their use.

3.2.3 Desired Built Form

3.2.3.1 Subdivision

Objectives

- O1 To enable the development of feasible built forms which also incorporate suitable side and rear setbacks at the upper levels to afford daylight and ventilation between buildings.

Provisions

- P1 Maintain a frontage of 20m - 40m along the Pacific Highway, which generally equates approximately to the amalgamation of two or three original allotments.
- P2 Maintain a 10m - 15m frontage in the remainder of the Locality Area (except residentially zoned land) consistent with two storey parapet shopfront scale, especially along Willoughby Road and Alexander Street.
- P3 Development on consolidated allotments with a frontage wider than that identified in P1 or P2 is to be broken down by articulation, design and detailing, change in materials and colours.

3.2.3.2 Form, massing and scale

Objectives

- O1 To ensure that density and scale is concentrated close to the metro station and along the Pacific Highway.
- O2 To ensure a positive transition in height and scale down from tall towers to neighbouring and adjoining lower density areas.

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- O3 To enable the lower levels of buildings to be converted between retail, commercial and community uses over time.
- O4 To increase amenity for the occupants of buildings by providing direct access to sunlight, daylight and natural ventilation.
- O5 To maximise year round solar access to existing and new public spaces to be created within the Planning Area.

Provisions

- P1 Buildings should generally transition in height from the Crows Nest Metro Station and Pacific Highway down to the surrounding areas and the lower scale development within the Crows Nest Town Centre, the Upper Slopes Neighbourhood and Crows Nest Neighbourhood.
- P2 Roof design presents a varied, composed and interesting skyline when viewed from a regional context.
- P3 Architectural detailing and ornamentation provides a rich visual texture and a symbolic reference to the history of the place, the building's use or occupant.
- P4 Consistent parapet facade heights are provided along Willoughby Road and the Pacific Highway.
- P5 Developments on land greater than 1,000sqm should consider the incorporation of internal courtyards adjacent to laneways and through site links to broaden the range and form of open space in the locality.
- P6 Development is to be designed to maximise year round solar access to existing and the new public spaces to be created within the Planning Area.
- P7 Tower elements located above the podium level should not exceed 45m in width, unless it can be adequately demonstrated that sufficient physical vertical articulation can be provided to break the tower elevation into two distinct elements.
- P8 Minimum floor to floor heights on land zoned *E2 Commercial Centre* or *MU1 Mixed Use* must be provided:
 - (a) Ground floor level: 4.0 – 4.5m
 - (b) First floor level: 3.6m
 - (c) Levels above: 3.0m (residential uses) or 3.6m (non-residential uses)
- P9 Residential floor to floor heights should generally not exceed 3.2m, except on the first floor level in accordance with P7 above.
- P10 Rooftop plant and structures should be designed and positioned to comprise a minor element of the roofscape and minimise any increases to the building's overall overshadowing impacts.

3.2.3.3 Setbacks

Objectives

- O1 Reinforce the spatial definition of streets and public spaces.
- O2 Emphasise the street as a distinct spatial entity and design the street wall frontage with an appropriate human scale and sense of enclosure for the street.
- O3 Ensure consistent street frontages along the street alignment.
- O4 Recognise the variation in street frontage heights throughout the centre and allow flexibility to respond to context.
- O5 Ensure adequate transition in scale between employment / mixed use zones and residential zones.



Provisions

Note. Additional setbacks may be required for any part of the building located above the podium level. Refer to Section 3.2.3.5 to Part F of the DCP.

- P1 Buildings are to be setback from all street frontages in accordance with the Building Setbacks Map (refer to Figure F-3.2-3). Where a street setback is not indicated on the Buildings Setback Map, setbacks are to be in accordance with:
- (a) Section 1.3.5 to Part E of the DCP for land zoned *R3 Medium Density Residential*; or
 - (b) Section 3.2.5 to Part E of the DCP for land zoned *R4 High Density Residential*.
- P2 A nil setback is permitted to all side and rear boundaries without a street frontage on land zoned *E2 Commercial Centre* or *MU1 Mixed Use*.
- P3 Despite P2, a minimum 6m setback is required where land zoned *E2 Commercial Centre* or *MU1 Mixed Use* directly adjoins land which is residentially zoned along the zone interface. This requirement does not apply if the land is separated by a public road.
- P4 Side and rear setbacks on residentially zoned land are required to be setback in accordance with Section 1.3.6 to Part E of the DCP.
- P5 Despite P1 and P2, setbacks may be varied to be consistent with the built form envelope approved as part of the approval for the Crows Nest Metro Station and Over Station Development (OSD).
- P6 Despite P1-P4, where a site contains a heritage item and that item is to be retained, the heritage item may maintain its existing setback.
- P7 Despite P1-P4, increased setbacks may be required for the podium levels contain residential uses to achieve adequate building separation in accordance with [SEPP \(Housing\) 2021](#).
- P8 Building alignments and setbacks should also respond to important elements of the nearby context including public spaces and heritage buildings, monuments and landscape elements, in order to complement the streetscape. In some places, this may require greater building setbacks than those specified in Figure F-3.2-3.



Figure F-3.2-3: Building Setbacks Map



3.2.3.4 Podium Heights

Objectives

- O1 To strengthen the urban form of the Centre with consistent street wall heights.
- O2 To achieve comfortable street environments for pedestrians in terms of daylight, scale, sense of enclosure and wind mitigation as well as a healthy environment for street trees.
- O3 To enhance the distinctive character of streets within the Centre.
- O4 To ensure the context of heritage items are not adversely eroded.

Provisions

- P1 Podiums are to be provided to in accordance with the Podium Heights Map (refer to Figure F-3.2-4).
- P2 Despite P1, corner sites are to maintain a consistent podium height to all street frontages to achieve consistent built form.
- P3 Despite P1, sites containing a heritage item and where that heritage item is to be retained the existing podium height may be retained.
- P4 Despite P1 podium heights may be varied to be consistent with the built form envelope approved as part of the approval for the Crows Nest Metro Station and Over Station Development (OSD). However, distinct architectural elements are to be incorporated to reflect the desired podium heights.
- P5 If required, podiums are to step down with the topography.



Figure F-3.2-4: Podium Heights Map



3.2.3.5 Above Podium Setbacks

Objectives

- O1 To protect daylight access at street level and permit views of sky from the street by providing setbacks above the podium that promote separation between buildings and assist in mitigating urban heat.
- O2 To ensure that built form achieves comfortable public domain conditions for pedestrians, with adequate daylight, appropriate scale, and mitigation of urban heat and wind effects of tower buildings.
- O3 To reinforce important elements of the local context including public spaces, heritage buildings, monuments and landscape elements.
- O4 To provide adequate privacy, access to light, air and outlook for the occupants of buildings, neighbouring properties and future buildings.
- O5 To ensure towers are sufficiently separated to be seen in the round and reduce the cumulative overshadowing impact of towers on neighbouring residential areas.
- O6 To ensure development does not prejudice the re-development of adjoining sites in the future.
- O7 To avoid awkward ziggurat style built form outcomes.

Provisions

- P1 All buildings are to be designed to provide setbacks above the podium in accordance with the Above Podium Setbacks Map (refer to Figure F-3.2-5). Setbacks above the podium are to be measured from the outer wall of the podium.
- P2 A minimum above podium setback to all side and rear boundaries is required as follows:
 - (a) 4.5m for buildings up to 8 storeys in height; or
 - (b) 6m for buildings from 9 storeys up to 18 storeys in height; or
 - (c) 12m for buildings over 18 storeys in height.
- P3 Despite P1 and P2, above podium setbacks may be varied to be consistent with the built form envelope approved as part of the approval for the Crows Nest Metro Station and Over Station Development (OSD).
- P4 Despite P2, the following minimum above podium setbacks, measured from the property boundary, is required where land zoned *E2 Commercial Centre* or *MU1 Mixed Use* directly adjoins land which is residentially zoned along the zone interface:
 - (a) 12m for buildings up to 8 storeys in height; or
 - (b) 15m for buildings over 8 storeys in height.

This requirement does not apply if the land is separated by a public road.
- P5 Despite P1, P2 and P3, increased setbacks above the podium may be required to achieve adequate building separation in accordance with [SEPP \(Housing\) 2021](#).
- P6 Only one step in the built form between the podium walls and tower is permissible.
- P7 An existing adjacent building, even if heritage listed, cannot be used to justify a reduced setback that would compromise the development potential of the adjacent site in the future.

Note. Above podium setbacks are measured to the outer face of any part of the building including balconies, architectural detailing and the like.



Figure F-3.2-5: Above Podium Setbacks Map



3.2.3.6 Active Frontages

Objectives

- O1 To provide for the amenity, interest and liveliness of the street environment and public open spaces.
- O2 To ensure a positive experience for pedestrians with the necessary fine grain environment of the street.
- O3 To enable sensory engagement with the street and public spaces.
- O4 To provide an active ground floor frontage that is accessible and integrated with the design of the public domain.
- O5 To maximise the extent of active frontages in the public domain.
- O6 To promote activity, connectivity and variety in the public domain.
- O7 To increase passive surveillance of the street and other public spaces and enhance safety.

Provisions

- P1 Buildings must contain active frontages to all street frontages, with the exceptions of public laneways.
- P2 Where a site has a direct frontage to an existing or proposed open space, an active frontage is to be provided to that interface. In particular, active frontages are to be provided to:
 - (a) Hume Street Park, including the link between Willoughby Road and Hume Street;
 - (b) Ernest Place, between Willoughby Road and Willoughby Lane; and
 - (c) Holtermann Reserve (proposed).
- P3 Active frontages are to be provided along both sides of through site links.
- P4 Active frontages to public laneways are encouraged where practical but not where they do not have an interface with residentially zoned land.
- P5 Where a site has multiple street frontages, all service and vehicular access points must not be provided off the primary street frontage.
- P6 Fire escapes and service doors must be seamlessly incorporated into the facade with quality materials.
- P7 Avoid the use of colonnades along all street frontages.

3.2.3.7 Solar access

Objectives

- O1 To ensure existing and future parks and public spaces in and around the Centre are afforded a high level of amenity through the maintaining and maximising of solar access during periods in the day when they are most used throughout the year.
- O2 To promote active and passive recreation to public spaces to service existing and planned population of the Centre and surrounds.
- O3 To ensure the successful growth and survival of trees and vegetation within the parks and public spaces.

Provisions

- P1 Development is to be designed to maximise year round solar access to existing and proposed new public spaces to be created within the Planning Area.
- P2 Development must not result in a net increase in overshadowing to the following existing and proposed public spaces between 10am and 3pm from the March Equinox to the September Equinox (inclusive):

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- (a) Hume Street Park, comprising all land located between Pole Lane, Willoughby Road, Clarke Street and Oxley Street and zoned *RE1 Public Recreation*; and
 - (b) Ernest Place, comprising all land within the Ernest Street road reserve located between Willoughby Road and Willoughby Lane and zoned *RE1 Public Recreation*; and
 - (c) Holtermann Reserve (proposed), comprising that part of Lot 1, DP 785343 and zoned *SP2 Infrastructure – Car Park*.
- P3 Development must not result in a net increase in overshadowing to the Willoughby Road road reserve, between Chandos Street and Falcon Street between 10am and 3pm from the March Equinox to the September Equinox (inclusive).
- P4 Despite P2 and P3, development on land located directly adjacent to the identified public spaces may result in a net increase in overshadowing of that space, but only if that development strictly complies with relevant built form envelope controls that apply to that land under both NSLEP 2013 and this DCP.

3.2.3.8 Awnings

Objectives

- O1 To maximise pedestrian comfort and continuous protection.

Provisions

- P1 Awnings must be provided to all street frontages on land zoned *E2 Commercial Centre* and *MU1 Mixed Use*, except laneways.

3.2.3.9 Car accommodation

Objectives

- O1 To minimise the impact on pedestrian safety.
- O2 Ensure that traffic generation is minimised.
- O3 To minimise the visual impact of vehicles.
- O4 Ensure high levels of accessibility.

Provisions

- P1 Where a property has a frontage to a laneway, vehicular access must be made from the laneway.
- P2 No vehicular access is permitted to:
 - (a) Willoughby Road,
 - (b) Pacific Highway, or
 - (c) Falcon Street.
- P3 Shared vehicular access to Shirley Road must be maintained to all properties between 286 and 306 Pacific Highway.
- P4 All off-street car parking must be provided underground, except when owned and operated by Council as a public car park.
- P5 The level of parking provided on sites in close proximity to the metro station should be minimised as far as practical.

3.2.4 Hume Street Park

Plan of Management

- P1 Development is not permitted on the Hume Street Park site:
 - (a) until a Plan of Management has been prepared for the site; and
 - (b) the development is consistent with the Plan of Management.



Diversity

- P2 The principal purpose is to provide a large recreational area and urban plaza with a variety of community, recreational and business purposes provided below ground level.

Form, massing and scale

- P3 Development is predominately located below ground, to ensure that the land is highly accessible for pedestrians and can be actively used as a recreational space and urban plaza.
- P4 Any development located above ground shall not exceed 1 storey in height.

3.2.5 27-57 Falcon Street

3.2.5.1 Desired Future Character, Design Objectives and Key Principles

- P1 Development is to respond to the scale and character of the existing development and desired future character of the surrounding area.
- P2 Built form, scale and massing is to transition in scale across the site from a mixed use, higher density typology in the western portion reflective of the Crows Nest Town Centre to a lower to medium density residential typology on the eastern portion.
- P3 Development should balance the provision of new residential flat buildings within a Town Centre, while maintaining a reasonable level of amenity, privacy and solar access for low density neighbouring residents on Alexander Lane, Falcon Street and in the Hayberry Conservation Area.
- P4 A mixed-use typology with medium rise residential flat buildings built to the boundary with commercial on ground level at the corner of Falcon Street and Alexander Lane. A residential typology to the eastern part of the site along Falcon Street setback from the street, with multi dwelling housing fronting Hayberry Lane to respond to the existing scale of the Hayberry Conservation Area.
- P5 Built form to transition to the existing lower scale development in the Hayberry Conservation Area.
- P6 Road widening along Alexander Lane with pedestrian amenity and road widening with a landscaped response and pedestrian amenity to Hayberry Lane.
- P7 Vehicular access from Alexander Lane with two-way access from / to Falcon Street.
- P8 A secure pedestrian through site link between Falcon Street and Hayberry Lane.

3.2.5.2 Desired Built Form

Objectives

- O1 To provide for increased opportunity for height and density in the growing Crows Nest Town Centre, close to public transport and services.
- O2 Building envelopes are to respond to the site's surrounding context which transitions in character from the Crows Nest Town Centre to the lower scale Heritage Conservation Area on Hayberry Street.
- O3 To achieve appropriate separation distances between existing and proposed buildings and ensure reasonable privacy and solar access is maintained to surrounding dwellings, mindful of the need for renewal at the site.
- O4 To ensure appropriate building lengths, a variety of building facades and a 'fine-grain' response to the public domain.

Provisions

Building Height

- P1 Buildings must not exceed the maximum height in storeys as shown on the Site Layout Plan at Figure F-3.2-6 and the following provisions:

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- (a) Building A - a maximum height of 6 storeys and stepping down in height to a maximum of 3 storeys at the Hayberry Lane frontage.
- (b) Building B - a maximum height of 6 storeys and stepping down in height to a maximum of 2-3 storeys at the Hayberry Lane frontage.
- (c) Building C - a maximum height of 4 storeys.
- (d) Building D - a maximum height of 3 storeys with a maximum 2 storey presentation to Hayberry Lane, with the third storey generally accommodated within the roof form.

P2 NSLEP 2013 may allow minor exceedances of the maximum heights stipulated on the Height of Buildings Map where it relates to plant and lift overruns only.

Street and Side Setbacks

P3 Building setbacks must, at a minimum, comply with the setbacks shown on the Site Layout Plan at Figure F-3.2-6 and the following provisions:

- (a) The following minimum setbacks are required to Falcon Street:
 - (i) Building A - 0m.
 - (ii) Building B - 0m.
 - (iii) Building C - 2m.
- (b) The following minimum setbacks are required to Hayberry Lane:
 - (i) Building A - 3m
 - (ii) Building B - 3m.
 - (iii) Building D - 1.5m.
- (c) The following minimum side setbacks are required to 56-63 Falcon Street:
 - (i) Building C - 4.5m.
 - (ii) Building D - 1.5m.
- (d) Building A is to be setback a minimum of 6m from the existing centreline of Alexander Lane.

3.2.5.3 Site Coverage

Objectives

- O1 To ensure that development is balanced and in keeping with the optimum capacity of the site acknowledging its unique size and location within the Crows Nest Town Centre at the interface between employment / mixed use zones and residential zones that accommodates a mix of building typologies.
- O2 To achieve appropriate building envelopes that ensure the development responds to its surrounding context and provides appropriate open space and landscaped area for residents and visitors.

Provisions

- P1 The maximum site coverage for this site is 65%.
- P2 For the purposes of P1, site coverage is to be determined in accordance with P2 to s.1.4.6 to Part E of the DCP.

3.2.5.4 Communal Open Space

Objectives

- O1 To provide high quality communal open space at ground level and on buildings with a reasonable level of outdoor amenity without reducing privacy to neighbouring dwellings.



O2 To provide a level of communal open space commensurate with *Apartment Design Guidelines* that is mindful of the site's unique location and building typologies.

O3 To ensure communal open space is useable.

Provisions

P1 Communal open space is provided in the locations shown on the Site Layout Plan at Figure F-3.2-6.

P2 Communal open space can be provided on the Building B rooftop only if the space is designed such that there is no potential for overlooking into private open space and its location will not create any noise issues for surrounding dwellings.

3.2.5.5 Landscaped Area

Objectives

O1 To ensure that landscaping is used to provide appropriate amenity for development and soften the appearance of buildings and their interface with the neighbouring dwellings and the public domain.

O2 To provide a level of landscaped area commensurate with *Apartment Design Guidelines* that is mindful of the site's unique location and building typologies.

Provisions

P1 The minimum landscaped area for the site is 20%.

P2 For the purposes of P1, landscaped area is to be determined in accordance with P2 to s.1.4.8 to Part E of the DCP.

3.2.5.6 Traffic, Access and Parking

Objectives

O1 To regulate traffic movements and reduce congestion on Falcon Street.

O2 To ensure that vehicular access is safe for motorists and pedestrians.

O3 To facilitate road widening along Alexander Lane.

O4 To facilitate road widening and the provision of a shared way along Hayberry Street.

O5 To create a safe, accessible and shared laneway network.

O6 To provide appropriate amount of basement parking spaces for residents, visitors and staff.

Provisions

P1 Vehicular access to the site must be from Alexander Lane and be located as far as practicable from Falcon Street.

P2 To facilitate vehicular access from Hayberry Lane, Alexander Lane is to be widened to allow for the provision of two-way traffic between Falcon Street and Hayberry Lane.

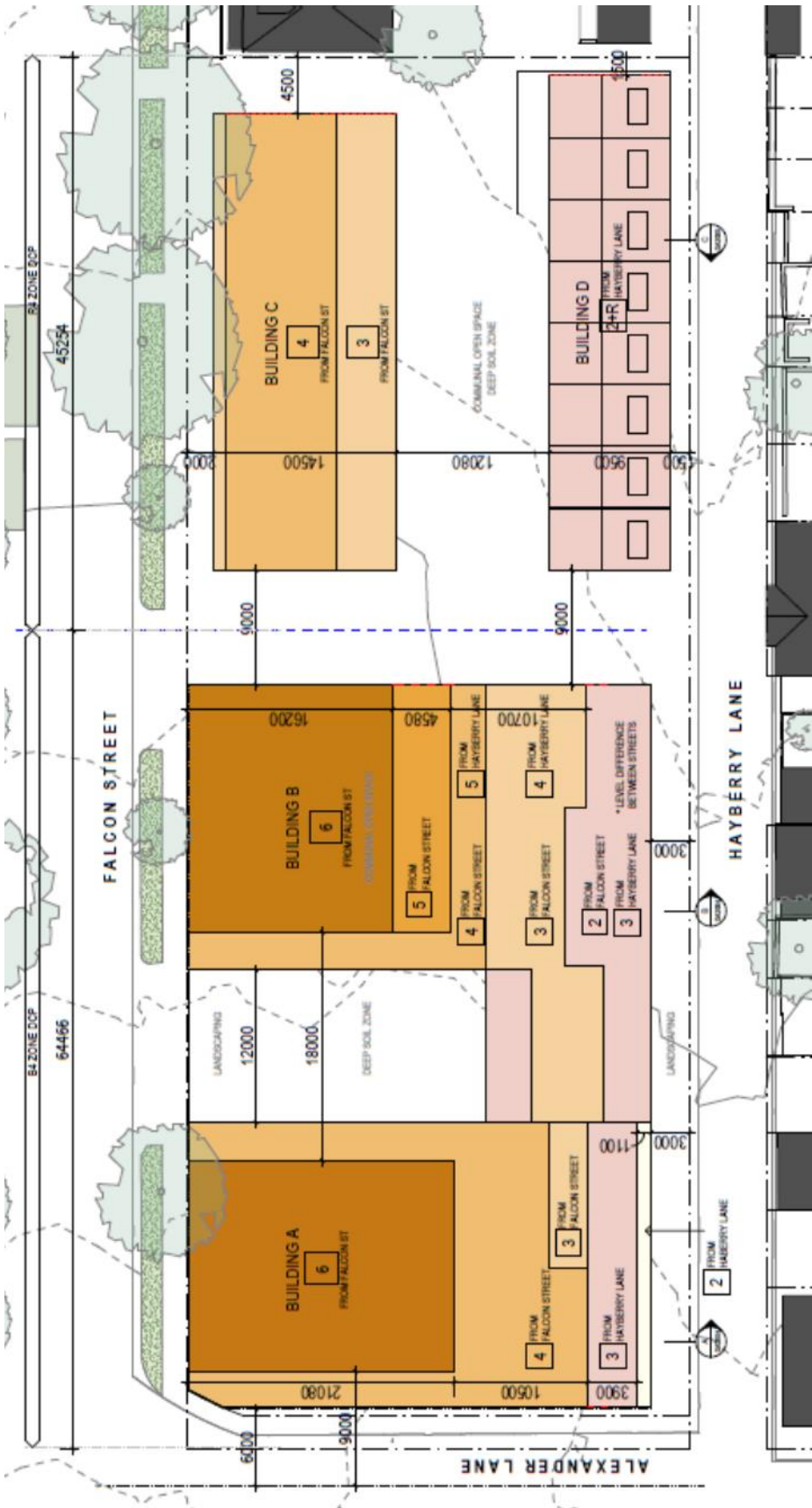
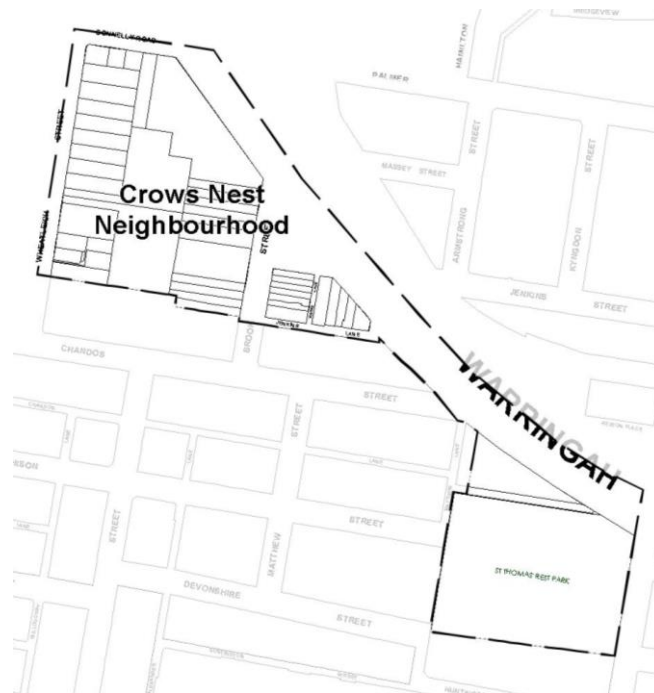


Figure F-3.2-6: Site Layout Plan

3.3 CROWS NEST NEIGHBOURHOOD



3.3.1 Significant Elements

Land Use

- P1 Predominantly residential accommodation.
- P2 Passive recreational spaces.

Topography

- P3 Gentle falls to the north-east towards the Warringah Expressway.

Views

- P4 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Vista north along Willoughby Road and Pacific Highway.

Identity / Icons

- P5 Warringah Expressway a major arterial thoroughfare.
- P6 St Thomas Rest Park.
- P7 Holtermann Estate Conservation Areas.

Subdivision

- P8 Traditional grid subdivision pattern interrupted by the juxtaposition of the Warringah Expressway.
- P9 A mix of narrow deep allotments reflecting detached and semi-detached housing forms and larger consolidated allotments reflecting multi dwelling housing and residential flat building forms.

Streetscape

- P10 Tree lined streets with grassed verges and concrete footpaths.
- P11 Buildings setback from the boundary and aligned with the street frontage.
- P12 Low brick fences.

3.3.2 Desired Future Character

Diversity

- P1 Predominantly a mix of dwelling houses, attached dwellings, multi dwelling houses and residential flat buildings according to zone.
- P2 Retention and enhancement of existing public open spaces.

3.3.3 Desired Built Form

Form, massing and scale

- P1 Retention of a low density residential character along Wheatley Street.

Access

- P2 Vehicle access on Brook Street should be carefully designed to minimise disruption to vehicular traffic.

Noise

- P3 Elevations of buildings fronting Chandos Street and Warringah Expressway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

3.4 HOLTERMANN ESTATE A CONSERVATION AREA



3.4.1 History

The Holtermann Estate Conservation Areas are part of original land grants to Alexander Berry, JR Hatfield and A Mosman. Extensive land purchases by BO Holtermann in the 1880s led to consolidated subdivision of large areas.

Holtermann's Estate sought to provide "comfortable working men's houses". The Holtermann Estate A Conservation Area was subdivided and offered for sale during the 1880s and 1890s.

Main period of construction-1884 to 1915.

3.4.2 Description

The Holtermann Estate A Conservation Area is the northern most section of the larger Holtermann Estate and is bounded by St. Thomas Rest Park, the Warringah Expressway and commercial development to the west.

The landform falls slightly to the north and west. Subdivision is determined by a grid pattern of wide streets, rear lanes and narrow cross lanes. Lot sizes are slightly larger than other parts of the Holtermann Estate though many lots have been developed for attached dwelling houses.

The area is characterised by its low scale of single storey, hipped roof, detached and attached dwelling houses that includes a mix of late 19th and early 20th century building styles, and restrained examples of Victorian Georgian, Filigree and Italianate, Federation Queen Anne and Federation Bungalow. There are also some Inter-War Californian Bungalow and Art Deco styles with post war residential flat buildings and modern infill housing.

Street verges are typically 3.5m wide and include grass with concrete or bitumen footpaths and crossings to off-street parking. Deep set sandstone kerbs remain in some locations. Houses to the high side of the street are often set on sandstone plinths with retaining walls to the street. Rear lanes are lined with fences, carports and garages with some development fronting the lanes.

There are long views along the main streets and cross views along the lanes.

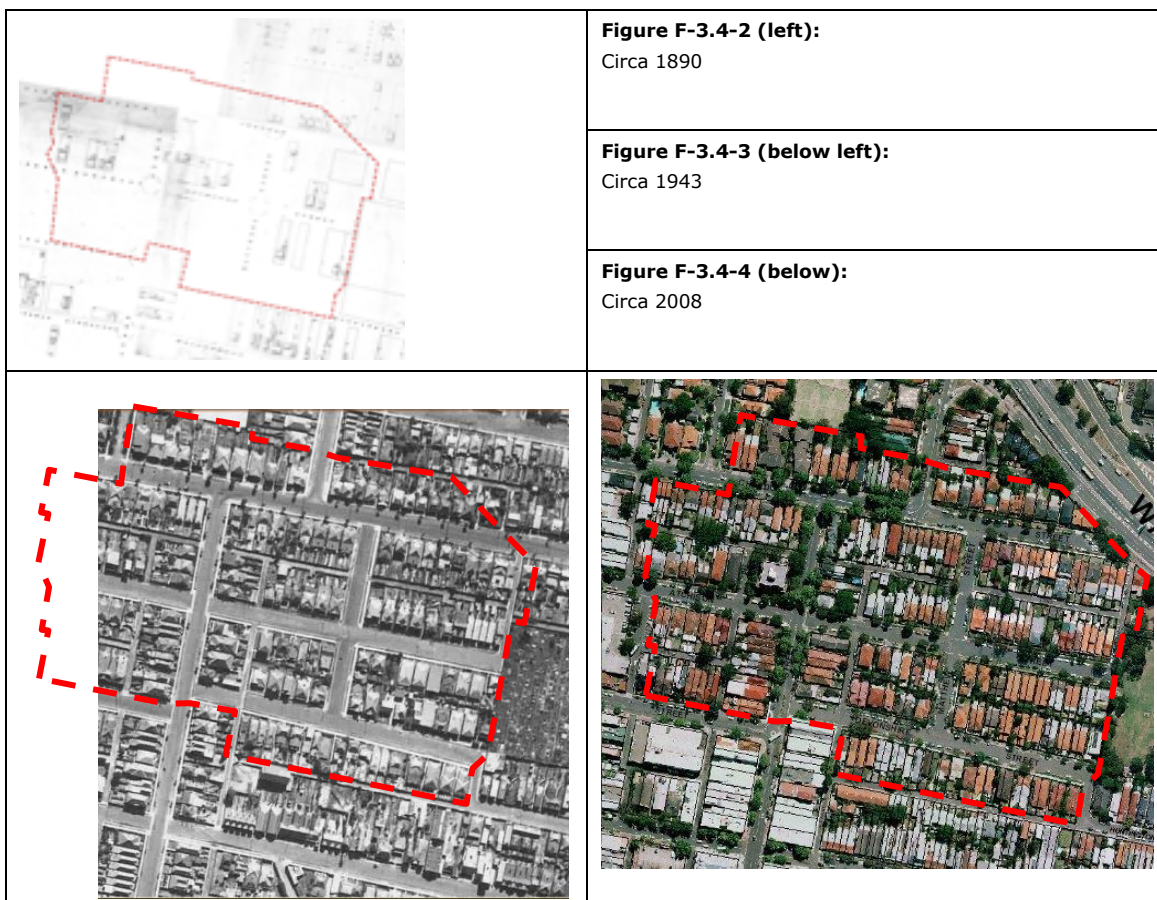
Front gardens contribute to the landscaping of the streets. Gardens follow the natural fall of the land with steps to the street on the high side. High and low scale street trees.

Section 3 - St Leonards / Crows Nest Planning Area

3.4.3 Statement of Significance

The Holtermann Estate A Conservation Area is significant:

- (a) as a late 19th century subdivision for speculative housing.
- (b) For its regular grid of streets, rear lanes and cross lanes.
- (c) For its consistent late 19th and early 20th century residential character and the unity of its low scale built form that derives from its regular grid subdivision pattern and its single storey, detached and attached dwelling houses in a mixture of late Victorian and early Federation styles.



3.4.4 Significant elements

Topography

P1 Slight falls to the north and west.

Subdivision

P2 Detached houses: 380m² lots with 10m frontage.

P3 Semi-detached and attached houses and some detached houses: 180m² to 260m² lots with 4-6m frontages. Long narrow lots with frontages to street and laneway (where they occur).

Streetscape

P4 Street trees align streets.

P5 Sandstone retaining walls relate to changes in level between streets and lots.



P6 Continuous grass verges and sandstone kerbs without vehicle crossings.

P7 Buildings addressing laneways have a low scale and service character.

Views

P8 Vistas along major streets and lanes.

3.4.5 Characteristic buildings

P1 Single storey, detached, semi-detached and attached dwelling houses.

3.4.6 Characteristic built elements

Siting

P1 Located towards the front of the block.

P2 Consistent setbacks.

Form, massing and scale

P3 Single storey with hipped and gabled roofs at the front, with skillion rear extensions.

P4 Reduced height and scale to rear.

P5 Open verandahs to front.

P6 Projecting front gables beside recessed verandahs.

P7 Dwelling houses in groups of identical design (detached, semi-detached and attached) often have continuous front verandahs.

P8 Strong skyline of simple pitched roofs and tall chimneys visible from street and rear lanes and stepped along the streets/lanes.

Roofs

P9 Hipped roofs pitched between 30 and 45 degrees without dormers or openings that can be seen from the street.

P10 Gabled ends for projecting bays to the street.

P11 Skillion roofs to rear extensions.

P12 Brick and rendered chimneys with terra cotta chimney pots.

External Materials

P13 Sandstone, timber weatherboards or face brick on sandstone foundations.

P14 Original rendered walls.

P15 Slate, corrugated metal and terra cotta tiled roofs.

P16 Timber windows, doors and joinery in a Victorian, Federation or Edwardian style.

P17 Original front garden landscaping.

Fences

P18 Original low front fences.

P19 Timber fences rear and side.

P20 Sandstone plinths, sandstone piers, metal palisade and gates, timber pickets, timber rails and mesh, pipe and mesh gates, original face brick with piers.

Car accommodation

P21 Located off rear lanes.



Section 3 - St Leonards / Crows Nest Planning Area

3.4.7 Uncharacteristic and undesirable buildings and built elements

- P1 Over-scaled, two storey additions; contemporary buildings with laneway frontages; two storey buildings to laneways; over-scaled and poorly detailed carports and garages; front and side dormers and rooflights; modified roof forms; removal of original detailing; verandah infill; rendered and painted face brickwork; modern facades; high walls and fences to the street.

3.5 HOLTERMANN ESTATE B CONSERVATION AREA



3.5.1 History

The Holtermann Estate Conservation Areas are part of original land grants to Alexander Berry, JR Hatfield and A Mosman. Extensive land purchases by BO Holtermann in the 1880s led to the consolidated subdivision of large areas.

Holtermann's Estate sought to provide "comfortable working men's houses". The area between West Street and Willoughby Road was subdivided and offered for sale during the 1880s and 1890s.

Main period of construction 1880-1915.

3.5.2 Description

The Holtermann Estate B Conservation Area includes the central portion of the larger Holtermann Estate.

The landform is generally level, with slight falls to the south. Subdivision is determined by a grid pattern of wide streets and narrow, rear lanes. Lot sizes vary and many lots have been developed for attached houses.

The area is characterised by its low scale of single storey, hipped roof, detached and attached dwelling houses that include a mix of late 19th and early 20th century building styles, and restrained examples of Victorian Georgian and Filigree, Victorian Italianate, Federation Queen Anne and Federation Bungalow. There are also some Inter-War Californian Bungalow and Art Deco styles with some post war residential flat buildings and modern infill housing. There are examples of high quality attached dwellings.

Street verges are typically 3.5m wide and include grass with concrete or bitumen footpaths. Deep set sandstone kerbs remain in some locations. Rear lanes are lined with fences, garages and carports with some remnant dunnies. The lane intersections are sometimes terminated by the side profile of a corner building oriented to the cross street.

Sophia Street provides diagonal views.

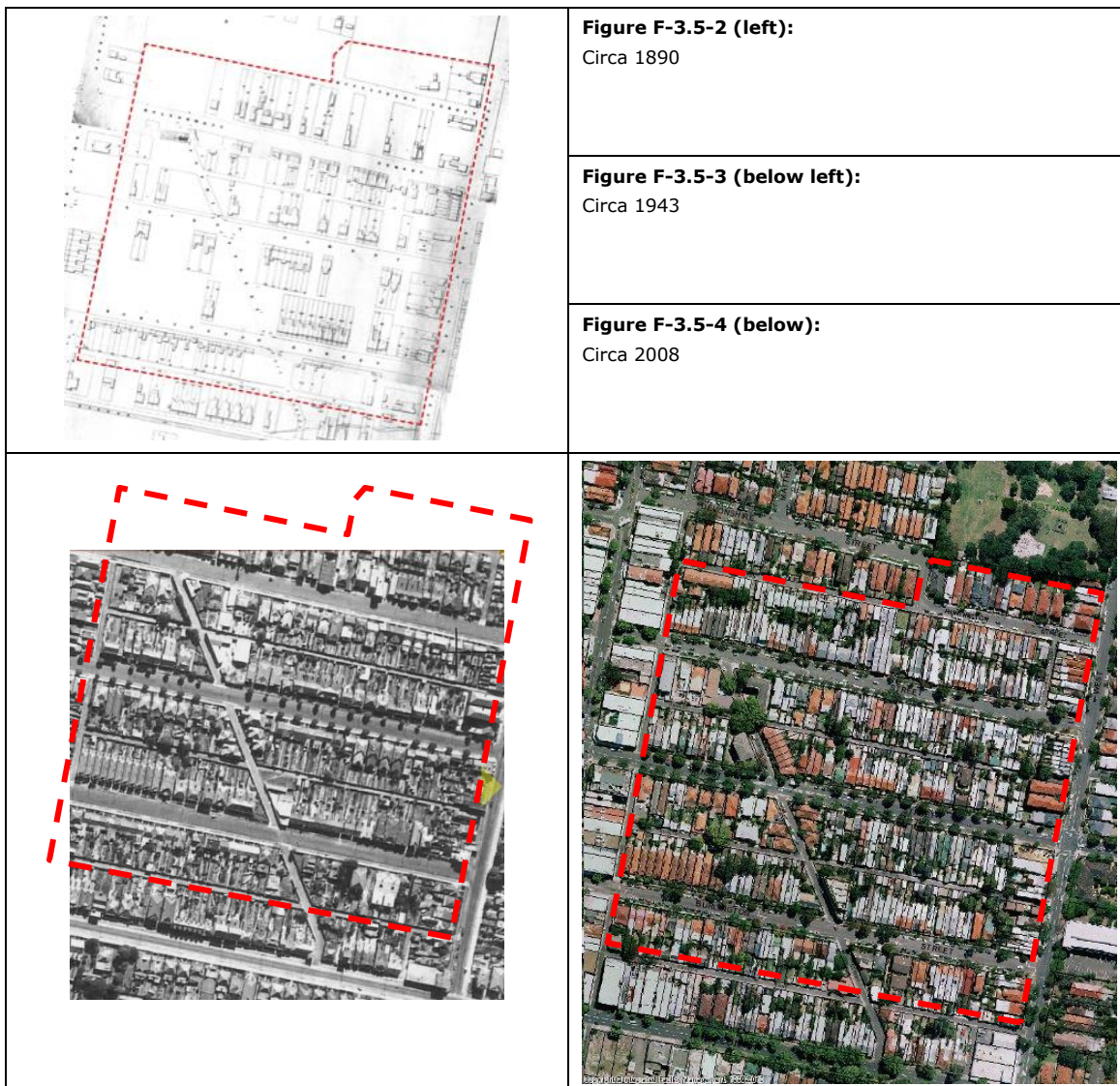
Front gardens contribute to the landscaping of the streets. Gardens follow the natural fall of the land with steps to the street on the high side. There are high and low scale street trees and shrubs.

Section 3 - St Leonards / Crows Nest Planning Area

3.5.3 Statement of Significance

The Holtermann Estate B Conservation Area is significant:

- (a) As a late 19th century subdivision for speculative housing.
- (b) For its regular grid of streets, rear lanes and cross lanes.
- (c) For its consistent late 19th and early 20th century residential character and the unity of its low scale built form that derives from its regular grid subdivision pattern and its single storey, detached and attached dwelling houses in a mixture of late Victorian and early Federation styles.



3.5.4 Significant elements

Topography

P1 Generally level, slight falls to the south east.

Subdivision

P2 Detached dwelling houses: 380m² lots with 10m frontage.



P3 Detached, semi-detached and attached dwelling houses: 180m² to 260m² lots with 4-6m frontages.

P4 Long narrow lots with frontages to street and laneway (where they occur).

Streetscape

P5 Street trees and shrubs align streets. Sandstone retaining walls relate to changes in level between streets and lots. Continuous grass verges and sandstone kerbs without vehicle crossings. Buildings addressing laneways have a low scale and service character.

Views

P6 Along Sophia Street.

3.5.5 Characteristic buildings

P1 Single storey, detached, semi-detached and attached dwelling houses.

3.5.6 Characteristic built elements

Siting

P1 Located towards the front of the block.

P2 Consistent setbacks.

Form, mass and height

P3 Single storey with hipped and gabled roofs with skillion rear extensions.

P4 Reduced height and scale to rear.

P5 Open verandahs to front.

P6 Projecting front gables beside recessed verandahs with decorative detailing.

P7 Dwelling houses in groups of identical design (detached, semi-detached and attached) which often have continuous front verandahs.

P8 Strong skyline of simple pitched roofs and tall chimneys visible from the street and rear lanes.

Roofs

P9 Hipped roofs pitched between 30 and 45 degrees without dormers or openings.

P10 Gabled ends for projecting bays to the street.

P11 Skillion roofs to rear extensions.

P12 Brick and rendered chimneys with terra cotta chimney pots.

External Materials

P13 Sandstone, timber weatherboards or face brick on sandstone foundations.

P14 Original rendered walls.

P15 Slate, terra cotta tiles, corrugated metal roofs.

P16 Original timber windows, doors and decorative joinery in a Victorian, Federation and Edwardian style.

P17 Original front garden landscaping.

Fences

P18 900-1600mm high to the street.

P19 1800mm high to laneways.



Section 3 - St Leonards / Crows Nest Planning Area

P20 Sandstone plinths, sandstone piers, metal palisade fences and gates, timber pickets, timber rails and mesh, pipe and mesh gates, original face brick with piers.

Car accommodation

P21 Located off rear lanes.

3.5.7 Uncharacteristic and undesirable buildings and built elements

P1 Over-scaled two storey additions; contemporary buildings with laneway frontages; two storey buildings to laneways; over-scaled and poorly detailed carports and garages; front and side dormers and rooflights; modified roof forms; removal of original detailing; verandah infill; rendered and painted face brickwork; modern facades; high walls and fences to the street.

3.6 HOLTERMANN ESTATE C CONSERVATION AREA



3.6.1 History

The Holtermann Estate Conservation Areas are part of original land grants to Alexander Berry, JR Hatfield and A Mosman. Extensive land purchases by B. O. Holtermann in the 1880s led to consolidated subdivision of large areas.

Holtermann's Estate sought to provide "comfortable working men's houses". The area between West Street and Willoughby Road was subdivided and offered for sale during the 1880s and 1890s.

The main period of construction across the Holtermann Estate was between 1884 and 1915.

3.6.2 Description

The Holtermann Estate C Conservation Area includes part of the southern end of the larger Holtermann Estate and is bounded by Falcon Street, West Street and the Pacific Highway.

The landform is generally level with some stepping across the streets. Lot sizes are irregular and the urban form is determined by a strongly defined grid pattern of wide streets and narrow rear lanes.

The area is characterised by modest, speculative cottages in the Victorian Georgian and Filigree, Victorian Italianate, Federation Queen Anne and Federation Bungalow styles. There are also some Inter-War Californian Bungalow and Art Deco styles and later infill development including the large campus of the Sydney Girls High School.

Street verges are typically 3.5m wide and include grass with concrete or bitumen footpaths. Deep set sandstone kerbs remain in some locations. Front cottage gardens contribute to the landscaping of the streets. Gardens are sometimes raised with centrally located steps to the street.

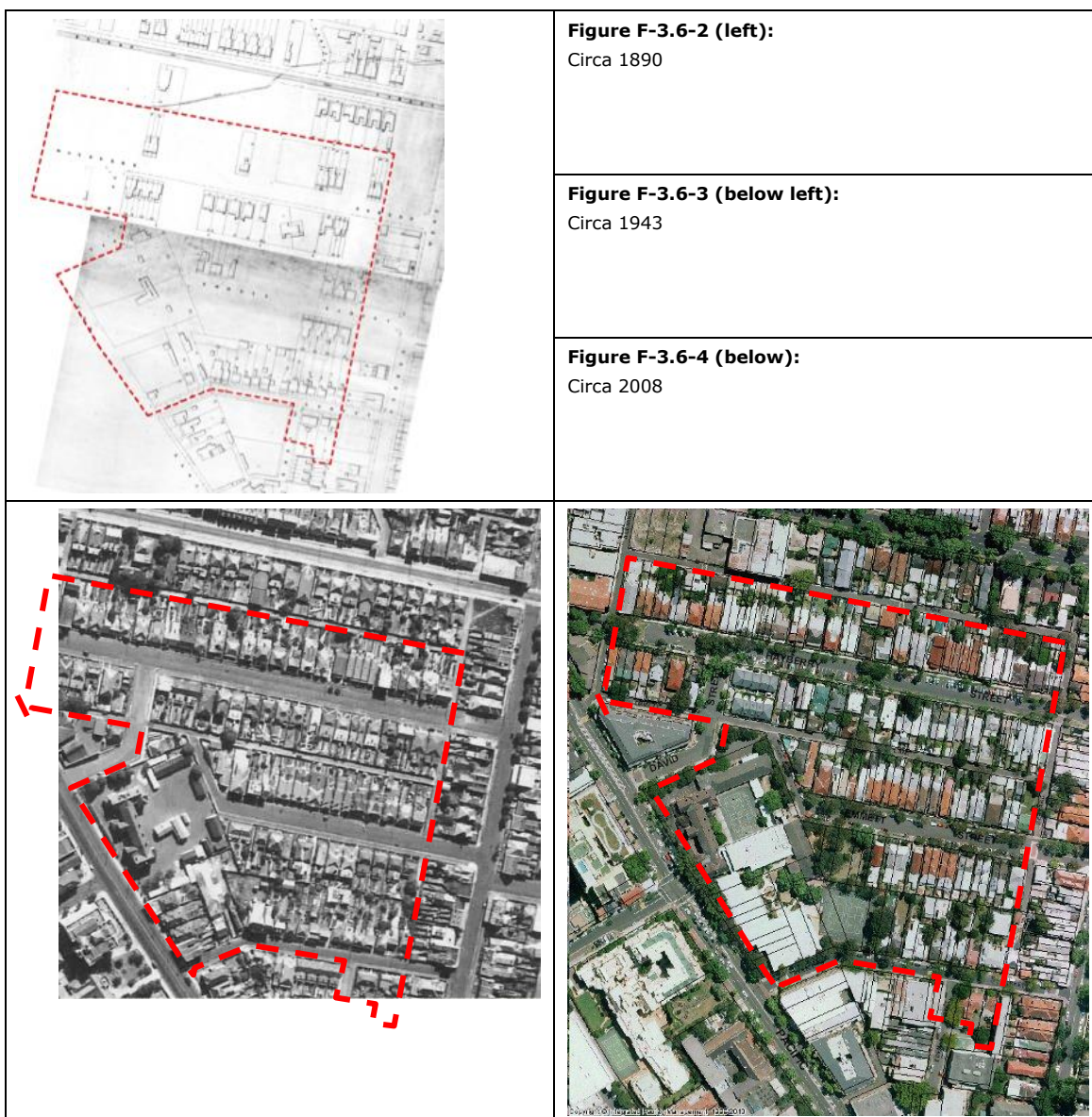
Rear lanes are important to the townscape and allow car access that helps the streets to maintain a pedestrian character. There are intrusive off street parking structures where rear lane access is not available. Other uncharacteristic and undesirable buildings and built elements include two storey additions constructed to the street, lot amalgamations and loss of original subdivision pattern, contemporary buildings with laneway frontages.

Section 3 - St Leonards / Crows Nest Planning Area

3.6.3 Statement of Significance

The Holtermann Estate C Conservation Area is significant:

- (a) For its late 19th and early 20th century residential character that is characterised by single storey, detached and semi detached dwelling houses of modest scale in a mixture of late Victorian and early Federation styles.
- (b) As an area that represents the working class residential development of North Sydney at the turn of the century.



3.6.4 Significant elements

Topography

P1 Generally level, slight falls to the north and west with stepped street.

Subdivision

P2 Long narrow lots with frontages to street and laneway (where they occur). Semi-detached houses and some detached house.



Streetscape

- P3 Sandstone retaining walls relate to changes in level between streets and lots.
- P4 Continuous grass verges and sandstone kerbs without vehicle crossings.
- P5 Lanes have an open, low scale and service character with lines of timber fences.

Views

- P6 Limited street views.

3.6.5 Characteristic buildings

- P1 Detached, late Victorian, Federation and Edwardian semi-detached dwelling houses.

3.6.6 Characteristic built elements

Siting

- P1 Located towards the front of the block, with gardens to rear.
- P2 Consistent setbacks.

Form, massing and scale

- P3 Predominantly single storey.
- P4 Rear extensions located within a single storey roof line of reduced height and scale to the main dwelling.
- P5 Dwelling houses with wide frontages have projecting bays to the street beside recessed verandahs.
- P6 Dwelling houses in groups of identical design (detached, semi-detached and attached) with continuous front verandahs.
- P7 Strong skyline of a series of simple pitched roofs and tall chimneys stepped along the streets/lanes and following the natural changes in landform.

Roofs

- P8 Hipped roofs pitched between 30 and 45 degrees and without dormer windows or openings that can be seen from the street.
- P9 Gabled ends for projecting bays to the street, skillion roofs to rear extensions.
- P10 Brick chimneys are unpainted, unrendered or rendered with detailing, or rough cast with chimney pots.

Materials

- P11 Walls: face brick, timber weatherboards or sandstone on sandstone foundations.
- P12 Roofing materials: unglazed terra cotta tiles, or slate and corrugated metal on Victorian cottages and rear extensions.

Windows and doors

- P13 Late Victorian, Federation and Edwardian.

Fences

- P14 Rusticated sandstone base walls, face brick, timber (vertical pickets and horizontal railing and wire fences) or metal palisade. Face brick or sandstone piers and base with metal palisade panels. Higher timber fences to rear.

Car accommodation

- P15 Located off rear lanes.

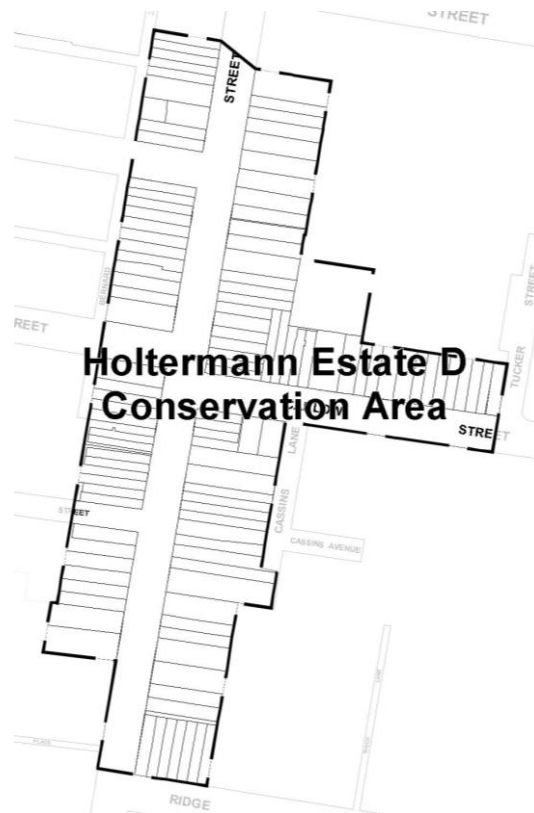


Section 3 - St Leonards / Crows Nest Planning Area

3.6.7 Uncharacteristic and undesirable buildings and built elements

- P1 Contemporary buildings and additions; two storey buildings to laneways; loss of original detail, painting and rendering of face brickwork; removal of original detailing, front and side dormers and rooflights.

3.7 HOLTERMANN ESTATE D CONSERVATION AREA



3.7.1 History

The Holtermann Estate Conservation Areas are part of original land grants to Alexander Berry, JR Hatfield and A Mosman. Extensive land purchases by B.O. Holtermann in the 1880s led to consolidated subdivision of large areas.

Holtermann's Estate sought to provide "comfortable working men's houses". The area between West Street and Willoughby Road was subdivided and offered for sale during the 1880s and 1890s.

The main period of construction across the Holtermann Estate was between 1884 and 1915.

3.7.2 Description

The Holtermann Estate D Conservation Area is set to both sides of the southern end of West Street and is defined by Falcon Street and Ridge Street.

The landform is generally level and the urban form is determined by a strongly defined grid pattern of wide streets and narrow rear lanes.

The Area is characterised by modest, speculative cottages that include a mix of late 19th and early 20th century building styles including Victorian Georgian and Filigree, Federation Queen Anne and Federation Bungalow. There are also some two storey Victorian Italianate and Victorian Filigree terraces and Inter-War, Californian Bungalow and Art Deco styles.

The mature street trees are also a prominent and unifying feature of the West Street streetscape.

Front cottage gardens contribute to the landscaping of the streets, and are typical of small lot development of the pre-war era.



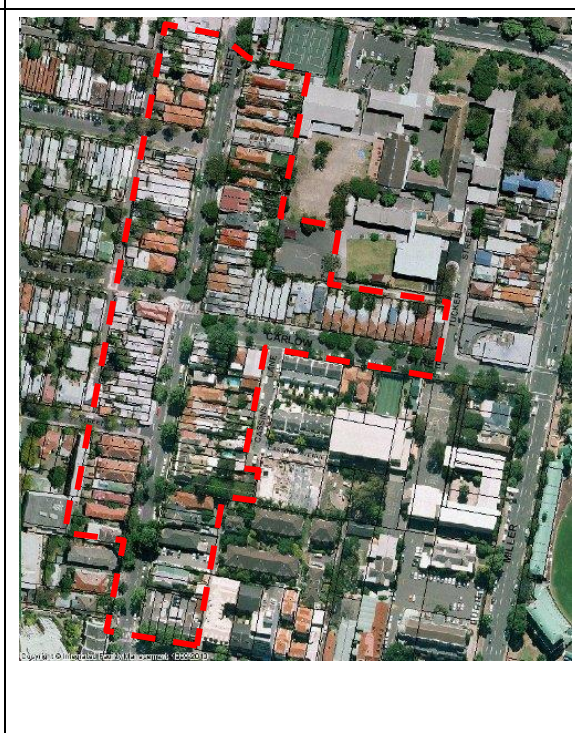
There are rear lanes that have a distinct character that is different to the streets and that allow car access.

Section 3 - St Leonards / Crows Nest Planning Area

3.7.3 Statement of Significance

The Holtermann Estate D Conservation Area is significant:

- (a) for its consistent late 19th and early 20th century residential character that is characterised by single storey dwelling houses of modest scale and two storey attached dwellings in a mixture of late Victorian and early Federation styles.
- (b) for its regular grid subdivision pattern, the level landform and development over a single main development period.

	Figure F-3.7-2 (left): Circa 1890
	Figure F-3.7-3 (below left): Circa 1943
	Figure F-3.7-4 (below): Circa 2008
	

3.7.4 Significant elements

Topography

P1 Generally level, slight falls to the north and west.



Subdivision

- P2 Long narrow lots with frontages to street and laneway (where they occur).

Streetscape

- P3 Sandstone retaining walls relate to changes in level between streets and lots. Continuous grass verges and sandstone kerbs without vehicle crossings. Street trees align streets. Buildings addressing laneways have an open, low scale and service character with lines of timber fences.

Views

- P4 Along West and Carlow Streets.

3.7.5 Characteristic buildings

- P1 A mixture of single storey detached and semi-detached dwelling houses and two storey attached dwellings.

3.7.6 Characteristic built elements

Siting

- P1 Located towards the front of the block.
P2 Consistent setbacks.

Form, massing and scale

- P3 Single storey detached and semi-detached dwellings.
P4 Two storey attached dwelling houses.
P5 Single storey, rear extensions within single storey roof line – reduced height and scale to rear of housing.
P6 Dwelling houses with wide frontages have projecting bays to the street beside recessed verandahs.
P7 Dwelling houses in groups of identical design often have continuous front verandahs.
P8 Strong skyline of a series of simple pitched roofs and tall chimneys stepped along the streets/lanes.

Roofs

- P9 Hipped roofs pitched between 30 and 45 degrees and without dormer windows or openings that can be seen from the street.
P10 Gabled ends for projecting bays to the street, skillion roofs to rear extensions.
P11 Brick chimneys are unpainted, unrendered or rendered with detailing, or rough cast with chimney pots.

Materials

- P12 Walls: face brick, timber weatherboards or sandstone on sandstone foundations. Where walls are painted darker shades are typically used for detailing.
P13 Roofs: unglazed terra cotta tiles, or slate and corrugated metal on Victorian cottages and rear extensions.

Windows and doors

- P14 Late Victorian, Federation, Edwardian and Inter War.

Fences

- P15 A mixed use of:
(a) low rusticated sandstone base walls, face brick;



Section 3 - St Leonards / Crows Nest Planning Area

- (b) timber (vertical pickets and horizontal railing and wire fences);
- (c) metal palisade;
- (d) face brick or sandstone piers and base with metal palisade panels.

Car accommodation

P16 Located off rear lanes.

3.7.7 Uncharacteristic and undesirable buildings and built elements

P1 Over-scaled, two storey additions; contemporary buildings with laneway frontages; two storey buildings to laneways; front and side dormers and rooflights; modified roof forms, removal of original detailing; verandah infill; rendered and painted face brickwork; modern facades; high walls and fences to the street, car parking in front setback, lot amalgamation and loss of original subdivision pattern.



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SECTION 4 CAMMERAY PLANNING AREA

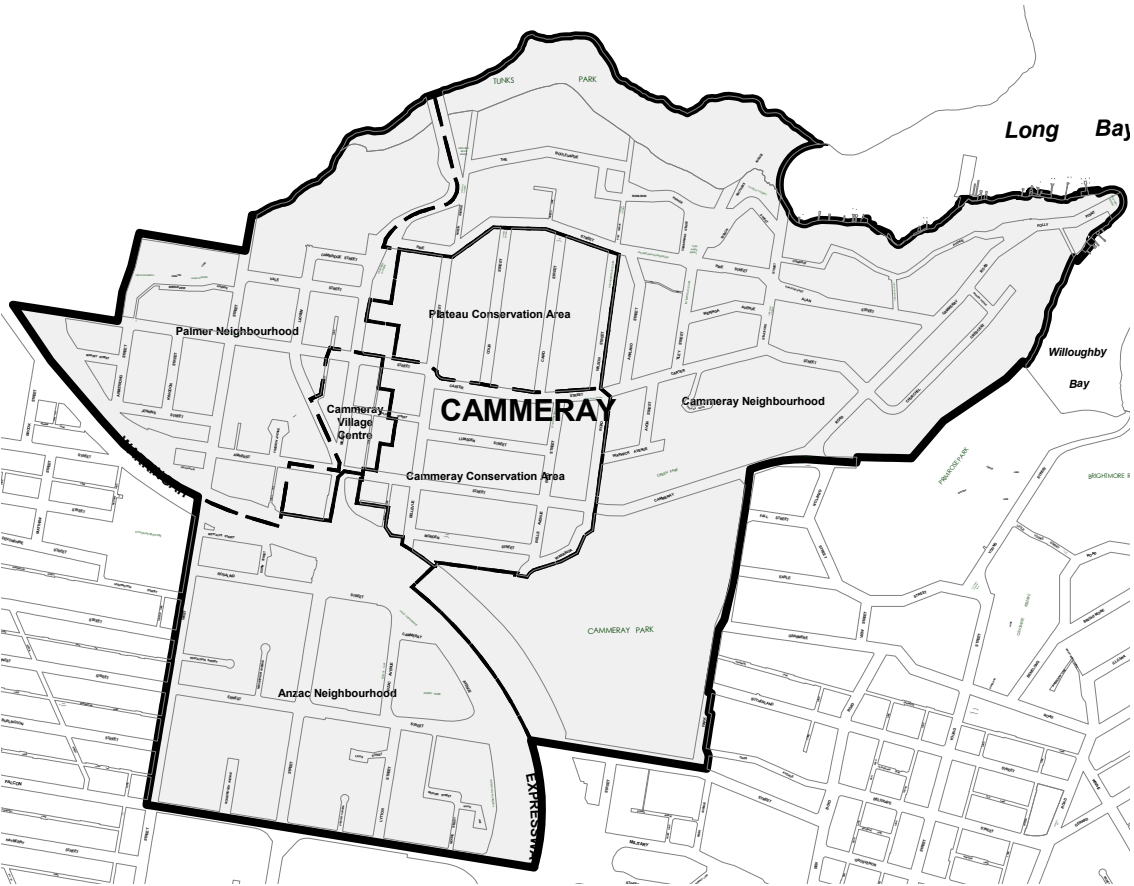
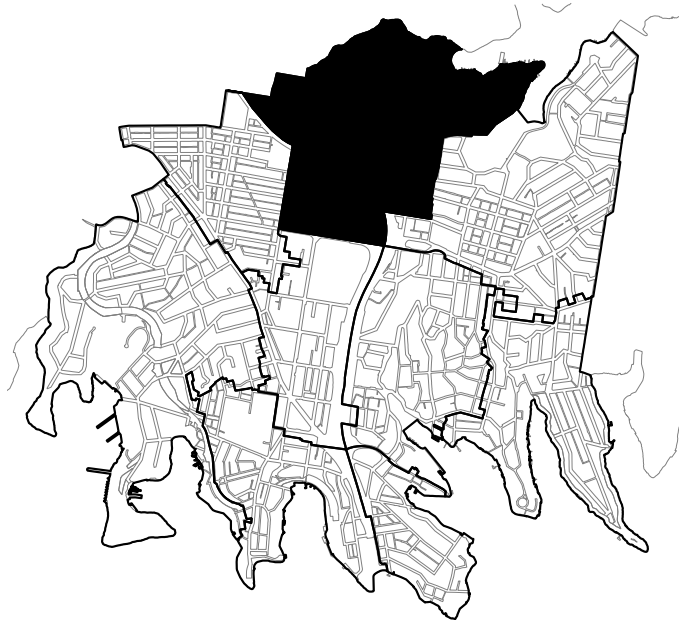


Figure F-4.1: Cammeray Planning Area and associated Locality Areas

4.0 CAMMERAY PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the Cammeray Planning Area.

The Planning Area is focussed around Cammeray Village, which is an active, pedestrian friendly shopping area that has small scale shops and provides street level activity with an lively pedestrian environment, where:

- local shops cater to the local community and are balanced between basic needs such as food and grocery, and recreation such as cafes and galleries
- development on both sides of Miller Street is unified through common elements
- there is safe and easy pedestrian movement across Miller Street

The surrounding residential neighbourhoods are diverse in nature, where:

- most of the existing dwelling houses and dual occupancies are retained
- capacity exists to accommodate some attached dwellings, multi dwelling housing and residential flat buildings close to existing public transport, services and facilities
- the density of residential development generally reduces the further away from Miller and Falcon Streets a property is located.

and where:

Function

- large areas of open space are used by locals and the wider regional population for recreation

Environmental Criteria

- the natural foreshores and water courses of Willoughby and Long Bay are conserved and protected, with pedestrian access to these areas is extended and improved

Section 4 - Cammeray Planning Area

- the scale and form of foreshore development protects and enhances the scenic, environmental and cultural qualities of the foreshore and adjoining lands
- major views from lookouts and other vantage points are not obscured by buildings or landscaping
- existing natural features such as rock outcrops and sandstone cliffs are conserved
- bushland and wetlands are protected from the adverse effects of development – such as stormwater runoff, spread of exotic plants and weeds, and visual impact of buildings and structures
- use of locally indigenous flora extends habitats for native birds and other fauna
- man made noise is limited, especially near foreshore areas and bushland
- noise insulation and orientation minimises noise impacts on developments close to the Warringah Expressway and main roads

Quality Built Form

- any development that occurs, reflects and reinforces the existing distinctive built form/ landscape areas and distribution of accommodation types
- the significance of heritage items and Conservation Areas are maintained

Quality Urban Environment

- public transport, cycling and walking are the preferred means of transport
- parking is managed to reduce impacts to local residents from regional commuter parking
- comfortable and safe pedestrian routes are maintained and extended
- cohesiveness throughout the area and its many built forms is achieved through landscaping and street tree planting
- backyards are provided for a variety of social and recreational activities
- public plazas provide a vibrant focal point for the local community

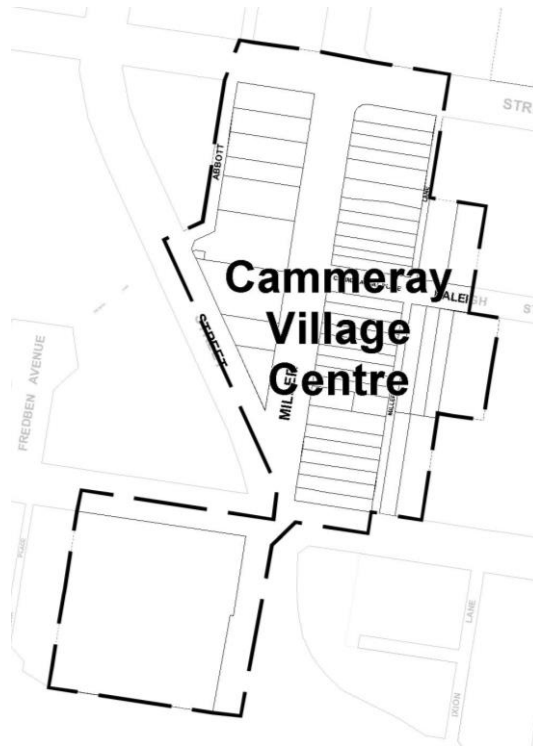
Efficient Use of Resources

- existing buildings and materials are conserved
- stormwater is retained for re-use onsite

The Cammeray Planning Area includes the following Locality Areas in which development is also subject to relevant provisions below:

- Section 4.1: Cammeray Village
- Section 4.2: Cammeray Neighbourhood
- Section 4.3: Anzac Neighbourhood
- Section 4.4: Palmer Neighbourhood
- Section 4.5: Plateau Conservation Area
- Section 4.6: Cammeray Conservation Area

4.1 CAMMERAY VILLAGE CENTRE



4.1.1 Significant Elements

Land Use

P1 Predominantly mixed commercial and residential development.

Topography

P2 Generally flat, straddling the topmost part of the ridge following the alignment of Miller Street.

Natural Features

P3 Forms topmost part of the ridge following the alignment of Miller Street.

Views

P4 The following views and vistas are to be preserved and where possible enhanced:

(a) District views to Middle Harbour from the upper levels of some buildings.

Identity / Icons

P5 Cammeraygal Place.

P6 Cammeray Square.

Subdivision

P7 Generally small narrow allotments in multiple ownerships to the east of Miller Street.

P8 Generally large consolidated allotments to the west of Miller Street.

Streetscape

P9 Wide, fully paved footpaths.

P10 Buildings built to street and laneway frontages.

P11 Continuous awnings along Miller Street.

Section 4 - Cammeray Planning Area

P12 Irregular planting of street trees.

P13 Active frontages to Miller Street and Raleigh Street.

Public transport

P14 Development to respond to the high level of accessibility to high frequency public bus services along Miller Street.

4.1.2 Desired Future Character

Diversity

P1 Generally small scale commercial activities, concentrated at the ground level, with a mix of commercial and residential uses above.

Active Streets

P2 The ground level of all buildings should be designed to activate streets, laneways and the public car parks to which they front.

P3 Active frontages must be provided to the ground floor level of buildings directly fronting Raleigh Street.

Traffic

P4 Development should minimise impacts to traffic flow on Miller Street.

P5 Development should ensure that pedestrian access continues to unify both sides of Miller Street.

4.1.3 Desired Built Form

Form, massing & scale

P1 Vertical design elements to reduce the visual bulk of buildings and create an appearance similar to traditional shops.

P2 Parapet heights and setbacks to be consistent along the Miller Street frontage.

P3 Where lots are consolidated, the built form reflects the former fine grain subdivision pattern.

Setback

P4 Setback 3m, at ground level from the Palmer Street frontage. Landscaping including trees is to be provided within this setback.

Podiums

P5 Podium of 8.5m (2 storeys) at street frontage with a setback of 2.5m above the podium.

Awnings

P6 Awnings to be provide along all street frontages.

Noise

P7 Elevations of buildings fronting Miller Street and the Warringah Freeway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

Car accommodation

P8 Car parking must be provided underground, with the exception of Council owned or operated public car parking.

P9 Trees in Council car park to be retained contribute to amenity of area.

4.2 CAMMERAY NEIGHBOURHOOD



4.2.1 Significant Elements

Land Use

P1 Predominantly residential development.

Topography

P2 Generally falling from the ridge along Carter Street and Cammeray Road down to the foreshores of Long Bay and Willoughby Bay.

P3 Steeper land generally adjacent to the foreshore areas.

Natural Features

P4 Natural vegetation and landforms, including shoreline with rock outcrops and native vegetation.

P5 Wetlands in the form of Coastal Saltmarsh along the foreshores of Willoughby Bay (refer to Section 3.2 – *Bushland* to Part B of the DCP).

Views

P6 The following views and vistas are to be preserved and where possible enhanced:

(a) Pine Street lookout (7), Tiley Street lookout (8), Stratford Street lookout (9), Churchill Crescent lookout (12), Folly Point lookout (13).

(b) District views to Middle Harbour and the suspension bridge over Tunks Park.

Identity / Icons

P7 Tunks Park.

P8 Long Bay.

Section 4 - Cammeray Planning Area

- P9 Primrose Park.
- P10 Bushland on slopes above Tunks Park and Primrose Park.
- P11 Suspension bridge over Tunks Park.
- P12 Willoughby Bay, Long Bay and foreshores.
- P13 Cammeray Park.
- P14 Warringah Expressway.

Subdivision

- P15 Regular grid patterns on the upper slopes of the Area.
- P16 Irregular grid pattern adjacent to the foreshore areas due to the steep and irregular topography.
- P17 Long narrow lots adjacent to the foreshores of Long Bay.

Streetscape

- P18 Narrow width roads and split carriageways adjacent to foreshore and Tunks Park with garages, carports and retaining walls built to the street.
- P19 Wide roads with grassed verges on the upper slopes of the Area and informal street planting.
- P20 Double rail timber fences.
- P21 Tall sandstone fences and sandstone retaining walls.
- P22 Buildings generally setback from the boundary with a skewed alignment to respective street frontages.

Public transport

- P23 Regular public bus services through the area.

4.2.2 Desired Future Character

Diversity

- P1 Primarily low-density residential development consisting of dwelling houses, semi-detached houses concentrated along the foreshore areas.
- P2 Multi dwelling housing and residential flat buildings concentrated towards the tops of the ridges.
- P3 The density of development along foreshore areas and in areas of steep terrain must be kept to a minimum.

4.2.3 Desired Built Form

Siting

- P1 Buildings should provide adequate separation to bushland and foreshore areas.
- P2 Buildings should not obstruct views from neighbouring properties. This is especially important in foreshore areas or sites that enjoy water views.
- P3 Buildings should not obstruct views and vistas from public places to the waterway.

Form, scale and massing

- P4 Buildings near foreshore areas should address the waterway.
- P5 Buildings on sloping land should be designed to follow the slope of the land, with minimum cut and fill to be undertaken.



Foreshore

- P6 Boat sheds are small in scale and do not dominate the foreshore frontage.
- P7 Development associated with boating activity is kept to minimum and is compatible with the surrounding uses.
- P8 Development adjoining foreshore or bushland areas (such as Tunks and Primrose Parks) use muted colours and non-reflective materials to ensure the scenic and environmental Qualities are enhanced.
- P9 Walls and fences along the foreshore should be kept low enough to allow views of private gardens from the waterway.

4.3 ANZAC NEIGHBOURHOOD



4.3.1 Significant Elements

Land Use

- P1 Predominantly residential accommodation.
- P2 Passive and active recreational spaces.
- P3 Educational and community facilities.

Topography

- P4 Falling from Miller Street eastwards towards the Warringah Expressway.

Views

- P5 The following views and vistas are to be preserved and where possible enhanced:
 - (a) District views towards Middle Harbour from the upper levels of some buildings.

Identity / Icons

- P6 Cammeraygal High School.
- P7 Warringah Freeway.
- P8 ANZAC Park Primary School.
- P9 ANZAC Park.

Subdivision

- P10 Rectilinear grid pattern.

Streetscape

- P11 Wide street reserves with grass verges, concrete footpaths and street trees.
- P12 London Plane trees to Miller Street.
- P13 Mixture of low full masonry and part masonry and timber paling fences.
- P14 Buildings setback from the boundary and aligned with the street frontage.



Public transport

- P15 Development to take advantage of high levels of accessibility to high frequency public bus services along Miller, Ernest and Falcon Streets.

4.3.2 Desired Future Character

Diversity

- P1 Predominantly medium to high density residential development such as attached dwellings, multi dwelling housing and residential flat buildings according to zone.
- P2 Any increase in residential density should be primarily focused on sites situated on Miller Street, Falcon Street and West Street which have good access to public transport.

4.3.3 Desired Built Form

Siting

- P1 Development to provide gardens within the front setback, especially facing busy roads.

Noise

- P2 Elevations of buildings fronting the Warringah Freeway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

4.4 PALMER NEIGHBOURHOOD



4.4.1 Significant Elements

Land Use

P1 Predominantly residential accommodation.

Topography

P2 Falling from Miller Street north-west towards Tunks Park.

Natural Features

P3 Bushland on slopes above Tunks Park

Views

P4 The following views and vistas are to be preserved and where possible enhanced:

- (a) Fred Hutley Reserve lookout Hamilton Avenue (1), Palmer Street lookout (2).
- (b) District views to the suspension bridge over Tunks Park.

Identity / Icons

- P5 Tunks Park.
- P6 Suspension Bridge over Tunks Park.
- P7 Warringah Freeway.

Subdivision

P8 Generally regular grid pattern.

Streetscape

- P9 Generally single storey built form appearance fronting West Street between Palmer Street and Amherst Street.
- P10 Tree lined streets with grassed verges and concrete footpaths.
- P11 A mixture of masonry and sandstone retaining walls built to street frontages and dividing split carriageways.



P12 Buildings setback from the boundary and aligned with the street frontage.

P13 Low rendered masonry retaining walls to the high side of streets.

P14 A mixture of paling and double rail timber fences.

Public transport

P15 Development to take advantage of high levels of accessibility to high frequency public bus services along Miller Street.

4.4.2 Desired Future Character

Diversity

P1 Predominantly medium to high density residential development such as attached dwellings, multi dwelling housing and residential flat buildings according to zone.

4.4.3 Desired Built Form

Siting

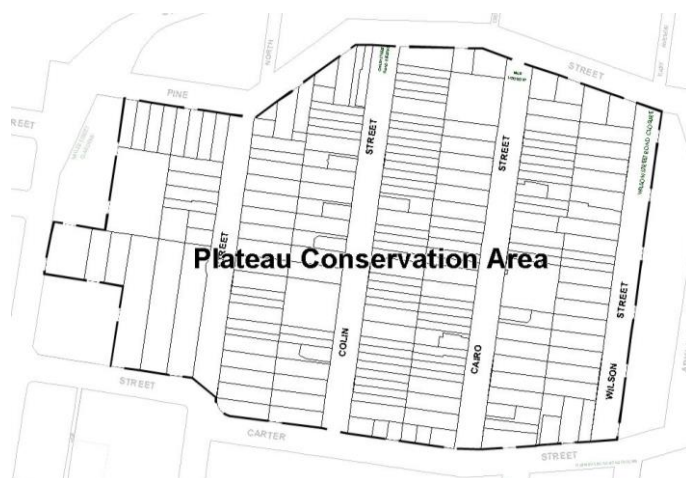
P1 Substantial gardens in front setback especially facing busy roads.

P2 Buildings should provide adequate separation to bushland areas.

Noise

P3 Elevations of buildings fronting the Warringah Freeway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

4.5 PLATEAU CONSERVATION AREA



4.5.1 History

The Plateau Conservation Area was subdivided as part of the 1899 *Cooliatta Estate* that was formed from the 23 acres grant to Alexander Macarthur in 1853.

In comparison with other parts of North Sydney the area was relatively remote, although the opening of the Suspension Bridge in 1892 and extension of the tramway along Miller Street in 1909 improved access.

The main period of development in the Conservation Area was in the early 20th century when the area developed slowly with modest houses on narrow blocks of land. Additional houses came after the First World War when the area was consolidated, particularly along the sloping northern boundary of Pine Street.

4.5.2 Description

The Plateau Conservation Area is located on a natural plateau at the northern end of Cammeray. The area is defined by escarpment along Pine Street, Wilson Street, Carter Street and Miller Street.

The landform slopes to the southeast and is a small plateau with escarpments to the north and east. The subdivision pattern is a regular grid of rectangular lots with dead end streets at the escarpments.

The Plateau Conservation Area is characterised by single storey Federation and Edwardian dwelling houses, reflecting the area's main period of development. The houses range from freestanding dwellings on large lots to small, semi-detached dwellings on narrow lots. There are also examples of Victorian Georgian style, weatherboard cottages.

The buildings generally have small setbacks from the street with cottage gardens and a mix of fence types.

The Brushbox street trees are characteristic of the area's main period of development, and give a strong regularity to the streetscape. Front gardens are well established, and often have extensive plantings to give an overall impression of a landscaped garden suburb.



4.5.3 Statement of Significance

The Plateau Conservation Area is significant:

- (a) For the unity of its subdivision history and consistency of the housing stock which is evident in the built form of the area.
- (b) For its largely intact residential form that illustrates small-scale housing including timber buildings, and which has survived without large scale intrusions.
- (c) For its strong landscape quality defined by street trees and front garden plantings that give an overall impression of a landscaped garden suburb.

4.5.4 Significant elements

Topography

P1 Level plateau with escarpments to the north and east.

Subdivision

P2 Regular rectangular lots.

Streetscape

P3 Street trees and street gardens.

Section 4 - Cammeray Planning Area

- P4 Sandstone kerbing, sandstone and concrete retaining walls, street gardens. Changes of level in topography.

Views

- P5 North facing lookouts: Bellevue Street, Colin Street, Carter Street, Cairo Street. Views from Colin, Wilson, Bellevue and Pine Street steps.

4.5.5 Characteristic buildings

- P1 Single storey, detached and semi-detached Federation and Edwardian dwelling houses sometimes in groups. Some Victorian cottages.

4.5.6 Characteristic built elements

Siting

- P1 To the front of the lot with large gardens to the rear.

Form, massing and height

- P2 Detached and attached dwellings with projecting bays and verandahs to street.
P3 Double fronted cottages with projecting bays and flanking verandahs.
P4 Single storey, rear extensions within single storey roof line. Reduced scale to the rear.

Roofs

- P5 Hipped roofs pitched between 30 and 45 degrees without dormer windows or openings that can be seen from the street.
P6 Gabled ends for projecting bays to the street, skillion roofs to rear extensions.
P7 Brick and rendered chimneys.

Materials

- P8 **Walls:** Weatherboards; Face brick on sandstone foundations
P9 **Roofs:** Slate and corrugated metal on older dwellings; Terracotta tiles with corrugated iron or corrugated sheet metal on rear extensions.
P10 Timber verandahs. Timber windows and doors.

Windows and doors

- P11 Consistent with building period. Timber.

Fences

- P12 Low height front fences and walls to 800mm in height.
P13 Timber fences to side and rear.
P14 Timber picket, stone, face brick and brick piers with timber pickets or horizontal rails.

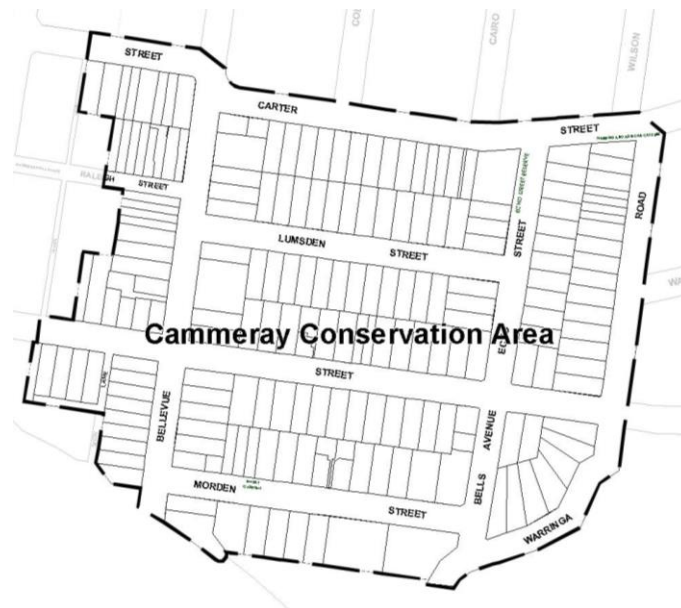
Car accommodation

- P15 No off street parking.

4.5.7 Uncharacteristic and undesirable buildings and built elements

- P1 Over-scaled additions; dormers to front roof slopes; carports and garages to the street; paved front gardens; high solid fences to the street; rendering and painting of face brick; loss of original detail; aluminium windows; contemporary buildings development.

4.6 CAMMERAY CONSERVATION AREA



4.6.1 History

Most of the Cammeray Conservation Area was subdivided in 1889 by Alexander Maxwell as the *Cooliatta Estate* on land granted to Alexander Macarthur in 1853. The south-western part of the area was called *Bells Paddock*.

The opening of the Suspension Bridge in 1892 and extension of the tramway along Miller Street in 1909 improved access to the area and provided a trigger for development. Most of the area was purchased as the Bell's Estate in 1909 after which time the area developed.

Very little redevelopment occurred in the area until the 1960s when uncharacteristic, three storey residential flat buildings were built in Morden Street.

4.6.2 Description

The Cammeray Conservation Area is bounded by Carter Street, the Freeway to the south, Warringa Road to the east, and Miller Street properties to the west.

The landform slopes down from the north towards the south and east and there are escarpments at Morden Street and Echo Street that form dead end roads.



The subdivision pattern is a regular grid that overlays the topography.

The characteristic buildings of the area are typically later Federation and Edwardian Queen Anne, reflecting the predominant period of development. Buildings are typically single storey, freestanding and constructed on a sandstone base with face brick walls and terracotta tiled hipped roof with asymmetrical street elevations with projecting gabled bays projection and a flanking verandah.

Street plantings and raised verges reinforce the cohesive character and regular pattern formed by the buildings and underlying lot pattern.

Several late Victorian residences are located in the northwest corner of the area, and timber houses are represented in Palmer, Bellevue and Raleigh Streets. There is some modern development to the southern edge.

Section 4 - Cammeray Planning Area

	<p>Figure F-4.5 (left): 1890 Subdivision Plan of the Cooliatto Estate</p> <p>Figure F-4.6 (below left): Circa 1943</p> <p>Figure F-4.7 (below right): Circa 2008</p>
	

4.6.3 Statement of Significance

The Cammeray Conservation Area is significant:

- (a) For the unity of its subdivision history which is evident in the built form of the area and that overlays its sloping and stepped topography;
- (b) As an area of modest Federation speculative builder housing set out in a geographically defined area that has survived without large-scale intrusion; and
- (c) For its landscape quality defined by street trees, stone boundary walls, rock escarpments, raised and front garden plantings that combine to give an impression of a landscaped garden suburb.

4.6.4 Significant elements

Topography

P1 Sloping towards east and south with sandstone escarpments.

Subdivision

P2 Regular grid pattern overlying topography.

Streetscape

P3 Houses at street level or raised above it.

P4 Consistent scale of housing addressing the street in garden setting.



P5 Raised verges, sandstone kerbing, sandstone and concrete retaining walls, street gardens.

P6 Street trees, including central plantings.

Views

P7 Carter Street lookout. Views from Morden Street to the south.

4.6.5 Characteristic buildings

P1 Single storey, free-standing, double fronted Federation dwelling houses.

P2 Late Victorian residences in the north-western portion.

4.6.6 Characteristic built elements

Siting

P1 Detached dwelling houses to the front of the lots with gardens to front and rear.

Form, massing and height

P2 Single storey, detached dwelling houses.

P3 Simple forms articulated with projecting bays and verandahs to front.

P4 Reduced bulk and scale to rear.

Roofs

P5 Hipped and gabled roofs pitched between 30 and 45 degrees with overhanging eaves.

P6 Gabled ends for projecting bays to the street.

P7 Skillion roofs to rear wings.

P8 Brick and rendered chimneys.

External Materials

P9 Timber or face brick on sandstone bases.

P10 Terracotta tiles with corrugated metal on rear wings. Slate and corrugated metal on older dwellings.

P11 Shingled or half-timbered gable roofed bays.

Windows and doors

P12 Late Victorian and Federation. Decorative leadlights and glazing.

Fences

P13 Low height front fences and walls to 800mm.

P14 Timber fences to rear and side.

P15 Stone, face brick and brick piers or timber pickets and horizontal rails.

Car accommodation

P16 Garages and carports located to the side and rear of dwellings.

P17 No garages or carports located in front of the building line.

P18 Single driveways.

4.6.7 Uncharacteristic and undesirable buildings and built elements

P1 Carports, garages or paving to front of lot; dormers to front of roofs; over-scaled additions; painting and rendering of face brickwork; high fences to street; and inappropriate detailing.



SECTION 5 NORTH CREMORNE PLANNING AREA

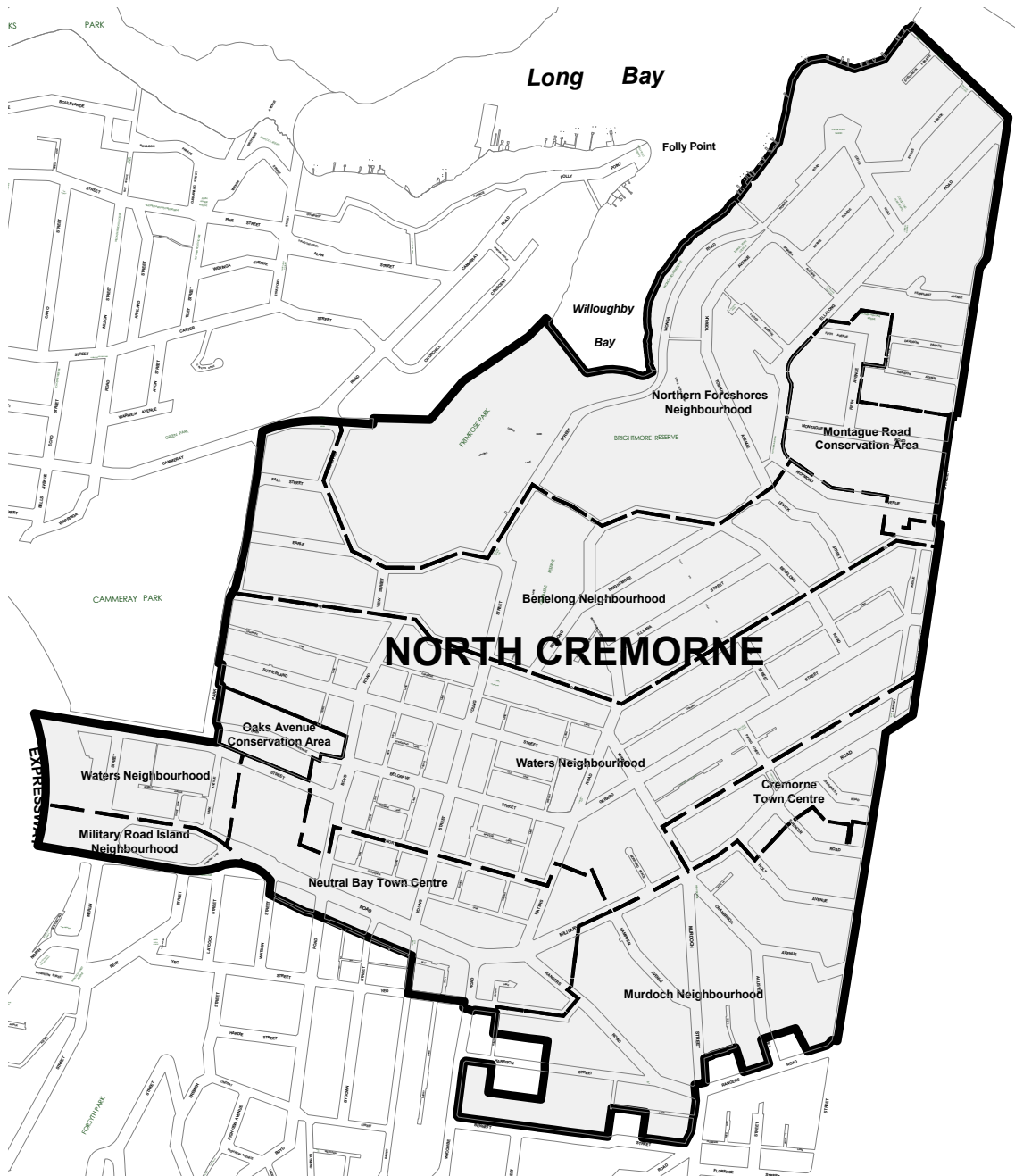
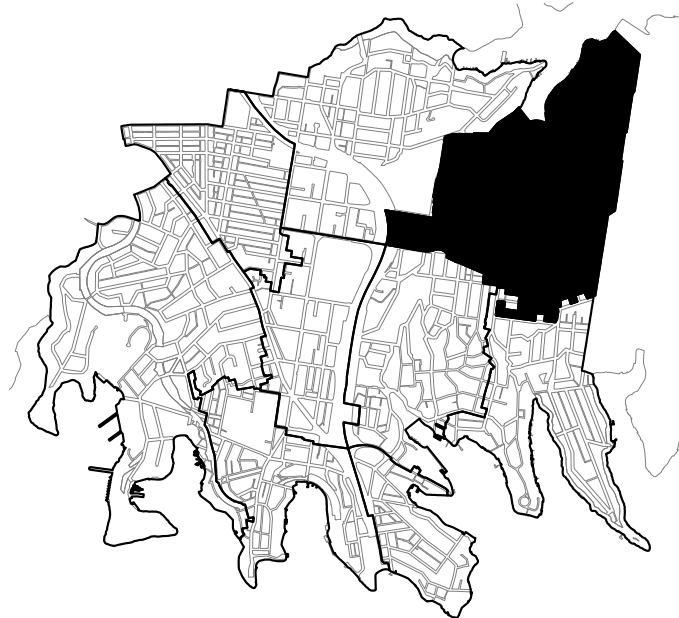


Figure F-5.1: North Cremorne Planning Area and associated Locality Areas

5.0 NORTH CREMORNE PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the North Cremorne Planning Area.

North Cremorne is a primarily residential neighbourhood providing a diverse range of housing forms for a mixed population. It is bound on its southern side by the Neutral Bay and Cremorne Town Centres, which are bustling places where people live, shop, eat, work and socialise providing a high level of amenity for all users.

Development within the Planning Area should result in:

- residential growth being provided in accordance with Council's Local Housing Strategy, with the growth concentrated within the Mixed Use zones of the Town Centres located on or in the vicinity of Military Road, and the remainder comprising of multi dwelling housing and residential flat buildings in the surrounding residential areas
- residential densities not being increased in foreshore areas and areas of steep terrain
- development within the *R2 Low Density Residential* zone being of a similar scale to existing characteristic development
- a wide range of residential types and sizes being distributed throughout the area according to zone
- a range of retail and commercial premises, services and facilities being available to the local community within the Town Centres

and where:

Function

- there are safe and accessible community facilities and meeting places
- the few non-residential uses operate without an adverse effect to the amenity of the residential neighbourhood

Section 5 - North Cremorne Planning Area

- services and facilities meet the needs of different population groups
- accessible and safe pedestrian routes are extended throughout the neighbourhood linking to the open spaces of Primrose Park and Brightmore Reserve
- additional public open space is provided for increased residential population
- public transport, cycling and walking are preferred means of transport
- local icons, cultural resources and heritage provide tangible evidence of the area's past and reflect the community's cultural values

Environmental Criteria

- parkland and natural foreshore areas are conserved, protected and easily accessible to pedestrians
- ecology of bushland and wetland areas are protected from adverse impacts of development such as stormwater runoff, dumping of fill soil and vegetation, leaching of fertilisers, spread of introduced plants and weeds and visual impact of structures
- habitat for native fauna is provided through the planting and maintenance of local flora
- good sunlight is available to both public and private spaces

Quality Built Form

- high quality residential accommodation in the Town Centre incorporates internal amenity for residents and energy efficient design
- open meeting places in the form of courtyards act as focal points, and are located in areas that provide relief from traffic noise
- residential development respects and maintains existing characteristic built form with buildings setback from all boundaries and landscaped front gardens, softening the built form
- the built environment is sympathetic to the topography and vegetation, allowing views of the surrounding area and Willoughby Bay
- heritage items are protected and significant streetscape elements are conserved in the Oaks Avenue and Montague conservation areas

Quality Urban Environment

- backyards are provided for a variety of practical and recreational needs of residents
- car parking does not adversely affect the character of the area and quality of residential streets
- front fences are low and offer good outlooks to house fronts and gardens

Efficient Use of Resources

- existing residential buildings are maintained to prevent unnecessary waste of building materials
- stormwater is captured and re-used on site

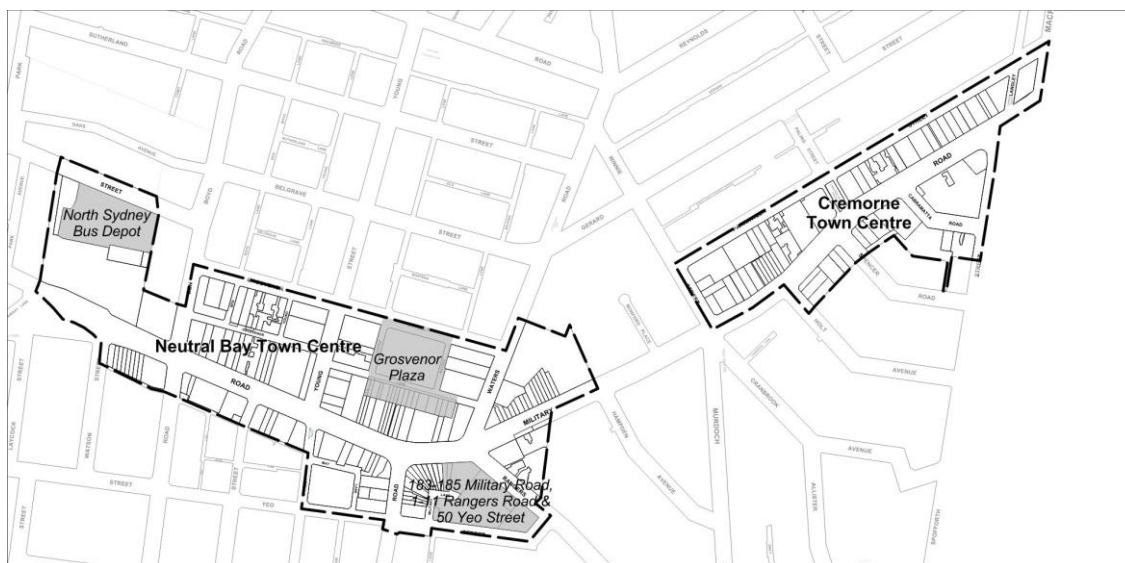
The North Cremorne Planning Area includes the following Locality Areas in which development is subject to the relevant provisions below:

- Section 5.1: Neutral Bay and Cremorne Town Centres
 - Section 5.1.4: North Sydney Bus Depot
 - Section 5.1.5: 1-11 Rangers Road
 - Section 5.1.6: Grosvenor Plaza



- Section 5.2: Military Road Island Neighbourhood
- Section 5.3: Waters Neighbourhood
 - Section 5.3.4: 14-16 Military Road
- Section 5.4: Benelong and Northern Foreshore Neighbourhood
- Section 5.5: Murdoch Neighbourhood
- Section 5.6: Montague Road Conservation Area
- Section 5.7: Oaks Avenue Conservation Area

5.1 NEUTRAL BAY AND CREMORNE TOWN CENTRES



5.1.1 Significant Elements

Land Use

P1 Predominantly mixed commercial and residential development.

Topography

P2 Generally flat, straddling the topmost part of the ridge along Military Road.

Natural Features

P3 Area forms the topmost part of a ridge following Military Road.

Views

P4 The following views and vistas are to be preserved and where possible enhanced:

- (a) District views to Middle & Sydney Harbours from the upper levels of some buildings.

Identity / Icons

P5 Orpheum Theatre is a community focal point and regional attraction that enhances the identity of the area.

P6 Military Road, a major regional thoroughfare.

Subdivision

P7 A diverse mixture of large consolidated sites intermixed with long narrow sites with dual frontages.

Streetscape

P8 Wide fully paved footpaths incorporating outdoor dining areas.

P9 Buildings built to street and laneway frontages.

P10 Continuous awnings along Military Road.

P11 Irregular planting of street trees.

P12 Active frontages to Military Road, Grosvenor Street.

P13 90° on-street parking to Parraween Street.



Public transport

- P14 Development is to take advantage of high levels of accessibility to high frequency public bus services along Military Road.

5.1.2 Desired Future Character

Diversity of activities, facilities, opportunities and services

- P1 Mixed commercial and residential development, primarily focused on Military Road.
- P2 A variety of commercial, retail, restaurants and cafes are provided at footpath level, non-residential or residential on the first floor and residential only on the upper floors.
- P3 Commercial activities should be maintained to all street frontages at ground level to stimulate pedestrian activity.
- P4 Activities should not have a detrimental impact to the safety and efficiency of vehicular traffic on Military Road.

5.1.3 Desired Built Form

Subdivision

- P1 Redevelopment sites should have a frontage of approximately 25m-30m.

Form, massing and scale

- P2 Generally 4-5 storeys.
- P3 5-6 storeys in the block bounded by Military Road, Cabramatta Road and Spofforth Street.
- P4 Larger facades are broken up with changes in building frontage alignment and architectural detailing to reflect the former subdivision patterns, especially fronting Military Road.

Public spaces and facilities

- P5 Outdoor dining areas:
- (a) are located within clearly defined spaces;
 - (b) are located away from main roads;
 - (c) are weather protected; and
 - (d) provide equal and unobstructed pedestrian movement.
- P6 Pedestrian arcades should be provided between Military Road and parallel laneways/streets to the north and south of Military Road to enhance pedestrian connectivity.
- P7 Views of shop fronts should not be obstructed from footpaths and roadways.
- P8 Encourage the retention and enhancement of trees within the public domain to improve public amenity.

Setbacks

- P9 Buildings should be built to all street frontages at ground level, except as follows:
- (a) Setback 1.5m from the northern side of Military Road, at ground level between Young Street and Waters Road, and
 - (b) Setback 1.5m at ground level from all laneways.

Podiums

- P10 Podium of 8.5m (two storeys) to Military Road, east of Hampden Avenue, with a setback of 3m above the podium.

Section 5 - North Cremorne Planning Area

- P11 Podium of 10m (three storeys) to Military Road, west of Hampden Avenue, with a setback of 3m above the podium.
- P12 Podium of 8.5m (two storeys) to laneway frontages and frontages not to Military Road, with a setback of 3m above the podium.
- P13 Podium of 8.5m (2 storeys) to the east and west of Barry Street Plaza, with a setback of 3m above the podium.
- P14 Provide adequate podium setbacks where a site adjoins residential or open space zones.

Building design

- P15 Ground floor access to shops is to be provided to all properties with a frontage to Military Road, a frontage to the Grosvenor Lane car park / plaza, and those with a dual frontage between Military Road and Parraween Street.
- P16 Building elements, materials, finishes, and windows should relate to neighbouring buildings.
- P17 Laneways should be provided with active frontages, wherever possible.

Car Parking & Access

- P18 Vehicular access from sites should not be provided to Military Road.

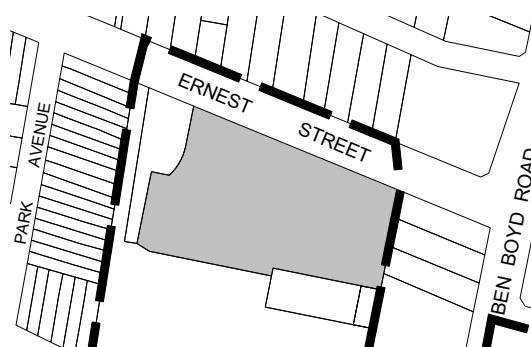
Noise

- P19 Elevations of buildings fronting Military Road and Ernest Street are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).
- P20 Services located at the rear of buildings at the interface with a residential zone should be carefully designed and located to ensure that they do not have a detrimental impact to the residential amenity of the neighbouring property.

Awnings

- P21 Awnings are provided along all street frontages.

5.1.4 North Sydney Bus Depot



Diversity of activities, facilities, opportunities and services

- P22 Continued operation of the bus depot as important public transport infrastructure.
- P23 If the bus depot ceases to operate, then the site should be used for mixed use development.
- P24 Incorporate community functions in the form of open space, through site links and / or other community uses.

- P25 Development provides active frontages to Ernest Street and internal public spaces.
- P26 Amalgamation with Big Bear site provides further opportunities for public benefit.
- P27 Redevelopment of an amalgamated Bus Depot and Big Bear site should be informed by a masterplan for the entire street block.

Form, massing and scale

- P28 Transition scale of built form down from central / southern portion of site to surrounding lower scale development and heritage conservation area.
- P29 Maximise solar access to internal public spaces.

Public Benefit

- P30 Provide public benefit with any proposed change to existing planning controls.
- P31 Affordable housing is a public benefit priority for the site.

Podiums

- P32 Podium of 10m (three storeys) to Ernest Street.

Ecologically Sustainable Development

- P33 In implementing ESD best practice, explore opportunities to incorporate ESD demonstration project in redevelopment.

Access

- P34 Create safe and active pedestrian links between Ernest Street and Military Road, particularly where possible via the Big Bear site.
- P35 Vehicular access minimises the impact on the flow of traffic along Ernest Street.

Noise

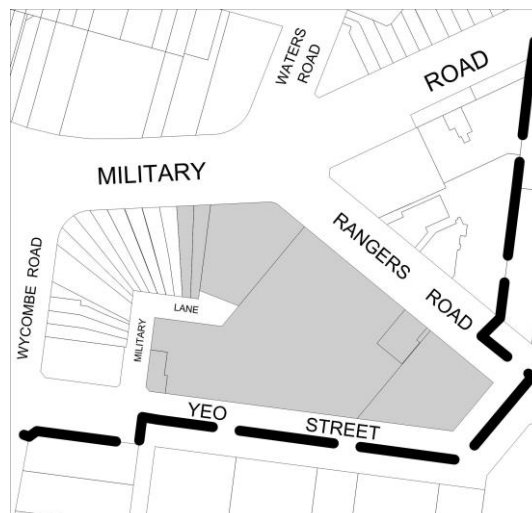
- P36 Elevations of buildings fronting Ernest Street are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of vegetation, cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

Heritage

- P37 Protect and respond architecturally to existing heritage items.
- P38 Subsurface archaeological material is assessed prior to excavation.

5.1.5 1-11 Rangers Road

* This section has been informed by the Neutral Bay Village Planning Study (2024).



Section 5 - North Cremorne Planning Area

5.1.5.1 Diversity

Objectives

- O1 To provide a mixture of uses that contribute to enhancing of the amenity, identity and desirability of Neutral Bay.
- O2 Ensure non-residential and residential uses are adequately separated to maximise the amenity for both residents and workers.
- O3 To increase access to open public spaces with high levels of amenity.

Provisions

- P1 Provide a mixed use building typology, with retail at the ground level, commercial space on the second storey and residential accommodation above.
- P2 Provide a fine grain of retail and other business premises fronting all street frontages, the future public plaza and where practical, to laneways.
- P3 Relocate the existing supermarket underground to assist in positively activating streets and future public plaza.
- P4 Create a new public plaza across both 183-185 Military Road and 1-7 Rangers Road with a minimum size of 1,000sqm.
- P5 Maximise opportunities to accommodate outdoor dining.
- P6 Provide a mixture of commercial tenancy sizes and flexible floorplates.

5.1.5.2 Form, massing and scale

Objectives

- O1 To ensure that density and scale is concentrated toward Military Road and provides a positive transition in height and scale down to the adjoining residential areas.
- O2 To maximise year round solar access to existing and new public open spaces to be created in the Planning Area.
- O3 To minimise overshadowing impacts on residential development to the south of Yeo Street.

Provisions

- P1 Allotments should be consolidated to establish suitable building footprints to accommodate additional height and the establishment of a new public plaza.
- P2 Height should transition from Military Road and the northern portion of Rangers Road down to Yeo Street.
- P3 Development addresses laneways with active uses, where practical.
- P4 Ensure that all dwellings on the southern side of Yeo Street receive a minimum of 2 hours sunlight to living areas at the winter solstice.
- P5 Building elements located above the podium level must not exceed 45m in horizontal width or length, unless punctuated with a significant visual break or articulation element.

5.1.5.3 Setbacks

Objectives

- O1 To reinforce the spatial definition of streets and public spaces.
- O2 To provide a wider footpath to Military Road to enhance pedestrian movement and comfort and provide outdoor dining opportunities.

Provisions

- P1 Buildings must be setback from the following road frontages:



- (a) 2.5m from Military Road, and
- (b) 0m from Rangers Road, except the ground level which is to be setback a minimum of 1.5m, and
- (c) 0m to Yeo Street and Military Lane.

P2 Buildings must be setback 0m to the public plaza.

P3 Buildings should be setback 0m from all side boundaries.

5.1.5.4 Podium Height

Objectives

- O1 To positively relate to the heritage context surrounding the site.
- O2 To create a human scaled interface within the public plaza and adjoining laneways.
- O3 To positively relate to the scale of development of the opposite side of Rangers Road and Yeo Street.

Provisions

P1 A podium must be:

- (a) 2 storeys in height to Military Road, the east-west alignment of Military Lane and to the proposed public plaza, and
- (b) 4 storeys to Rangers Road, and
- (c) 3 storeys or a maximum of 10m, whichever is the lesser, to Yeo Street and the north-south alignment of Military Lane.

5.1.5.5 Above Podium Setbacks

Objectives

- O1 To provide a consistent street frontage and minimise overshadowing impacts to the public plaza.
- O2 Minimise overshadowing impacts to residential properties on the southern side of Yeo Street.
- O3 To reduce the scale and bulk of buildings when viewed from the public domain.

Provisions

P1 The following minimum setbacks (measured from the outer wall of the podium) must be provided above the podium level:

- (a) 3m to Military Road, Rangers Road and Military Lane.
- (b) 3m to Yeo Street, with a further 3m setback to any part of the building above 5 storeys.
- (c) 3m to any podium facing the public plaza and through site link.
- (d) 0m to the entire length of the side boundary between 183-185 Military Road and 181A Military Road, with 6m above 6 storeys.
- (e) 6m to the side boundary between 1-7 Rangers Road and 9-11 Rangers Road, along the Rangers Road frontage.
- (f) 0m to the side boundary between 1-7 Rangers Road and 9-11 Rangers Road, along the Yeo Street frontage

Note. Setbacks are measured to the outer face of any part of the building including balconies, architectural detailing and the like.

Despite P1(e) and (f), a 6m setback is required to the entire length of the side boundary between 1-7 Rangers Road and 9-11 Rangers Road, if the through-site link is not provided open to the sky.

Section 5 - North Cremorne Planning Area

5.1.5.6 Public Plaza

Objectives

- O1 To create additional publicly accessible space.
- O2 To ensure that the space is sufficiently sized to provide a mixture of passive and active recreational opportunities.
- O3 To maximise the amenity of the public space.
- O4 Maximise the activation of the space.
- O5 To increase landscaping and tree canopy within the Neutral Bay Town Centre.

Provisions

- P1 A public plaza of at least 1,000sqm is to be provided across both 183-185 Military Road and 1-7 Rangers Road. The minimum area of the plaza is to exclude any pedestrian through-site link between the plaza itself and Yeo Street and is to open to the sky.
- P2 The public plaza is to be located adjacent to the Military and Rangers Road intersection.
- P3 At least 50% of the public plaza must be able to receive solar access between 10am and 1pm at the Winter Solstice.
- P4 All ground level frontages to the public plaza are to be appropriately activated.
- P5 Provide a variety of functions to activate the space, including outdoor dining, public seating, community lawn, playground, water feature, public artwork, landscaping, fixed bicycle parking.
- P6 If 183-185 Military Road is to be redeveloped in isolation from 1-7 Rangers Road, then the building at 183-185 Military Road must be setback at the ground level from its eastern boundary, to ensure that appropriate pedestrian access arrangements can be made independently, without reliance on reciprocal rights of way between 183-185 Military Road and 1-7 Rangers Road.

5.1.5.7 Through-site Link

Objectives

- O1 To improve pedestrian permeability through the area.
- O2 To provide clear visual connections between different parts of the Neutral Bay Town Centre.
- O3 To maximise casual surveillance over the space.

Provisions

- P1 A minimum 6m wide through-site link is to be provided between the proposed public plaza and Yeo Street to enable pedestrians to traverse between the intersection of Military Road and Rangers Road and Yeo Street.
- P2 The through-site link should be open to the sky to maximise legibility in wayfinding through the area and avoid the creation of extensive wall lengths along Yeo Street.
- P3 Despite P1 and P2, a through-site link may only be built over, but only where the consent authority is satisfied that the through-site link is widened to a minimum of 8m and have a minimum 7m clearance to any storey located above.
- P4 The ground level of buildings fronting the through-site link are to be appropriately activated.
- P5 The through-site link is to be publicly accessible at all times.
- P6 The through-site link should be integrated with the proposed public plaza.



5.1.5.8 Activation

Objectives

- O1 To provide for the amenity, interest and liveliness of the street environment and public open spaces.
- O2 To ensure a positive experience for pedestrians with the necessary fine grain environment of the street.
- O3 To enable sensory engagement with the street and public spaces.
- O4 To provide an active ground floor frontage that is accessible and integrated with the design of the public domain.
- O5 To maximise the extent of active frontages in the public domain.
- O6 To promote activity, connectivity and variety in the public domain.
- O7 To increase passive surveillance of the street and other public spaces and enhance safety.

Provisions

- P1 Buildings must contain active frontages to all street frontages, with the exceptions of public laneways.
- P2 Despite P1, active frontages to public laneways are encouraged where practical.
- P3 Buildings which have a direct interface with the proposed public plaza are to provide an active frontage to that interface.
- P4 Active frontages are to be provided along both sides of all through-site links.
- P5 Where a site has multiple street frontages, all service and vehicular access points must not be provided off the primary street frontage.
- P6 Fire escapes and service doors must be seamlessly incorporated into the facade with quality materials.

5.1.5.9 Access

Objectives

- O1 To prioritise pedestrian movements along Military Road, Rangers Road and Yeo Street.

Provisions

- P1 No vehicular access is to be provided from Military Road or Rangers Road.
- P2 Vehicular access should be provided from Military Lane.
- P3 Despite P2, vehicular access may be provided from Yeo Street subject to adequate demonstration that it will not present any adverse conflicts to the traffic flow on Yeo Street.
- P4 Where practical, commercial and residential vehicular access should be separated.
- P5 All service access is to be provided from Military Lane.
- P6 The number of retail and commercial customer spaces for the Woolworths site redevelopment (1-7 Rangers Road and 50 Yeo Street), is to be limited to 180 parking spaces.
- P7 There be the provision of a single ingress and egress on Yeo Street in order to promote pedestrian safety.

5.1.5.10 Landscaping

Objectives

- O1 To minimise privacy impacts of residents within and adjoining the development.

Section 5 - North Cremorne Planning Area

- O2 To soften the appearance of buildings and their interface with neighbouring buildings and the public domain.
- O3 To minimise heat island effects.

Provisions

- P1 Terrace planting is to be provided to soften the building appearance and contribute to the overall aesthetic of building facades.

Perimeter planting is to be established along the edges of the podium and rooftop terraces.

Where practical, landscaping should be used to screen any rooftop plant.

Consideration is to be given to the use of green roofs and green walls.

5.1.6 Grosvenor Lane Car Park

** This section has been informed by the Grosvenor Lane Planning Study 2014*



Diversity

- P1 Provide a fine-grain of retail and other frontages at the interface with the public realm, including the public plaza, to maximise variety of uses.
- P2 Provide opportunities for outdoor dining.
- P3 Provide active frontages to the plaza and where possible to laneways.

Public Domain

- P4 Create a public plaza on the current public car park site.
- P5 Implement shared zones or widen footpaths where possible to improve pedestrian safety and amenity.
- P6 Design plaza to be flexible and able to accommodate passive recreation and special events.
- P7 Design of built form should facilitate the revitalisation and improvement of the public domain along Military Road.
- P8 Upgrade adjoining laneways to complement the public plaza.



P9 Provide quality, active pedestrian links between Military Road and the public plaza.

Setbacks

P10 Set back buildings 1.5m at ground level on all street and laneway frontages.

Built Form

P11 Minimise the impact of development on the public domain and plaza.

P12 Minimise the impact of development on surrounding residential land.

P13 Podium of 10m (three storeys) to Grosvenor Street, with a setback of 3m above the podium.

P14 Mitigate noise from Military Road in design of through site link and built form.

Access and Parking

P15 Relocate public parking underground and provide additional public car spaces.

P16 Maintain existing laneway network or provide adequate alternative for small scale loading, short term parking and vehicular circulation.

P17 Loading facilities should not impact on amenity of plaza and should ideally be provided underground.

Public Infrastructure

P18 Upgrade or relocate Neutral Bay Community Centre as part of any redevelopment incorporating the existing site.

P19 Provide improved bus stop infrastructure on Military Road.

Heritage

P20 Protect and respond architecturally to heritage items within the area.

5.2 MILITARY ROAD ISLAND NEIGHBOURHOOD



5.2.1 Significant Elements

Land Use

P1 Predominantly residential development in the form of semi-detached dwellings.

Topography

P2 Generally flat with a slight fall to the south to Falcon Street.

Natural Features

P3 Area forms the topmost part of a ridge following Military Road.

Views

P4 The following views and vistas are to be preserved and where possible enhanced:

- (a) Potential district views to Middle and Sydney Harbours from the upper levels of some buildings.

Identity / Icons

P5 Military Road, a major regional thoroughfare.

P6 Falcon Street, a major regional thoroughfare.

Subdivision

P7 Regular grid pattern.

P8 Site areas of 200m² to 350m².

Streetscape

P9 Wide one way streets.

P10 Street trees.

Public transport

P11 New development to maximise the use of the area's high level of accessibility to high frequency public bus services.

5.2.2 Desired Future Character

Diversity

P1 Residential flat buildings with some mixed use, according to zone.



5.2.3 Desired Built Form

Subdivision

- P1 All land should be consolidated to create a single allotment of sufficient size to create a series of residential flat buildings which can be appropriately located behind large landscaped buffers.

Siting

- P2 Towards the north, having regard to the acquisition of land for road widening purposes.

Setbacks

- P3 Setback on Falcon Street frontage incorporates substantial landscaping and creates a buffer to traffic.

Podiums

- P4 Podium of 13m (four storeys) to all elevations of a building, with a setback of 3m above the podium.

Form, massing and scale

- P5 Large facades are broken up with changes in building frontage alignment and architectural detailing.
- P6 Internal courtyard or landscaped open space with solar access between 11am and 3pm, 21st June.

Access

- P7 Maintain pedestrian crossings across Military Road and Falcon Street.
- P8 Vehicle access is provided off Military Road.
- P9 Vehicular access for service and delivery vehicles and resident/occupant vehicles is combined.
- P10 Regional traffic is concentrated on Falcon Street and Military Road to north of the street block is a local road.

Noise

- P11 Elevations of buildings fronting Military Road and Falcon Streets are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

5.3 WATERS NEIGHBOURHOOD



5.3.1 Significant Elements

Land Use

P1 Predominantly residential development.

Topography

P2 Gentle fall to the north from the ridge aligning with Military Road.

Views

P3 The following views and vistas are to be preserved and where possible enhanced:

- (a) along Park Avenue of Cammeray Park.
- (b) Vistas to Middle Harbour down Ben Boyd Road and Young Streets.
- (c) District views to Middle Harbour from the upper levels of some buildings.

Identity / Icons

- P4 SCEGGS – Redlands School.
- P5 Cammeray Park.
- P6 Military Road, a major arterial thoroughfare.
- P7 Ernest, Belgrave Road and Gerard Streets, a sub-arterial thoroughfare.

Subdivision

P8 A regular grid pattern with street blocks generally running east / west, with a large number of properties having dual frontages associated with a large number of laneways.

Streetscape

- P9 Wide tree lined streets with grass verges and concrete paths.
- P10 Narrow fully paved laneways.
- P11 A mixture of tall and low rendered masonry fences to the street, with taller fences generally located along Ernest, Belgrave and Gerard Streets in front of low and medium density housing.
- P12 Buildings setback from the boundary and aligned with the street frontage.

Public transport

- P13 Development should take advantage of the high level of public bus services operating along Military Road, Ernest Street, Belgrave Street and Gerard Street.

5.3.2 Desired Future Character

Diversity

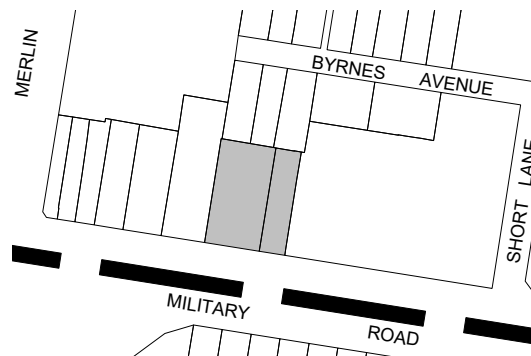
- P1 Predominantly medium to high density residential accommodation, generally comprising attached dwellings, multi-dwelling housing and residential flat buildings, according to zone.
- P2 The density of development generally reduces the further away a property is located from Military Road.

5.3.3 Desired Built Form

Form, massing and scale

- P1 Future development of high density housing must have a sympathetic relationship to other surrounding development in terms of height, bulk and scale, privacy and access to views (for example stepping down to lower height).
- P2 Buildings adjacent rear laneways should be ancillary to the main building on a site.

5.3.4 14-16 Military Road



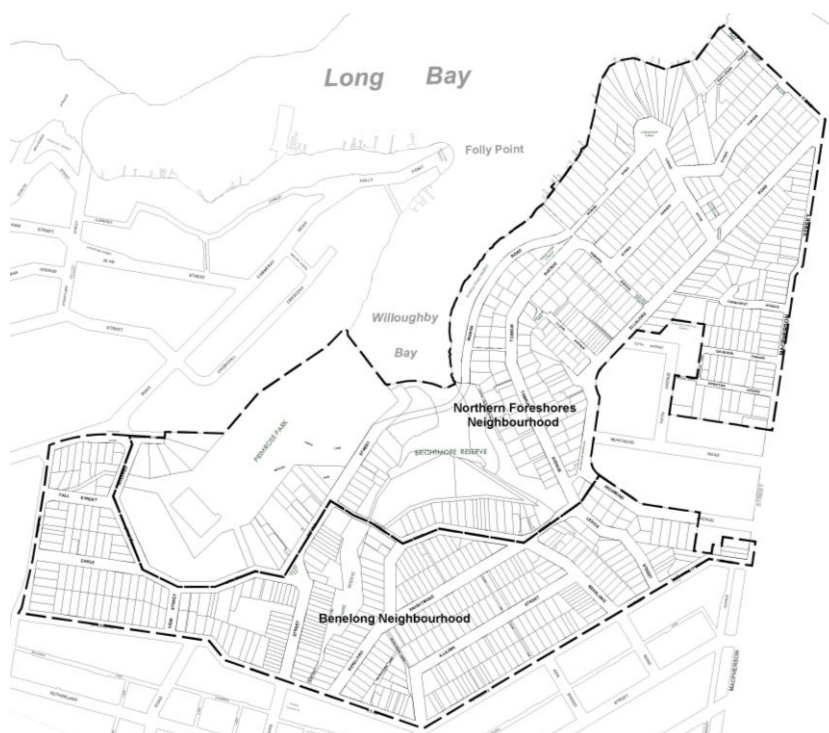
Noise

- P1 Where 14-16 Military Road, Neutral Bay is to be used for non-residential purposes, an acoustic wall is to be erected, which:
- (a) matches the height, alignment, design, materials and finishes of the existing acoustic wall over 18-38 Military Road, Neutral Bay; and
 - (b) extends for the entire length of the northern boundary and its southern face is setback a minimum of 483mm from the common boundary with No.s 1 to 5 Byrnes Avenue; and
 - (c) extends for the entire length of the western boundary and is setback a minimum of 3m from the eastern alignment of the residential flat building on the adjoining allotment at 12 Military Road.

Visual impact

- P2 The setback between the western boundary of 14-16 Military Road, Neutral Bay and the acoustic wall required by P1 above, must be landscaped with vegetation capable of growing to the height of the acoustic wall to ensure effective screening.

5.4 BENELONG AND NORTHERN FORESHORE NEIGHBOURHOODS



5.4.1 Significant Elements

Land Use

- P1 Predominantly residential accommodation.
- P2 Passive and active recreation areas.

Topography

- P3 Generally falling to the north and northwest to the foreshores of Willoughby Bay.
- P4 Generally flat over the public open space areas adjacent to the foreshore.
- P5 Steep slopes to the southern and western ends of Primrose Park and residential properties in the vicinity of the foreshore.

Natural Features

- P6 Remnant natural shoreline areas in North Cremorne.
- P7 Remnant bushland of Primrose Park, Brightmore Reserve and Willoughby Bay
- P8 Wetlands in the form of Coastal Saltmarsh along the foreshores of Willoughby Bay (refer to Section 3.2 – Bushland to Part B of the DCP).
- P9 Natural sandstone cliffs adjacent to the western edge of Primrose Park.

Views

- P10 The following views and vistas are to be preserved and where possible enhanced:
 - (a) District views to Middle Harbour from the upper levels of some buildings.
 - (b) Primrose Park Lookout (10), Earle Street Lookout (11), Tobruk Avenue Lookout (14), Lodge Road Lookout (15), Ellalong Road Lookout (17).



Identity / Icons

P11 Primrose Park and Brightmore Reserve.

P12 Willoughby Bay.

Subdivision

P13 Irregular grid pattern, informed by the irregular topography of the area.

Streetscape

P14 Tree lined streets with grassed verges and concrete footpaths.

P15 Split carriageways with large sandstone retaining walls.

P16 A mixture of low picket, paling, double rail timber and masonry fences. Continuity of particular fencing types in certain streets.

P17 Buildings generally setback from the boundary with garages and carports built to the boundary in the Northern Foreshores Area.

Public transport

P18 Limited access to public bus services through the Area.

5.4.2 Desired Future Character

Diversity

P1 Primarily low density residential accommodation, generally comprising dwelling houses, semi detached houses and dual occupancies.

P2 Potential for limited attached dwellings and multi dwelling housing according to zone.

P3 Enhancement of existing passive and active recreation areas.

5.4.3 Desired Built Form

Subdivision

P1 The long narrow lots in Lodge Road are not to be subdivided because of adverse impacts on drainage, landscaping, views and the natural foreshore.

Form, massing and scale

P2 Minimise the building footprint to preserve natural features, native vegetation and rock outcrops, particularly on land adjoining or near foreshores.

P3 Buildings should provide adequate separation to bushland and foreshore areas.

P4 Densities should not be increased in areas of steep terrain. Development on sloping land should be designed to follow the fall of the land.

Roofs

P5 Development should maintain low pitched roofs.

P6 Flat roofs may be considered on Wonga Road and Ryries Parade to retain views for neighbouring property. However they will not be permitted to gain an additional storey.

Views

P7 Buildings should not obstruct views and vistas from public places to the waterway.

P8 Development should not adversely restrict views from neighbouring properties. This is especially important in foreshore areas or sites that enjoy water views.

Setback

P9 Front: 3m



Section 5 - North Cremorne Planning Area

P10 Rear: 10m

Foreshore

- P11 Development adjacent to the foreshore is small in scale and does not dominate the foreshore frontage.
- P12 Development associated with boating activity is kept to a minimum and is compatible with the surrounding land uses.
- P13 Development adjoining foreshore or bushland areas (such as at Wonga Road) use muted colours and non-reflective materials, such as brick and timber to ensure the scenic and environmental qualities are enhanced.

Murdoch Neighbourhood

The map displays a complex network of streets. Key streets include Street, Avenue, Road, and Boulevard. The central area is labeled 'Murdoch Neighbourhood' in bold black text. The map is oriented with North at the top.

Section 5 - North Cremorne Planning Area

5.5.2 Desired Future Character

Diversity

- P1 Primarily high density residential accommodation, generally comprising residential flat buildings.
- P2 Limited opportunities for attached dwellings dwelling houses, semi-detached houses, dual occupancies and multi dwelling housing.
- P3 The density of development generally reduces the further away a property is located from Military Road.

5.5.3 Desired Built Form

Setback

- P1 Front: 5m
- P2 Buildings should be setback to all side boundaries in accordance with Part B of the DCP, except as follows:
 - (a) A minimum of 2m from the eastern boundary of No. 4 Rangers Road to retain existing trees.
- P3 Rear 10m.
- P4 Residential flat buildings have substantial front, rear and side setbacks.

5.6 MONTAGUE ROAD CONSERVATION AREA



5.6.1 History

The Montague Road Conservation Area was originally part of Alfred Thrupp's land grant, which was later acquired by Daniel Cooper and his heirs. In 1891 a large 25 acres of the land was purchased by James Ernest Holt and later subdivided. The upper portion to Macpherson Street was developed as the *Surrey Estate* and placed for auction in 1907 and 1910.

Despite subdivisions in the area in the 1890's, little development occurred until the early 20th century because of its remoteness from the main development areas of North Sydney. The area's development was facilitated by the construction of tramways which increased its accessibility.

5.6.2 Description

The Montague Road Conservation Area is a residential neighbourhood that is characterised by small to medium scale houses setback from the street.

The topography of the area is a sloping plateau with a bluff along Ellalong Avenue. Richmond Avenue runs along the contours with the other roads in a grid pattern sloping with the topography. Montague Road is a wide Street with grass verges.

Street trees vary in size and species.

The area has views from the high vantage points to Willoughby Bay. Frontages often have retaining walls in a variety of finishes and low picket fences.

The conservation area is characterised by the good quality, single and two storey detached and semi-detached buildings on medium to large size lots in the Federation Queen Anne styles interspersed with some California Bungalows with articulated facades and front verandahs with decorated timber detailing.

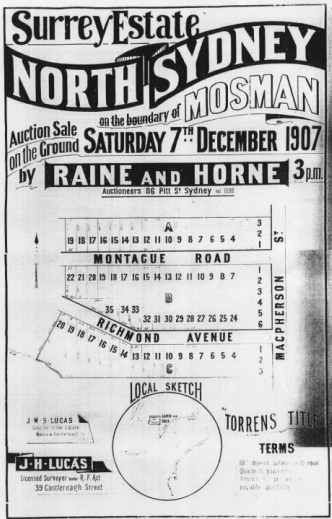


Characteristic building materials are face brick with brick or sandstone bases, roughcast render, fibro. Most houses have brick chimneys with terracotta chimney pots.

Section 5 - North Cremorne Planning Area

5.6.3 Statement of Significance

The Montague Road Conservation Area is significant:

- (a) As an example of a subdivision that responds to the topography and the resulting built form that features stepped and elevated housing forms.
- (b) As a consistent and intact early 20th century residential area with medium sized detached and attached Federation Queen Anne houses and California bungalows.
- (c) For the quality and collective significance of the buildings within the area.

	<p>Figure F-5.2 (left): Circa 1890</p>
	<p>Figure F-5.3 (below left): Circa 1943</p>
	<p>Figure F-5.4 (below right): Circa 2008</p> 

5.6.4 Significant elements

Topography

P1 Plateau sloping down towards Willoughby Bay.

Subdivision

P2 Rectilinear subdivision with boundary to street frontage.



Streetscape

- P3 Split carriageway and changes in level in Ellalong Road and Richmond Avenue. Low timber fences. Slot views over and between buildings to the city and Harbour Bridge.
- P4 Double rail timber fences.

Views

- P5 Brothers Memorial Park Fifth Avenue Lookout, Ellalong Road Lookout, Richmond Avenue Lookout. Street views along Richmond and Fifth Avenues.

5.6.5 Characteristic buildings

- P1 Detached single storey dwelling houses on small lots.
- P2 Federation, Queen Anne and California Bungalow styles.

5.6.6 Characteristic built elements

Siting

- P1 Principal frontage oriented and parallel to the street, forward towards front boundary.
- P2 Front setback 4-5 m.
- P3 Side setback 1-1.5 m.
- P4 Rear setback 5-6 m

Form, massing and scale

- P5 Characteristic height - single storey.

Roofs

- P6 Pitched between 30 and 45 degrees, small scale and with a variety of roof forms. Terracotta tile or slate with corrugated metal to rear extensions.
- P7 Smaller roof forms to the rear.

External Materials

- P8 Dry pressed face brick, sandstone base courses.

Windows, doors and joinery

- P9 Consistent with building period and style. Timber.

Fences

- P10 Low in scale (800mm max). Face brick or timber vertical picket timber fences.

Car accommodation

- P11 Located behind the main building line (i.e. not verandahs) or located at the rear with hipped and/or gabled roof forms.

5.6.7 Uncharacteristic elements

- P1 Uncharacteristic developments in the area include unsympathetic alterations, second floor additions, attic room conversions, with dormers or skylights on front roof plane; residential flat buildings and battle-axe multi-dwelling housing developments; parking and large areas of paving in the front setback.

5.7 OAKS AVENUE CONSERVATION AREA



5.7.1 History

The Oaks Avenue Conservation Area was originally part of Alfred Thrupp's land grant. The southern half of Oaks Avenue was subdivided in 1887 by J Cooper, who bought the land from Thrupp, as the "Oaks Brickworks Estate". This land was then leased to Oliver Clews, who built seven "Brick and Iron" houses on the southern side of the road in 1889. The land on the northern side of the road was vacant at this time.

The remaining houses on the southern side of Oaks Avenue and houses on the northern side were built between 1899 and 1901. The first appeared in Council's 1902 Valuation Book.

5.7.2 Description

The Oaks Avenue Conservation Area is a discrete residential neighbourhood that is characterised by small lots.

Oakes Avenue is a relatively short street, with a slight level change along its length. The buildings on the northern side of the street are set down to those on the southern side.


Street vegetation is a mix of box and eucalypt trees planted in the early to mid 20th century. Street and foreground planting contribute to the sense of place.

The Conservation Area is characterised by the consistent row of single storey, modest sized houses. A number of the lots are splayed to Oaks Avenue leading to stepped building alignments and setbacks.

5.7.3 Statement of Significance

The Oaks Avenue Conservation Area is significant:

- (a) For the unity of its subdivision pattern and the resulting built form.
- (b) As a consistent and intact early 20th century residential area with a mix of Federation and 1920s single storey houses.
- (c) As a remnant of the former "Oakes Brickworks Estate".

	<p>Figure F-5.5 (left): Circa 1890</p>
	<p>Figure F-5.6 (below left): Circa 1943</p>
	<p>Figure F5.7 (below right): Circa 2008</p>
	

5.7.4 Significant elements

Topography

P1 Slight slope to the north.

Subdivision

P2 Detached dwelling houses on regular subdivision with narrowest boundary to street.

P3 Typically narrow allotments and approximately 450sqm in area.

Streetscape

P4 Sandstone kerbs, grass verges and concrete paths. Street trees planted in verges mainly Box and Eucalypts. Low walls in stone and timber.

Views

P5 Views to Cammeray Park at the top of the street.

5.7.5 Characteristic buildings

P1 Single storey, moderately scaled Federation and Queen Anne dwellings.

Section 5 - North Cremorne Planning Area

5.7.6 Characteristic built elements

Siting

- P1 Principal frontage oriented to the street with garden to the front.
- P2 Front setback 4 – 5 m,
- P3 Side setback 1 – 1.5 m.
- P4 Rear setback 5 – 6 m.

Form, massing and scale

- P5 Single storey with hipped and gabled roofs.
- P6 Reduced height to rear.
- P7 Simple forms articulated with projecting bays and verandahs to the front.

Roofs

- P8 Hipped and gabled. Pitched between 30 to 45 degrees.

Materials

- P9 Face brick walls, sandstone base courses.
- P10 Terracotta tile roofs with corrugated metal to rear extensions.
- P11 Timber windows, doors and joinery.
- P12 Tessellated tiles to front verandahs.

Windows and doors

- P13 Consistent with building period and style. Timber.
- P14 Step through double hung windows to front with narrow sidelights and multi paned fanlights.

Fences

- P15 Low front fences to 800mm in height.
- P16 Sandstone, face brick, timber vertical pickets and iron.
- P17 Gates are offset from stairs and front door.

Car accommodation

- P18 No garages or driveways located to the front.

5.7.7 Uncharacteristic buildings

- P1 Garages and carports, large rear additions, changes to original front fences, contemporary flat buildings, two storey attached dwellings and carriage style developments. Other unsympathetic changes are verandah enclosures, high front brick fences, cement rendering and painting face brick or stone walls.



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SECTION 6 SOUTH CREMORNE PLANNING AREA

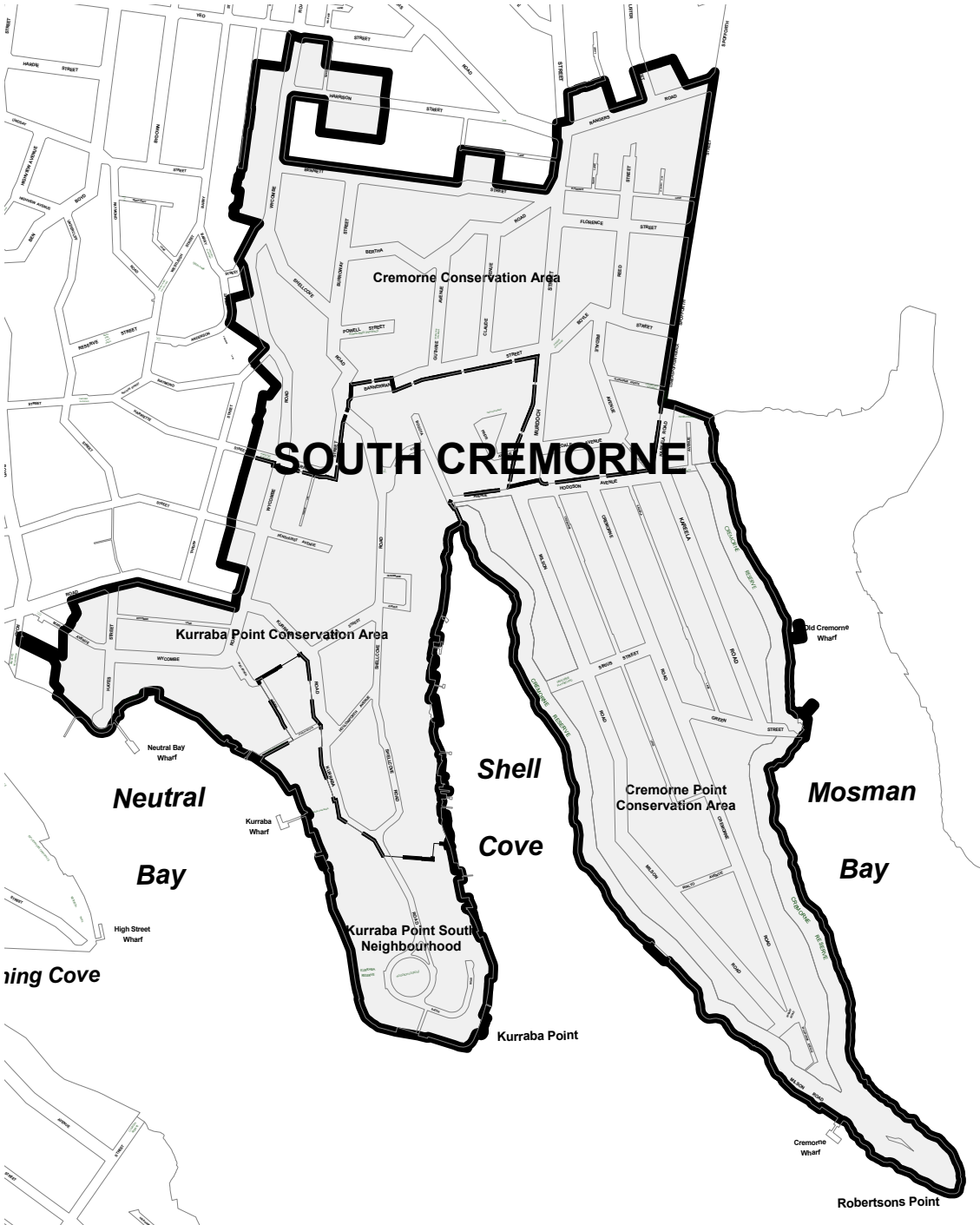


Figure F-6.1: South Cremorne Planning Area and associated Locality Areas

6.0 SOUTH CREMORNE PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the South Cremorne Planning Area.

South Cremorne is a green, leafy area sympathetic to its harbourside setting. The design of new buildings is to be sympathetic to the landscape and character of buildings within the locality, complementing existing building forms in respect to massing, composition, materials, colours and maintaining the original subdivision pattern.

Development within the Planning Area should result in:

- no substantial change to residential densities
- no significant change in intensity of development
- a wide range of single household residential types being distributed in a number of distinctive built form/landscape areas
- the conservation of features which contribute to the local identity

and where:

Function

- a limited number of non-residential uses, such as small scale convenience stores and cafes, coexist with the residential character without adverse effect
- areas of open space are used by locals and the wider regional population for a variety of recreational purposes
- comfortable and safe pedestrian routes are maintained and extended
- public transport, cycling and walking are the preferred means of transport
- through traffic is discouraged and commuter parking managed through parking schemes
- local identity, icons and heritage are conserved

Section 6 - South Cremorne Planning Area

Environmental Criteria

- the remaining natural foreshores and water courses are conserved and protected, and pedestrian access to these areas is extended and improved
- bushland is protected from the adverse effects of development – such as stormwater runoff, spread of exotic plants and weeds, and visual impact of structures
- additional street trees and onsite landscaping contribute to amenity while minimising impact on solar access and residents views
- arrangement of building forms and street pattern allows good sunlight access to properties
- stormwater management measures prevent runoff and pollution of the harbour
- boat sheds and other structures along the foreshore minimise bulk and avoid clutter. Materials and colours utilised are unobtrusive
- use of locally indigenous flora extends habitats for native birds and other fauna
- man made noise is limited to protect the ambience of the natural environment

Quality Built Form

- development reflects and reinforces the existing distinctive built form/landscape areas and distribution of accommodation types
- development in conservation areas incorporates design, architectural detailing, materials and colours sympathetic with the area
- buildings address streets, reserves and waterways where appropriate
- cohesiveness throughout the area is achieved through landscaping and street tree planting
- views from lookouts and other vantage points are not obscured by structures or landscaping
- pedestrian routes such as footpaths and “shortcuts” are maintained as local landmarks important to the community

Quality Urban Environment

- Cremorne Reserve is protected and enhanced by complementary planting and landscaping in adjoining private gardens
- backyards are used for a variety of activities particularly for families with children and for the practical and recreation needs of residents in residential flat buildings

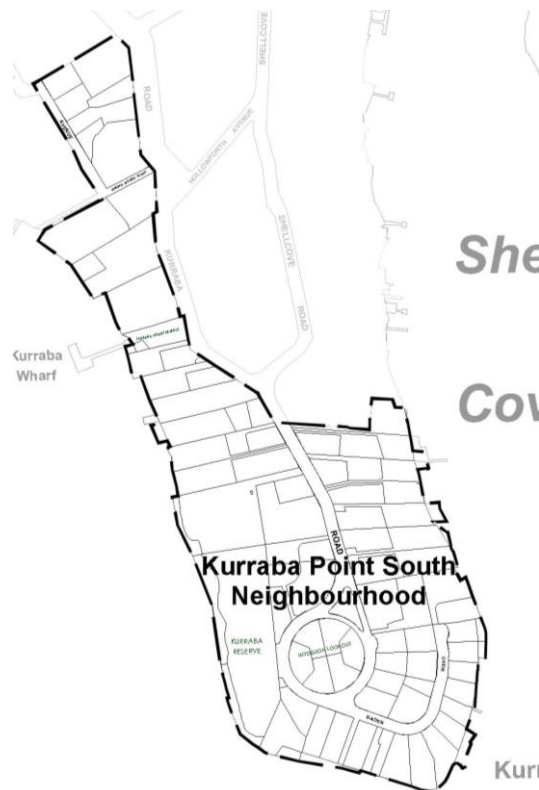
Efficient Use of Resources

- stormwater is retained for reuse on site.

The South Cremorne Planning Area includes the following Locality Areas in which development is subject to relevant provisions below:

- Section 6.1: Kurraba Point South Neighbourhood
- Section 6.2: Kurraba Point Conservation Area
- Section 6.3: Cremorne Conservation Area
- Section 6.4: Cremorne Point Conservation Area

6.1 KURRABA POINT SOUTH NEIGHBOURHOOD



6.1.1 Significant Elements

Land Use

- P1 Predominantly residential accommodation.
- P2 Passive and active recreational spaces.

Topography

- P3 Falls from the ridge along Kurraba Road down to the foreshores of Sydney Harbour.
- P4 Steep falls generally adjacent to the foreshore.

Natural Features

- P5 Low topographic profile and steady slopes with sections of shallow topography.
- P6 Along the eastern side of Kurraba Point, generally retained edges, some small jetties and boatsheds, mixed with natural shoreline.
- P7 Natural vegetation, varying degrees of private landscaping of native and introduced species.

Views

- P8 The following views and vistas are to be preserved and where possible enhanced:
 - (a) views from streets and reserves to Sydney Harbour and beyond.
 - (b) Hodgson lookout (41), Spains lookout (42), Kurraba Wharf lookout (43).

Identity / Icons

- P9 Hodgson Lookout.

Section 6 - South Cremorne Planning Area

P10 Sydney Harbour.

Subdivision

P11 Irregular grid pattern following the topography of the land.

P12 Deep narrow lots adjacent to the foreshore.

Streetscape

P13 Narrow carriageways with fully paved verges for pedestrians.

P14 Tall rendered masonry retaining walls built to street frontages on the high side of the street, interrupted with garage openings.

P15 Buildings generally setback a minimum from the boundary with a skewed alignment to respective street frontages

Public transport

P16 Development takes advantage of high levels of access to regular public ferry services from Kurraba Wharf.

6.1.2 Desired Future Character

Diversity

P1 Primarily medium to high density residential accommodation, generally comprising attached dwellings, multi dwelling housing and residential flat buildings according to zone.

P2 Limited opportunities for low density housing, generally comprising dwelling houses and dual occupancies.

P3 Any increases in density should be concentrated in the vicinity of Kurraba Wharf.

6.1.3 Desired Built Form

Siting

P1 Buildings address both the street, as well as the foreshore.

P2 In narrow streets, towards rear boundary (subject to foreshore building line) to produce a more open streetscape.

P3 Sites should provide front setbacks to allow for soft landscaping.

Colours and materials

P4 Development adjoining foreshore or bushland areas (such as at Wonga Road) use muted colours and non-reflective materials, such as brick and timber to ensure the scenic and environmental qualities are enhanced.

6.2 KURRABA POINT CONSERVATION AREA



6.2.1 History

The Kurraba Point Peninsula was part of the land grant to Alfred Thrupp, later acquired by Daniel Cooper. It remained in the Cooper family until the 1880s when long-term leases began to be converted to freehold.

Early development was limited to a small number of waterfront residences near the present Hayes Street, established in the 1830s. Leasehold properties became available from the 1850s and from the 1860s a number of large mansions were built on Kurraba Point. Industry in the area was minimal: a quarry on Kurraba Point operated in the 1840s to supply stone to Fort Denison; later it was used as a soap works then a ferry depot.

The improvement in transport opened the area for development. Waterfront properties were taken up with large, architect designed homes, some later replaced with residential flat buildings in the 1920s. Most of the earlier subdivisions have been subdivided for infill development.

6.2.2 Description

The Kurraba Point Conservation Area is bounded by the Kurraba Road, Wycombe Road, Bannerman Road and the shore of Shell Cove.

The landform follows the slope of the land towards the water to Neutral Bay and Shell Cove with roads following the contour lines.

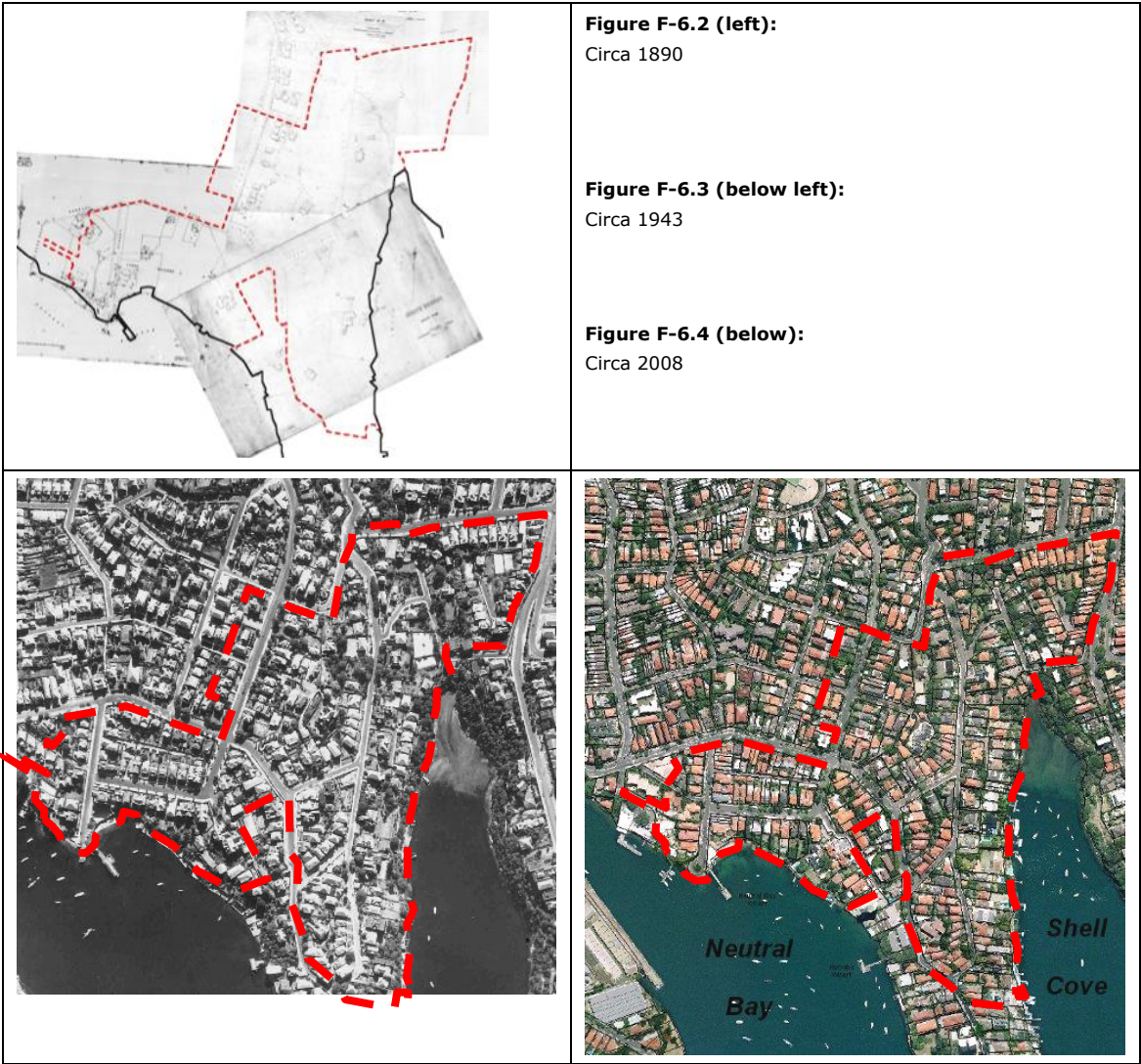
The higher section has a more regular subdivision pattern but the sloping part have an irregular subdivision determined by the street pattern and topography. Some later battle axe blocks to the waterfront sites. Lot sizes vary and many are developed for large architect designed houses with residential flat buildings on sites close to the water.

The area is characterised by the medium to large Federation and Edwardian homes with some earlier Victorian development, inter war dwelling houses and 1930s residential flat buildings. There some very fine examples of the period particularly along, Shell Cove Road, Kurraba Road, Hayes Street Wallaringa Road including Nutcote, home of the author May Gibbs. The group of dwelling houses and reserve in Bannerman Street and Prior Avenue form an unusual subdivision with a private reserve and tennis courts.

Section 6 - South Cremorne Planning Area

Elevated sites provide extensive views to Sydney Harbour and there are views from the Bogota Avenue, Kurraba Wharf, down Hayes Street and Lower Wycombe Road.

Front gardens contribute to the landscaping of the street. Gardens follow the slope of the site with stone walls to the street. There are street trees that add to the leafy character of the area.



6.2.3 Statement of Significance

The Kurraba Point Conservation Area is significant:

- (a) As an early harbourside suburb that has retained significant elements of its major 19th century development phase with good examples of later periods of development including some fine inter war flat buildings.
- (b) For the large number of architecturally distinguished homes some that have a strong relationship with the water and include the works of B. J. Waterhouse, Jefferson Jackson and Walter Liberty Vernon.
- (c) For its irregular subdivision pattern with irregular street pattern and widths that allied to the topography and plantings and mature trees add to a high quality luxuriant character.



6.2.4 Significant Elements

Topography

P1 Undulating topography with falls to Neutral Bay and Shell Cove.

Subdivision

P2 Rectilinear subdivision to upper level. Irregular subdivision to the lower slopes.

Streetscape

P3 Irregular street pattern following the contours. Street trees and stone walls align streets and paths.

Views

P4 Prior Avenue Lookout, Bogota Avenue Lookout, Kurraba Wharf Lookout. Views from Hayes Street and Lower Wycombe Road.

6.2.5 Characteristic buildings

P1 Federation and Edwardian detached dwelling houses. Inter war dwelling houses. Inter war residential flat buildings.

6.2.6 Characteristic built elements

Siting

P1 Centrally on lots with front and rear garden.

Form, massing and scale

P2 Single and two storey, detached dwelling houses with hipped and gabled roofs.

P3 Skillion wings and reduced height and scale to rear.

P4 Open verandahs to front.

P5 Strong skyline of simple, pitched roofs and chimneys visible from the street stepped along the sloping streets.

P6 Multi-level residential flat buildings.

Roofs

P7 Pitched between 30 and 45 degrees with some use of parapets to the street.

P8 Skillion roofs to rear extensions.

P9 Brick and rendered chimneys.

External Materials

P10 Face and rendered brick on sandstone base.

P11 Slate, terracotta tiles, corrugated metal roofs.

P12 Original timber windows doors and decorative joinery.

Windows, doors and joinery

P13 Consistent with building period and style. Timber.

Fences

P14 Original low scale front fences. Timber fences to rear and side. Sandstone walls, timber gates; timber pickets; timber rails; face brick with piers.

Car accommodation

P15 To the side or rear of dwellings.



Section 6 - South Cremorne Planning Area

6.2.7 Uncharacteristic and undesirable buildings and built elements

- P1 Contemporary residential flat buildings; contemporary buildings and additions; over-scaled additions; over-scaled and poorly detailed carports and garages to the street frontages; front and side dormers and rooflights; extensive glazing; glazed balustrades; removal of original detailing; infilled verandahs; rendered and painted face brickwork; modern facades; high walls and fences to the street.

6.3 CREMORNE CONSERVATION AREA



6.3.1 History

The Cremorne Conservation Area was contained within the Alfred Thrupp grant of 1815, which was subsequently acquired by Daniel Cooper. The Coopers did little to improve the property but commenced leasing land around 1850.

From the 1870s, with Military Road constructed and the ferry services available on the Harbour, demand for residential land boomed. This area was promoted as highly desirable and included large blocks of land on which expensive mansions were built.

Freehold land was available from the 1890s and such development continued until the early twentieth century. The Neutral Bay Land Company, who employed architects such as Walter Liberty Vernon and William Wardell to prepare house designs, developed an area of Wycombe and Shell Cove Roads north of Harriette Street, which features substantial, architecturally distinguished housing of the 1890s.

6.3.2 Description

The Cremorne Conservation Area is located on the sloping ground above Shell Cove and extends to Spofforth Street the east to the boundary of the LGA.

The landform slopes to the harbour with an irregular pattern of streets that follow the contours and slopes. The subdivision pattern varies with irregular lots that follow the topography.

The Conservation Area is characterised by a mix of small, medium and substantial late Victorian and Federation era dwelling houses with two and three storey, Inter-war residential flat buildings. Buildings are generally set in established gardens.

Section 6 - South Cremorne Planning Area



6.3.3 Statement of Significance

The Cremorne Conservation Area is significant:

- (a) as a consistent late 19th/early 20th Century residential area with a mix of Victorian, Federation one and two storey housing, mixed with Inter-war residential flat buildings,
- (b) as a largely intact early 20th Century suburb retaining much of the urban detail and fabric seen in gardens, fencing, street formations, use of sandstone, sandstone kerbing, natural rock faces, wide streets and compatible plantings, all of which are a response to the topography of the area.

6.3.4 Significant elements

Topography

P1 Sloping sites falling towards Shell Cove.



Subdivision

P2 Irregular lots following the street pattern and topography.

Streetscape

P3 Split roads. Double rail timber fences. Street reservation planting.

Views

P4 Bertha Road lookout (19), Guthrie Avenue lookout (20), Boyle Street (Hunts and Cremorne Point lookouts - 21, 22).

P5 Views from Lower Murdoch Street, Glade Avenue, Reed Street, Burroway Street, Wycombe and Shell Cove Road intersection, Barry Street and Wycombe Road intersection.

6.3.5 Characteristic buildings

P1 Single and two storey detached late Victorian, Federation and Edwardian dwelling houses. Inter war residential flat buildings.

6.3.6 Characteristic built elements

Siting

P1 To the middle of the lot with gardens to the front and rear.

Form, massing and scale

P2 Two storey terraces. Single and two storey detached dwellings. Reduced scale to the rear. Low, multi-storey residential flat buildings.

Roofs

P3 Pitched, hipped and gables roofs pitched between 30 and 45 degrees with skillion roofs to rear. Brick and rendered chimneys. Parapets to flat roofed residential flat buildings.

Materials

P4 **Walls:** Face and rendered brick on sandstone foundations; dark brick to Inter-war residential flat buildings.

P5 **Roofs:** Slate; terracotta tile and corrugated metal to the rear; flat roofs to residential flat buildings.

P6 Timber verandahs and Federation and Arts and Crafts detailing.

Windows and doors

P7 Consistent with building period and style. Timber.

Fences

P8 Low scale 900-1200 mm height; sandstone walls; metal palisade; timber pickets.

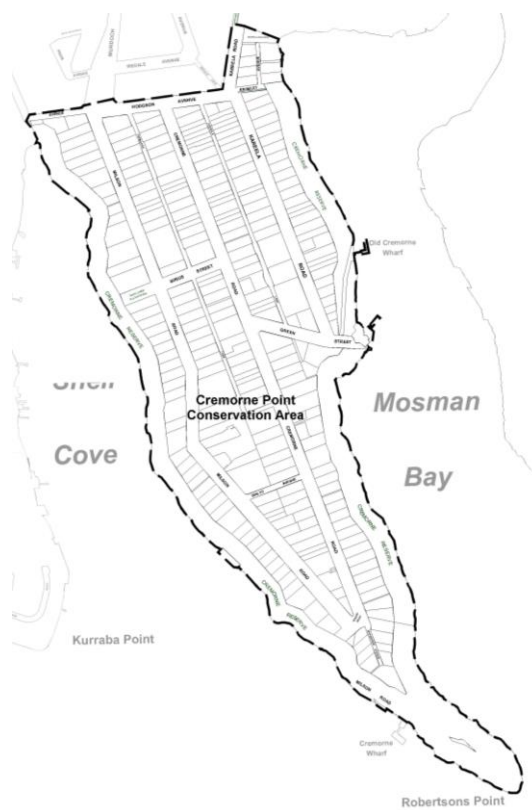
Car accommodation

P9 Set back from the main building line.

6.3.7 Uncharacteristic and undesirable buildings and built elements

P1 Over-scaled additions; dormers and skylights to front roof slopes; roof terraces; carports and garages to the street; paved hardstand areas within front setbacks high solid fences to the street; rendered and painted face brick; loss of original detail; contemporary buildings and additions and residential flat buildings.

6.4 CREMORNE POINT CONSERVATION AREA



6.4.1 History

Little development occurred on the peninsula until the 1850s. An area on Cremorne Point was leased to Clarke and Woolcott to establish the "Cremorne Gardens" recreation ground. It closed after a short time leaving the area as a well-known picnic spot on the harbour. A number of subdivisions were planned in the 1870s and 1880s. Coal mining was proposed after coal was found in the 1890s.

During the 1880s the Government demanded and eventually acquired a 100 foot foreshore reservation for public recreation after many years of negotiations, including a court case. Cremorne Point Reserve was gazetted in 1905. Subdivision finally occurred from this time and residential development was rapid thereafter with the major phase of development taking place in the first 20 years of the century.

Access to the peninsula was principally by water and tram. Cremorne Point opened as a tram terminus in 1911, with a line from Cremorne Junction to the ferry wharf. The line operated until 1956. Two ferry wharves operated from the earliest development of the area at Old Cremorne and Cremorne Point.

6.4.2 Description

The Cremorne Point Conservation Area is located on a peninsula between Shell Cove and Mosman Bays, and is bounded by Hodgson Street to the north.

The landform slopes southwards from a central ridgeline and down to the bays on each side. The street pattern follows the topography with roads and service lanes following the ridge line with secondary lanes giving access to the lower sites close to the water. Some streets have terraced footpaths set above or below the roads with exposed rock outcrops.

The Cremorne Point Conservation Area is characterised by large, single and two storey, freestanding Federation and Edwardian dwellings, reflecting the area's main period of

development. Some dwelling houses have their primary elevation to the water with garaging and carports to the rear. There are also examples of single storey Inter-war dwelling houses with multi-storey Edwardian and Inter-war residential flat buildings. There is considerable Post-war infill development of single dwelling houses and multi-storey residential flat buildings.

The buildings generally have irregular setbacks from the street with level or terraced gardens and a mix of fencing types. There is extensive use of stone in boundary and retaining walls.

The main roads are well landscaped with nature strips and street plantings. Front gardens are well established, and often have extensive plantings to give an overall impression of a landscaped, garden suburb. The peninsula has a landscaped public reserve to its perimeter that is unusual in the Sydney area.

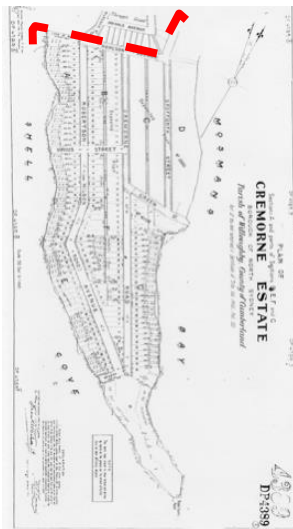


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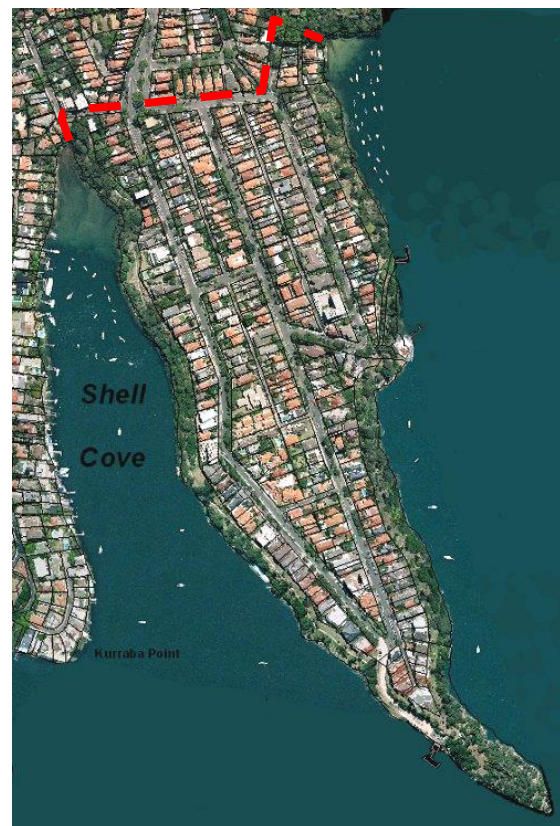
Circa 1890

Figure F-6.9 (below left):

Circa 1943

Figure F-6.10 (below):

Circa 2008



Section 6 - South Cremorne Planning Area

6.4.3 Statement of Significance

The Cremorne Point Conservation Area is significant for:

- (a) Consistent early 20th Century residential area with a mix of Federation and 1920s one and two storey housing mixed with inter-war residential flat buildings of two to three storeys, all built on large allotments with a strong orientation to the water.
- (b) Unique foreshore reserve that predates the residential subdivision, which demonstrates the concern for recreation, public access and suburban amenity, and the importance of headlands in the visual character of Sydney Harbour.
- (c) The visual unity derived from its subdivision history that is still apparent.

6.4.4 Significant elements

Topography

- P1 Low ridgeline along a peninsula between two bays that slopes to the water.

Subdivision

- P2 Regular rectangular lots.

Streetscape

- P3 Continuity of fences and landscaping. Sandstone kerbing, sandstone and concrete walls, street gardens.

Views

- P4 Cremorne Reserve Walk lookouts (many, including 34, 35, 38, 40), Hodgson Avenue lookout (24), Bromley Avenue lookout (25), Bogota Avenue lookout (26), Cremorne Road lookout (27), Sirius Park lookout (28), Kareela Lane lookout (29), Green Street lookout (30), Cremorne Lane lookout (31), St Chads lookout (32), Milson Road lookout upper level (33), Wallenundgal lookout, Milson Road (36), Wharf Road lookout (37), Kareela Lane lookout (29). Views from Rialto Avenue, Kareela Road.
- P5 Cremorne Reserve is protected and enhanced by complementary planting and landscaping in adjoining private gardens.

6.4.5 Characteristic buildings

- P1 Single and two storey, detached Federation and Edwardian dwelling houses. Edwardian and Inter-war residential flat buildings.

6.4.6 Characteristic built elements

Siting

- P1 To the middle of the lot with gardens to the front and rear.
- P2 Buildings sited to retain street views above and to the side to harbour.
- P3 Front setback 7.9m; 10-15 m east side of Kareela Rd and west side of Cremorne Rd between Sirius St and Hodgson Ave.
- P4 Side setbacks of 1.5 m or 5 m where lots are amalgamated.

Form, massing and scale

- P5 Single and two storey detached dwellings. Double elevations to waterfront properties. Reduced scale to the rear on non waterfront properties. Multi-storey residential flat buildings.



Roofs

- P6 Hipped and gables roofs pitched between 30 and 45 degrees without dormer windows or openings that can be seen from the street. Brick and rendered chimneys.
- P7 Flat or pitched roofs; parapet walls to Inter-War residential flat buildings.

Materials

- P8 **Walls:** Face and rendered brick on sandstone foundations; shingle spandrels and gables; fibre cement sheeting, battened gables and rough cast render.
- P9 **Roofs:** Slate; terracotta tile and corrugated metal; flat roofs to some residential flat buildings.
- P10 Timber verandahs and Federation and Arts and Crafts detailing.

Windows and doors

- P11 Consistent with building period and style. Timber.

Fences

- P12 Low scale to 900 mm height. Up to 1.2 m for large Federation residences.
- P13 Sandstone walls; timber pickets.

Car accommodation

- P14 Parking provided off rear lanes.

6.4.7 Uncharacteristic and undesirable buildings and built elements

- P1 Over-scaled additions; dormers and skylights to front roof slopes; roof terraces; carports and garages covering more than 1/3 of the street frontage; high solid fences to the street; rendering and painting of face brick; extensive glazing; glazed balustrades; loss of original detail; contemporary buildings and additions; and residential flat buildings.



SECTION 7 NEUTRAL BAY PLANNING AREA

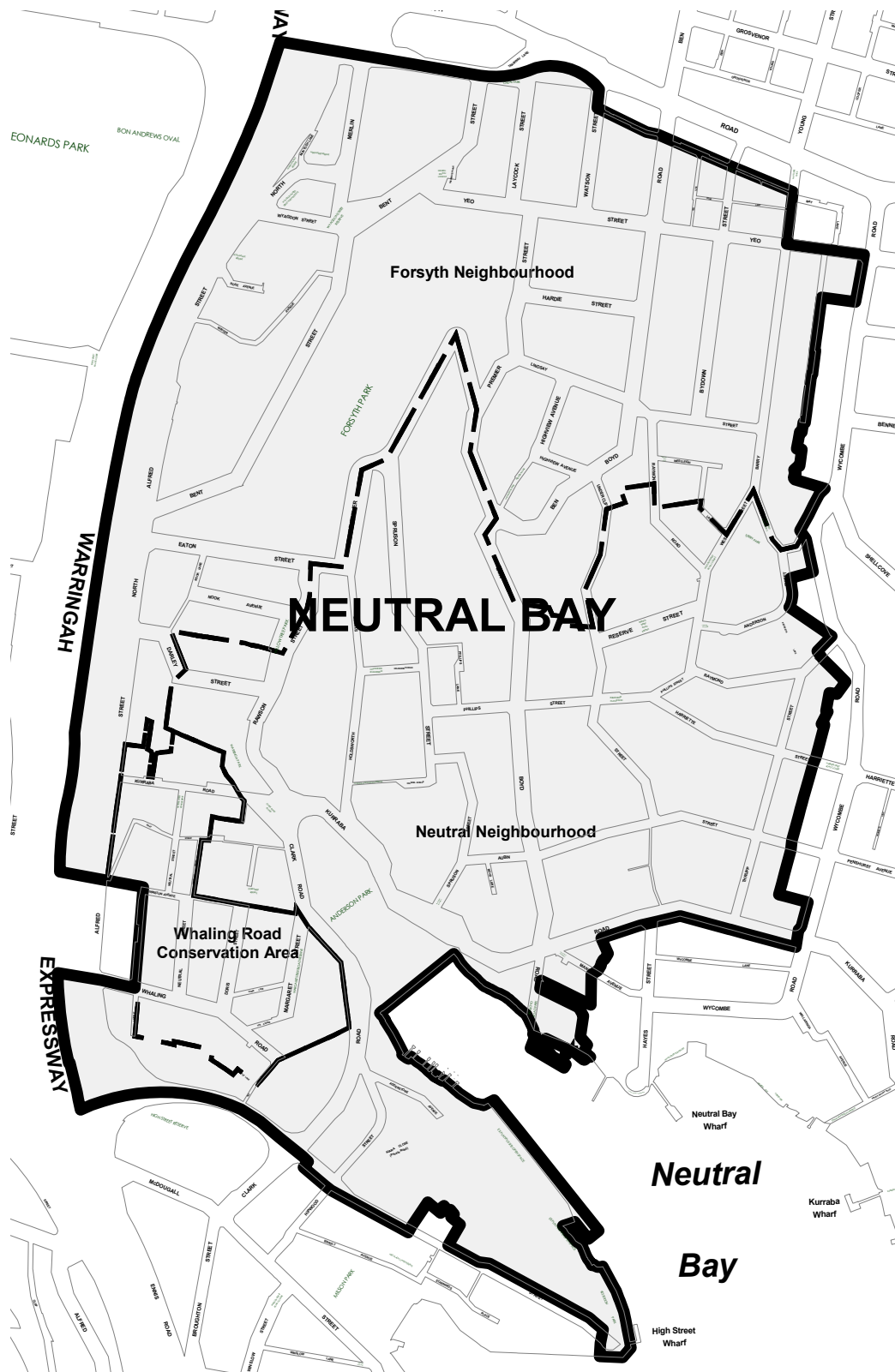
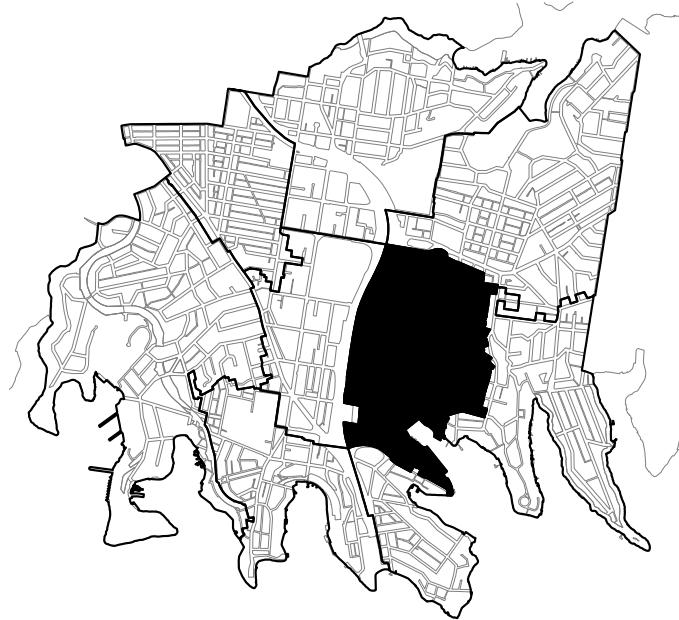


Figure F-7.1: Neutral Bay Planning Area and associated Locality Areas

7.0 NEUTRAL BAY PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the Neutral Bay Planning Area.

Neutral Bay is a diverse residential neighbourhood. Lower density development exists in the neighbourhood where there are small allotments, conservation areas and heritage items. Small shops, community and school facilities cater for the local residents. The waterfront and harbour setting of the Neutral Bay Area provide passive and active recreation pursuits, access to transportation, to other parts of the harbour via ferries and contribute to the natural and scenic qualities of the neighbourhood.

Development in the Planning Area should result in:

- residential growth occurring in accordance with Council's Local Housing Strategy, principally in the high density residential zones situated in the vicinity of Military Road
- a wide range of residential types and sizes being distributed throughout the area
- a scattered range of shops, services and facilities being available to the local community
- any alterations and additions being of a similar scale to existing buildings

and where:

Function

- there are safe and accessible community facilities and meeting places
- a few non-residential uses operate without adverse effect on the residential neighbourhood
- accessible and safe pedestrian routes are extended from Forsyth Park to the foreshore at Anderson Park
- public transport, cycling and walking are the preferred means of transport in an effort to reduce through traffic within Neutral Bay.

Section 7 - Neutral Bay Planning Area**Environmental Criteria**

- parkland and natural foreshore areas are conserved and protected and pedestrian access to these is incorporated and improved
- good sunlight is available to public spaces and parks
- ecology of bushland is protected from adverse impacts of development such as stormwater runoff, dumping of fill soil and vegetation, leaching of fertilisers, spread of introduced plants and weeds and visual impact of structures
- habitat for native fauna is abundant through planting local flora
- preservation and enhancement of tree coverage in line with Council's Urban Forest Policy

Quality Built Form

- new development is consistent with the existing built form
- buildings are of a lower scale and stepped on sloping terrain with vegetation abundant on steep slopes
- buildings are set back from all boundaries and have landscaped front gardens, softening the built form
- views of surrounding areas and the harbour are available through sites and achieved through setbacks of the built form from all boundaries and low, open fencing
- heritage items are protected, and significant streetscape elements are conserved in the Whaling Road conservation area
- wharf areas in Neutral Bay have a high level of amenity, have active uses nearby and provide a safe point for commuters to and from the area

Quality Urban Environment

- backyards are used for private recreational needs of residents in apartment buildings, particularly for families and children
- front fences are low and offer good visual surveillance to house fronts and gardens

Efficient Use of Resources

- existing buildings are generally maintained to prevent unnecessary waste of building materials
- storm water is captured on site and used to water gardens

The Neutral Bay Planning Area includes the following Locality Areas in which development is subject to relevant provisions below:

Section 7.1: Fortsytth Neighbourhood

Section 7.2: Neutral Neighbourhood

Section 7.2.4: HMAS Platypus

Section 7.3: Whaling Road Conservation Area

7.1 FORSYTH NEIGHBOURHOOD



7.1.1 Significant Elements

Land Use

- P1 Predominantly residential accommodation.
- P2 Passive and active recreational spaces.
- P3 Educational establishments.

Topography

- P4 Generally falls to the south-east from the ridge along Military Road and from the Warringah Freeway.
- P5 Steep falls occurring adjacent to northern and western sides of Forsyth Park.

Natural Features

- P6 Remnant bushland at the northern end of Forsyth Park.

Views

- P7 The following views and vistas are to be preserved and where possible enhanced:
 - (a) District views from streets and reserves to Sydney Harbour and beyond.
 - (b) Kenneth Bolton Lookout (44), Bent & Yeo Streets Lookout (45), Merlin Street Lookout (46), Bent Street Lookout (47), Forsyth Park Lookout (48)

Identity / Icons

- P8 Warringah Freeway, a major arterial thoroughfare.
- P9 Military Road, a major arterial thoroughfare.
- P10 Forsyth Park.

Subdivision

- P11 Rectilinear grid pattern on the upper slopes.

Section 7 - Neutral Bay Planning Area

P12 Irregular subdivision pattern west of Forsyth Park due to the undulating and steep topography.

Streetscape

P13 To the west of Forsyth Park:

- (a) A mixture of one and two way streets.
- (b) Split carriageways separated by sandstone retaining walls and double rail timber fences.
- (c) Buildings setback from the boundary behind low fences.

P14 To the east of Forsyth Park:

- (a) Tree lined streets with grassed verges and concrete footpaths.
- (b) Garages and carports built to the boundary on the high side of the street.
- (c) Buildings setback from the boundary with a skewed alignment to the street.

Public transport

P15 Development is to take advantage of high levels of accessibility to high frequency public bus services along Military Road.

7.1.2 Function

Diversity

- P1 Predominantly high density residential accommodation in the form of residential flat buildings concentrated in the vicinity of Military Road to take advantage of high levels of public transport.
- P2 Some pockets of attached dwellings and multi dwelling housing according to zone.
- P3 Some pockets of federation style dwelling houses and semi-detached dwellings in Laycock, Bydown and Barry Streets.

7.1.3 Desired Built Form

Form, massing & scale

P1 Buildings to step down in height in accordance with the terrain.

Noise

P2 Elevations of buildings fronting Military Road and Warringah Freeway are to be designed and incorporate design features to minimise traffic noise transmission (e.g. the use of cavity brick walls, double glazing, minimal glazing, solid core doors, concrete floors, enclosed balconies etc).

Fences

P3 Buffer planting screens impact from Warringah Expressway.

7.2 NEUTRAL NEIGHBOURHOOD



7.2.1 Significant Elements

Land Use

- P1 Predominantly residential accommodation.
- P2 Passive and active recreational spaces.
- P3 Small scale commercial activities.

Topography

- P4 The northern portion of the Area generally falls to the south down to the foreshores of Sydney Harbour.
- P5 The southern portion of the Area generally falls to the east down to the foreshores of Sydney Harbour.
- P6 Steep topography to the north of Anderson Park.

Natural Features

- P7 Remnant natural shoreline areas in Neutral Bay, Anderson Park and Kesterton Park.
- P8 Natural outcrops of exposed rock and cliff faces.

Views

- P9 The following views and vistas are to be preserved and where possible enhanced:
 - (a) views from streets and reserves to Sydney Harbour and beyond.

Section 7 - Neutral Bay Planning Area

- (b) Westleigh Street Lookout (49), Reserve Street Lookout (50), Anderson Street Lookout (51), Phillip Street Lookout (52), Holdsworth Road Lookout (53), Spruson Street Lookout (54).

Identity / Icons

- P10 Anderson Park.
- P11 Former HMAS Platypus site, including:
 - (a) Existing wharf structure,
 - (b) Sandstone cuttings, and
 - (c) Heritage items and aspects of heritage significance are incorporated in development and reflect the history of the site as a gas works and naval installation.
- P12 Sydney Harbour.

Subdivision

- P13 Irregular grid pattern generally aligning with the topography.

Streetscape

- P14 Tree lined streets with grassed verges and concrete footpaths.
- P15 Split carriageways, with pedestrian linkages through.
- P16 Generally a mixture of low sandstone, masonry and timber picket fences, with higher fences on the lower side of the street.
- P17 Buildings generally setback, with some instances of garages built to the boundary.

Public transport

- P18 Development to take advantage of high levels of access to regular public ferry services from High Street and Neutral Bay Wharfs and bus services along Clarke and Kurraba Roads.

7.2.2 Desired Future Character

Diversity

- P1 Predominantly low density residential accommodation in the form of dwelling houses, semi-detached houses and dual occupancies.
- P2 Pockets of attached dwellings, multi dwelling housing and residential flat buildings according to zone.
- P3 Small scale commercial and retail premises according to zone.
- P4 Maintaining and enhancing recreational and cultural facilities.

Foreshore

- P5 Wharf areas are made active, safe and accessible.
- P6 Wharf areas in Neutral Bay have a high level of amenity, have active uses nearby and provide a safe point for commuters to and from the area

7.2.3 Desired Built Form

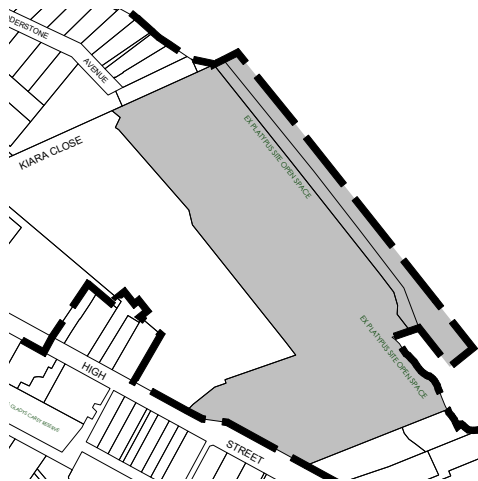
Form, massing and scale

- P1 Development should be carefully designed to follow the topography of the land, with buildings on sloping sites.

Foreshore

- P2 Development on sites on or near the foreshore should be carefully designed to not restrict water views from neighbouring property or from public areas.
- P3 Development adjacent to the foreshore is small in scale and does not dominate the foreshore frontage.
- P4 Development associated with marinas and boating activity is kept to a minimum and is compatible with the surrounding land uses.
- P5 Development adjoining foreshore areas use muted colours and non-reflective materials, such as brick and timber to ensure the scenic and environmental qualities are enhanced.

7.2.4 HMAS Platypus



Diversity

- P1 A mix of cultural, recreational, community and commercial uses. Potential uses include cafes/dining, training/education, performance/events, offices, studios, conference/function/meeting places, neighbourhood shops and small scale maritime industrial uses.
- P2 Existing wharf to be retained and adaptively reused.
- P3 Existing buildings to be adaptively reused where possible.
- P4 Existing buildings are to be reduced in scale where appropriate to provide a better relationship with neighbouring development.
- P5 Provision of a sequence of terraces, streets, squares and gardens for public enjoyment by the waterfront.
- P6 Public open spaces are to be connected internally and to external existing pedestrian links.

Heritage

- P7 Heritage items and aspects of heritage significance are incorporated in development and reflect the history of the site as a gas works and naval installation.
- P8 The heritage significance of structures on the site is to be maintained and conserved through the implementation of policies within a conservation management plan for the site.

Contamination

- P9 The site is remediated through implementation and measures outlined in a remediation report and a review of remediation works as outlined in Section 11 -Contamination and Hazardous Materials to Part B of the DCP.

Section 7 - Neutral Bay Planning Area

Views

- P10 Views along pedestrian access from High Street of Neutral Bay and Sydney Harbour. View opportunities from a lookout higher on the site.
- P11 View corridors from High Street and Kiara Close to Sydney Harbour between and over buildings.

Solar access

- P12 Sunlight corridors between and over buildings to foreshore open space.
- P13 Buildings designed to maximise solar access to surrounding residential properties and public areas during winter months.

Siting

- P14 New development to be sited towards the south-western boundary to allow a substantial setback from the foreshore to enable continual public access along the foreshore.

Setback

- P15 Buildings adjoining public open space areas are setback 6m to achieve a buffer with parkland/foreshore.

Form, massing and scale

- P16 Two or three storey at High Street and the foreshore.
- P17 Five storeys against cliff.
- P18 Development is stepped down to open space along foreshore resulting in minimised overshadowing of public areas.
- P19 Development on High Street relates to the existing two storey historic attached dwellings.

Roofs

- P20 Pitched between 25 and 35 degrees along High Street frontage.
- P21 Flat on lower site maximising view opportunities.

Access

- P22 Continuous pedestrian access is to be provided along the entire foreshore frontage.
- P23 Pedestrian links are to be provided to Kesterton Park and High Street.

Car accommodation

- P24 The existing car park accessed off Kiara Close may be retained if the number of parking spaces does not increase and access to the car park is managed / controlled to prevent casual parking.
- P25 Any new car parking should be located underground or within buildings.
- P26 Unenclosed parking should be suitably screened from public view.
- P27 Disabled parking should be provided to enable public access to the foreshore and open spaces.

Traffic generation

- P28 A traffic study must be submitted which outlines strategies to minimise the impact from traffic generated by the site.

7.3 WHALING ROAD CONSERVATION AREA



7.3.1 History

Prior to the construction of the Warringah Freeway, the Whaling Road Conservation Area was the eastern edge of the North Sydney township. The steep escarpment, now marked by Clark Road, was the physical barrier for the area.

The area was part of a land grant to James Milson in 1839 part of which was subdivided in the 1870s by Milson's daughter Sophia Shairp.

During the latter half of the 19th century early farms and the occasional large residence were subdivided and developed for small-scale housing. By the early 20th century the remaining larger properties were subdivided for speculative development that led to the construction of rows of single storey, semi-detached bungalows.

The area was largely developed by 1930 and remained unchanged until the 1960s when construction of the Warringah Freeway isolated the area from North Sydney.

7.3.2 Description

The Whaling Road Conservation Area is defined by the Warringah Expressway and the escarpment edge to Clark Road.

The landform slopes steeply to the north east, creating dramatic views, and resulting in stepped forms and house. The urban form is strongly influenced by the topography, and by the main phases of development.

The area is a discrete residential neighbourhood that is characterised by dwelling houses on small lots. The subdivision pattern is regular and irregular reflecting the topography with has small lots. Streets follow the slope and contours of the area with short streets with a number of steep, dead-end streets. Doris Street is split level for most of its length, and Margaret Street is not accessible to vehicles.

Section 7 - Neutral Bay Planning Area

The area is characterised by small scale housing, one to two storey Victorian Georgian, Victorian Filigree and Federation style dwellings forming a dense urban pattern. There are high quality groups of single and two storey attached dwellings.

There are small pocket of public open space, some pedestrian links. Fencing is low and characteristically of timber giving coherence throughout the area.

Doris Street has remnant cobbling showing through contemporary road surfacing.



7.3.3 Statement of Significance

The Whaling Road Conservation Area is significant:

- (a) For its unity that relates to its subdivision history and which is evident in the development and streetscape of the area.



- (b) As a consistent and intact Victorian and Federation residential area that consists of modest housing on small lots.
- (c) As a largely intact late 19th and early 20th century subdivision that retains much of the urban fabric and detail associated with its development over time such the street formations, sandstone kerbing, fencing, gardens and a strong relationship to topography.
- (d) For the quality and collective significance of the buildings within the area.

7.3.4 Significant elements

Topography

- P1 Slopes steeply to north east, escarpment to Clark Road.

Subdivision

- P2 Irregular and regular rectilinear subdivision reflecting the topography and original subdivision pattern with boundary to street frontage.

Streetscape

- P3 Streets following the slope or contours. Changes in level in Margaret, Doris, Bray and Neutral Streets. Sandstone retaining walls and steps. Street reservation plantings. Pedestrian access via a network of footpaths and steps that connect with Anderson Park, North Sydney and the foreshore with double rail timber fences.

Views

- P4 Neutral Street Lookout, Margaret Street Lookout. Street views along Whaling Road, Doris Street and Neutral Street. Slot views over and between buildings to the city and Harbour Bridge.

7.3.5 Characteristic buildings

- P1 Detached dwelling houses on small lots – single storey Victorian Georgian, Victorian Filigree and Federation.
- P2 Two storey attached dwellings.

7.3.6 Characteristic built elements

Siting

- P1 Principal frontage oriented and parallel to the street, forward towards front boundary.
- P2 Front setback 4- 5m.
- P3 Side setback 1 – 1.5m.
- P4 Rear setback 5m.

Form, massing and scale

- P5 Single storey detached and semi-detached dwelling houses.
- P6 Double fronted dwelling houses with projecting gabled bay and flanking verandahs.
- P7 Two storey attached dwellings with front verandahs.
- P8 Matching single storey attached dwellings.

Roofs

- P9 Pitched between 30 and 45 degrees.
- P10 Gabled, hipped and gabled.



Section 7 - Neutral Bay Planning Area

Materials

P11 **Walls:** A mixture of sandstone, weatherboard, face brick on sandstone bases and rendered brick on stone bases.

P12 **Roofs:** Slate, corrugated metal, terracotta tile.

Windows and doors

P13 Consistent with building period and style. Timber.

Fences

P14 Low to 900 mm.

P15 Metal palisade on low stone plinths; low brick; timber picket fences.

Car accommodation

P16 Generally no off street parking.

P17 Garages and carports located off rear lanes.

7.3.7 Uncharacteristic elements

P1 Contemporary and 1960's residential flat buildings; over-scaled additions; carports garages and paved hardstands built forward of the building line; over-scaled and front dormers and skylights; large rear decks; enclosure of verandahs; high solid front fences; rendering and painting of face brickwork; loss of original detail.



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SECTION 8 KIRRIBILLI PLANNING AREA

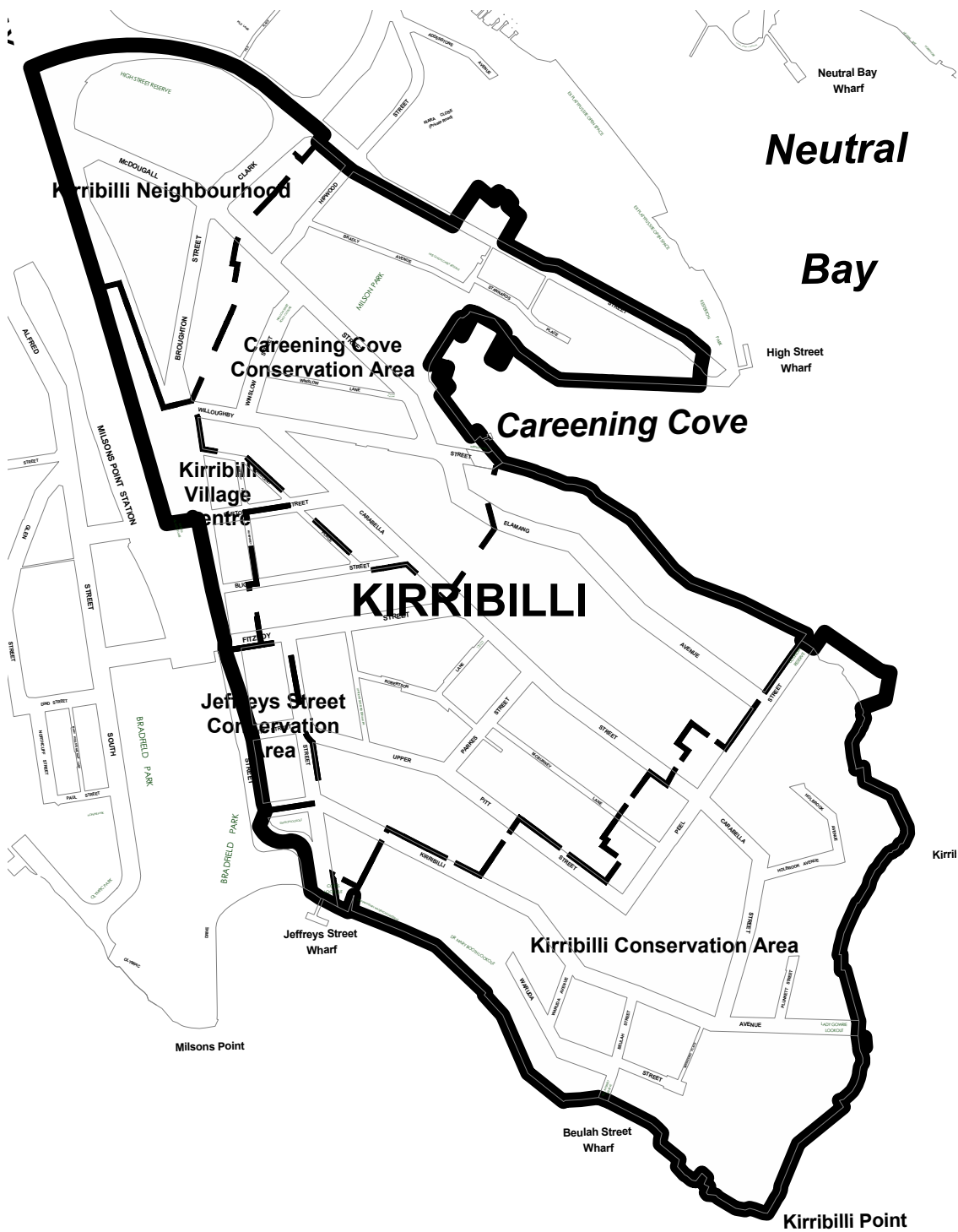
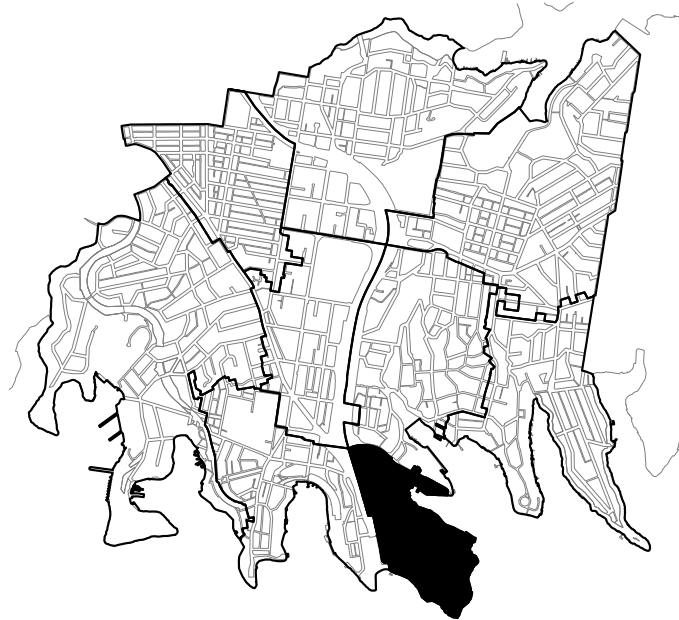


Figure F-8.1: Kirribilli Planning Area and associated Locality Areas

8.0 KIRRIBILLI PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the Kirribilli Planning Area.

Kirribilli is located on the foreshores of Sydney Harbour with spectacular views of the Sydney CBD, the Sydney Opera House and Sydney Harbour Bridge. Bradfield Park, at the base of the Sydney Harbour Bridge, provides unique outlooks and a place for outdoor recreation and softens the dense built form on the foreshores. The focal point of the Kirribilli peninsula is the Kirribilli Village Centre, a compact and lively area with a community centre, local shops and outdoor cafes that serve the needs of the local community. The village is surrounded by a predominantly residential area with a small number of other uses such as education, transport, maritime activities and community facilities. Conservation areas are often associated with prominent landmarks such as Admiralty House and Kirribilli House.

Development within the Planning Area should result in:

- limited growth with no substantial increase in residential densities.
- there is no significant change in low density residential or conservation areas.

and where:

Function

- the community centre in the Kirribilli Village provides a meeting place for the local community.
- a limited number of non-residential uses coexist peacefully with residential uses without adverse impacts on amenity.
- local shops provide basic needs (groceries, newsagents, hairdressing) for the local community and recreation opportunities (galleries, cafés) without adverse effects.
- open space caters for a variety of recreational needs.
- through traffic is discouraged, to avoid traffic congestion, and commuter parking is managed.

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- the Village Centre is lively with an appropriate number of outdoor cafes and restaurants.
- laneways provide rear access for dwellings and local shops in the Kirribilli Village Centre.
- pedestrian links to Milsons Point and public transport facilities are upgraded and extended.
- bus, train, and ferry services are improved to encourage greater patronage.
- traffic calming strategies are implemented to avoid traffic problems.
- new development has minimal impact on traffic flows and the demand for parking.
- public transport, cycling and walking are the preferred modes of transport.
- the pedestrian tunnels to Arthur Street in Milsons Point and Kirribilli Station are maintained as local landmarks.

Environmental Criteria

- the natural areas of Sydney Harbour foreshore are conserved with improved and extended pedestrian access.
- the impacts of non-residential uses, such as noise and air pollution, are minimised.
- solar access is maximised to open space areas in public and private domains.
- extensive views of the Sydney Harbour and the Sydney CBD from open space areas, lookouts, private dwellings are shared and not obscured by structures and landscaping.
- public open space is provided for outdoor activities and for people to enjoy harbour views.

Quality Built Form

- any development that occurs reflects and reinforces the existing built form and distribution of accommodation types.
- landscaping and street plantings complement the built form to create cohesiveness throughout the area.
- heritage items, icons, contributory items and conservation areas are conserved and maintained with an appropriate curtilage.
- development on the foreshore enhances views to Kirribilli from Sydney Harbour.

Quality Urban Environment

- backyards are used for a variety of activities, particularly for families with children and for the practical and recreational needs of residents.

Efficient Use of Resources

- existing buildings are maintained and adaptively reused to prevent unnecessary waste of building materials.
- stormwater is retained for reuse on site.
- energy consumption and waste disposal is minimised by all land uses.

The Kirribilli Planning Area includes the following Locality Areas in which development is subject to relevant provisions below :

- Section 8.1: Kirribilli Village Centre
- Section 8.2: Kirribilli Neighbourhood



Section 8.2.4: McDougall Street (northern side)

Section 8.2.5: 3-11 McDougal Street (Greenway Flats)

Section 8.2.6: James Milson Village

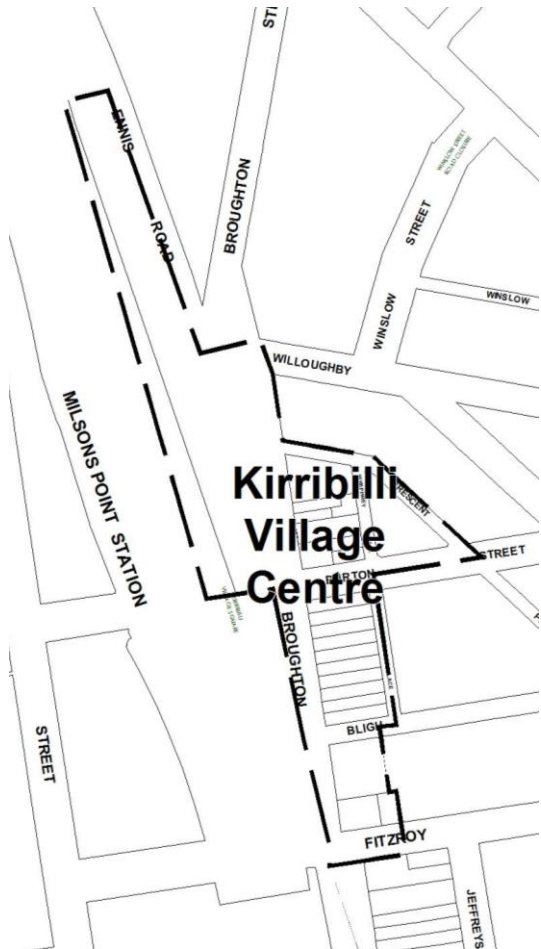
Section 8.3: Careening Cove Conservation Area

Section 8.4: Kirribilli Conservation Area

Section 8.5: Jeffreys Street Conservation Area



8.1 KIRRIBILLI VILLAGE CENTRE



8.1.1 Significant Elements

Land Use

P1 Predominantly commercial and mixed commercial and residential development.

Topography

P2 Generally flat, forming the topmost part of the ridge to Kirribilli Peninsula.

Natural Features

P3 Forms the topmost part of the ridge to Kirribilli Peninsula.

Views

P4 The following views and vistas are to be preserved and where possible enhanced:

- (a) Views of the Sydney CBD, Sydney Opera House and Sydney Harbour from Broughton Street.

Identity / Icons

- P5 Sydney Harbour Bridge including pedestrian tunnels to Milsons Point Station / Arthur Street in Milsons Point and the Ennis Road Bays.
- P6 Bradfield Park.
- P7 Sydney Harbour.



- P8 The pedestrian tunnels to Arthur Street in Milsons Point and Kirribilli Station are maintained as local landmarks.

Subdivision

- P9 Generally rectilinear lots with narrow frontages.
P10 Leased spaces under the Sydney Harbour Bridge.

Streetscape

- P11 Wide fully paved footpaths.
P12 Iconic street trees along Broughton Street, Ennis Road (n.b. also refer to the Harbour Bridge Conservation Management Plan and heritage inventory sheets).
P13 Buildings built to the boundary with active frontages.
P14 Continuous awnings to Broughton Street and Ennis Road.
P15 Safe and effective pedestrian crossing at intersection of Broughton and Burton Streets.

Public transport

- P16 Development should capitalise on the Area's high level of access to public train, bus and ferry services.

8.1.2 Desired Future Character

Diversity

- P1 The community centre in the Kirribilli Village provides a meeting place for the local community.
P2 The Village Centre is lively with an appropriate number of outdoor cafes and restaurants.
P3 Predominantly mixed commercial and residential development, with retail premises located at ground level and shop top housing above.
P4 There should be limited redevelopment with no substantial increase in density or intensity.
P5 Adaptive reuse of the Ennis Road Bays to ensure the activation of the western side of the Village.
P6 All street frontages at the ground level are to be activated by adjoining commercial activities.
P7 Laneways provide rear access for dwellings and local shops in the Kirribilli Village Centre.

8.1.3 Desired Built Form

Form, massing & scale

- P1 Existing shopfronts are restored to original form with nineteenth century two storey shopfronts with parapet.
P2 New shopfronts reflect the scale and proportions of the historical two storey parapet shopfront character of Kirribilli Village.
P3 No window walls or curtain walls above awnings.
P4 Shopfronts to provide continuous, solid street awning (no glass) between ground and first floor.
P5 No dormers or first floor decks on front elevation.
P6 No balconies, verandahs or the like to project over footpaths.
P7 Development on the foreshore enhances views to Kirribilli from Sydney Harbour.



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Windows and doors

P8 Small windows with timber joinery at first floor level on front façade.

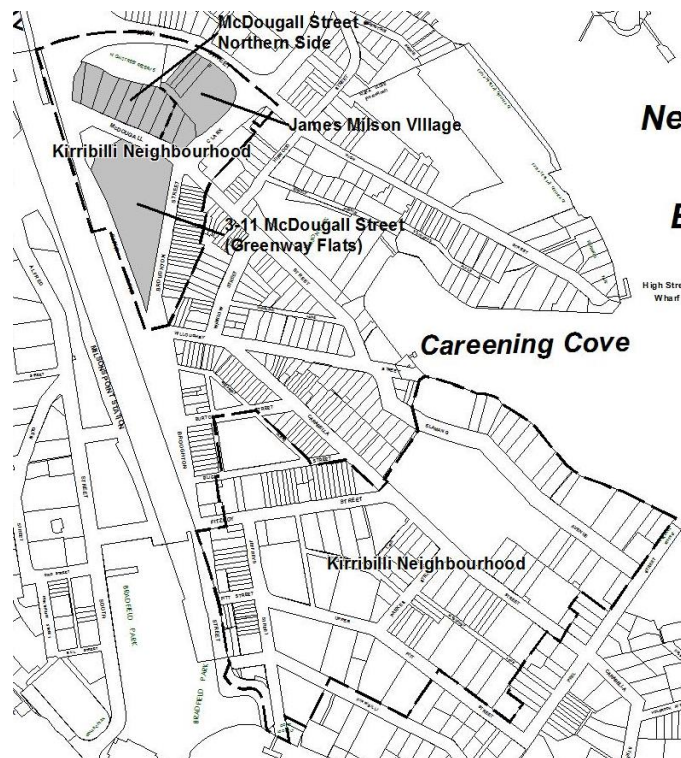
Car accommodation

P9 No direct vehicular access to Broughton Street.

P10 Occupant car parking and loading and unloading service areas are to be accessed off Humphrey Place where possible.

P11 Vehicular access is generally by way of shared or amalgamated vehicular access points where possible.

8.2 KIRRIBILLI NEIGHBOURHOOD



8.2.1 Significant Elements

Land Use

- P1 Predominantly residential development.
- P2 Educational establishments.

Topography

- P3 Generally falls to the south-west and north east from the ridge along Carabella Street down to the foreshores of Sydney Harbour.

Natural Features

- P4 Sydney Harbour frontage.

Views

- P5 The following views and vistas are to be preserved and where possible enhanced:
 - (a) District views from streets and reserves to Sydney Harbour and beyond.
 - (b) Slot views of Sydney Harbour from Elamang, McDougall, Upper Pitt Streets and Kirribilli Avenue.
 - (c) Views of the harbour from Robertson Lane Lookout (61); Bradfield Park Lookout No. 2 (68), Copes Lookout (66); Jeffreys Street Lookout (65), Stanton Lookout (67).

Identity / Icons

- P6 Sydney Harbour.
- P7 Sydney Harbour Bridge.
- P8 St Aloysius College.

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P9 Loretto Convent School.

P10 Bradfield Park.

P11 Greenway Flats.

Subdivision

P12 Site areas of 230m² - 750m²

Streetscape

P13 Narrow streets with limited landscaping.

P14 Split carriageways with sandstone retaining walls and one way movement.

P15 High masonry fences except to heritage items.

P16 Buildings generally setback a minimum from the boundary with a skewed alignment to respective street frontages.

P17 Garages built to the boundary.

Public transport

P18 Development should capitalise on the Area's high level of access to public train, bus and ferry services.

8.2.2 Desire Future Character

Diversity

P1 Predominantly high density residential accommodation in the form of residential flat buildings, according to zone.

P2 Limited potential for low to medium density residential accommodation in the form of attached dwellings, dwelling houses, dual occupancies, multi dwelling housing and semi-detached dwellings.

P3 Educational establishments.

P4 Limited redevelopment is envisaged for this Area with no substantial increase in density.

8.2.3 Desired Built Form

Form, massing and scale

P1 Development is to generally conform with the provisions contained within Part B – *Development Controls* of the DCP.

P2 Educational establishments are to reflect the scale and massing of development on adjoining properties at its interface with the adjoining property.

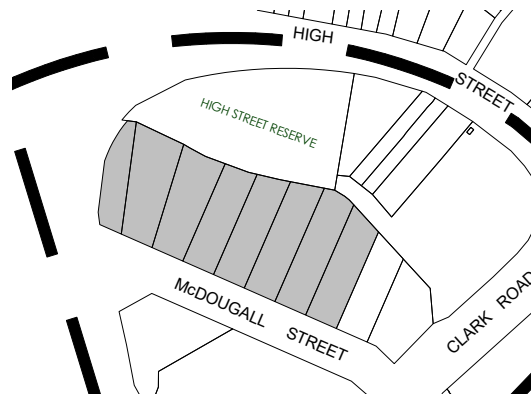
Materials

P3 **Walls:** Masonry, face brick.

P4 **Windows:** Timber framed.

P5 **Roofs:** Terracotta tiles, slate.

8.2.4 McDougall Street (Northern Side)



Diversity

P1 High density residential accommodation in the form of residential flat buildings.

Subdivision

P2 Sites amalgamated with frontages of 25m-35m.

P3 No isolated properties (sites with frontage less than 25m).

Streetscape

P4 Existing street trees in McDougall Street.

P5 Additional street plantings in accordance with Council's Street Tree Strategy.

P6 Significant trees and plantings in front gardens.

Siting

P7 Towards the front boundary to provide substantial setback from public open space off High Street.

Setbacks

P8 Front: 5m.

Side: 5-7m.

Rear: 15m.

P9 Side setback between No.s. 16-18 McDougall.

Form, massing and scale

P10 Five (5) storeys, with the topmost storeys setback from the levels below.

P11 Single residential building to McDougall Street and Public Open Space area off High Street.

Roofs

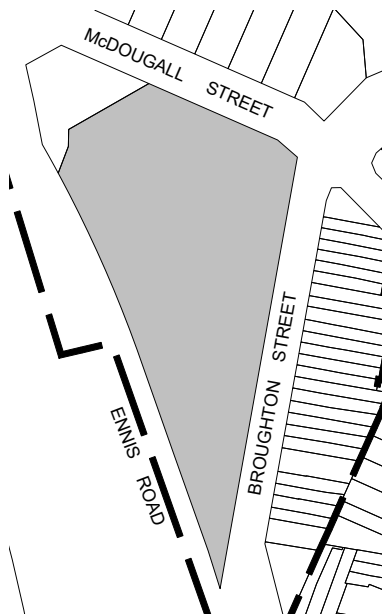
P12 Flat roof to allow views from the Bradfield Highway to Sydney Harbour.

Gardens

P13 Substantial plantings including trees in front garden complement street trees.

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8.2.5 3-11 McDougall Street (Greenway Flats)



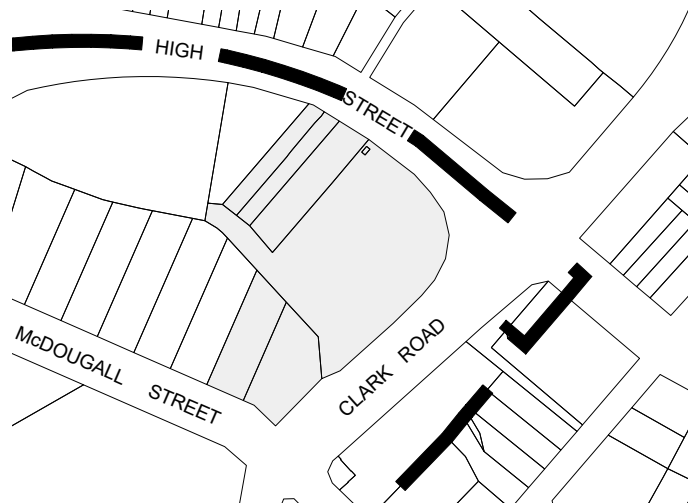
Diversity

- P1 High density residential accommodation in the form of residential flat buildings.
- P2 Future redevelopment of the site is not to result in a significant change in scale or bulk.

Form, massing and scale

- P3 Must not have a site coverage exceeding 25%.
- P4 Must not have an un-built upon area exceeding 15%.
- P5 Must have a minimum landscaped area of 60%.

8.2.6 James Milson Village



Diversity

- P1 High density residential accommodation in the form of seniors housing.
- P2 Future redevelopment of the site is not to result in a significant change in scale or bulk.



Form, massing and scale

- P3 Must not have a site coverage exceeding 60%.
- P4 Must not have an un-built upon area exceeding 15%.
- P5 Despite any other provision of this plan, if the land is used for seniors housing, it must have a minimum landscaped area of 25%.

8.3 CAREENING COVE CONSERVATION AREA



8.3.1 History

The Careening Cove Conservation Area includes a section of the Robert Ryan grant (later acquired by Robert Campbell), part of the James Milson grant, and the southern side of the Crown subdivision that was intended to be used for whaling purposes from 1830. This section of the Ryan Grant was subdivided in the 1860s but development did not gain momentum until the 1880s.

Milson's land remained within the family until the 1880s, after which sections were subdivided for residential development. Milson Park was created during the 1890s by reclamation of the head of Careening Cove. The peninsula, along which High Street runs, was sold by the Government to private owners for the development of whaling industries. No whaling occurred but a number of houses were built in the 1860s and 1870s overlooking the Cove.

The south side of the tip of the peninsula was a significant wharf area, notable for its use by the Wunderlich Company for the importation of Marseilles roofing tiles in the 1890s. This foreshore was later used for ship and boat building and repair, and subsequently subdivided for housing in the early 1920s (particularly to the south side of Stannards Place).

8.3.2 Description

The Careening Cove Conservation Area is located around the bay forming Careening Cove and bounded by High Street, Broughton Street and Crescent Place.

The landform slopes to the Bay with a flat area of reclaimed land close to the Cove. The subdivision pattern varies with a grid of rectangular lots that are cut to suit the diagonal street pattern and topography.

The Conservation Area is characterised by two storey, late Victorian terraces, single and two storey Federation and Inter-War dwelling houses and residential flat buildings. The area retains groups of picturesque industrial buildings with associated jetties, slipways, moorings and equipment. The Ensemble Theatre was one of the first recycled industrial buildings in Sydney.



8.3.3 Statement of Significance

The Careening Cove Conservation Area is significant:

- (a) as a largely consistent early 20th century residential area with an unusual and irregular pattern of street layout and irregular subdivision pattern that give the area a particular character.
- (b) as retaining much of the urban detail and fabric seen in gardens, fencing, street formations, use of sandstone for retaining and building bases, sandstone kerbing and natural rock faces.
- (c) for the amphitheatre like form around the reclaimed Milson Park and the head of the bay.
- (d) for the remaining waterfront industrial and recycled industrial development that gives the area much of its character.

8.3.4 Significant elements

Topography

P1 Amphitheatre form falling to Careening Cove.

Section 8 - Kirribilli Planning Area

Subdivision

P2 Irregular lots following the street pattern and topography.

Streetscape

P3 Changes in street level. Sandstone retaining walls. Milson Park. Double rail timber fences.

P4 Street trees including Jacarandahs in McDougall Street.

Views

P5 Harbour views from Miss Gladys Carey Reserve lookout (57), High Street Lookout (58), Milson Park; views of the valley from Winslow Street (59). Views of Milson Park and Careening Cove from Hipwood and McDougall Streets and Bradly Avenue; View of Sydney Harbour from Stannards Place, Willoughby Road and Winslow Lane.

P6 Slot views between and over buildings.

8.3.5 Characteristic buildings

P1 A mixture of:

- (a) Two storey late Victorian terraces.
- (b) Single and two storey, detached Federation and Edwardian houses.
- (c) Inter-War residential flat buildings.

8.3.6 Characteristic built elements

Siting

P1 To the middle of the lot with gardens to the front and rear.

P2 Front setback of 3 – 5m.

P3 Side setback of 0 – 1m.

P4 Rear setback of 8m.

Form, massing and scale

P5 Two storey attached dwellings.

P6 Single and two storey detached dwelling houses.

P7 Reduced scale to the rear.

P8 Low, multi-storey Inter-War residential flat buildings.

Roofs

P9 Pitched, hipped and gables roofs pitched between 30 and 45 degrees with skillion roofs to rear.

P10 Parapets to flat roofed residential flat buildings.

P11 Brick and rendered chimneys.

Materials

P12 Walls: Face and rendered brick on sandstone foundations.

P13 Roofs: Slate, terracotta tile and corrugated metal sheeting at rear; flat roofs to residential flat buildings.

P14 Timber verandahs and Federation and Arts and Crafts detailing.

Windows and doors

P15 Consistent with building period and style. Timber.



Fences

P16 Low to 900mm in height.

P17 Sandstone walls; timber pickets.

Car accommodation

P18 Located off rear lanes.

8.3.7 Uncharacteristic elements

P1 Over-scaled additions; dormers and skylights to front roof slopes; roof terraces carports and garages to the street; high solid fences to the street; rendering and painting of face brick; paved front gardens, loss of original detail; modern infill development and residential flat buildings.



In 1800 the Kirribilli Peninsula was granted to Robert Ryan but was acquired shortly after by Robert Campbell, merchant. The land was partly leased to James Milson for many years.

From 1842, the Campbells began to lease and sell the land, with one of the first sales going to Col. Gibbes who erected a substantial house "Wotonga" (now part of Admiralty House) on the tip of the peninsula.

Over the next three decades, most of the waterfront was occupied by large houses with substantial properties fronting the foreshore. In the early part of the 20th century the large land holdings were subdivided and culminated in the boom of residential flat buildings close to the ferry wharves. The replacement of earlier dwelling houses with residential flat buildings, however, has created some uncharacteristic elements within the Kirribilli Conservation Area.

The Kirribilli Conservation Area is located on the lower section of the Kirribilli peninsula and has a long waterfront to Sydney Harbour.

The landform slopes to the Harbour with an irregular pattern of streets that follow the contours and slopes. The subdivision pattern varies with irregular lots that follow the topography.

The Conservation Area is characterised by a mix of small, medium and substantial late Victorian, Federation era houses with Inter-War dwelling houses and residential flat buildings with modern residential flat buildings on some of the waterfront sites. Buildings are integrated with the slope and generally have a homogenous scale with established gardens. Some earlier Victorian villas survive in Kirribilli Street and Upper Pitt Street. Also located on the waterfront are Kirribilli and Admiralty Houses.

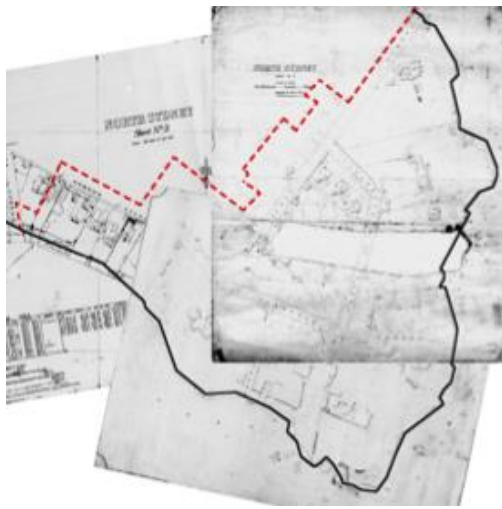


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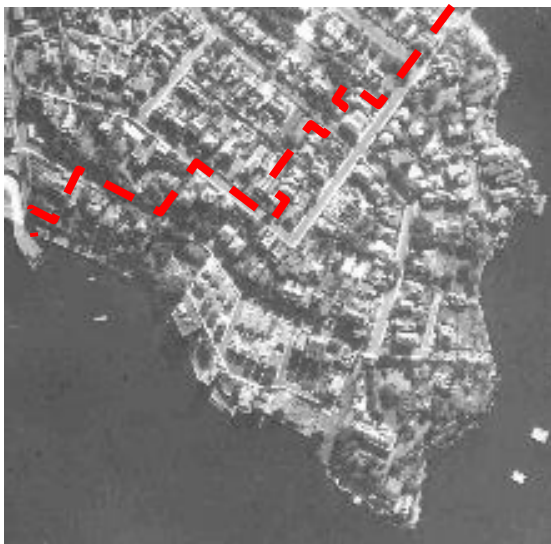
Circa 1890

Figure F-8.6 (below left):

Circa 1943

Figure F-8.7 (below right):

Circa 2008



8.4.3 Statement of Significance

The Kirribilli Conservation Area is significant:

- (a) as a consistent early 20th century residential area with a mix of Federation and one or two storey Inter War dwelling houses and two or three storey residential flat buildings on large allotments with a strong orientation to the water.
- (b) as a largely intact early 20th century suburb retaining much of the urban detail and fabric seen in gardens, fencing, street formations, use of sandstone and later reinforced concrete "naturale" fencing, sandstone kerbing, natural rock faces, wide streets and compatible plantings.
- (c) for its unity derived from its subdivision history which is still clearly seen in the development of the area.
- (d) as containing the important government buildings Kirribilli House and Admiralty House.

Section 8 - Kirribilli Planning Area

8.4.4 Significant elements

Topography

P1 Sloping site falling to Sydney Harbour.

Subdivision

P2 Irregular lots following the street pattern and topography.

Streetscape

P3 Changes in level. Sandstone kerbing.

P4 Street trees.

Views

P5 Views of Sydney Harbour Bridge, City of Sydney, Opera House from Beulah Street.

P6 Lookout (63), Dr Mary Booth Lookout (64); views of Sydney Harbour from the Lady.

P7 Gowrie Lookout (62), Colinda Reserve Lookout (60). Views of Sydney Harbour from Holbrook Avenue, Peel Street, Kirribilli Avenue.

8.4.5 Characteristic buildings

P1 A mixture of:

(a) Victorian Villas.

(b) Single and two storey detached late Victorian, Federation and Edwardian houses.

(c) Federation and Inter-war dwelling houses and residential flat buildings.

8.4.6 Characteristic built elements

Siting

P1 Forward on lot with larger rear garden. Foreshore properties address the street.

P2 Front setback 3m.

P3 Side setback 1.5 – 2m.

P4 Rear setback 10 – 15m.

Form, massing and scale

P5 Detached, single storey villas.

P6 Single and two storey detached dwellings.

P7 Reduced scale to the rear.

P8 Three storey multi storey residential flat buildings as viewed from the street.

Roofs

P9 Pitched, hipped and gables roofs pitched between 30 and 45 degrees with skillion roofs to rear.

P10 Brick and rendered chimneys.

P11 Parapets to flat roofs on Inter-War residential flat buildings.

Materials

P12 **Walls:** Face and rendered brick on sandstone foundations; dark brick to Inter-war residential flat buildings.

P13 **Roofs:** Slate; terracotta tile and corrugated metal; flat roofs to residential flat buildings.



P14 Federation and Arts and Crafts detailing.

Windows and doors

P15 Consistent with building period and style. Timber.

Fences

P16 Low scale to 800mm.

P17 Face brick; timber pickets; horizontal railings.

Car accommodation

P18 Parking at rear off laneways or set back behind the main building line.

8.4.7 Uncharacteristic elements

P1 Over-scaled additions; dormers and skylights to front roof slopes; roof terraces; carports and garages in front setbacks; high solid fences to the street; rendering and painting of face brick; paving of front gardens; extensive glazing; glazed balustrades; loss of original detail; modern infill development and residential flat buildings.



8.5 JEFFREYS STREET CONSERVATION AREA



8.5.1 History

The Jeffreys Street Conservation Area is an interesting remnant of the early development of Milson’s Point that was developed from the 1860’s onwards and was largely established by the late 1890’s. The area was a grid pattern of streets stretching from Albert Street to Jeffreys Street that was lined with single, two and three storey houses and that was associated with the land occupied by James Milson.

The construction of the Sydney Harbour Bridge resulted in the demolition of the western portion of the area but there is sufficient built form, street pattern and historical information to demonstrate the relationship of the current built form to its earlier state.

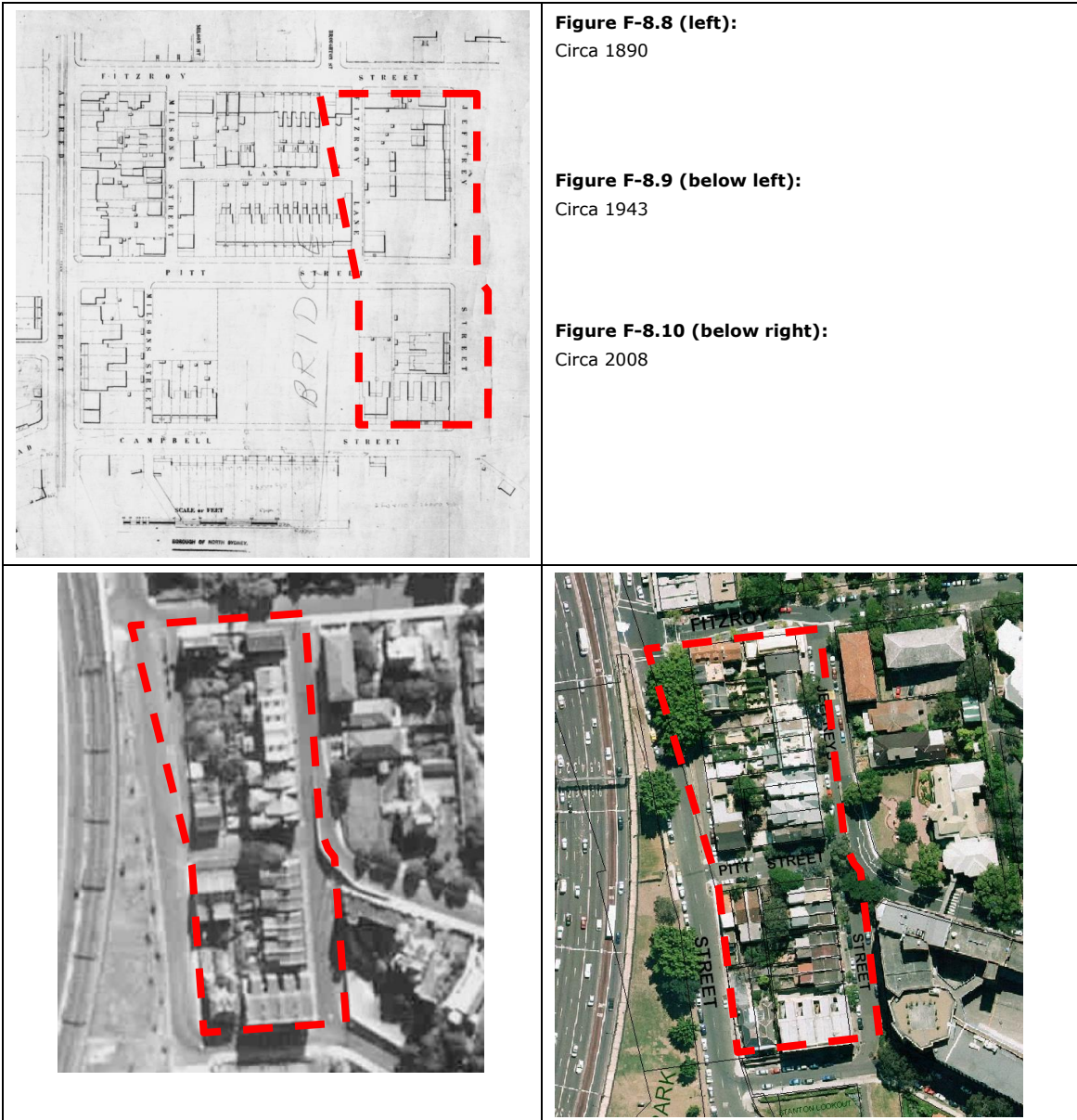
The area has association with James Milson and the Milson family, a prominent local merchant and an important figure in the development of North Sydney. It also has association with the Robert Campbell, an important local merchant and developer Robert Ryan who was granted 120 acres including the subject area in 1800.

8.5.2 Description

The Jeffreys Street Conservation Area is bounded by Fitzroy Street, Jeffreys Street, Kirribilli Avenue and Broughton Street. The landform slopes down from the north towards the south and west and there is a sandstone retaining wall at Jeffreys Street and Kirribilli Avenue. The subdivision pattern is a regular grid that overlays the topography with streets running down or across the slope.

The characteristic buildings of the area are typically mid to late Victorian and Federation residential terraces, reflecting the predominant period of development. Buildings are typically two or three storey, terraces with pitched and gabled tiled roof with two storey verandahs and skillion rear wings. Street plantings and raised verges reinforce the cohesive

character and regular pattern formed by the buildings and underlying lot pattern. There is some modern development to the northern and southern edges.



8.5.3 Statement of Significance

The Jeffrey Street Conservation Area is significant:

- (a) for the unity of its subdivision history which is evident in the built form of the area and that overlays its sloping and stepped topography.
- (b) as an area of mid to late Victorian and Federation speculative terraces that are a remnant of the early development of the area prior to the clearances for the construction of the Sydney Harbour Bridge.

8.5.4 Significant elements

Topography

P1 Sloping towards west and south.

Section 8 - Kirribilli Planning Area

Subdivision

P2 Regular grid pattern overlying topography.

Streetscape

P3 Sandstone retaining walls.

P4 Houses at street level or raised above it.

P5 Consistent scale of housing faces the street in urban setting.

P6 Narrow verges, sandstone and concrete kerbing, brick walls, no street gardens.

P7 Street trees.

Views

P8 Sweeping view of Sydney Harbour from Kirribilli Avenue to Broughton Street.

P9 Views from Jeffreys Street to the south and Pitt Street to the west.

8.5.5 Characteristic buildings

P1 Two and three storey mid to late Victorian and Federation attached dwellings.

8.5.6 Characteristic built elements**Siting**

P1 Dwellings to the front of the lots or at the street with gardens/yard to the rear.

Form, massing and scale

P2 Two and three storey attached dwellings.

P3 Reduced bulk and scale to the rear.

P4 Simple forms articulated with verandahs to front and skillion wings to the rear.

Roofs

P5 Gabled roofs pitched between 20 and 35 degrees.

P6 Gabled ends for side elevations.

P7 Skillion roofs to rear wings.

P8 Brick and rendered chimneys.

Materials

P9 **Walls:** Painted and face brick; rendered masonry on some sandstone bases.

P10 **Roofs:** Slate, terra cotta and concrete roof tiles; corrugated metal on rear wings.

Windows and doors

P11 Mid to late Victorian and Federation.

P12 French doors to first floor verandahs.

Fences

P13 Low height front fences and walls.

P14 Masonry fences to rear and side.

P15 Face and rendered brick; metal palisade.

Car accommodation

P16 Garages and carports to side and rear.

P17 No garages or carports in front of the building line.



8.5.7 Uncharacteristic Elements

- P1 Over-scaled dormers; roof decks and terraces; 'Juliette balconies'; enclosed balconies and verandahs; extensive glazing and glazed balustrades; flat roofs to service wings; painted face brick; removal of garden setting.



SECTION 9 LAVENDER BAY PLANNING AREA

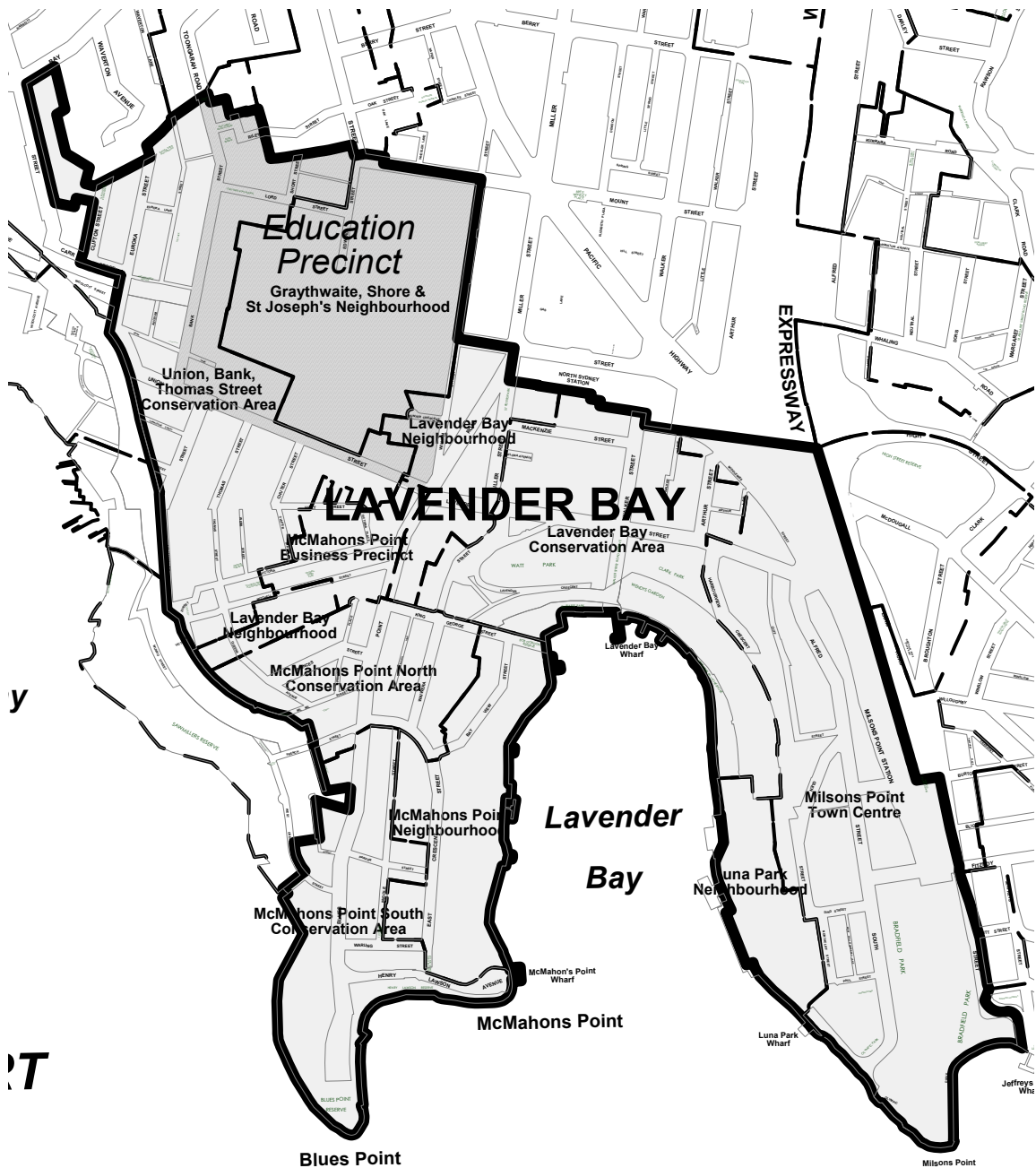


Figure F-9.1: Lavender Bay Planning Area and associated Locality Areas

9.0 LAVENDER BAY PLANNING AREA CHARACTER STATEMENT



The following statement identifies the existing character and the desired future outcome for development in the Lavender Bay Planning Area.

The Lavender Bay Planning Area is a diverse area reflected by the very wide range of land uses occurring within the Area, including a mixture low, medium and high density residential accommodation, commercial premises, light industry, education establishments, places of worship and public recreational facilities. Many of these land uses are located in a leafy setting with strong links to Sydney Harbour and are often associated with landmark buildings such as Graythwaite, the Shore School and St Peter's Church.

The Planning Area is noted for its historical character arising from the retention of much of its original subdivision pattern and good examples of largely intact mid 19th century and early 20th Century buildings. Blues Point Road in McMahon's Point is a popular village centre enjoyed by local residents and visitors to the area with its outdoor cafes, galleries and small specialty shops.

A large portion of the Education Precinct is located in the Planning Area, which consists of landmark educational establishments such as the Australian Catholic University (ACU), the Sydney Church of England Grammar School (Shore) and North Sydney Demonstration School and supports a growing student population and workforce of teaching and support staff.

Milsons Point is on the shores of Sydney Harbour and consists of a large concentration of mixed residential and commercial towers located at the base of the Sydney Harbour Bridge, surrounded by landmarks such as Luna Park, Bradfield Park and North Sydney Pool.

Development within the Planning Area should result in:

- any residential growth being in accordance with the North Sydney Local Housing Strategy, with high density residential accommodation mainly being accommodated within the mixed use zone at Milsons Point, with no substantial change in the other residential and productivity support areas.
- a wide range of single household residential types being distributed in a number of distinctive built forms/landscape areas, including purpose-built student accommodation to support the functioning of the Education Precinct.

Section 9 - Lavender Bay Planning Area

- any retail premises being of a scale to cater to the local community and which provide a balance between basic (e.g. food and groceries) and recreational (e.g. cafes and galleries) needs.
- the conservation of features which positively contribute to the local identity.
- a vibrant and engaging Education Precinct with high quality built form, safe pedestrian networks and a range of formal and informal public spaces for students to study, socialise and engage with the local community.

and where:

Function

- the community centre is in an accessible location and provides a meeting place for the local community and is part of North Sydney's network of community and cultural centres.
- non-residential uses such as public recreation, schools, light industrial, places of public worship, railway and shops coexist peacefully with the residential character.
- public open space areas are utilised by local residents and the wider regional population for a variety of social and recreational activities.
- access to the Harbour foreshores is improved with pedestrian links from Luna Park to the public reserve in Munro Street, Waverton Park and Smoothey Park.
- public transport, cycling, and walking are the preferred means of transport.
- through traffic is discouraged from using the already overloaded road network and commuter parking managed through parking schemes.
- local identity, icons and heritage are conserved.
- Luna Park is a public recreational and amusement park, enjoyed by local, regional and international users, that is easily accessed by public transport.
- the boardwalk adjacent to Luna Park on the foreshore is always accessible to the general public maintaining links to the wharf, other landing facilities and adjoining foreshore land.
- local identity, icons and heritage are preserved.
- man-made features such as the railway and pedestrian cuttings through the approach to Sydney Harbour Bridge are maintained as local landmarks that are important to the community.

Environmental Criteria

- the remaining natural foreshores are conserved with improved pedestrian access to the foreshore.
- public open space is protected from the adverse effects of development – such as stormwater runoff, spread of introduced plants and weeds, and visual impact of structures.
- both residential and non-residential land uses minimise noise and air pollution.
- solar access is maximised to open space areas in public and private domains.
- natural features (rock formations, trees) are conserved and maintained.
- major views from Luna Park, lookouts, and other vantage points are not obscured by structures or landscaping.

Quality Built Form

- any development that occurs reflects and reinforces the existing distinctive built form/landscape areas and distribution of accommodation types.



- buildings in Milsons Point are designed to preserve views and prevent wind tunnels.
- there is adequate open space within Luna Park for passive recreation and free movement within the park.
- the character, bulk and scale of new development within Luna Park complement the original features of Luna Park and enhance its appearance when viewed from within and Sydney Harbour.
- development within Luna Park is complementary to redevelopment of adjacent SRA land.
- there is an appropriate built form on the foreshore to maintain the significance of Sydney Harbour.

Quality Urban Environment

- traffic flows are managed to promote pedestrian amenity and there are improved pedestrian links between Kirribilli and Milsons Point.
- through traffic is discouraged to prevent traffic congestion.
- development promotes the safety of people that is enhanced by good street lighting.
- public open space provides recreational opportunities and acts as a buffer to the high density development.
- the demand for on-street parking is managed by allowing only appropriate levels of development.
- backyards are used for a variety of activities particularly for families with children and for practical and recreation needs of residents in residential flat buildings.

Efficient Use of Resources

- stormwater is retained for reuse on site.
- existing buildings are maintained and adaptively reused to prevent unnecessary waste of building materials.

Public Domain

- buildings and street furniture are compatible with unique features of Milsons Point including Luna Park, the Olympic Pool and Sydney Harbour.
- street furniture and landscaping, outdoor advertising in McMahon's Point and Kirribilli respect the historical character of the area.
- educational establishments are oriented to the public domain to provide increased surveillance and activation.
- streetscape improvements within the Education Precinct occur in accordance with the *North Sydney Centre Public Domain Strategy* and *Education Precinct Public Domain Masterplan*.

The Lavender Bay Planning Area includes the following Locality Areas in which development is subject to the relevant provisions below:

Section 9.1: Milsons Point Town Centre

Section 9.1.4: 52 Alfred Street

Section 9.2: McMahon's Point Business Precinct

Section 9.3: Luna Park Neighbourhood

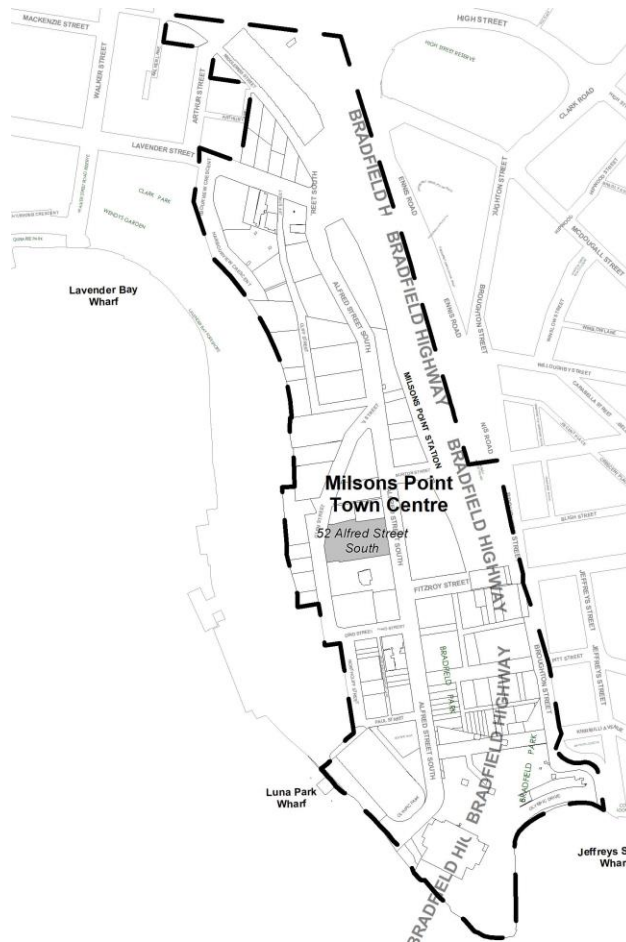
Section 9.4: Graythwaite, Shore & St Joseph's Neighbourhood



Section 9 - Lavender Bay Planning Area

- Section 9.5: Lavender Bay Neighbourhood
- Section 9.6: McMahons Point Neighbourhood
- Section 9.7: McMahons Point North Conservation Area
 - Section 9.7.8: Blues Point Village
- Section 9.8: McMahons Point South Conservation Area
 - Section 9.8.8: 1 Henry Lawson Avenue
- Section 9.9: Lavender Bay Conservation Area
- Section 9.10: Union, Bank, Thomas Street Conservation Area

9.1 MILSONS POINT TOWN CENTRE



9.1.1 Significant Elements

Land Use

- P1 Predominantly mixed commercial and residential development.
- P2 Passive and active recreational spaces.
- P3 Major road and railway infrastructure.

Topography

- P4 Generally falls to the south and south west from the Sydney Harbour Bridge down to Sydney Harbour.
- P5 Steep falls and cliffs adjacent to and along the western boundary of the Area.

Natural Features

- P6 Interface with Sydney Harbour.

Views

- P7 Major views from Luna Park, lookouts, and other vantage points are not obscured by structures or landscaping.

Section 9 - Lavender Bay Planning Area

- P8 The following views and vistas are to be preserved and where possible enhanced:
- (a) District views from streets and reserves to Sydney Harbour and beyond.
 - (b) Views to Lavender Bay from Luna Park Lookout (71), Harbour View Crescent.
 - (c) Views to Sydney Harbour from Bradfield Park No. 1 Lookout (69), Olympic Park Lookout (70); Alfred Street, Paul Street.

Identity / Icons

- P9 Luna Park.
- P10 North Sydney Pool.
- P11 Sydney Harbour Bridge and approaches.
- P12 Bradfield Park.
- P13 Sydney Harbour.

Subdivision

- P14 Large consolidated allotments reflective of the Area's previous commercial nature.
- P15 Heritage items have small rectilinear lots, reflecting original subdivision patterns.

Streetscape

- P16 Narrow footpaths and streets, except to Alfred Street which has wide fully paved verges incorporating street trees.
- P17 Buildings built to street and laneway frontages.

Public transport

- P18 Development should capitalise on the Area's high level of accessibility to high frequency public train, bus and ferry services.

9.1.2 Desired Future Character

Diversity

- P1 Medium to high-rise mixed residential and commercial development, built boundary to boundary, with setbacks at laneways, above podium and to public spaces.
- P2 Variety of different sized non-residential spaces and land uses which serve the local needs of residents (including convenience stores, cafes, medical centres etc).
- P3 Development for residential accommodation should be in accordance with Council's Local Housing Strategy, with limited growth envisaged for the area.
- P4 Ground floors of mixed use development to operate land uses that promote pedestrian activity.
- P5 Existing heritage items shall be protected and retained where practical.
- P6 Provide a balance between the working and resident populations of the town centre, to ensure an active environment throughout the day.
- P7 Where existing commercial buildings are to be refurbished, retention of the existing level of commercial floor space is encouraged.

Through-site pedestrian links

- P8 Pedestrian access is provided from Alfred Street to Glen Street:
- (a) between 68 and 72 Alfred Street,
 - (b) along the southern boundary of 52 Alfred Street.

Traffic Management

- P9 Existing one-way vehicular movements are maintained along Cliff Street.



9.1.3 Desired Built Form

Subdivision

- P1 Minimum frontage of 30m.

Form, massing and scale

- P2 Buildings step down from maximum LEP heights on the ridge of the peninsula (along Alfred Street from Lavender Street to Dind Street) to 10m on the shores of Lavender Bay.

Setbacks

- P3 Buildings are built to the street boundary at ground level, except as specified below:
- (a) Setback of 1.5m from a laneway at ground level.
- P4 Preserve and create spaces between buildings above podium height that will offer views of the Harbour and its foreshore areas to help break up the wall of development along Alfred Street.

Podiums

- P5 Podium of 13m (four storeys) to all buildings fronting Alfred Street, with a setback of 3m of all parts of the building located above the podium.
- P6 Podium of 10m (three storeys) to all buildings fronting Cliff or Glen Streets, Harbourview Crescent or railway land with a setback of 3m of all parts of the building located above the podium.
- P7 Podium of 10m (three storeys) to all buildings fronting any laneway with a setback of 3m of all parts of the building located above the podium.

Building design

- P8 Materials used are painted render, masonry, concrete with natural colours.
- P9 Buildings address every street frontage with no blank walls to streets, lanes, public space or railway land.

Skyline

- P10 Ancillary equipment, plant rooms are not visible from Sydney Harbour.
- P11 Existing buildings are recycled to accommodate mixed use developments.

Street furniture, landscaping works, public art

- P12 Street furniture, landscaping and/or public art adjacent to Luna Park and the Olympic Pool are compatible with their role as centres of entertainment and recreation.
- P13 Man-made features such as the railway and pedestrian cuttings through the approach to Sydney Harbour Bridge are maintained as local landmarks that are important to the community.

Noise

- P14 Elevations of buildings facing the Sydney Harbour Bridge and its approaches are to be designed and incorporate features to minimise traffic and railway noise transmission (e.g. using design features such as cavity brick walls, double glazing, minimal glazing, solid core doors and concrete floors etc).

Wind Speed

- P15 Buildings are designed not to create wind tunnel effects along Alfred, Glen, Paul, Dind, Northcliff Streets and Harbourview Crescent.

Reflected Light

- P16 Reflected light for motorists on the Sydney Harbour Bridge and the Bradfield Highway is no greater than 20% nor is there excessive artificial light from outdoor advertising.

Section 9 - Lavender Bay Planning Area

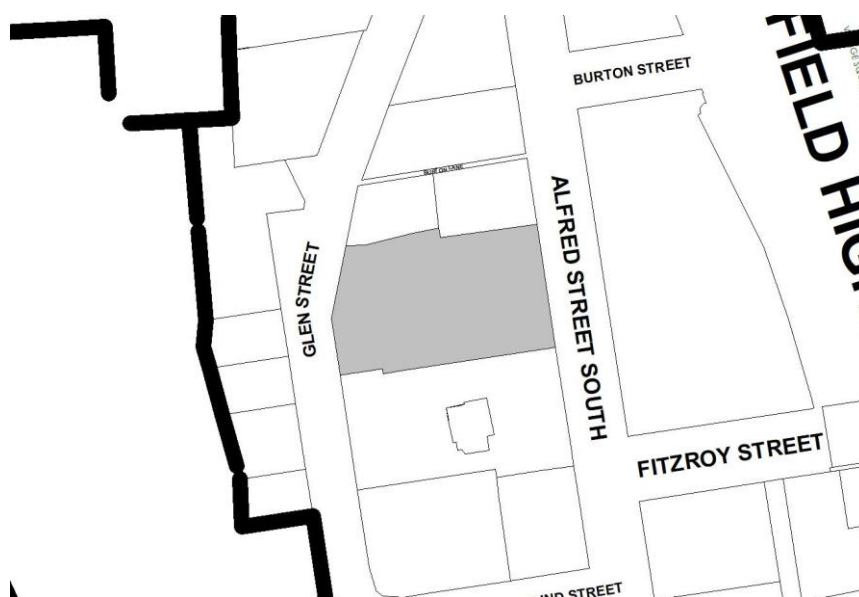
Solar access

- P17 There is no increase in overshadowing of Bradfield Park, Luna Park, and North Sydney Pool between 12 noon and 3pm.

Views

- P18 Spaces between buildings preserve views to Sydney Harbour, Sydney Opera House and Lavender Bay.
- P19 Buildings step down in height from ridge of the peninsula to a lower height on the foreshore to maintain views.
- P20 Preserve existing views of Lavender Bay and Sydney Harbour along the railway track from the north side of the Sydney Harbour Bridge to North Sydney Station.

9.1.4 52 Alfred Street, Milsons Point



9.1.4.1 Desired Future Character, Design Objectives and Key Principles

- P1 Development is to respond to the scale and character of existing development and desired future character of the surrounding area.
- P2 Built form, scale and massing is to reflect the dual frontage character of the block and residential building typologies.
- P3 Deliver a mixed-use development with active frontages to Alfred Street and Glen Street.
- P4 Development is to ensure that view loss, overshadowing and other amenity impacts on neighbouring residential buildings and impacts on heritage and the public domain are minimised.
- P5 Development is to maximise solar access to Bradfield Park.
- P6 An improved pedestrian through-site link between Alfred Street and Glen Street with active frontages.

9.1.4.2 Desired Built Form

Objectives

- O1 To provide for increased opportunity for height and density in the Milsons Point Town Centre, in close proximity to public transport and services.



- O2 To ensure that solar access to Bradfield Park is maximised.
- O3 To ensure appropriate separation distances between existing and proposed buildings and ensure reasonable privacy, solar access and views are maintained to surrounding dwellings.
- O4 To positively relate to the heritage context surrounding the site.
- O5 To maximise view sharing across the site, particularly view protection to the south from 37 Glen Street.

Provisions

Solar access

- P1 Any development at 52 Alfred Street must not result in a net increase in overshadowing to Bradfield Park between 12 noon and 3pm.

Street and Side Setbacks

- P2 Buildings must be setback:
- (a) 0m to Alfred Street and Glen Street, and
 - (b) A minimum 6m to the southern boundary, and
 - (c) A minimum 3m setback to the northern boundary where the site directly adjoins 37 Glen Street.

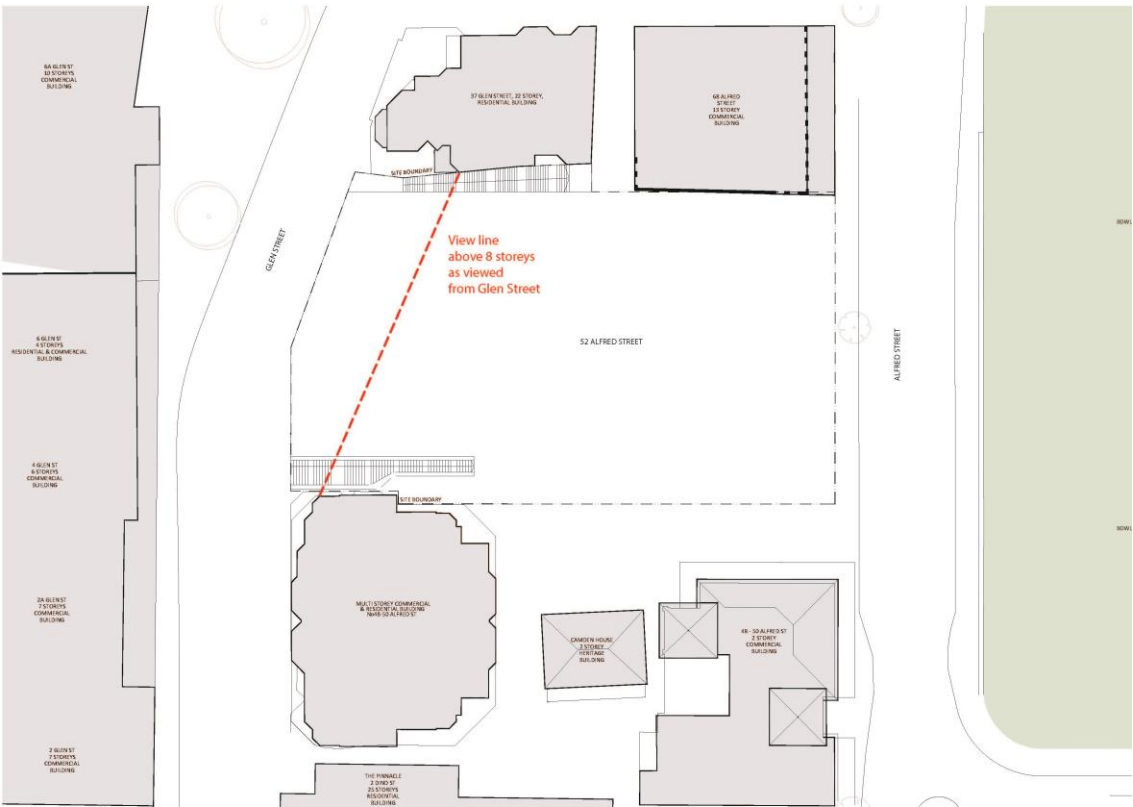
Podium Height

- P3 Despite any other provision of this DCP, a podium must:
- (a) maintain a consistent overall height across the entirety of the site; and
 - (b) be 2 storeys in height along its southern boundary and positively relate to the height of the heritage listed Camden House at 56 Alfred Street to its south; and
 - (c) be 2 storeys in height fronting Alfred Street; and
 - (d) not exceed 4 storeys fronting Glen Street.

Above Podium Setbacks

- P4 The following minimum setbacks must be provided above the podium:
- (a) 3m to the site's Alfred Street frontage, and
 - (b) 3m to the site's Glen Street frontage, and any part of a building located above RL 44 (approximately 8 storeys) as viewed from Glen Street, must not be constructed westwards of a view line established from the eastern edge of living area windows to 37 Glen Street (located approximately 12.8m east of the Glen Street boundary projecting southwards across 52 Alfred Street site to the north-western corner of the residential flat building known as "The Milson" fronting Glen Street at 48-50 Alfred Street (approximately 3m east from the Glen Street boundary); and
 - (c) 9m to the site's southern boundary up to 8 storeys in height and 12m for any storeys located above; and
 - (d) 9m to the site's northern boundary up to 8 storeys in height and 12m for any storeys located above, for that part of the site located directly adjacent to 37 Glen Street.
- P5 Despite provision P4 (c) and (d), increased setbacks may be required to achieve adequate building separation in accordance with SEPP 65, protect views from adjacent residential buildings (37 Glen Street) and help break up the wall of development along Alfred Street.

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Through-site pedestrian link

- P6 The following through-site link is to be provided, retained or enhanced:
- (a) An east-west link from Alfred Street to Glen Street adjacent to the southern boundary of 52 Alfred Street. This link must be a minimum of 6m wide.

Parking & Access

- P7 The development must ensure existing levels of vehicular access, servicing and parking provision required for the effective operation of the adjoining development at 48-50 and 56 Alfred Street South are maintained.

9.2 McMAHONS POINT BUSINESS PRECINCT



9.2.1 Significant Elements

Land Use

- P1 Diverse range of residential accommodation, home occupation, light industrial, commercial and retail activities.

Topography

- P2 Generally falls to the south.

Views

- P3 The following views and vistas are to be preserved and where possible enhanced:
(a) Views to Balls Head Bay along Mitchell and Victoria Streets.

Identity / Icons

- P4 Commodore Hotel.
P5 McMahon's Point Community Centre.

Subdivision

- P6 A mixture of large consolidated lots in single ownership and small lots in individual ownership.
P7 Irregular subdivision pattern.

Streetscape

- P8 Street tree plantings on Blues Point Road and Lavender Street complement mature plantings and landscaping at the front of residential premises.
P9 Narrow streets and lanes with narrow footpaths predominate off Blues Point Road
P10 A mixture of tall and low brick and sandstone fences.
P11 Buildings built to the boundary, interrupted by garage / vehicular access openings.

Public transport

- P12 Development is to take advantage of the high levels of accessibility to public train and bus services.

9.2.2 Desired Future Character

Diversity

- P1 Mix of modern 2-3 storey industrial and commercial buildings, 2-3 storey Victorian terraces that cater for residential and light industrial purposes, modern medium

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density attached dwellings and some 3-4 storey inter-war and post-war residential flat buildings according to zone.

P2 Mitchell Street Park provides recreation/green space for workers and residents.

P3 McMahon's Point community centre is in an accessible location and provides a meeting place for the local community and is part of North Sydney's network of community and cultural centres.

P4

Traffic Management

P5 Existing one-way vehicular movements are maintained along Victoria and Mitchell Streets.

9.2.3 Desired Built Form

Form, massing and scale

P1 Attached dwellings have symmetrical facades.

P2 Site coverage of buildings on Mitchell and Victoria Streets is more than 80% of the allotment.

Siting

P3 Buildings on Victoria and Mitchell Streets are predominantly located towards the front boundary of the allotment.

Setbacks

P4 **Front:** Between 0m - 2m on Victoria and Mitchell Streets.

P5 **Side:** Between 0m - 2m on Victoria and Mitchell Streets.

Materials, colours

P6 Materials include face brick, sandstone, masonry, timber and/or painted render.

P7 Materials, architectural detail and colours of modern residential development are sympathetic to heritage items and character of Victoria and Mitchell Streets.

Car accommodation

P8 Locate all car spaces, garages or carports at the rear of the property if rear lane access is available.

P9 Short term metered on-street parking for commuters and visitors.

9.3 LUNA PARK NEIGHBOURHOOD



9.3.1 Significant Elements

Land Use

- P1 Entertainment and recreational activities.
- P2 Luna Park is a public recreational and amusement park, enjoyed by local, regional and international users, that is easily accessed by public transport.

Topography

- P3 Generally flat with steep cliff along the eastern boundary of the Area.

Natural Features

- P4 Foreshores of Sydney Harbour, Cliffs on the eastern boundary of Luna Park with fig and coral trees above.

Views

- P5 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Views to Sydney CBD, Sydney Opera House, Sydney Harbour Bridge, Sydney Harbour and Lavender Bay.

Identity / Icons

- P6 Coney Island.
- P7 Crystal Palace.

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- P8 Luna Park Face and Towers at front entry.
- P9 Wild Mouse.
- P10 Luna Park Gateway.
- P11 Fig and coral trees on cliff top.
- P12 Mirror Maze.
- P13 Palais de Danse.
- P14 Sea Walls.
- P15 Cliff Tunnel and Chamber.
- P16 Sydney Harbour.

Subdivision

- P17 Large consolidated allotments.

Streetscape

- P18 Open paved areas to the Face of Luna Park.
- P19 Strong physical and visual connection with Sydney Harbour.

Public transport

- P20 Development should capitalise on the Area's high level of accessibility to high frequency public train, bus and ferry services.
- P21 The primary mode of travel to the Area by public train, bus and ferry services.

9.3.2 Desired Future Character

Diversity

- P1 Amusements, recreational facilities, shops, take-away food shops, multi storey mixed use buildings, temporary structures and kiosks, street furniture, weather shelters and boardwalks.

9.3.3 Desired Built Form

Access

- P1 Olympic Drive is main entry to Luna Park.
- P2 "Midway" pedestrian street provides access to recreational facilities.
- P3 The boardwalk adjacent to Luna Park provides 24 hour public access to the foreshores of Lavender Bay maintaining links to the wharf, other landing facilities and adjoining foreshore land.
- P4 There is adequate open space within Luna Park for passive recreation and free movement within the park.
- P5 The character, bulk and scale of new development within Luna Park complement the original features of Luna Park and enhance its appearance when viewed from within and Sydney Harbour.
- P6 Development within Luna Park is complementary to redevelopment of adjacent SRA land.
- P7 There is an appropriate built form on the foreshore to maintain the significance of Sydney Harbour.

Siting

- P8 A service area (6m width) is provides at the base of the cliff.
- P9 Buildings are independent of the cliff face.



Form, massing and scale

- P10 Low scale development to Northcliffe Street to preserve views to Sydney Harbour.
- P11 Low buildings (approximately 2-3 storey) are provided on the foreshores, except for structures for amusements, turrets, flagpoles.
- P12 Buildings including plant facilities are no higher than the cliff face.
- P13 The footprint of buildings are no greater than the footprint of Coney Island.
- P14 The bulk and scale of buildings complement the heritage items and its role as an amusement park.
- P15 Views of Luna Park to and from Sydney Harbour and vistas within Luna Park are maintained.
- P16 At least 50% of the site is public area.
- P17 New amusement rides are located within building envelope up to 100% if rides are "open" structures.

Fences

- P18 Fences are transparent (up to 90%) in materials sympathetic to the character of Luna Park.

Materials, colours

- P19 Materials, architectural details and colours of new structures complement heritage items.

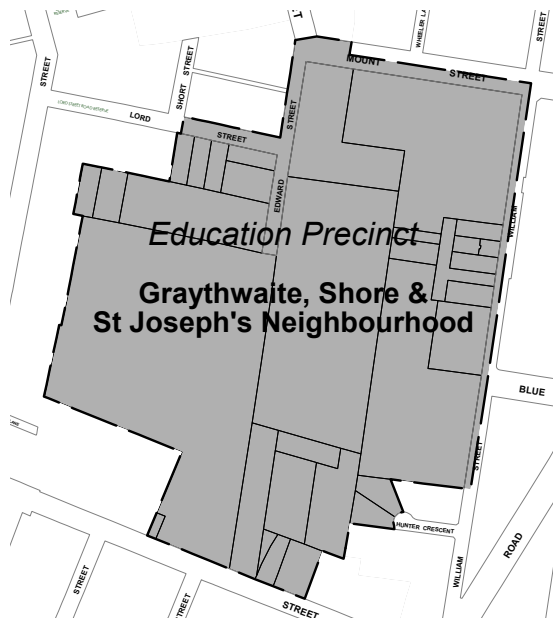
Detail

- P20 New developments in accordance with the Management Plan for Luna Park adopted by Minister for Land and Water Conservation on 12 March 1998.
- P21 Geo-technical and structural reports are submitted with any application for development to evaluate impact on heritage, cliff face, sea walls.

Car accommodation

- P22 Car parking is provided underground.
- P23 Parking does not affect stability or appearance of cliff or cliff face.

9.4 GRAYTHWAITE, SHORE & ST JOSEPH'S NEIGHBOURHOOD



9.4.1 Significant Elements

Land Use

- P1 Educational establishments.
- P2 Place of public worship.

Topography

- P3 Generally flat across the northern portion of the Area and falls to the south across the southern portion of the site.
- P4 Terraced lands across the Graythwaite site.

Natural Features

- P5 Shrubberies and trees in the grounds of Graythwaite (Giant Bamboo, Moreton Bay and Port Jackson Figs, Washington Palms, Small fruit fig; Cook Pine; Firewheel tree; Jacaranda; English Oak; Monterey pine; Coral trees, Camphor laurels; Brush Box).
- P6 On the Graythwaite site:
 - (a) Pond, well and underground cistern.
 - (b) Three natural springs.
 - (c) Areas above former air raid shelters.
 - (d) Embankment.

Views

- P7 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Distant views of Sydney CBD, Sydney Harbour Bridge and Sydney Harbour.
 - (b) Views of the mansion and substantial landscaping from Union Street.

Identity / Icons

- P8 Graythwaite Mansion and grounds.



P9 Sydney Church of England Grammar School (Shore).

P10 St Joseph's Convent.

Subdivision

P11 Large consolidated land holdings.

Streetscape

P12 Fully paved verges with street trees to Union, Edward and Lord Streets.

P13 Buildings setback from the boundary and aligned to the street on Edward and Lord Streets with low open fences.

P14 Buildings built to the boundary along William and Mount Streets.

Public transport

P15 Development is to take advantage of the high levels of accessibility to public train and bus services.

9.4.2 Desired Future Character

Diversity

P1 Predominantly educational establishments.

P2 Places of public worship and associated activities.

P3 The intensity of development reduces the further away from William Street it is located.

P4 The open landscaped setting of the Graythwaite site is retained.

P5 Consideration is given to making some of the buildings on the Graythwaite site available for community use.

P6 Provide a range of high quality, purpose-built student accommodation in and around the Education Precinct, within 400m to a tertiary institution, 800m to a railway station, and 400m to a bus stop used by a regular bus service.

P7 Educational establishments are oriented to the public domain to provide increased surveillance and activation.

P8 Streetscape improvements within the Education Precinct occur in accordance with the North Sydney Centre Public Domain Strategy and Education Precinct Public Domain Masterplan.

Accessibility and permeability

P9 Provide a student pick up and drop off route through the Sydney Church of England Grammar School (Shore), between Union Street and William Street.

Archaeology

P10 Archaeological relics on the Graythwaite site are protected and can be used to shed light on its development or add to understanding of past uses.

9.4.3 Quality Built Form

Subdivision

P1 The grounds of Graythwaite form the curtilage to the mansion and should not be subdivided. The landscaped terraces should not be broken up or separated from the mansion.

Siting

P2 New buildings on the Graythwaite site are located to the north, north east and north west of Graythwaite Mansion.

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- P3 New buildings are to maintain view corridors to Sydney Harbour, Parramatta River and Parramatta.

Form, massing and scale

- P4 Graythwaite, a grand Victorian Italianate mansion on a large prominent urban property is to be conserved and reused in accordance with the Conservation Management Plan for the site. Any future use must be non-intrusive and maintain the heritage fabric of the site. An interpretive feature or explanation may be incorporated into the site.
- P5 Scale of development reduces in intensity the further away from William Street it is located.
- P6 New buildings are subordinate to massing and scale of Graythwaite Mansion, are lower in height and have a smaller footprint.

Roofs

- P7 Roofs are pitched between 30 - 45 degrees made of either slate or terracotta tiles.

Windows and doors

- P8 Windows are timber framed with traditional vertical proportions.

Materials, colour, detail

- P9 Buildings are constructed of either face brick, masonry, timber and/or sandstone.
- P10 Colours used are browns, greens, grey for infill buildings. Colour scheme to heritage buildings in traditional schemes.
- P11 Architectural detail, external finishes of any new building are compatible with the Graythwaite collection of building but not a copy.

Fences

- P12 Fences to Graythwaite frontages to be based on historical evidence.
- P13 Fencing includes open timber picket fences, low brick or stone wall or a hedge.

Gardens

- P14 Historic plantings and significant trees are retained, including giant bamboo, figs, pines and remnant vineyards.
- P15 The lower, middle landscaped terraces on the Graythwaite site are retained as open space for recreational purposes.
- P16 Historic cultural features including sandstone stairs, pond, well, cistern and WWII bunkers are retained and interpreted.
- P17 Natural springs retained.

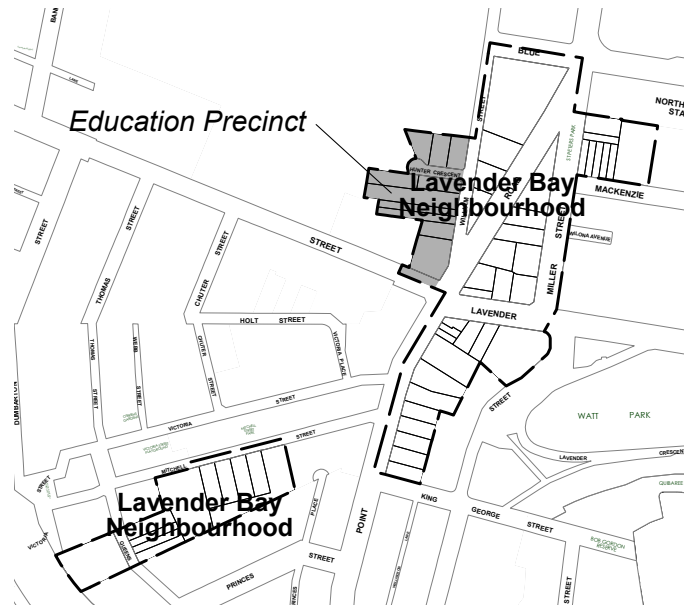
Car accommodation

- P18 Car spaces or underground parking is available to accommodate cars.

Design Principles

- P19 Educational establishments must address and activate public domain areas including streets, pedestrian links, laneways and public spaces.

9.5 LAVENDER BAY NEIGHBOURHOOD



9.5.1 Significant Elements

Land Use

- P1 Predominantly residential development.
- P2 Places of public worship.
- P3 Mixed commercial and residential development.

Topography

- P4 Generally falls to the south from Blue Street.
- P5 Steep falls occur to the east of Blues Point Road.

Views

- P6 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Views of Lavender Bay and Sydney Harbour Bridge from intersection of Miller and Lavender Streets.
 - (b) Views of Harbour Bridge from St Peter's Park Lookout (79).

Identity / Icons

- P7 St Peter Church.
- P8 Sydney Church of England Grammar School (Shore School).

Subdivision

- P9 Irregular subdivision pattern due to the streets following the irregular topography.

Streetscape

- P10 Low masonry fences and retaining ways to the street frontage.
- P11 Fully paved verges with street trees.
- P12 Buildings generally setback a minimum from the boundary with a skewed alignment to respective street frontages.



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Public transport

- P13 Development is to take advantage of the high levels of accessibility to public train and bus services.

9.5.2 Desired Future Character

Diversity

- P1 Predominantly a mixture of medium and high density residential accommodation comprising attached dwellings, multi dwelling housing and residential flat buildings, according to zone.
- P2 Limited mixed commercial and residential development with small scale shops at the ground level and residential above.
- P3 Provide a range of high quality, purpose-built student accommodation in and around the Education Precinct, within 400m to a tertiary institution, 800m to a railway station, and 400m to a bus stop used by a regular bus service.

9.5.3 Desired Built Form

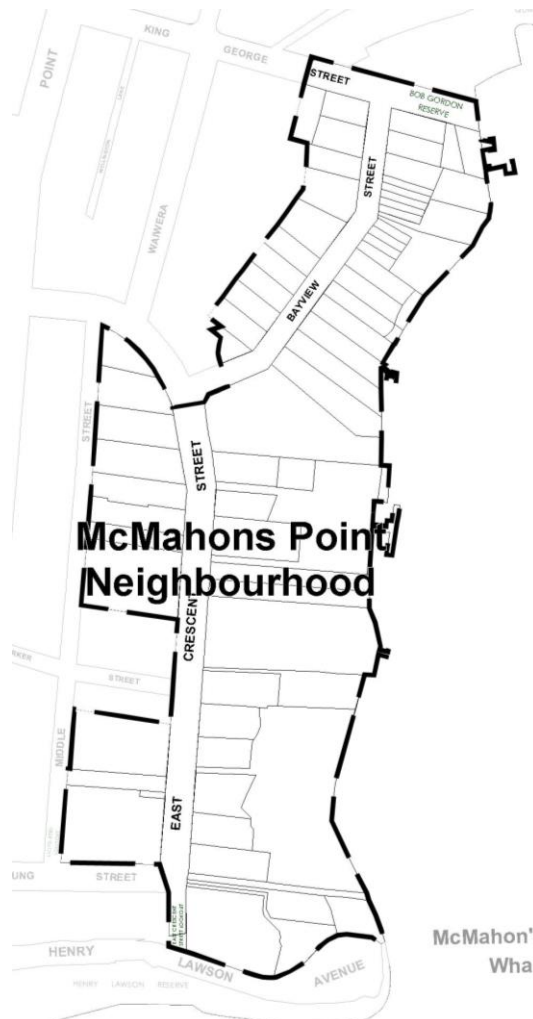
Form, massing and scale

- P1 Development is to generally conform with the provisions contained within Part B – *Development Controls* of the DCP.
- P2 Development in the *E1 Local Centre* zone is to:
- (a) have symmetrical facades fronting the primary street frontage.
 - (b) Maintain the nineteenth century two storey shopfronts incorporating parapets and awnings to the primary street frontages.

Design Principles

- P3 Educational establishments must address and activate public domain areas including streets, pedestrian links, laneways and public spaces.

9.6 McMAHONS POINT NEIGHBOURHOOD



9.6.1 Significant Elements

Land Use

P1 Predominantly residential accommodation.

Topography

P2 Steep falls from Middle Street down to the foreshores of Lavender Bay.

P3 Terraced land adjacent to foreshore.

P4 Vertical sandstone cuttings to Henry Lawson Avenue Steep falls occur adjacent to Forsyth Park.

Natural Features

P5 Lavender Bay frontage.

Views

P6 The following views and vistas are to be preserved and where possible enhanced:

(a) District views from most properties to Sydney Harbour and beyond.

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- (b) Views to Lavender Bay from King George Street Lookout (80), Extensive views to Sydney Harbour from East Crescent Reserve Lookout (81), Lloyd Rees Lookout (82).

Identity / Icons

- P7 Lavender Bay.
P8 Continuous sea wall to Lavender Bay.

Subdivision

- P9 Residential flat buildings: Site areas of 700m² - 800m².
P10 Other residential accommodation: Site areas of 230m² to 450m².

Streetscape

- P11 Split carriageways separated by sandstone walls and double rail timber fences.
P12 Low rendered masonry fences to the lower side of the street.
P13 Sandstone retaining walls to the high side of the street interrupted by garages built to the boundary.
P14 Buildings generally setback a minimum from the boundary with a skewed alignment to respective street frontage.
P15 Fully paved verges with limited landscaping.

Public transport

- P16 Development should capitalise on the Area's high level of accessibility to high frequency public train, bus and ferry services.

9.6.2 Desired Future Character

Diversity

- P1 Medium density residential accommodation comprising attached dwellings and multi-dwelling housing within the northern portion of the Area, according to zone.
P2 High density residential accommodation comprising of residential flat buildings concentrated within the southern portion of the Area, according to zone.

9.6.3 Desired Built Form

Form, massing and scale

- P1 Development is to generally conform with the provisions contained within Part B – *Development Controls* of the DCP.

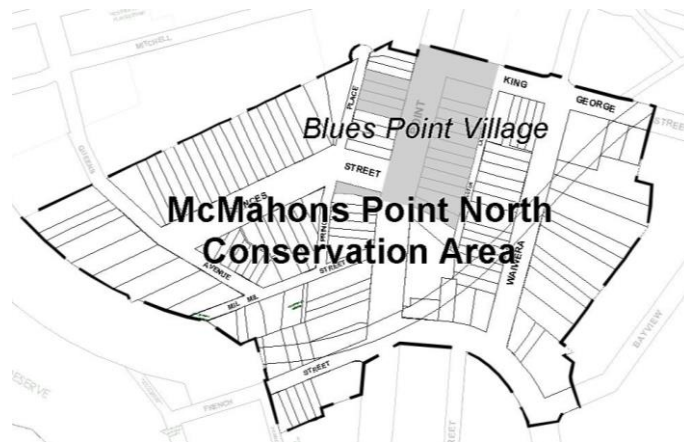
Setback

- P2 Properties located on the foreshore have setbacks of 15m or greater from the shore line.

Materials, colours

- P3 Buildings are constructed of timber, sandstone, face brick, masonry and/or painted render.

9.7 McMAHONS POINT NORTH CONSERVATION AREA



9.7.1 History

Land in the McMahon's Point North Conservation Area was granted to William Blue in 1817 and remained in the family until the 1850s. The estate was then progressively subdivided, with the earliest developments occurring at the eastern end.

Blues Point Road was one of the earliest roads in the district, being gazetted from 1839 as a thoroughfare from the ferry wharf to the St Leonard's township (now North Sydney).

The middle and southern sections of the peninsula were subdivided by the 1870s. The foreshores were used for boat and ship building from Blues and McMahon's Point. A tramway was extended to McMahon's Point in 1909, further stimulating development, particularly along Blues Point Road.

9.7.2 Description

The topography of the locality slopes down from North Sydney towards Blues Point. Blues Point Road runs down the ridge to the Blues Point peninsula with the landform falling away on either side. A steep escarpment runs parallel to the western side of Waiwera Street.

The subdivision pattern is not strongly related to the topography with irregular lots and street patterns.

The area is characterised by Victorian, Federation and Inter-war period residential and retail development. Buildings are characteristically small to moderate sized group developments of attached dwellings and semi-detached dwellings.

Characteristic building materials include dark brick, painted or rendered brick and weatherboards. Roofs are tiled, corrugated iron and sometimes slate. Details include use of sandstone in retaining walls and timber and plaster façade detailing.

Princes Street is an intact example of a Federation subdivision and Blues Point Road is a significant commercial strip in North Sydney.



9.7.3 Statement of Significance

The McMahon's Point North Conservation Area is significant:

- (a) For its consistent character and unity that derives from its dense urban subdivision pattern and history and which is still clearly seen in the development of the area.
- (b) As a predominantly early 20th century precinct with a mix of Federation and 1920's one and two storey housing and a very fine grouping of early 20th century buildings along Blues Point Road.
- (c) For the streetscape qualities of the Blues Point Road shops.
- (d) For its high quality streetscapes, particularly in Princes and Waiwera Streets.

9.7.4 Significant elements

Topography

- P1 Sloping down from North Sydney towards Blues Point, Blues Point Road follows the ridge of the peninsula.

Subdivision

- P2 Irregular subdivision to the outer streets with boundary to street frontage. Regular subdivision along Blues Point Road.

Streetscape

- P3 Many streets are split with stone kerbing and walls.
- P4 Formal street plantings are located within the road reserve in Princes Street.
- P5 Double rail timber fences.



Views

- P6 King George Street lookout: views to Lavender Bay. Views to Berry's Bay via Mil Mil Street Lookout and French Street.
- P7 Slot views over and between buildings throughout area.

9.7.5 Characteristic buildings

- P1 Detached and semi-detached dwelling houses, commercial buildings along Blues Point Road. Victorian Filigree, Federation Queen Anne, Free Style, Bungalow, Californian Bungalow, workers cottages.
- P2 Two storey, Victorian Italianate/Filigree attached dwellings with verandahs to the street.
- P3 Single storey and two storey Victorian and Edwardian commercial buildings.

9.7.6 Characteristic built elements

Siting

- P1 Forward on lot with larger rear garden.
- P2 Consistent setbacks.

Form, massing and scale

- P3 Single storey detached and attached dwellings.
- P4 Two storey, attached dwellings.
- P5 Two storey, commercial development with parapets to the street.

Roofs

- P6 Pitched between 30 and 45 degrees with some use of parapets to the street, skillion roofs to rear extensions.

Materials

- P7 **Walls:** Timber weatherboards, sandstone face brick, rendered brick.
- P8 **Roofs:** Slate, terracotta tiles, corrugated metal.

Windows and doors

- P9 Mid to late Victorian, Federation and Edwardian. Timber.

Fences

- P10 Low sandstone, brick and timber palisade. Metal palisade on sandstone plinths.

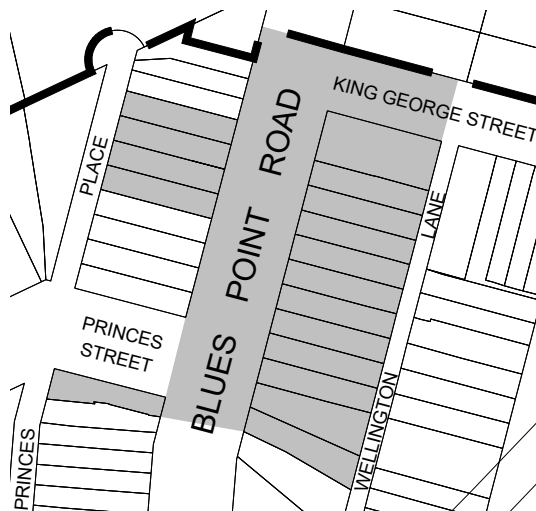
Car accommodation

- P11 Located off rear lanes.

9.7.7 Uncharacteristic elements

- P1 Modern infill development; painting of face brickwork; high fences to street; modern awnings to shops removal of traditional details, verandah infill, modernised facades, roof decks, over-scaled dormers, alterations to original roof form, advertising signage, garden paving.

9.7.8 Blues Point Village



Diversity

- P1 Predominantly mixed commercial and residential development.
- P2 Small scale shops or small scale shops with shop top housing at first floor level.
- P3 Development should be limited with no substantial increases in density.
- P4 Ground floor should be retained as shops or a similar use, to provide street level activity.

Subdivision

- P5 Site areas of 160m² – 350m²

Streetscape

- P6 Wide pavements to accommodate outdoor dining.
- P7 Awnings to street frontage elevations over footpaths.

Form, massing and scale

- P8 Generally 2-storey attached dwelling style buildings with ground floor shopfronts, gabled roof and first floor balcony.
- P9 Buildings have symmetrical facades.

Windows and doors

- P10 Buildings have double hung sash windows with a vertical emphasis.
- P11 Doors have two or four panels.

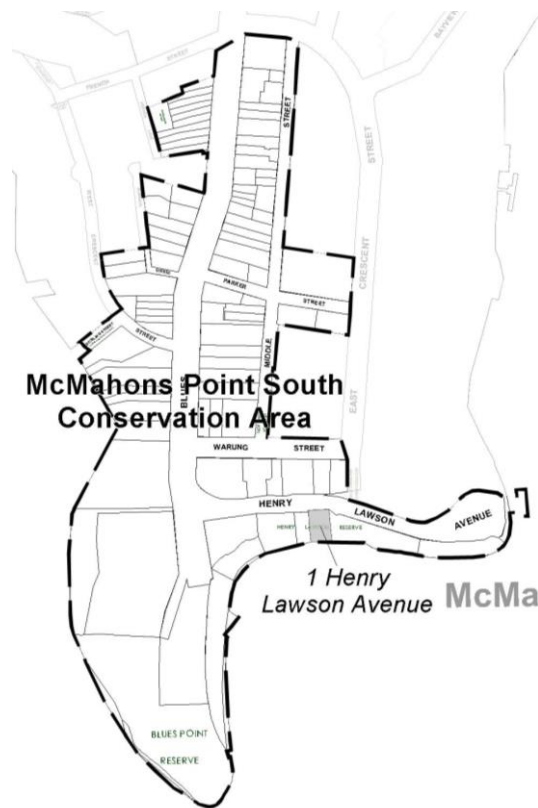
Materials, colours

- P12 Materials include face brick, sandstone or timber.

Car accommodation

- P13 Car spaces, garages, or carports are located at the rear of the property where rear lane access is available.

9.8 McMAHONS POINT SOUTH CONSERVATION AREA



9.8.1 History

Land in the McMahon's Point South Conservation Area was granted to William Blue in 1817 and remained in the family until the 1850s. The estate was then progressively subdivided, with the earliest developments occurring at the northern end.

Blues Point Road was one of the earliest roads in the district, being gazetted from 1839 as a thoroughfare from the ferry wharf to the St Leonards township (now North Sydney).

The middle and southern sections of the peninsula were subdivided by the 1870s. The foreshores were used for boat and ship building from Blues and McMahon's Point. A tramway was extended to McMahon's Point in 1909, further stimulating development, particularly along Blues Point Road.

9.8.2 Description

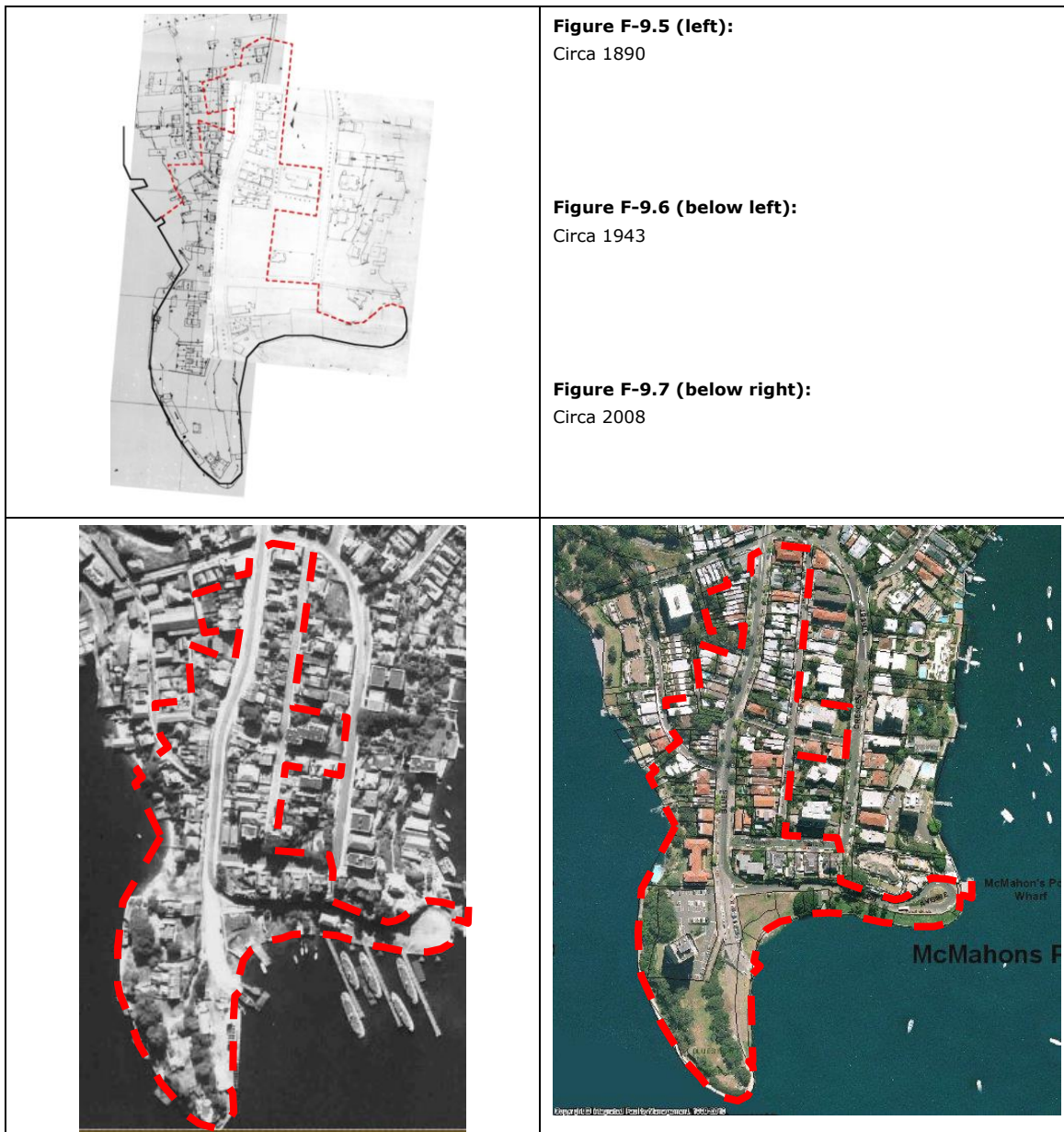
The topography of the locality slopes down from North Sydney towards Blues Point. Blues Point Road runs down the ridge to the Blues Point peninsula with the landform falling away on either side. There are expansive views down Sydney Harbour and towards the Sydney CBD, Berry's Bay and Balmain from the foreshore that is defined by an escarpment along the northern side of Henry Lawson Avenue.

Lot sizes in the area are generally large. There is a mix of architectural periods and styles in the mainly residential building stock. There are buildings from 1840s stone cottages to Federation dwelling houses and residential flat buildings and Inter-war buildings.

The buildings are typically small to moderate size with taller buildings on the higher ground and at the point and include group developments of attached dwellings and semi-detached dwelling houses. Some have rear lane access.

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Street trees include Jacarandas, mixed native species and the occasional palm, and are mature and substantial on the major streets. Front gardens supplement street plantings and give a sense of continuity between the side streets and Blues Point Road. Other urban elements such as stone retaining walls and rocky outcrops combine with avenue plantings to create a sense of place.



9.8.3 Statement of Significance

The McMahons Point South Conservation Area is significant:

- For its connection to the early grant to William Blue and the Harbour crossing point at Blues Point, the early waterfront industry of Sydney Harbour, both of which can be interpreted by physical remnants.
- As a mid 19th to early 20th century residential area with a mix of Victorian, Federation, 1920's and 1930's housing mixed with some Inter-war residential flat buildings.



9.8.4 Significant elements

Topography

P1 Sloping down from the north towards Blues Point.

Subdivision

P2 Mostly rectilinear subdivision with boundary to street frontage.

Streetscape

P3 Sandstone kerbing and walls.

P4 Street trees are a key feature in Waiwera, Miller and Lavender Streets.

P5 Double rail timber fences.

Views

P6 Blues Point Reserve and Lloyd Rees lookouts: views to Lavender Bay and Sydney Harbour.

P7 Views to Berry's Bay via Dowling Street Lookout.

9.8.5 Characteristic buildings

P1 Detached dwelling houses, semi-detached dwellings, attached dwellings, 20th Century residential flat buildings according to zone. Victorian, Federation and Inter-war period detailing.

9.8.6 Characteristic built elements

Siting

P1 Forward on lot with larger rear garden.

P2 Consistent setbacks.

Form, massing and scale

P3 Detached and attached dwellings vertically proportioned with massing determined by historic subdivision pattern.

Roofs

P4 Pitched between 30 and 45 degrees with some use of parapets to the street, skillion roofs to rear extensions.

Materials

P5 High proportion of masonry or solid surfaces to glazed surfaces.

Windows, doors and joinery

P6 Timber frames windows and doors have traditional styles and proportions according to architectural style of the building.

P7 Face brick, terracotta tiles, corrugated metal, slate, timber joinery, sandstone base courses.

Fences

P8 Low, 900mm max. height (timber) or 1m (iron palisade).

Car accommodation

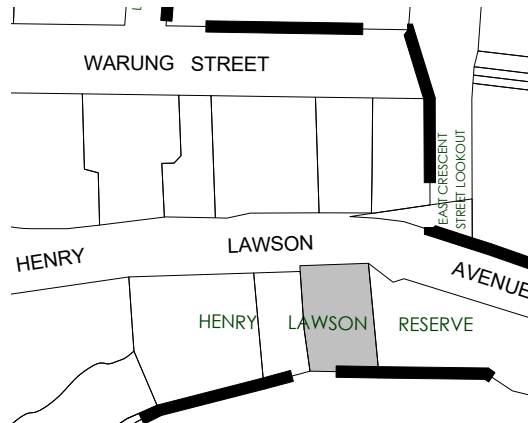
P9 Car spaces or carports located off rear lanes.

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9.8.7 Uncharacteristic elements

- P1 Post-war residential flat buildings and modern infill development; garages to front setback, roof lights and dormers in front and side roof pitches, removal of original detail, garden paving, verandah infills.

9.8.8 1 Henry Lawson Avenue



Diversity

- P1 To be integrated into the regional public open space network created along the entire southern foreshore of McMahons and Blues Points.
- P2 The site is redeveloped with cognisance of the former use of the site.

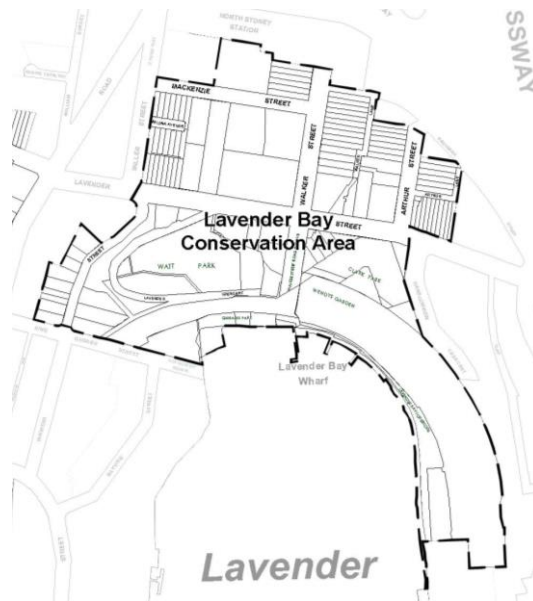
Form, massing and scale

- P3 Heritage listed slipways and sea walls to be retained.

Colours and materials

- P4 Development uses muted colours and non-reflective materials, such as brick and timber to ensure that the scenic and environmental qualities are preserved.

9.9 LAVENDER BAY CONSERVATION AREA



9.9.1 History

The Lavender Bay Conservation Area includes land that was granted to William Blue in 1817 (south of Lavender Street), the Milson's grant (east of Walker Street) and the Crown's Township subdivision.

Land in the township area was dedicated to Christ Church, St Leonards in 1872, and to the Catholic Church and school, St Francis Xavier's that opened in 1881. Milson's land was mainly used as the grounds of "Brisbane House" and a section on the east side of Walker Street was subdivided in the 1870s.

John Carr acquired most of the land at the head of Lavender Bay from the Blue family in the 1850s. A number of large mansions were built including "Quiberee", "Neepsend" and "Hellespont". Some was later reserved for the construction of the railway in 1883, after which it was given over for parkland. Watt Park includes remnant plantings from the former gardens of these homes.

A public bathing reserve on the east side of the end of Walker Street became Clark Park in the early 20th century. A large portion of the waterfront was used as boat building and repair yards from the late 19th century to the late 20th century.

9.9.2 Description

The Lavender Bay Conservation Area is bounded by the commercial development to the north, the Sydney Harbour Bridge approaches and Lavender Bay.

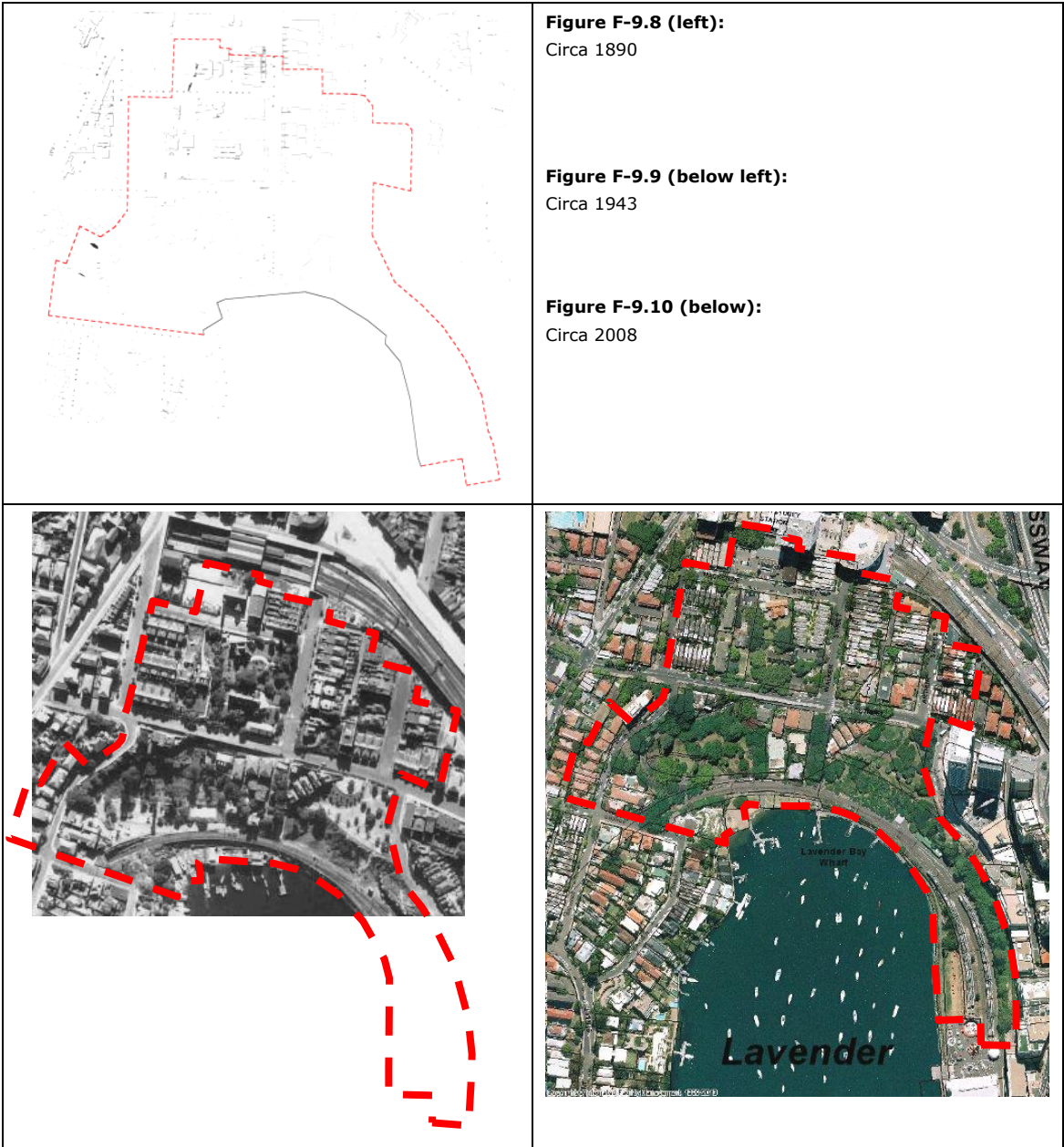
The landform is level to the north with a rock escarpment along Lavender Bay Road and a steep slope to the water. To the upper plateau the subdivision is determined by a grid pattern of wide streets and narrow rear lanes. Lot sizes vary and many are developed for attached dwellings. To the centre of the plateau are landscaped church sites. Below the escarpment the pattern is determined by the steep landform with its remaining Victorian estates overlaid with a network of steep, pedestrian steps and irregular cross paths.

The area is characterised by the Victorian development of Gothic and Romanesque churches and detached mansions down the centre of the area. There is an overlay of single, two and three storey attached dwellings to the upper level in the Victorian Free Classical, Victorian Italianate and Federation Arts and Crafts style. There are single storey cottages and Inter-war, Spanish Mission style flats. The railway viaduct and boardwalk provides a strong edge to the water side reserve.

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The elevated site provides extensive views to the south over Sydney Harbour and there are views up and down the pedestrian steps.

Front gardens contribute to the landscaping of the street. Gardens follow the slope of the site with stone walls to the street. There are street trees and a band of remnant Victorian plantings to the parks and gardens across the centre of the site.



9.9.3 Statement of Significance

The Lavender Bay Conservation Area is significant:

- (a) As a late 19th Century residential area with rare groupings of high quality, stepped terrace style housing on the slopes above Lavender Bay overlaid on religious and Victorian Mansion estates.
- (b) For the quality and collective significance of the buildings within the area.
- (c) For the natural topography of rock escarpments modified with steep pathways and sandstone retaining walls and the remnant Victorian plantings.

- (d) For the fine and extensive open space area along the foreshore of Lavender Bay which incorporates public walking paths around the waterfront and the railway viaduct constructions.
- (e) For the impressive and important views to Sydney Harbour from Watt and Clark Parks.

9.9.4 Significant Elements

Topography

- P1 Upper plateau with slight falls to the south, rock escarpment and natural basin sloping down to Lavender Bay.

Subdivision

- P2 Large church sites and former mansion estates.
- P3 Rectilinear subdivision to upper level with long narrow lots with detached and semi-detached dwelling houses and attached dwellings.
- P4 Irregular subdivision to the lower slopes.
- P5 Unique subdivision pattern and private road in Wilona Avenue.

Streetscape

- P6 Street trees and stone walls align streets and paths.
- P7 Lanes have a low scale and service character.
- P8 Public parks to the lower slopes with sandstone features and walling.

Views

- P9 Harbourview Crescent Lookout, Clark Park Lookout, Arthur Street Lookout, Lower Walker Street Lookout, Waiwera Street Lookout.
- P10 Views to Sydney Harbour, Sydney CBD, Sydney Harbour Bridge from Lavender Street, Lavender Crescent, Waiwera Street, Watt and Clark Parks.

9.9.5 Characteristic buildings

- P1 A mixture of:
 - (a) Detached Victorian mansions;
 - (b) Victorian and Federation religious buildings; and
 - (c) Victorian and Federation detached, semi-detached dwelling houses and attached dwellings.

9.9.6 Characteristic built elements

Siting

- P1 Forward on lot with larger rear garden.
- P2 Consistent setbacks.

Form, massing and scale

- P3 Two storey detached houses.
- P4 Single, two and three storey with gabled roofs.
- P5 Skillion wings and reduced height and scale to rear.
- P6 Open verandahs to front. Houses in groups of identical design.
- P7 Strong skyline of simple, pitched roofs and chimneys visible from the street and rear lanes stepped along the sloping streets.

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Roofs

- P8 Pitched between 30 and 45 degrees with some use of parapets to the street.
- P9 Skillion roofs to rear extensions.
- P10 Brick and rendered chimneys.

External Materials

- P11 Rendered brick and face brick on sandstone base.
- P12 Slate, terracotta tiles, corrugated metal roofs.
- P13 Original timber windows doors and decorative joinery.

Windows and doors and external joinery

- P14 Victorian Federation and Edwardian. Timber.

Fences

- P15 Original front fences.
- P16 Timber fences rear and side.
- P17 Sandstone plinths, sandstone piers, metal palisade and gates, timber pickets, timber rails and mesh, pipe and mesh gates, original face brick with piers.

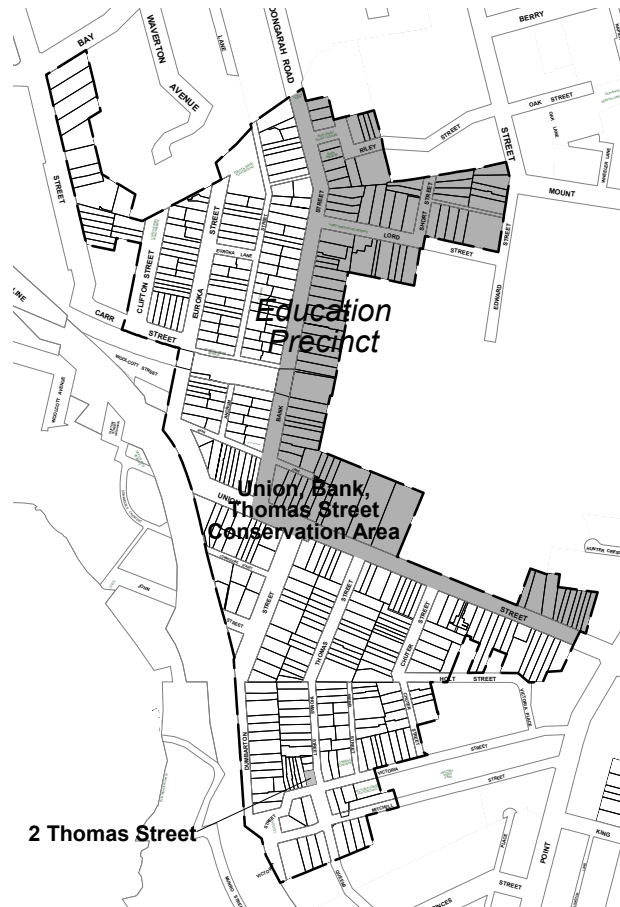
Car accommodation

- P18 Located off rear lanes.

9.9.7 Uncharacteristic elements

- P1 Inter-War residential flat buildings; modern infill development; over-scaled additions; contemporary buildings with laneway frontages; over-scaled and poorly detailed carports and garages; front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street.

9.10 UNION, BANK, THOMAS STREET CONSERVATION AREA



9.10.1 History

The Union/Bank/Thomas Street Area is loosely contained within two residential subdivisions of the mid-nineteenth century. The land on the south side of Union Street was part of the William Blue grant, inherited by his daughter Susannah, and subdivided c. 1859 by her husband, William Chuter.

Land on the northern side of Union Street was granted to Thomas Walker who later sold it to Edwin Sayers. Sayers built *Euroka Villa* on the Graythwaite site in Union Street. The western facing slopes of Sayer's land was subdivided during the construction of the railway in two phases, first during the 1890s and again in the 1930s. Despite the railway intrusion, the area retains its nineteenth century buildings and streetscape form.

The area experienced significant upheaval during the construction of the railway in two phases, first during the 1890s and again in the 1930s. Despite the railway intrusion, the area retains its nineteenth century buildings and streetscape form.

9.10.2 Description

The Union, Bank Thomas Street Conservation Area is bounded to the east by the North Sydney Commercial area where the land rises to the crest of North Sydney and to the west by the railway line along the foreshore of Berry's Bay.

The areas on either side of Union Street have distinct characters within the Conservation Area. The subdivision pattern is irregular to the south of Union Street reflecting the topography and street pattern with various street widths reflecting different phases of development. The subdivision pattern is more regular to the north of Union Street reflecting the flatter topography. The area has a close subdivision pattern, with cross streets and

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laneways that give a dense, urban texture. The area is characterised by long north/south streets, with streets north of Union Street running mostly across the steeply graded contours.

Characteristic buildings of the area include 19th and early 20th century cottages, including attached, semi-detached and detached houses. The buildings are typically one to two storeys on small lots interspersed with pockets of larger, two storey Victorian terraces, early 20th century housing, and three storey Inter-war residential flat buildings.

A range of architectural styles are represented in the Conservation Area, including Victorian Georgian, Victorian Regency, Victorian Filigree, Victorian Italianate, Federation Queen Anne and Federation Arts and Crafts, most of which are interpreted in a simplified manner.

There are pocket parks and lookouts distributed throughout the area. The townscape character is also defined by regular, processional planting in the street reserves, and by extensive use of stone elements within streets and street formations.

The topography of the locality facilitates expansive views down streets running south off Union Street, including Dumbarton and Thomas Streets.

	<p>Figure F-9.11 (left): Circa 1890</p> <p>Figure F-9.12 (below left): Circa 1943</p> <p>Figure F-9.13 (below right): Circa 2008</p>
	

9.10.3 Statement of Significance

The Union, Bank, Thomas Street Conservation Area is significant:

- (a) As the largest area of mid to late Victorian buildings in the North Sydney area including a substantial number of individually significant buildings, groups of buildings and unique streetscapes in the local context.
- (b) For the clarity of its subdivision history that is still clearly seen in variations of road widths between sections of streets and changes of directions of the subdivision pattern in response to the subdivision of earlier estates.
- (c) For the way development has responded to the topography through stepped building forms and excavation in some locations.
- (d) For its fine streetscapes with intact rows of Victorian residences, especially in Thomas, Union and Chuter Streets.
- (e) For its relatively large number of surviving timber residences.

9.10.4 Significant elements

Topography

P1 Area slopes to the west with escarpments to the north.

Subdivision

P2 Irregular subdivision to the south of Union Street with boundary to street frontage.

P3 Mostly rectilinear subdivision to the north of Union Street with boundary to street frontage.

P4 Restricted lot sizes.

Streetscape

P5 Varying width streets following the contours or slopes.

P6 Sandstone retaining walls and kerbing.

P7 Double rail timber fences.

Views

P8 Dumbarton Street lookout, Commodore Crescent lookout, Lord Street lookout.

P9 Dumbarton Street, Commodore Street, John Street: views to Berry's Bay.

P10 Slot views over and between buildings throughout area.

9.10.5 Characteristic buildings

P1 A mixture of:

- (a) 19th and early 20th Century cottages, including attached, semi-detached and detached single storey dwellings in the Victorian Georgian, Victorian Regency, Victorian Filigree, Victorian Italianate.
- (b) Two storey, Victorian Filigree, Victorian Italianate attached dwellings.
- (c) Early 20th Century single and two storey Federation Queen Anne and Federation Arts and Crafts housing.
- (d) Three storey Inter-war residential flat buildings.

9.10.6 Characteristic building elements

Siting

P1 Forward on lot with larger rear garden.

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P2 Consistent setbacks.

Form, massing and scale

P3 Single and two storey, detached and semi-detached dwellings with verandahs.

P4 Two and three storey attached dwellings with front verandahs.

P5 Rear additions behind and below the ridge line, submissive in scale.

Roofs

P6 Pitched between 30 and 45 degrees with some use of parapets to the street, skillion roofs to rear extensions.

P7 Hipped and gabled roofs to cottages; projecting gables to street.

P8 Stone, brick and rendered brick chimneys.

Materials

P9 **Walls:** Sandstone; weatherboards; face brick and rendered masonry on sandstone bases.

P10 **Roofs:** Slate; corrugated metal; terracotta tiles.

Windows and doors

P11 Original Victorian, Federation and Edwardian.

Fences

P12 Low stone, brick and rendered walls.

P13 Metal palisade on low stone plinths.

P14 Timber picket fences.

Car accommodation

P15 Located off rear lanes.

P16 Single car parking in modest structures.

9.10.7 Uncharacteristic elements

P1 Over-scaled additions; Carports and garages to front of lot; dormers and conditions to front and side roofs; removal of original details; painting and rendering of face brickwork; high fences to street; inappropriate fence details; paving of gardens.

9.10.8 Future built elements

P1 Provide a range of high quality, purpose-built student accommodation in and around the Education Precinct, within 400m to a tertiary institution, 800m to a railway station, and 400m to a bus stop used by a regular bus service.

P2 Educational establishments must address and activate public domain areas including streets, pedestrian links, laneways and public spaces.



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SECTION 10 WAVERTON / WOLLSTONECRAFT
PLANNING AREA

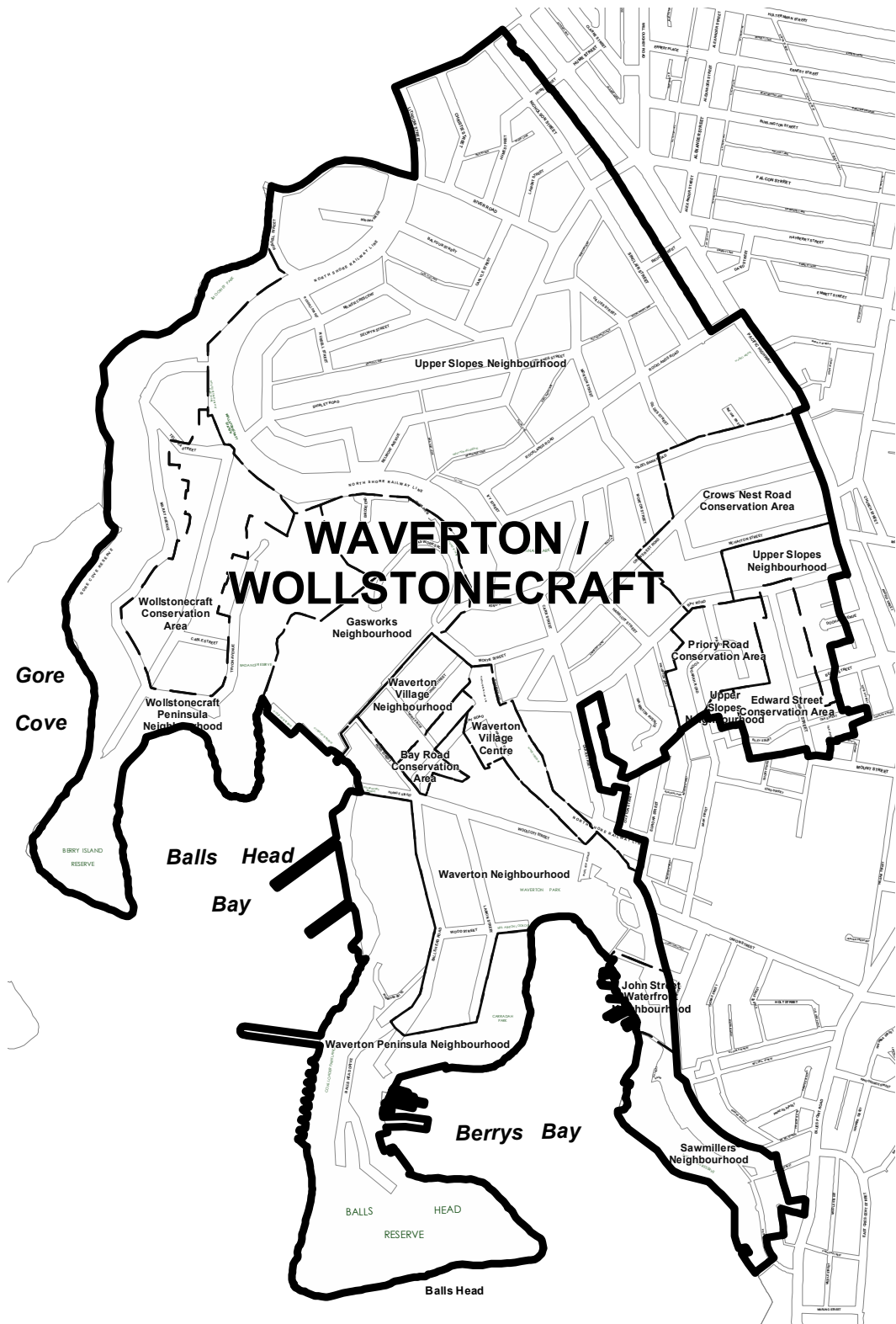


Figure F-10.1: Waverton / Wollstonecraft Planning Area and associated Locality Areas

10.0 WAVERTON / WOLLSTONECRAFT PLANNING AREA CHARACTER STATEMENT



The Planning Area generally comprises a diverse residential neighbourhood ranging from low density residential development adjacent to the foreshore areas of Sydney Harbour to high density residential development generally on the upper slopes and in close proximity to railway stations. The suburbs of Waverton and Wollstonecraft essentially align with the two ridges/peninsulas that project out into Sydney Harbour. The Area is also physically divided by the North Shore Railway line. Both neighbourhoods are in a pleasant setting, as a result of buildings being setback from boundaries, onsite landscaping, street trees and strong links to Sydney Harbour.

A large portion of the Education Precinct is located in the Planning Area, which consists of landmark educational establishments such as the Australian Catholic University (ACU), the Sydney Church of England Grammar School (Shore) and North Sydney Demonstration School and supports a growing student population and workforce of teaching and support staff.

The foreshores of the Planning Area are generally protected from development by recreational and bushland buffers, with the minor exception of maritime industrial activities which are reliant on a land-water interface.

Development within the Planning Area, should result in:

- residential growth being provided in accordance with Council's Local Housing Strategy, predominantly comprising attached dwellings, multi dwelling housing and residential flat buildings in the appropriate zones
- redevelopment of sites respects the existing built form and maintains the character of the area. This includes any alterations and additions to existing buildings
- a wide range of residential types being distributed in a number of distinctive built forms and landscape areas, including purpose built student accommodation to support the functioning of the Education Precinct.

Section 10 - Waverton / Wollstonecraft Planning Area

- local shops cater to the local community and are balanced between basic needs of food and grocery, and social needs- such as cafes and galleries
- a vibrant and engaging Education Precinct with safe pedestrian networks, high quality built form and a range of formal and informal public spaces for students to study, socialise and engage with the local community.
- future maritime uses having a minimal impact on residential amenity
- features contributing to local identity are promoted and conserved

and where,

Function

- a community centre being provided in an accessible location in the Waverton Neighbourhood, provides a meeting place for the local community and is part of North Sydney's network of community and cultural centres
- a limited number of non-residential uses such as maritime industrial, defence, hospital and educational establishments coexist peacefully with the residential character without adverse effect
- large areas of open space are used by local residents and the wider regional population for a variety of social and recreational needs
- comfortable and safe pedestrian routes are maintained and extended to achieve a continuous route from Smoothey Park to Waverton Park and on to McMahon's Point
- public transport, cycling and walking are the preferred means of transport
- through traffic is discouraged from using the already overloaded road network and commuter parking managed through parking schemes

Environmental Criteria

- the remaining natural foreshores and water courses are conserved and protected, and pedestrian access to these is extended and improved
- bushland is protected from the adverse effects of development – such as stormwater runoff, spread of introduced plants and weeds, and visual impact of structures
- use of local flora extends habitats for native birds and other fauna
- mechanical noise and other industrial noise is controlled, to protect the ambience of natural features

Quality Built Form

- any development reflects and reinforces the existing distinctive built form/landscape areas and distribution of accommodation types
- cohesiveness is achieved in the area and through landscaping and street tree planting
- development in foreshore areas is carefully designed to consider the existing topography and not disrupt views from neighbouring properties
- significant views from lookouts and other vantage points are not obscured by structures or landscaping
- man-made features such as the railway cutting at Waverton Station and the tank cuttings on the BP site are maintained as local landmarks important to the community
- educational establishments are oriented to the public domain to provide increased surveillance and activation



Quality Urban Environment

- backyards are used for a variety of activities particularly for families with children and for the practical and recreation needs of residents in apartments

Efficient Use of Resources

- existing buildings are maintained to prevent unnecessary waste of building materials
- stormwater is retained for reuse on-site

Public Domain

- streetscape improvements within the Education Precinct occur in accordance with the *North Sydney Centre Public Domain Strategy* and the *Education Precinct Public Domain Masterplan*

The Waverton / Wollstonecraft Planning Area includes the following Locality Areas which are subject to the relevant provisions further below:

- Section 10.1: Waverton Village Centre
- Section 10.2: Upper Slopes Neighbourhood
 - Section 10.2.4 Newlands Lane Open Space Area
- Section 10.3: Wollstonecraft Peninsula Neighbourhood
 - Section 10.3.4 12 Shirley Road
- Section 10.4: Gasworks Neighbourhood
- Section 10.5: Waverton Village Neighbourhood
- Section 10.6: Waverton Neighbourhood
- Section 10.7: Waverton Peninsula Neighbourhood
 - Section 10.7.4 HMAS Waterhen – Upper Terrace
 - Section 10.7.5 Former Quarantine Depot
 - Section 10.7.6 Former Woodleys Boatyard
- Section 10.8: John Street Waterfront Neighbourhood
- Section 10.9: Sawmillers Neighbourhood
- Section 10.10: Wollstonecraft Conservation Area
- Section 10.11: Crows Nest Road Conservation Area
- Section 10.12: Edward Street Conservation Area
- Section 10.13: Priory Road Conservation Area
- Section 10.14: Bay Road Conservation Area



10.1 WAVERTON VILLAGE CENTRE



10.1.1 Significant Elements

Land Use

P1 Predominantly mixed commercial and residential development.

Topography

P2 Moderate falls to the south.

P3 Large vertical cutting through sandstone to accommodate railway line.

Natural Features

P4 Wakelin Reserve maintained as public park.

P5 Creekline into Wakelin Reserve is conserved.

Views

P6 The following views and vistas are to be preserved and where possible enhanced:

- (a) Unobstructed views, from the Bay Road Bridge, of the railway cutting, tunnel and cliff face.
- (b) Vistas towards Balls Head and water views along Balls Head Road.
- (c) Views toward Railway Station building from Bay Road and Crows Nest Road.
- (d) Views from Wakelin Reserve towards Sydney Harbour.

Identity / Icons

P7 North Shore railway line.

P8 Waverton Railway Station.

Subdivision

P9 Irregular subdivision pattern.



Streetscape

- P10 Paved street verges.
- P11 Buildings built to the boundary.
- P12 Continuous awnings to the southern side of Bay Road.

Public transport

- P13 Development is to take advantage of the high levels of accessibility to public train and bus services.

10.1.2 Desired Future Character

Diversity

- P1 Predominantly mixed-use development with retail and commercial premises on ground floor (fronting Bay Road), non-residential or residential accommodation on the first floor, and residential above.
- P2 A community centre is established over the SRA land to the south of the Bay Road.
- P3 Development to the north of Bay Road is to comprise a small commercial building set back from Bay Road in a large landscaped open area and used for a plant nursery, café, or childcare.
- P4 Man-made features such as the railway cutting at Waverton Station are maintained as local landmarks important to the community.

Access

- P5 Pedestrian access is provided from Bay Road through to Carr Street.

10.1.3 Desired Built Form

Subdivision

- P1 No further subdivision of land north of Bay Road.

Form, massing and scale

- P2 One storey to the north of Bay Road.
- P3 To the south of Bay Road:
 - (a) One to two storeys at the Bay Road frontage.
 - (b) Any additional storeys are set back at least 6m from the Bay Road frontage above the second storey with little impact on views, overshadowing and privacy.
- P4 Building bulk and facades are articulated providing gaps for natural light and direct sunlight to penetrate onto Waverton Station platforms.
- P5 Where possible, the building to the north of Bay Road should be retained in its current form.

Siting

- P6 Building sited to maintain access to North Shore Railway.
- P7 Views from Wakelin Reserve, surrounding residential properties, and solar access (particularly to Waverton Station platforms), considered when siting building.
- P8 Development to the north of Bay Road is to be located towards the street frontage of the block, with large side and rear yards.

Setback

- P9 To the north of Bay Road:
 - (a) **Front:** 5m.



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(b) **Side:** 3m.

(c) **Rear:** 20m.

P10 To the south of Bay Road:

(a) **Front:** 0m.

(b) **To Wakelin Reserve:** 3m minimum.

(c) Setbacks from boundaries that adjoin or are near residential properties and areas of open space land, are maintained to protect the amenity of that land.

Solar Access

P11 Dwellings and ground level retail plaza of Waverton Station north east side of 100 Bay Road have good solar access.

Fences

P12 Buildings and front gardens to the north of Bay Road are seen through and over front fences.

Car accommodation

P13 Emergency vehicle access is provided to the North Shore railway line.

P14 Parking is located underground.

P15 Only one access point is provided from Bay Road adjacent to Wakelin Reserve to the SRA Land south of Bay Road.

10.2 THE UPPER SLOPES



10.2.1 Significant Elements

Land Use

- P1 Diverse range of low, medium and high density residential accommodation.
- P2 Passive and active recreational spaces.
- P3 Educational establishments.
- P4 Health services facilities.

Topography

- P5 Generally falls from the ridge following the Pacific Highway down to the south-west.

Natural Features

- P6 Remnant bushland in Smoothey Park.

Views

- P7 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Vistas to Sydney Harbour along Crows Nest Road are maintained.
 - (b) Remaining views from Brennan Park are maintained.



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Identity / Icons

- P8 Mater Hospital.
- P9 Bradfield College.
- P10 North Sydney Demonstration School.
- P11 Pacific Highway, a major regional thoroughfare.
- P12 North Shore railway line.
- P13 Brennan Park.
- P14 Smoothey Park.

Subdivision

- P15 A diverse mixture of allotment sizes and shapes reflective of the street alignments which follow the undulating topography of the land.

Streetscape

- P16 Wide streets with trees set into the carriageway.
- P17 Grass verges and concrete paths.
- P18 Buildings setback and generally aligned with the street frontage.
- P19 Low masonry fences.
- P20 Garages built to the boundary along the southern side of Rocklands Road and set into the slope of the land.

Public transport

- P21 Development is to take advantage of the high levels of accessibility to public train and bus services.

10.2.2 Desired Future Character

Diversity

- P1 Predominantly residential accommodation, comprising:
 - (a) Residential flat buildings on garden lots, according to zone.
 - (b) Attached dwellings and multi dwelling housing, according to zone.
 - (c) Dwelling houses and dual occupancies, particularly in the Balfour and Carlyle Street area.
 - (d) Rows of semi-detached housing in Sinclair Street.
- P2 Supporting community facilities such as educational establishments and health care facilities.
- P3 A vibrant and engaging Education Precinct with safe pedestrian networks, high quality built form and a range of formal and informal public spaces for students to study, socialise and engage with the local community.
- P4 Provide a range of high quality, purpose-built student accommodation in and around the Education Precinct, within 400m to a tertiary institution, 800m to a railway station, and 400m to a bus stop used by a regular bus service.
- P5 Streetscape improvements within the Education Precinct occur in accordance with the North Sydney Centre Public Domain Strategy and the Education Precinct Public Domain Masterplan.

10.2.3 Desired Built Form

- P1 Development in the form of residential flat buildings and multi dwelling housing in the relevant zones.
- P2 Educational development on the Bradfield College land is residential in scale, similar to attached dwellings and smaller residential flat buildings – with bulk and scale of larger buildings broken down into a number of elements.
- P3 Educational establishments must be oriented and designed to address, surveil and activate the public domain areas including streets, pedestrian links, laneways and public spaces.

10.2.4 Newlands Lane Open Space Area



Objectives

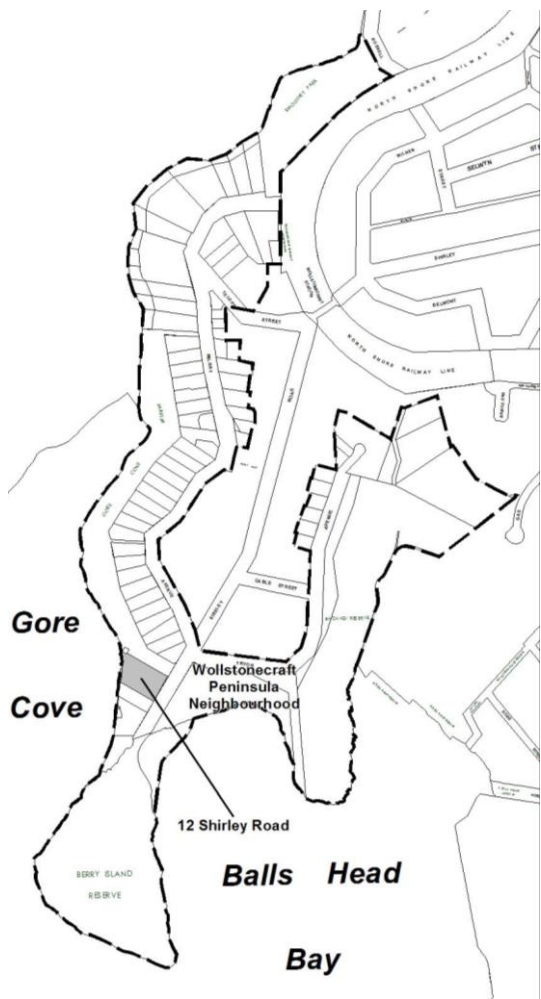
- O1 Development should maintain the established open landscaped setting provided by the area.
- O2 Development should maintain the established scenic, environmental and cultural qualities of Newlands Lane.
- O3 Restrict development within the open space area subject area to works relating to the provision of landscaping or open space.
- O4 Development should encourage a native bushland setting.

Provisions

- P1 Development should not be undertaken in the open space building line area identified by shading in the figure to Sub-section 10.2.4 to Part F of the DCP.
- P2 Fences of a height of no more than 900mm may be permitted.



10.3 WOLLSTONECRAFT PENINSULA NEIGHBOURHOOD



10.3.1 Significant Elements

Land Use

- P1 Residential accommodation.
- P2 Passive and recreational spaces.

Topography

- P3 Generally falls from the ridge following Shirley Road down to the foreshores of Balls Head Bay and Gore Cove.
- P4 Steep topography in the vicinity of the shoreline.

Natural Features

- P5 Wetlands in the form of Coastal Saltmarsh along the foreshores of Gore Cove (refer to Section 3.2 – Bushland to Part B of the DCP).
- P6 Remnant natural shorelines.
- P7 Remnant bushland adjacent to the foreshore.

Views

- P8 The following views and vistas are to be preserved and where possible enhanced:



- (a) District views to Sydney Harbour.
- (b) Bridge End Lookout (110), Tryon Avenue Lookout (111), Berry Island Lookouts (112-114), Gore Cove Track Lookouts (115).

Identity / Icons

- P9 Berry Island Reserve.
- P10 Badangi Reserve.
- P11 Gore Cove bushland and wetlands.
- P12 Former Gas Works site.
- P13 Gore Cove.
- P14 Balls Head Bay.

Subdivision

- P15 Irregular grid subdivision reflective of the streets following the topography.

Streetscape

- P16 Buildings setback from the boundary with a skewed alignment to respective street frontages.
- P17 Split carriageways, raised verges and double rail timber fences.
- P18 Remnant sandstone guttering.
- P19 Brick and sandstone retaining walls built to the street alignment on the high side.

Public transport

- P20 Development is to take advantage of the high levels of accessibility to public train services.

10.3.2 Desired Future Character

Diversity

- P1 Predominantly low density residential accommodation comprising of detached dwelling houses.
- P2 Development should generally not result in any significant increase in dwelling density.
- P3 Maintaining and enhancing existing public foreshore spaces.

Access

- P4 Providing continuous public access along the foreshores of Gore Cove and Balls Head Bay.

10.3.3 Desired Built Form

Form massing and scale

- P1 Bungalow style to some large "mansion" style dwellings.
- P2 Buildings generally of brick and tile construction within a landscaped setting.
- P3 Development on sloping sites to follow topography of the land.
- P4 Development should be designed to not water disrupt the views from neighbouring properties.

10.3.4 12 Shirley Road



Objectives

- O1 To provide continuous public foreshore access along the length of Wollstonecraft Peninsula.

Provisions

- P1 Once the entire site ceases to be used by the State government as a research station for fisheries purposes, land directly adjacent to the foreshore of Gore Cove is to be handed back to the general public to enable continuous public access across the entire length of the Gore Cove foreshore.

10.4 GASWORKS NEIGHBOURHOOD



10.4.1 Significant Elements

Land Use

- P1 High density residential accommodation.
- P2 Passive and active recreational spaces.

Topography

- P3 Steep falls south-west to the foreshore, with flatter areas adjacent to the foreshore.
- P4 Vertical man-made sandstone cliffs.

Natural Features

- P5 Balls Head Bay.

Views

- P6 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Views to Balls Head Bay and beyond.
 - (b) Bridge End Lookout (110), Tryon Avenue Lookout (111), Berry Island Lookouts (112-114), Gore Cove Track Lookouts (115).

Identity / Icons

- P7 Former gasworks buildings.
- P8 Balls Head Bay.
- P9 Sandstone cliffs.

Subdivision

- P10 Large consolidated allotments and strata subdivision.

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Streetscape

- P11 Wide streets, with no on-street parking.
- P12 Grassed verges.
- P13 Buildings setback from the boundary with a skewed alignment to respective street frontages.

Public transport

- P14 Development is to take advantage of the high levels of accessibility to public train services.

10.4.2 Desired Future Character

Diversity

- P1 Predominantly residential flat buildings.

Foreshore

- P2 Retain and enhance public access across foreshore areas.

10.4.3 Desired Built Form

- P1 Future development of should maintain the existing character of the area with no substantial increase in density.
- P2 Development to step down to follow topography of the land.
- P3 Development should be designed to not disrupt water views from neighbouring properties.

10.5 WAVERTON VILLAGE NEIGHBOURHOOD



10.5.1 Significant Elements

Land Use

- P1 Predominantly residential development.
- P2 Limited mixed commercial and residential development and community facilities.

Topography

- P3 Gentle falls to the south-west between McKye Street and Whatmore Street.
- P4 Steep falls to the south-west between Whatmore Street and Ross Street.

Views

- P5 The following views and vistas are to be preserved and where possible enhanced:
 - (a) From the end of Tunks Street to Sydney Harbour and beyond.
 - (b) McKye Street Lookout (95), King Street Lookout (96).

Identity / Icons

- P6 Former gas works site.

Subdivision

- P7 Site areas of 400 - 460m².
- P8 Long narrow allotments.

Streetscape

- P9 Tall sandstone retaining walls built to street alignment.
- P10 Low masonry, stone and picket fences.
- P11 Buildings setback from the boundary with a skewed alignment to respective street frontages.

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P12 Grass verges, concrete footpaths and street trees.

Public transport

P13 Development to take advantage of high levels of access to public train and bus services.

10.5.2 Desired Future Character

Diversity

P1 Predominantly low density residential accommodation comprising dwelling houses, semi-detached dwellings, dual occupancies.

P2 Development should generally not result in any significant increase in dwelling density.

10.5.3 Desired Built Form

Form, massing and Scale

P1 Generally one storey with potential for a second storey set into the fall of the land.

P2 Where possible, any extensions should be contained within a one storey roof-line.

Car accommodation

P3 On-street or to the side where width of lot permits.

P4 No onsite parking structures forward of front building line, except along the northern side of Ross Street, but only where it is set into the fall of the land and hidden from view.

10.6 WAVERTON NEIGHBOURHOOD



10.6.1 Significant Elements

Land Use

- P1 Diverse range of residential accommodation.
- P2 Passive and active recreational spaces.

Topography

- P3 Gentle falls to the foreshores of Berrys Bay.
- P4 Wide flat areas adjacent to Berrys Bay.
- P5 Steep falls from Ross Street to Balls Head Bay.
- P6 Flat areas following the ridge along Balls Head Road.

Natural Features

- P7 Berrys Bay.
- P8 Balls Head Bay.
- P9 Natural shoreline topography and bushland adjacent Balls Head Bay.
- P10 Significant stands of mature trees within Waverton Park.
- P11 Carradah Park.
- P12 HMAS Waterhen.

Views

- P13 The following views and vistas are to be preserved and where possible enhanced:
 - (a) District views to Sydney CBD, Sydney Harbour and beyond.

Section 10 - Waverton / Wollstonecraft Planning Area

- (b) Views to Iron Cove Bridge from Balls Head Road.
- (c) Waverton Park Lookout (98), Will Ashton Lookout (99), Balls Head Lookouts (100 -109).

Identity / Icons

- P14 North Shore railway line.
- P15 Coal Loader and former BP oil tank cut outs.
- P16 Waverton Park.
- P17 Berrys Bay.
- P18 Balls Head Bay.

Subdivision

- P19 Irregular grid pattern reflective of the streets aligning with the topography.

Streetscape

- P20 Low fences to Balls Head Road, with high fences to other road frontages.
- P21 Buildings setback from the boundary with a skewed alignment to respective street frontages.

Public transport

- P22 Development is to take advantage of the high levels of accessibility to public train and bus services.

10.6.2 Desired Future Character

Diversity

- P1 Dwelling houses on the upper portions of the ridge of Waverton Peninsula.
- P2 Multi dwelling housing and residential flat buildings on Woolcott and Ross Streets.
- P3 A community centre being provided in an accessible location in the Waverton Neighbourhood, provides a meeting place for the local community and is part of North Sydney's network of community and cultural centres.
- P4 Man-made features such as the tank cuttings on the former BP site are maintained as local landmarks important to the community.

Access

- P5 Providing continuous public access along the foreshores of Balls Head Bay and Berrys Bay.

10.6.3 Desired Built Form

Form, massing and scale

- P1 Low density and scaled development located along the ridge tops of the Area with higher density development located elsewhere.

The map illustrates the Waverton Peninsula Neighbourhood, bounded by the ocean to the west and south. Key streets include WOOD STREET, WOODLOTT STREET, LARSON STREET, and WOODLOTT STREET. The area is divided into several sections: the northern section is labeled 'HMAS Waterhen - Upper Terrace'; the central section is labeled 'Waverton Peninsula Neighbourhood'; the southern section is labeled 'Former Woodleys Boatyard'; and the eastern section is labeled 'Former Quarantine Depot'. The map also shows the 'Balls Head' area to the south and the 'Waverton Peninsula' to the north. The 'Balls Head' area is further divided into 'Balls Head Reserve' and 'Balls Head'.

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Section 10 - Waverton / Wollstonecraft Planning Area

Natural Features

- P6 Remnant natural shoreline areas.
- P7 Remnant bushland in Balls Head Reserve.

Views

- P8 The following views and vistas are to be preserved and where possible enhanced:
 - (a) Views from the end of Larkin Street, from the rock outcrop towards the Sydney CBD, Sydney Harbour and Parramatta River.
 - (b) Views through the Area from streets, adjoining open space and nearby residences.
 - (c) Horace Street Lookout (97).
 - (d) Significant public views of Iron Cove and the Parramatta River from Horace Street, Balls Head Road and the cliff top.

Identity / Icons

- P9 Tank cuttings on the former BP site.
- P10 Balls Head Reserve.
- P11 HMAS Waterhen.
- P12 The Coal Loader.
- P13 Carradah Park.
- P14 Berrys Bay.
- P15 Balls Head Bay.

Subdivision

- P16 Generally 1ha or greater.

Streetscape

- P17 Remnant sandstone guttering.
- P18 Informal streetscapes.
- P19 Buildings setback from the boundary with a skewed alignment to respective street frontages.

Public transport

- P20 Development is to take advantage of the high levels of accessibility to public train and bus services.
- P21 Potential to accommodate new ferry services.

10.7.2 Desired Future Character

Diversity

- P1 Maintained and enhanced passive recreational spaces.
- P2 Community based activities within existing recreational spaces.
- P3 Maintaining a working harbour with maritime based industrial, commercial and recreational buildings and structures, hard stand, wharves and jetties, administrative buildings, car parking.
- P4 Future maritime uses having a minimal impact on residential amenity.
- P5 Man-made features such as the tank cuttings on the former BP site are maintained as local landmarks important to the community.



Access

- P6 Enhanced continuous public access along the foreshores.

10.7.3 Desired Built Form

Siting

- P1 Maritime buildings are predominantly located against the cliff with a setback from the cliff for access and maintenance.
- P2 Areas adjacent to the foreshore are generally free of buildings.
- P3 Buildings are sited to avoid obstructing public view corridors to Iron Cove and the lower reaches of the Parramatta River and views to Sydney Harbour.

Form, massing and scale

- P4 Generally 1-3 storeys.
- P5 The height of buildings is generally kept below cliff lines where possible on low terraces adjoining the foreshore.
- P6 Building height is minimised to preserve public and private views.
- P7 Generally monolithic structures.
- P8 Large open areas including landscaping and hard stand areas.
- P9 Buildings follow the natural topography of the land enhancing the scenic qualities of the foreshores.
- P10 Power lines are underground minimising visual clutter.

Colours and Materials

- P11 Modern, lightweight materials.
- P12 Muted colours to minimise the visual impact when viewed from the Harbour.

Roof

- P13 Flat or curved.
- P14 Flat roofs to preserve views where appropriate.
- P15 Buildings on the upper terrace to HMAS Waterhen fronting Balls Head Road have a similar roof form to those on the eastern side of Balls Head Road.

Fences

- P16 Retention of existing sandstone fences and walls.
- P17 Transparent fences (minimum 80%) screened with landscaping.

Car accommodation

- P18 Onsite parking area close to Balls Head Road, to be screened from public view with landscaping.

Traffic management

- P19 A traffic report is to be submitted with all development applications which are likely to result in an increase in vehicular traffic. The report is to demonstrate that:
- (a) additional traffic flows from the development is safely accommodated on existing local streets.
 - (b) the environmental capacity of roads west of the Pacific Highway (including Bay, Crows Nest Hazelbank Roads and Woolcott Street) leading to the Waverton Peninsula has been satisfactorily investigated.

10.7.4 HMAS Waterhen - Upper Terrace



The site is currently used by Defence as a secure car park and storage compound. The community is opposed to the intensification of any other Defence functions onto this site and desires a buffer between the primary Defence functions adjacent to the foreshore and residential development on the opposite side of Balls Head Road. The community would prefer that the site ultimately becomes future public open space to provide that buffer. However, if the site is redeveloped for Defence purposes, the following provisions outline the preferred outcome if the site is redeveloped. The principle aim is to ensure that any development is appropriate in scale to the site and nearby homes.

Diversity

- P1 Low intensity Defence purposes which do not affect the amenity or safety of adjoining residential premises (e.g. defence accommodation).

Siting

- P2 Buildings are to address Horace Street and Balls Head Road.

Setbacks

- P3 Incorporate similar front setbacks to dwellings on eastern side of Balls Head Road.
P4 Incorporate similar separation distances between buildings to dwellings on eastern side of Balls Head Road.

Form, massing and scale

- P5 Maximum of 1 storey.
P6 Small scale consistent with the built form on the eastern side of Balls Head Road.

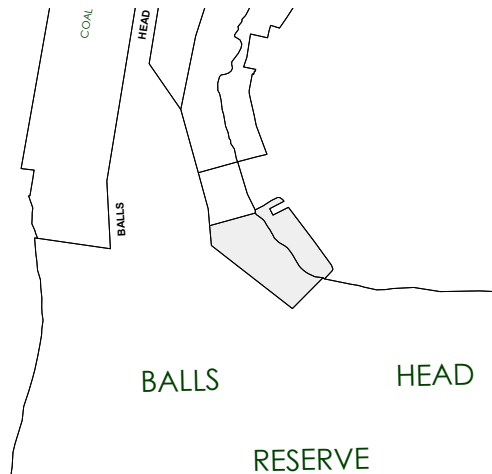
Colours and Materials

- P7 Incorporate similar building materials and colours to the residential buildings on the eastern side of Balls Head Road.

Roof

- P8 Incorporate similar roof form to those on the eastern side of Balls Head Road.

10.7.5 Former Quarantine Depot



Planning Policies

- P1 In accordance with Council's resolution made on 28 May 2012, the principles of the *Waverton Peninsula Masterplan* are to be applied to this site.

Diversity

- P2 Low intensity maritime related industrial, commercial and recreational activities having a minimal impact on residential amenity.
- P3 Community facilities.

Form, massing and scale

- P4 Maintain the scale of existing buildings and structures.
- P5 Any alterations and additions to be minimal in scale.
- P6 No increase to the length of adjacent wharves.
- P7 Reductions in landscaped area will generally not be supported, unless addressing accessibility issues across the site.

Heritage

- P8 Development must conform with the Conservation Management Strategy that applies to the site.

Access

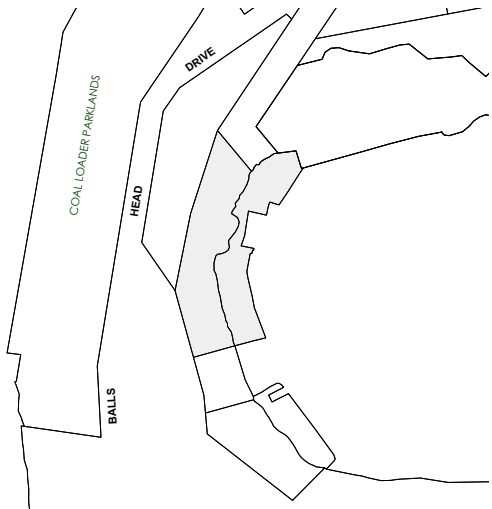
- P9 Public access across the foreshore portions of the site should be encouraged where practical.
- P10 Travel lifts to access the site will not be permitted.
- P11 Public access.

Car parking

- P12 There shall be no increase in on-site car parking provision.



10.7.6 Former Woodleys Boatyard



Planning Policies

- P1 In accordance with Council’s resolution made on 28 May 2012, the principles of the *Waverton Peninsula Masterplan* are to be applied to this site.

10.8 JOHN STREET WATERFRONT NEIGHBOURHOOD



10.8.1 Significant Elements

Land Use

P1 Maritime industrial and commercial activities.

Topography

P2 Generally flat below a large man-made sandstone cliff.

Natural Features

P3 Berrys Bay.

Views

P4 The following views and vistas are to be preserved and where possible enhanced:

(a) views to Sydney Harbour and beyond.

Identity / Icons

P5 Berrys Bay.

P6 Sandstone cliff.

P7 North Shore railway line.

Subdivision

P8 Large consolidated parcels.

Streetscape

P9 Munro street is narrow with no on-street parking and fully paved verges.

P10 John Street is narrow with fully vegetated verges and double rail timber fences.

P11 Buildings built close to the boundary with a low masonry wall and cycle fencing above.

10.8.2 Desired Future Character

Diversity

- P1 Waterfront industrial buildings and structures, hard stand, wharves and jetties, administrative buildings, car parking.
- P2 Future maritime uses having a minimal impact on residential amenity.

Access

- P3 Providing continuous public access along the foreshores of Berrys Bay via railway lands.

10.8.3 Desired Built Form

Form, massing and scale

- P1 Large scale industrial buildings reflective of their foreshore location.
- P2 Building height is minimised to preserve public and private views.
- P3 The height of buildings generally kept below cliff lines where possible on low terraces adjoining the foreshore.
- P4 Large open areas including landscaping and hard stand areas.

Siting

- P5 Buildings are generally located against the cliff with a setback from the cliff for access and maintenance.
- P6 Areas adjacent to the foreshore are generally free of buildings.

Fences

- P7 Open fencing screened with landscaping.

Colours and materials

- P8 Modern, lightweight materials.

Roof

- P9 Flat or curved.
- P10 Flat roofs to preserve views where appropriate.

Car accommodation

- P11 To be screened from public view with landscaping.

10.9 SAWMILLERS NEIGHBOURHOOD



10.9.1 Significant Elements

Land Use

- P1 Residential accommodation.
- P2 Passive and active recreational spaces.

Topography

- P3 Steep falls to the foreshores of Berrys Bay with flat areas directly adjacent to the shoreline.

Natural Features

- P4 Berrys Bay.
- P5 Remnant natural shoreline areas.
- P6 Re-established bushland areas to the foreshore.

Views

- P7 The following views and vistas are to be preserved and where possible enhanced:
 - (a) District views to Sydney Harbour and beyond.

Identity / Icons

- P8 North Shore railway line.
- P9 Sawmillers Reserve.
- P10 Berrys Bay.

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Subdivision

P11 Large consolidated lots to the north of the Area and smaller lots to the south.

Streetscape

P12 Narrow streets with concrete paths on one side of the street.

P13 High stone and masonry fences.

P14 Buildings setback from the boundary with a skewed alignment to respective street frontages.

Public transport

P15 Development is to take advantage of the reasonable levels of accessibility to public train, bus and ferry services.

10.9.2 Desired Future Character

Diversity

P1 Predominantly a mixture of multi dwelling housing, attached dwellings and residential flat buildings according to zone.

P2 Development should not result in any significant increase in dwelling density.

P3 Maintaining and enhancement of passive recreational spaces with continual foreshore access.

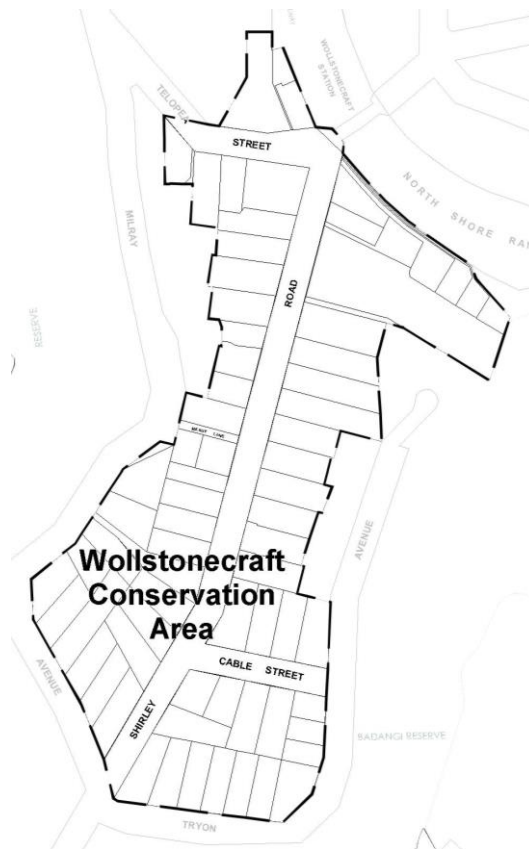
10.9.3 Desired Built Form

Form, massing and scale

P1 Development is to step down with the topography to ensure the retention of views to the waters of Sydney Harbour.

P2 Development to be setback from the foreshore to enable continuous public foreshore access.

10.10 WOLLSTONECRAFT CONSERVATION AREA



10.10.1 History

The Wollstonecraft Conservation Area was part of the land granted to Edward Wollstonecraft in 1825 and subsequently known as the Berry Estate. Berry and Wollstonecraft were business partners and related by marriage.

The Wollstonecraft peninsula was one of the last areas of the Wollstonecraft/Berry Estate to be developed. It was subdivided from 1912 and the prime building land, near the railway, was developed with large Federation style homes.

The major phase of development was after World War One, when Federation or Californian Bungalow and Inter-war residential flat building styles were common. A number of uncharacteristic residential flat buildings and multi-dwelling housing were built on the fringes of the peninsula after World War II when further subdivisions along Milray and Tryon Avenues occurred.

10.10.2 Description

The Wollstonecraft Conservation Area is part of the Wollstonecraft peninsula that lies between Gore Cove and Balls Head Bay. The area has bushland reserve to the east, south and west and is separated from the areas to the north by the North Shore railway line.

Shirley Road runs along the ridge and the landform falls to the south and away to east and west off the ridge. Shirley Road and Milray Avenue incorporate level changes across their width. The subdivision pattern is irregular and partly determined by the topography with no rear lanes. Milray and Tryon Avenues are subordinate circuit streets that run with the contours around the peninsula.

The Conservation Area is characterised by detached residential development on large garden lots in a mix of Federation, Federation Queen Anne, Arts and Crafts, Olde English and

Section 10 - Waverton / Wollstonecraft Planning Area


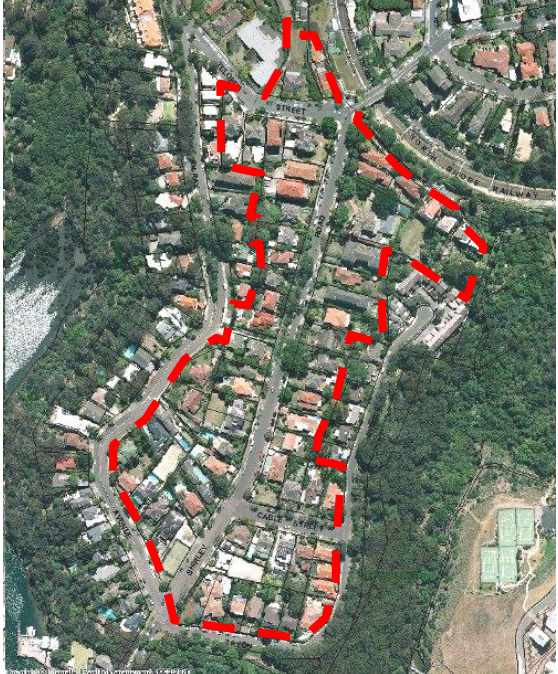
Californian bungalow styles and, interspersed with some Inter-war art deco residential flat buildings. The houses are typically large, and set well back from the street in established gardens, some with tennis courts. Most are on original lots. Houses to the high side of the street are often set on plinths.

Street verges are typically 3.5 m wide and include grass and concrete footpaths and crossings to off street parking. There is low shrub and tree planting in the street reservation and on the grass verge. The street lines are reinforced by the stone and brick fences that follow the slope of the streets.

The vista along Shirley Road terminates dramatically at Berry Island, an area of regenerated bushland. There are also expansive views of Sydney Harbour and Berry Island from the end of Shirley Road.

Planting in front gardens contributes to the landscape.

Uncharacteristic elements include modern residential flat buildings; over-scaled, two storey additions; over-scaled and poorly detailed carports and garages; front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; rendered high walls and fences to the street.

	
<p>Figure F-10.2 (above): Circa 1943</p>	<p>Figure F-10.3 (above): Circa 2008</p>

10.10.3 Statement of Significance

The Wollstonecraft Conservation Area is significant:

- (a) As a substantially intact residential subdivision from the early 20th century that retains much of its urban detail and fabric in gardens, street fencing and use of sandstone.
- (b) As a consistent and largely intact early 20th century residential area with a mix of high quality buildings and particularly the Federation and Inter-War housing on large lots.
- (c) For its unity that is derived from its subdivision history and which is evident in the development and built form.



10.10.4 Significant elements

Topography

- P1 Peninsula form, falling away on either side of the ridge to Gore Cove and Balls Head Bay.

Subdivision

- P2 Rectangular blocks with narrowest boundary to street.

Streetscape

- P3 Changes in level, split road and path formations. Sandstone and brick street fences reinforce road layout. Post and rail fencing.

Views

- P4 Tryon Avenue lookout. Views along Shirley Road and from the lower end over Berry Island and the harbour. Views through bush to the harbour from Cable Street and Tryon Avenue. Slot views to the harbour and foreshore over and between buildings.

10.10.5 Characteristic buildings

- P1 Single storey and two storey dwelling houses on garden lots.
P2 Inter-war residential flat buildings.

10.10.6 Characteristic built elements

Siting

- P1 Middle of lot (or slightly forward), generous front and side gardens with trees, lawns and shrubs.
P2 Consistent setbacks.

Form, massing and scale

- P3 Single and two storey with hipped and gabled roofs with rear extensions.
P4 Reduced height and scale to rear.
P5 Complex massing, roof form and detail to larger houses.
P6 Strong skyline of pitched roofs and chimneys visible from street and stepped along the streets.

Roofs

- P7 Pitched and hipped between 30 and 45 degrees with slate and terracotta tiled roofs, without dormers or openings that can be seen from the street.
P8 Gabled ends for projecting bays to the street.
P9 Skillion roofs to rear extensions.
P10 Brick and rendered chimneys with terracotta chimney pots.

External Materials

- P11 Sandstone, face brick, roughcast render sandstone foundations.
P12 Textured brick to Inter war residential flat buildings.
P13 Slate and terracotta tiled roofs.
P14 Timber windows, doors and joinery in a Federation, Edwardian and Inter War style.
P15 Leadlight windows. Original front and side garden landscaping.

Fences

- P16 Original low front fences.



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P17 Low sandstone walls, timber pickets, timber rails and mesh, pipe and mesh gates, original face brick with piers.

Car accommodation

P18 Side drives with garages and parking located behind the building line.

10.10.7 Uncharacteristic elements

P1 Over-scaled additions; Carports and garages to front of lot; dormers and rooflights to front and side roofs; removal of original details, painting and rendering of face brickwork; high walls and fences to street; inappropriate fence details, paving of gardens

10.11 CROWS NEST ROAD CONSERVATION AREA



10.11.1 History

The Crows Nest Road Conservation Area was part of the land granted to Edward Wollstonecraft in 1825 and subsequently known as the Berry Estate when it passed to Alexander Berry. Wollstonecraft named the estate Crows Nest Farm and built a large house there, Crows Nest House. The Conservation Area extends across what was the 1890s curtilage of the 1850s residence.

Parts of the estate and the house were bought by Sir John Hay in 1897 and parts of the estate were subdivided. The subdivisions progressively alienated the land around the house. Crows Nest House dwelling remained the residence of Hay and then his widow, Lady Jessie Hay.

Residential building covenants applied over the subdivision proscribing siting, type and quality of houses to be approved by the Trustees of the Estate or their architects. Lady Hay's will made provision for a school, shown on the 1932 subdivision plan and now known as the North Sydney Demonstration School.

The final subdivision occurred in 1934 after the death of Lady Hay and Crows Nest House was demolished. A land parcel known as the "Branthwaite Estate" was subdivided in 1935 separately to the rest of the Crows Nest House grounds.

10.11.2 Description

The Crows Nest Road Conservation Area is a residential area characterised by medium to large detached houses on garden lots and is bounded to the east by the Pacific Highway, the multi-storey units to the north and the North Sydney Demonstration School to the south.

The area is relatively level, sloping slightly from the ridge of the Pacific Highway towards the west, though the lower end of Crows Nest Road is terraced. Street layout and subdivision is determined by the Estate boundaries with a linear pattern of large, regular and rectangular lots with wide street frontages and no rear lanes.

The area is characterised by its single and two storey individual (often architect designed) houses in a range of Inter-war styles intended to be viewed from at least three sides. Facades are modulated by strong, recessed and projecting geometrical forms consistent with each building style. The house styles are unified by the regular subdivision pattern, building materials, massing and setbacks and by the high quality of construction and design. The earlier houses to the periphery tend to be smaller and less distinct.

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Street verges are typically 3.5m wide grass nature strips and concrete footpaths with crossings for off street parking. Hazelbank Road is on the slope and houses to the high side of the street are set above the footpath.

Introduced and native species are planted on grass verges and within roads, most dating from the 1920/1930 subdivision, forming avenues that unify the streetscape. The large plane trees planted along Hazelbank Road are among the most significant of the area.

Front gardens contribute to the landscaped character of the area with large areas of lawn, introduced plant species, and straight stone-flagged or brick paved paths leading to the house. Gardens are often open at street level or above low retaining walls.

Uncharacteristic elements include over-scaled, two storey additions; over-scaled and poorly detailed carports and garages to the street; front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street.

	<p>Figure F-10.4 (left): Circa 1890</p> <p>Figure F-10.5 (below left): Circa 1943</p> <p>Figure F-10.6 (below): Circa 2008</p>
	

10.11.3 Statement of Significance

The Crows Nest Road Conservation Area is significant:

- (a) As an excellent example of Inter-war garden suburb planning and subdivision for non-speculative residential development.
- (b) For its rarity in the locality as a largely intact and consistent residential area from the 1920 – 1930 eras that retains a fine range of Inter-war houses of one and two storeys, built on large lots with intact gardens and single garages.
- (c) For its strong landscape qualities that combine from the established trees, grassed verges and large open front gardens that provide unity to the streetscape.
- (d) For its association with Crows Nest House, and potential for evidence of the site of the historic house.

10.11.4 Significant elements

Topography

P1 Generally level, sloping away from Pacific Highway to the west.

Subdivision

P2 Detached dwelling houses: approximately 800m² lots with 18m to 22m frontage.

P3 Large rectilinear lots, narrowest boundary to street frontage.

Streetscape

P4 Street trees planted in verges and carriageways, mainly Plane Trees, Brushbox and Melaleucas.

P5 Low retaining walls of sandstone and face brick, stepped in height following changes in street level.

P6 Deep grass verges, concrete paths and concrete kerbs.

Views

P7 Street vistas along Hazelbank Road, Crows Nest Road, McHatton Street and Bay Road.

P8 Slot views to harbour along Mc Hatton Street.

10.11.5 Characteristic buildings

P1 Single storey Californian Bungalows, Arts and Crafts and Queen Anne Revival houses, and houses on 1921 subdivision.

P2 Single and two storey houses of Old English, Georgian Revival, Mediterranean, Art Deco and International styles on the later subdivision.

10.11.6 Characteristic built elements

Siting

P1 Dwellings set well back from the front of the block.

P2 Principal building frontage oriented to and parallel to the street alignment.

P3 Consistent setbacks.

Form, massing and scale

P4 Single and two storey buildings with hipped and gabled roofs.

P5 Reduced height and scale to the rear.

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- P6 Dwelling houses of individual design, well modulated on front and side facades with recessed and projecting geometric forms, such as bays, verandahs and cantilevered balconies, consistent with architectural style of the house.

Roofs

- P7 Hipped and gabled with low pitch for Californian Bungalow and Mediterranean styles, steeper pitch (35-45 degrees) for Georgian Revival and Old English styles without dormer or openings viewed from street.
- P8 Flat roofs concealed behind parapet walls for International styles.
- P9 Face brick chimneys.

Materials

- P10 Sandstone bases; face brick; original rendered walls.
- P11 Slate; glazed terracotta tiled roofs.
- P12 Timber windows, doors and joinery.
- P13 Original front garden landscaping stone-flagged driveways and flagging paths.

Windows and doors

- P14 Narrow vertical windows and doors within solid masonry walls.
- P15 Entrance doors with porches or porticos contained under a separate roof with arched openings.
- P16 Casements or double-hung sash windows (except International style houses).
- P17 Timber window shutters for the revival house styles of old English/Tudor and Georgian.
- P18 Horizontal steel windows and doors for International style houses.
- P19 Stripped and relatively heavy Inter-war detailing around window arrangements, the front door and gable ends, such as wrought iron grills to windows, masonry balustrades.

Fences

- P20 Low sandstone and brick walls.

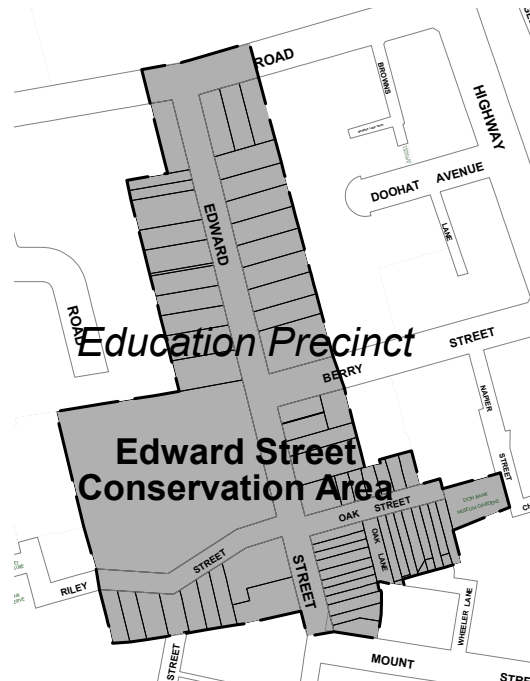
Car accommodation

- P21 Located to side drives.
- P22 Garages and porte-cocheres set back behind the front building line.

10.11.7 Uncharacteristic elements

- P1 Over-scaled two storey additions; over-scaled and poorly detailed carports and garages to the street; front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street.

10.12 EDWARD STREET CONSERVATION AREA



10.12.1 History

The Edward Street Conservation Area includes land from *The Priory* and *Rockleigh Grange*, both of which were sold from the Wollstonecraft Estate in the 1840s. It also includes parts of a later (1850s) subdivision by Alexander Berry, a partner of Edward Wollstonecraft.

The 1850s subdivision was designed for small township building blocks and various small scale dwelling houses and attached dwellings were built at the southern end of Edward Street as a result.

Blocks at the northern end of Edward Street were purchased and consolidated by Capt. RF Pockley who built "Doohat". The three large properties were sold during the 1880s and sections of the estates subdivided and sold during the 1890s. Residential development then followed in the early 20th century.

10.12.2 Description

The Edward Street Conservation Area runs each side of Edward Street with larger areas to the south including the Catholic University site. It is bounded to the east by high rise along the Pacific Highway, the North Sydney School to the north and by the steep slope to the west.

Edward Street runs along the top of a ridge and is flat to the east falling steeply to the west along Riley Street. Subdivision is determined by the street layout and topography with rectangular and irregular lot sizes, some developed for attached housing with no rear lanes.

The area is characterised by each phases of development and groupings of identical rows of houses. The early phase of development is represented by the Don Bank Group; small scale, attached, single storey weatherboard and brick houses in the Victorian Georgian and Italianate style. These buildings create an intimate 19th century atmosphere and context for the Victorian Filigree style developments at the southern end and the later detached dwellings at the northern end in the Federation Queen Anne and Inter War styles. There is some two storey Victorian and Federation attached dwellings houses and residential flat

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buildings and later infill developments. The Catholic University occupies the west of the conservation area and contains *Rockleigh*, modern buildings and car parking.

Street verges are 3.5 metres wide to Edward and Berry Street with grass and concrete footpaths and crossovers for parking. Houses to the high side of the street are set on brick and sandstone plinths and the houses to Riley Street are set on elevated sites with high, sandstone retaining walls to the street.

There are views from Edward Street to the CBD and to the west to the Harbour. The axial view north along Edward Street looks directly to the stone gateway of the original Lady Hay’s Estate.

The land steps across Edward Street with a high side to the east with houses and a low side to the west and there are low street trees. Front gardens contribute to the landscaping of the street and there are remnant trees in the former *Rockleigh Estate*.



10.12.3 Statement of Significance

The Edward Street Conservation Area is significant:

- (a) As an intact example of early township development in North Sydney.



- (b) As a subdivision of the *Priory* and *Rockleigh Grange* Estates.
- (c) For its grouping of intact and commonly detailed mid Victorian and Federation houses.

10.12.4 Significant elements

Topography

- P1 Top of ridge, flat to the east falling away to west.

Subdivision

- P2 Rectilinear subdivision with narrower boundary to street frontage.
- P3 Irregular lots to slope.

Streetscape

- P4 Split streets and changes in level.
- P5 Double rail timber fences.
- P6 Strong skyline of simple pitched roofs and tall chimneys without dormers or openings to the street.

Views

- P7 Street views along Edward and Riley Streets.
- P8 Slot views over and between buildings to western Sydney from Edward Street.

10.12.5 Characteristic buildings

- P1 A mixture of:
- (a) Single storey semi-detached and attached Victorian Georgian and Filigree dwelling houses.
 - (b) Single storey Federation and Queen Anne style detached dwelling houses to common detail.
 - (c) Two storey Federation residential flat buildings.

10.12.6 Characteristic built elements

Siting

- P1 Located forward on the lot with large rear gardens.
- P2 Consistent setbacks.

Form, massing and scale

- P3 Single storey detached, semi-detached and attached dwellings with projecting bays and verandahs to the street.
- P4 Skillion rear wings.
- P5 Reduced height and scale to rear.
- P6 Dwellings in groups of identical design (detached, semi-detached and attached).

Roofs

- P7 Gabled and hipped between 30 and 45 degrees.
- P8 Gable ends to side.
- P9 Skillion roofs to rear extensions. Brick and rendered chimneys.

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External Materials

- P10 Face brick, original rendered masonry walls.
- P11 Slate, corrugated metal, terracotta tiles roofs timber joinery on sandstone base course.
- P12 Timber windows with original coloured glass and leadlight, doors and joinery.
- P13 Cast iron lacework.
- P14 Original front garden landscaping.

Windows, doors and joinery

- P15 Victorian, Federation and Edwardian. Timber.

Fences

- P16 Original low front fences.
- P17 Timber fences to rear.
- P18 Sandstone plinths, sandstone piers, metal palisade and gates, timber pickets, original face brick with piers.

Car accommodation

- P19 To street with paved hardstand areas. No garages or carports in front of building line.

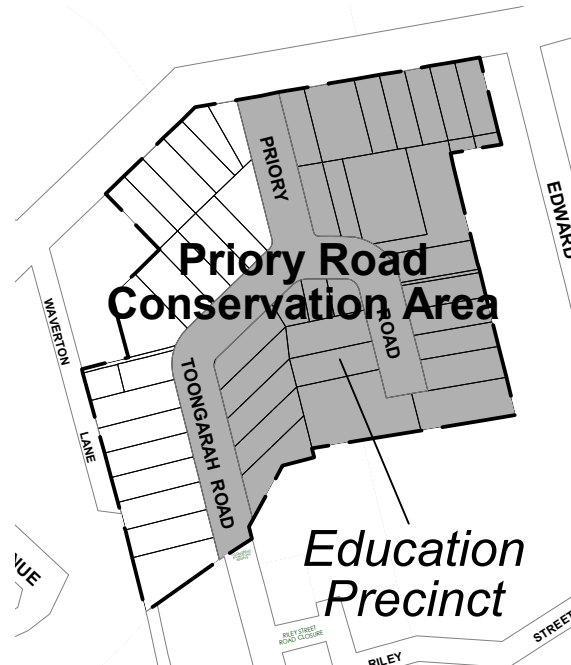
10.12.7 Uncharacteristic elements

- P1 Uncharacteristic elements include modern, multi-storey commercial buildings; over-scaled, two storey additions; carports and garages in front setback, over-scaled and poorly detailed carports and garages; paving of front setback for car parking, front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street.

10.12.8 Future character and built elements

- P1 A vibrant and engaging Education Precinct with safe pedestrian networks, high quality built form and a range of formal and informal public spaces for students to study, socialise and engage with the local community.
- P2 Educational establishments must be oriented and designed to address, surveil and activate public domain areas including streets, pedestrian links, laneways and public spaces.
- P3 Cluster education-related development along Edward Street between Berry Street and Oak Street.
- P4 Where educational establishments provide night classes, these activities should be concentrated along Napier Street and Berry Street, within easy walking distance of public transport.
- P5 Provide a range of high quality, purpose-built student accommodation in and around the Education Precinct, within 400m to a tertiary institution, 800m to a railway station, and 400m to a bus stop used by a regular bus service.
- P6 Streetscape improvements within the Education Precinct occur in accordance with the North Sydney Centre Public Domain Strategy and the Education Precinct Public Domain Masterplan.

10.13 PRIORY ROAD CONSERVATION AREA



10.13.1 History

The Priory Road Conservation Area was part of the land granted to Edward Wollstonecraft in 1825 and subsequently known as the Berry Estate. Berry and Wollstonecraft were business partners and related by marriage.

Colonel George Barney (Commander of Royal Engineers) leased the Priory Estate from Berry in the mid-1830s. In 1843 he purchased an extra one and a half acres, which included "Toongarah", forming an estate of about 10 acres.

"The Priory" was built in the 1840s at a time when the north shore was becoming more attractive as a residential location for the mansions built between the 1830s and the 1860s

In 1913 the estate was subdivided into 40 lots forming Toongarah Road and Priory Road. At this time both of the original dwelling houses, "Toongarah" and "The Priory" were extant. Further subdivisions occurred in the 1920s and 1930s.

"Toongarah" was purchased by SCEGGS school in 1917. The land was subdivided in 1939 into six blocks, and "Toongarah" was demolished. The original "Priory" building was incorporated into flats.

10.13.2 Description

The Priory Road Conservation Area is a small residential area bounded by Bay Road and the escarpment and reserve to the end of Toongarah Road.

The landform falls to the south with the southern end of strongly defined by the cliff. The subdivision pattern is determined by the wide, horseshoe shaped road and their being cul de sacs. Lot sizes are irregular though generally rectangular. The road system has been affected by the topography with Priory and Toongarah Roads falling down the slight hill and finishing at dead ends. Both ends are relatively wide.

The area is characterised by detached dwelling houses on medium sized lots and a few residential flat buildings on larger lots. The majority of buildings are single storey and were built between 1915 and 1928 soon after subdivision in the Inter-war Californian Bungalow style. There are a few Federation era buildings, 1930s flats and several buildings from the

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1940s built after the subdivision of the site of “Toongarah”. There are modern infill residential flat buildings.

Streets verges are typically 3.5 m wide and include grass with concrete footpaths and kerbs. Street trees and plantings in the private gardens are important to the character of the area. The area is heavily planted with trees.

There are views to trees and rooftops of the Bank Street valley, and to the Sydney CBD and long views from the lookout at the end of Toongarah Road.

Uncharacteristic elements include modern residential flat buildings; modern dwelling houses; over-scaled, two storey additions; over-scaled and poorly detailed carports and garages; front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; garages and car ports to the street; high walls and fences to the street.

The materials used in the Conservation Area are consistently dark face brick, unglazed terracotta tiled hipped and gabled roofs, and timber framed casement windows. Front fences are generally low and typically in brick to match the house style. Priory Road also features some sandstone retaining walls. The setbacks from the street allow for small front gardens.

	<p>Figure F-10.10 (left): Circa 1890</p> <p>Figure F-10.11 (below left): Circa 1943</p> <p>Figure F-10.12 (below right): Circa 2008</p>
	



10.13.3 Statement of Significance

The Priory Road Conservation Area is significant:

- (a) As a subdivision of the 1840s curtilage of the early mansions, "The Priory" and "Toongarah".
- (b) As a largely intact and consistent early 20th century residential area that has a mix of Inter-war buildings, including bungalows and residential flat buildings in original condition.
- (c) For its quiet landscape setting derived from the mature street plantings, grassed verges, small open front gardens, low front fences and backdrop of trees in the valley below.
- (d) For its contained character derived from the cul-de-sac street pattern, the topography and the consistency of building form and materials.

10.13.4 Significant elements

Topography

- P1 Sloping southwards to the cliff edge.

Subdivision

- P2 Detached dwelling houses: 380m² lots with 10m frontage.
- P3 Detached, semi-detached houses and attached houses: 180m² to 260m² lots with 4-6m frontages.
- P4 Generally rectangular blocks with irregular shaped blocks on the corners and along the eastern side of Toongarah Road.

Streetscape

- P5 Street trees planted in verges, mainly small eucalypts, Plane Trees and paperbarks.
- P6 Pedestrian linkages from Priory Road to Edward Street, and from Toongarah Road to Waverton Lane.

Views

- P7 Views of trees and houses in the valley below, and limited views to the tops of the higher buildings in North Sydney, mostly from private domain.
- P8 Views from Toongarah Street lookout over the valley to Sydney CBD.

10.13.5 Characteristic buildings

- P1 Detached, single storey inter-war Californian bungalows on garden lots.
- P2 1930s/40s residential flat buildings.

10.13.6 Characteristic built elements

Siting

- P1 To the front of block.
- P2 Dwellings on the eastern side of Toongarah Road are oriented to side boundaries.
- P3 Consistent setbacks.

Form, massing and scale

- P4 Single storey with hipped and gabled roofs with skillion rear extensions.
- P5 Reduced height and scale to rear.
- P6 Open verandahs to front.

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Roofs

- P7 Hipped and gabled, pitched between 30 and 40 degrees without dormers to front and sides. Brick and rendered chimneys with terracotta chimney pots.

External Materials

- P8 Sandstone bases, face brick walls, wall shingles, original render and roughcast render. Textured and dark brick to residential flat buildings.
- P9 Slate, terracotta tiles, asbestos cement sheet, corrugated metal roofs.
- P10 Original timber windows, doors and decorative joinery.
- P11 Original front garden landscaping.

Windows, doors and external joinery

- P12 Federation and Inter-war casements and double hung sash. Timber.

Fences

- P13 Low timber pickets, timber rails and mesh original face brick with piers.

Car accommodation

- P14 Located off street with crossovers and garages and carports at side or rear. No garaged or carports in front of building line.

10.13.7 Uncharacteristic elements

- P1 Uncharacteristic elements include modern, multi-storey commercial buildings; over-scaled, two storey additions; carports and garages in front setback, over-scaled and poorly detailed carports and garages; paving of front setback for car parking, front and side dormers and rooflights; removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street.

10.13.8 Future built elements

- P1 Provide a range of high quality, purpose-built student accommodation in and around the Education Precinct, within 400m to a tertiary institution, 800m to a railway station, and 400m to a bus stop used by a regular bus service.
- P2 Educational establishments must address and activate public domain areas including streets, pedestrian links, laneways and public spaces.

10.14 BAY ROAD CONSERVATION AREA



10.14.1 History

The Bay Road Conservation Area was part of the land granted to Edward Wollstonecraft in 1825 and subsequently known as the Berry Estate when it passed to Alexander Berry, a business partner and related by marriage.

The Conservation Area is adjacent to Waverton Station, which opened in 1893 as part of the St Leonard's to Milson's Point line.

The section of Bay Road was originally known as Crows Nest Road as it gave access to the Crows Nest Estate from Berrys Bay.

The western side of Bay Road was subdivided from 1883-1888. It was part of land owned by the Colonial Sugar Refining Company which operated a sugar works, one of the earliest industries along the foreshore. The Sugar Company Estate included Ross Street, Whatmore Street, Tunks Street and McKye Street.

The oldest building in the Conservation Area appears to be the Victorian cottage behind the Uniting Church at 75 Bay Road. The buildings at 116, 118 and 122 Bay Road are part of an area that was subdivided in 1910.

10.14.2 Description

The Bay Road Conservation Area is located at the end of Bay Road and is bounded to the east by the North Shore railway line.

The landform is relatively flat with the land rising to the north east and falling to the south west. It has an enclosed character due to the topography, being located at the base of two hills, and from the narrowing of the street. At the end of Bay Road there are views of Balls Head Bay and the Balmain peninsula beyond.

The subdivision pattern is regular but follows the curve of the street with tapering lots to the south.

The area has a village atmosphere focusing on Waverton Railway Station.

Characteristic buildings in the area are single storey, semi-detached dwelling houses dating from the Federation period. The area also contains a small strip of Federation period commercial buildings.

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There are some modern intrusions on the eastern and western side of Bay Road, and some contemporary residential flat buildings.

Street plantings reinforce the village-like atmosphere of the area.

	<p>Figure F-10.13 (left): Circa 1890</p> <p>Figure F-10.14 (below left): Circa 1943</p> <p>Figure F-10.15 (below right): Circa 2008</p>
	

10.14.3 Statement of Significance

The Bay Road Conservation Area is significant:

- (a) As a Federation period pocket of commercial and residential development, and one of the few intact areas of early development in the Waverton area.
- (b) For its enclosed village-like character that is a function of the topography, narrowing of the street.

10.14.4 Significant elements

Topography

P1 At the base of two hills, falling towards Balls Head Bay.



Subdivision

- P2 Regular subdivision with tapering lots to the south and irregular shaped blocks at the corners.

Streetscape

- P3 Dwellings raised above street level north of Whatmore Street corner. Higher footpath level north of Whatmore Street, with winding steps to street level.
- P4 Widened footpath at shops with outdoor seating.
- P5 Street trees on footpath and adjacent to narrowed road section and traffic calming devices.

Views

- P6 Views to Balls Head Bay from southern end of road.

10.14.5 Characteristic buildings

- P1 A mixture of:
- (a) single storey Federation detached and semi-detached dwelling houses on garden lots.
 - (b) two storey, Federation commercial buildings with parapets.

10.14.6 Characteristic built elements

Siting

- P1 Detached dwelling houses located towards the front of the block and addressing the street but leaving a small rear yard.
- P2 Shops built to street boundary.
- P3 Consistent setbacks.

Form, massing and scale

- P4 Single storey, detached dwelling houses with simple forms articulated with projecting bays and verandahs to front and a reduced scale and bulk to the rear.
- P5 Two storey commercial buildings with parapets and clearly defined "front" of building, oriented to the street.

Roofs

- P6 Hipped and gabled between 30 and 45 degrees with overhanging eaves. Skillion roofs to rear wings. Parapets on shops. Variety of chimney designs.

External Materials

- P7 Face brick on sandstone bases, roughcast render. Terracotta tiles with corrugated metal on rear wings.

Windows and doors

- P8 Federation style. Lead light decorative glazing. Timber.

Fences

- P9 Low height front fences in a variety of materials – timber, stone, iron palisade.
- P10 Timber fences to side and rear.

Car accommodation

- P11 No garages or carports in front of the building line.



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10.14.7 Uncharacteristic elements

- P1 Carports to front of lot; dormers to front of roofs; over-scaled additions; painting and rendering of face brickwork; removal of original details, alterations to original roof forms high fences to street; inappropriate fence details.
- P2 Modern infill development.



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