

AGENDA

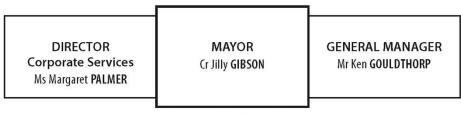
Council Chambers 14 May 2020

The **3731**st **MEETING** of **COUNCIL** will be held at the Council Chambers, 200 Miller Street, North Sydney at **6:30 pm** on Monday 18 May 2020. The agenda is as follows.

KEN GOULDTHORP GENERAL MANAGER

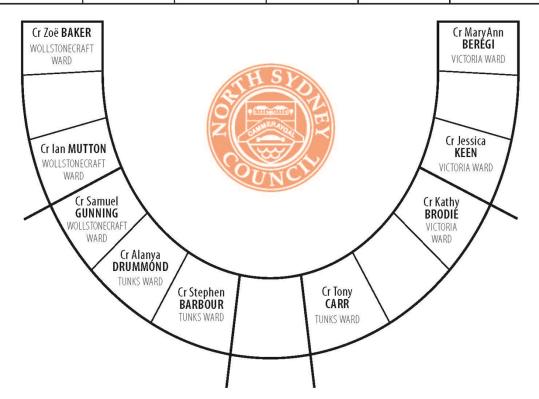
North Sydney Council is an Open Government Council. The records of Council are available for public viewing in accordance with this policy, with the only exception being made for certain confidential documents such as legal advice, matters required by legislation not to be divulged, and staff matters.





COUNCIL OFFICERS

DIRECTOR	DIRECTOR	DIRECTOR	DIRECTOR	MANAGERS	MANAGERS
Open Space &	Community &	Engineering &	City Strategy		
Environmental	Library Services	Property Services	Mr Joseph HILL		
Services Mr Robert EMERSON	Mr Martin ELLIS	Mr Duncan MITCHELL	mi sosepii iii L		



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1.	Opening Meeting
2.	Acknowledgement of Country
3.	Apologies and Applications for Leave of Absence by Councillors
4.	Confirmation of Minutes
5.	Disclosures of Interest

6. Mayoral Minutes

MAYORAL MINUTE

6.1. North Sydney Olympic Pool and Easing of COVID-19 Restrictions

North Sydney Olympic Pool (NSOP) was closed on 26 March 2020 in accordance with the Public Health Order mandating the closure of swimming pools. As a result of the forced closure, advice at the time that the measures were expected to be in place for at least 6 months, the deteriorated structural condition of the eighty-four (84) year old NSOP facility and its essential redevelopment scheduled to commence later this calendar year, the General Manager correctly brought forward redundancy/redeployment arrangements for all NSOP permanent staff.

On Friday 8 May 2020, the Prime Minster advised that, "Australians had earned an early mark" from the success of flattening the curve and announced a three-stage strategy to easing restrictions:

- Step 1 will focus on carefully reopening the economy and giving Australians an opportunity to return to work and social activities including gatherings of up to ten (10) people, up to five (5) people in the family home and some local and regional travel.
- Step 2 builds on this with gatherings of up to twenty (20) people and more businesses re-opening, including gyms, beauty services and entertainment venues like galleries and cinemas:
- Step 3 will see transition to COVID safe ways of living and working with gatherings of up to one hundred (100) people permitted. Arrangements under Step 3 will be the "new normal" while the virus remains a threat. International travel and mass gatherings over one hundred (100) people remain restricted.

Under the three-step plan, State Governments will continue to make decisions on individual circumstances and local conditions. It is envisaged that there will be at least a month between steps. The Premier subsequently announced that Step 1 will commence in NSW from Friday 15 May.

Under Step 1, outdoor swimming pools can open with severe restrictions including meeting the Step 1 gathering limitations of a maximum of ten (10) people at the facility, toilets/amenities remaining closed and no more than one (1) person per swimming lane. The restrictions, through to and including Step 3, where gatherings will still be limited to one hundred (100) people, are envisaged to continue to severely restrict the normal use of outdoor facilities such as NSOP.

Throughout the COVID-19 challenges North Sydney Council has continued to ensure that public facilities are available to the maximum extent possible and avoided self-imposing additional restrictions beyond that required by Public Health Orders. Indeed, I am proud of the staff, who under the leadership of the General Manager and executive team, have continued to provide full customer services and maintained productivity with a roster arrangement enabling a mix of work from home and work from the office. The organisation has proven to be nimble and innovative in altering its work practices while continuing to provide normal services together with a range of additional services in the new world of online/remote services initiated by COVID-19.

Council remains committed to the essential redevelopment of the eighty-four (84) year old NSOP facility commencing towards the end of 2020. In the interim, I would like to continue in the manner we have throughout COVID-19 and provide restricted access for lap swimming in line with the restrictions applied by the Health Authorities. This would be facilitated, initially, through a pre-booking/registration arrangement.

Prior to commencing lap swimming it will be necessary to complete the twelve (12) monthly structural engineering review of the pool shell to ensure public safety. I've been advised that this inspection can be completed within a week, all things going well, at a cost of \$9,800. There will also be an ongoing operational cost to Council of this temporary arrangement with heating of the pool alone costing in the order of \$500 per day. Nevertheless, I believe it is important for community well-being and mental health that Council continues to provide recreational facilities to the extent practicable with COVID-19 restrictions.

The Mayor of Lane Cove Council has confirmed that the redevelopment of their 50m pool is expected to be completed by the end of September. This timeline should work nicely for pool users as when the NSOP is closed for redevelopment (expected around the end of September), pool users will have an alternative option to continue their swimming.

I therefore recommend:

- **1. THAT** Council undertake the 12-monthly structural engineering review of the pool shell and subject to the results of the review not identifying significant further deterioration, the North Sydney Olympic Pool outdoor pool be made available for lap swimming, initially on a pre-booking basis.
- **2. THAT** Council continue to progress the redevelopment of NSOP as a priority in order to address the deteriorated condition of the Facility and return it to full operation as quickly as possible, post COVID-19.

COUNCILLOR JILLY GIBSON MAYOR

- 7. Reports of Committees
- 7.1. Governance and Finance Committee Minutes 11 May 2020

AUTHOR: Ian Curry, Manager Governance & Committee Services

ENDORSED BY: Carrie Chan, Executive Governance Manager

ATTACHMENTS:

1. Minutes - Governance & Finance Committee - 11 May 2020 [7.1.1 - 6 pages]

PURPOSE:

Council is required to consider the Minutes of this Committee under the Code of Meeting Practice.

EXECUTIVE SUMMARY:

This report presents the recommendations of the last meeting of the Governance and Finance Committee held on 11 May 2020 for Council adoption. The minutes are attached for information.

FINANCIAL IMPLICATIONS:

The financial implications of each item are discussed in the individual reports.

RECOMMENDATION:

- **1. THAT** the 2020 Customer Satisfaction Survey Findings be received (*GF01*).
- **2. THAT** Council recognises that the positive result of the Survey could not have been achieved without the efforts of staff. *(GF01)*
- **3. THAT** Council note the requirements of the IPC Guideline No. 1 and OLG Circular 19-21, issued in respect of returns of interests by Councillors and designated staff. (GF02)
- **4. THAT** the Returns of Interest by Councillors be published on Council's website. (GF02)
- **5. THAT** the Quarterly Review of the Operational Plan 1 January to 31 March 2020 be received. *(GF03)*
- **6. THAT** Council notes the remedial action for those projects that were not on track for the period ending 31 March 2020. *(GF03)*
- 7. THAT the Quarterly Budget Review Statement March 2020 report be received. (GF04)
- **8. THAT** the revised 2019/20 budget be adopted. (GF04)
- **9. THAT** the amended Operational Plan & Budget Preparation Timeline be adopted. (GF05)
- **10. THAT** the Council 2020 Schedule of Meetings be amended to provide for an additional Governance & Finance Committee meeting on 1 June 2020. *(GF05)*

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 5. Our Civic Leadership
- 5.1 Council leads the strategic direction of North Sydney

BACKGROUND

In accordance with Council's Code of Meeting Practice:

20.24 The minutes of meetings of each Committee of the Council must be confirmed at a subsequent meeting of the committee.

The Charter of the Governance and Finance Committee states:

11.3 The recommendations arising at Committee meetings are to be submitted to the next available Council meeting for adoption. Any Councillor, irrespective of whether that Councillor is a member that Committee may be the mover or seconder of such recommendations.

CONSULTATION REQUIREMENTS

Community engagement is not required.

DETAIL

This report presents the recommendations of the last meeting of the Governance and Finance Committee held on 11 May 2020 for Council adoption. The minutes are attached for information.

Minute Book Page No 7

NORTH SYDNEY COUNCIL

REPORT OF GOVERNANCE & FINANCE COMMITTEE

SUBJECT: PROCEEDINGS OF COMMITTEE AT MEETING HELD IN THE SUPPER ROOM AT THE COUNCIL CHAMBERS, NORTH SYDNEY, ON MONDAY 11 MAY 2020 AT 6.00PM.

PRESENT

Councillor Gibson in the Chair, Councillors Barbour, Brodie, Drummond, Keen and Mutton.

Staff: Ken Gouldthorp, General Manager

Margaret Palmer, Director Corporate Services Marcelo Occhiuzzi, Acting Director City Strategy Darren Goode, Manager Accounting Services

Jenny Gleeson, Manager Integrated Planning & Special Projects Ian Curry, Manager Governance and Committee Services

Visitors: James Parker and Christine Dening (Jetty Research, Item 1)

Apologies were received from Councillor Tony Carr.

At the commencement of business (6.34pm) those present in the Supper Room were:

The Mayor, and Councillors Drummond and Mutton.

Those participating remotely were:

Councillors Barbour, Brodie and Keen.

Declarations of Interest

Nil.

6. Minutes

The Minutes of the previous meeting held on 10 February 2020, copies of which had been previously circulated, were taken as read and confirmed.

The Motion was moved by Councillor Brodie and seconded by Councillor Barbour.

This is Page No 7 of the Minutes of the Governance & Finance Committee Meeting of the North Sydney Council held on 11 May 2020.

Voting was as follows:

For/Against 6/0

Councillor	Yes	No	Councillor	Yes	No
Gibson	Y		Barbour	Y	
Beregi	Abs	sent	Drummond	Y	
Keen	Y		Gunning	Abs	sent
Brodie	Y		Mutton		
Carr	Abs	sent	Baker	Abs	sent

7. GF01: 2020 Customer Satisfaction Survey Findings

Report of Jenny Gleeson, Manager Integrated Planning & Special Projects

This report presents the key findings of the 2020 Customer Satisfaction Survey regarding resident and business satisfaction with Council's key service areas as well as topical questions (Attachment 1).

Council conducts independent community-wide surveys on a periodic basis to measure the community's satisfaction with its services and to obtain information on the importance of those services to the community. This assists with the prioritisation of funding of Council activities (services and projects). The 2020 survey was conducted by Jetty Research.

The mean score of residents' overall satisfaction with Council was 5.4 (83% satisfied) out of a possible 7, a 0.2 increase on 2016 (up from 5.2) and the highest ever result across the seven surveys conducted between 2004 and 2020.

The mean score of business overall satisfaction with Council was 5.0 (65% satisfied) out of a possible 7, a 0.3 mean point score increase on 2016.

Consistent with previous years, the 2020 survey was also used to gather representative feedback to inform Council's next Integrated Planning and Reporting (IP&R) Review, including the *North Sydney Community Strategic Plan* (CSP) and *Delivery Program*. The survey results will also be used to inform the *End of Term Report*, which is to be presented to the last meeting of the outgoing term of Council.

The survey was funded from the 2019/20 non-recent budget. There are no immediate financial implications as a result of this survey. The survey findings will be used to review services and programs in preparation for the next review of Council's IP&R suite of plans, which will occur following commencement of the next term of Council.

Recommending:

1. THAT the 2020 Customer Satisfaction Survey Findings be received.

James Parker and Christine Dening (Jetty Research) provided a Briefing to the Committee on this matter

A Motion was moved by Councillor Barbour and seconded by Councillor Mutton,

- 1. THAT the 2020 Customer Satisfaction Survey Findings be received.
- **2. THAT** Council recognises that the positive result of the Survey could not have been achieved without the efforts of staff.

The Motion was put and carried.

Resolved to Recommend:

- **1. THAT** the 2020 Customer Satisfaction Survey Findings be received.
- **2. THAT** Council recognises that the positive result of the Survey could not have been achieved without the efforts of staff.

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Voting was as follows:

For/Against 6/0

Councillor	Yes	No	Councillor	Yes	No
Gibson	Y		Barbour	Y	
Beregi	Abs	sent	Drummond	Y	
Keen	Y		Gunning	Abs	sent
Brodie	Y		Mutton		
Carr	Abs	sent	Baker	Abs	sent

8. GF02: Returns of Interest by Councillors and Designated Staff – IPC Guidelines

Report of Ian Curry, Manager Governance and Committee Services

Disclosing pecuniary interests of councillors and other designated decision makers furthers openness, transparency and accountability in local government. It also facilitates the identification and management of potential conflicts of interest that might arise where councillors and staff participate in decisions from which they may derive, or be perceived to derive, personal or financial benefit.

The purpose of this report is to advise Councillors of the requirement by the Information and Privacy Commissioner (IPC) for making the disclosure of the returns of interest of Councillors and designated persons publicly available on our website.

Recommending:

- **1. THAT** Council note the requirements of the IPC Guideline No. 1 and OLG Circular 19-21, issued in respect of returns of interests by Councillors and designated staff.
- **2. THAT** the Returns of Interest by Councillors be published on Council's website, subject to redaction of personal information.

A Motion was moved by Councillor Mutton and seconded by Councillor Gibson,

- **1. THAT** Council note the requirements of the IPC Guideline No. 1 and OLG Circular 19-21, issued in respect of returns of interests by Councillors and designated staff.
- **2. THAT** the Returns of Interest by Councillors be published on Council's website.

The Motion was put and carried.

Resolved to Recommend:

- **1. THAT** Council note the requirements of the IPC Guideline No. 1 and OLG Circular 19-21, issued in respect of returns of interests by Councillors and designated staff.
- **2. THAT** the Returns of Interest by Councillors be published on Council's website.

Voting was as follows:

For/Against 6/0

Councillor	Yes	No	Councillor	Yes	No
Gibson	Y		Barbour	Y	
Beregi	Abs	sent	Drummond	Y	
Keen	Y		Gunning	Abs	sent
Brodie	Y		Mutton		
Carr	Abs	sent	Baker	Abs	sent

9. GF03: Operational Plan Quarterly Performance Review: January to March 2020

Report of Sarah Malcolm, Corporate Planning Coordinator

The *North Sydney Council Delivery Program* 2018/19-2020/21 was adopted on 25 June 2018. The *Operational Plan* 2019/20 i.e. Year 2 of the Delivery Program was adopted on 24 June 2019 and commenced 1 July 2019.

This Quarterly Review includes a status update of the adopted Delivery Program

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projects.

Attachment 1 details the performance against projects by operational view (by Division) for the period 1 January to 31 March 2020. Of the 302 total projects scheduled to commence this quarter, 286 (94%) projects are on track (green status) and 13 (6%) are behind schedule (red and amber status projects). Note: 3 projects discontinued (black) and are not included in the % analysis.

Financial performance information is detailed within the Quarterly Budget Review Statement (QBRS) presented separately to Council (refer Item GF04).

Recommending:

- **1. THAT** the Quarterly Review of the Operational Plan 1 January to 31 March 2020 be received.
- **2. THAT** Council notes the remedial action for those projects that were not on track for the period ending 31 March 2020.

The Recommendation was moved by Councillor Keen and seconded by Councillor Barbour.

The Motion was put and carried.

Resolved to Recommend:

- **1. THAT** the Quarterly Review of the Operational Plan 1 January to 31 March 2020 be received.
- **2. THAT** Council notes the remedial action for those projects that were not on track for the period ending 31 March 2020.

Voting was as follows:

For/Against 6/0

Councillor	Yes	No	Councillor	Yes	No
Gibson	Y		Barbour	Y	
Beregi	Abs	sent	Drummond	Y	
Keen	Y		Gunning	Abs	sent
Brodie	Y		Mutton		
Carr	Abs	sent	Baker	Abs	sent

10. GF04: Quarterly Budget Review - March 2020

Report of Darren Goode, Manager Accounting Services

This report should be read in conjunction with Item GF03 Operational Plan Performance Review. It also supplements the information provided to the Council via the COVID-19 Impacts - Part 2 report (Item GF03) The purpose of this report is to inform Council of the results of the review of the budget for the quarter ended 31 March 2020 and to seek approval to make the necessary adjustments to the 2019/20 budget to reflect the current projected estimates of income and expenditure for the year.

The forecast operating result before capital grants and contributions for the year is a deficit of \$3.4 million which is a deterioration of \$6 million from the \$2.6 million surplus forecast at the end of the December quarter. This has been largely caused by the impact of the COVID-19 pandemic on Council's revenue streams. Actual income and expenditure were both on target with the budget projections prior to the onset of the pandemic. The adjustments to the budget outlined in this report have increased the drawdown from reserves to ensure that the budget remains balanced to \$18.2 million. The forecast balance of cash reserves as at 30 June 2020 is now \$49.3 million.

The net effect of the adjustments to the 2019/20 revised budget outlined in this report was the budget remaining in a balanced position at the end of the March quarter with net transfers from reserves of \$18.2 million.

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Recommending:

- 1. THAT the Quarterly Budget Review Statement March 2020 report be received.
- 2. THAT the revised 2019/20 budget be adopted.

The Recommendation was moved by Councillor Mutton and seconded by Councillor Brodie.

The Motion was put and carried.

Resolved to Recommend:

- 1. THAT the Quarterly Budget Review Statement March 2020 report be received.
- 2. THAT the revised 2019/20 budget be adopted.

Voting was as follows:

For/Against 6/0

Councillor	Yes	No	Councillor	Yes	No
Gibson	Y		Barbour	Y	
Beregi	Abs	sent	Drummond	Y	
Keen	Y		Gunning	Abs	sent
Brodie	Y		Mutton		
Carr	Abs	sent	Baker	Abs	sent

11. GF05: COVID-19 Financial Impacts - Part 2

Report of Margaret Palmer, Director Corporate Services, Darren Goode, Manager Accounting Services and Jenny Gleeson, Manager Integrated Planning & Special Projects

The purpose of the report is to provide an update on the preparation of the *Draft 2020/21 Operational Plan & Budget*. The Novel Coronavirus (COVID-19) pandemic has significantly impacted Council's financial position. This report supplements information provided to the Council on 27 April 2020 (Item 12 COVID-19 Financial Impacts), as well as the March 2020 quarterly budget review statement (Item GF04). Council's Budget Preparation Timetable was prepared prior to COVID-19. To assist councils to respond, the NSW Government has made regulatory changes regarding Integrated Planning and Reporting (IP&R) statutory deadlines, including a one-month extension to adopt the *2020/21 Operational Plan & Budget* (i.e. by 31 July 2020).

The additional available time to prepare the *Draft 2020/21 Operational Plan & Budget* is necessary to ensure the pandemic impacts are fully integrated into Council's financial planning. Attachment 1 details the amended Operational Plan & Budget Preparation Timeline, which requires amendment to Council's 2020 Schedule of Meetings, with an additional Governance & Finance Committee meeting on 1 June 2020. This will enable Councillors the opportunity to discuss the *Draft 2020/21 Operational Plan & Budget*, and the *Amended Resourcing Strategy* prior to the June Council Meeting, which will seek endorsement to publicly exhibit the plans.

The current known financial implications are detailed throughout the body of this report; and will be further reported upon in the draft 2020/21 budget.

Recommending:

- **1. THAT** the amended Operational Plan & Budget Preparation Timeline be adopted.
- **2. THAT** the Council 2020 Schedule of Meetings be amended to provide for an additional Governance & Finance Committee meeting on 1 June 2020.

The Recommendation was moved by Councillor Mutton and seconded by Councillor Barbour.

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Page No 12

The Motion was put and carried.

Resolved to Recommend:

- **1. THAT** the amended Operational Plan & Budget Preparation Timeline be adopted.
- **2. THAT** the Council 2020 Schedule of Meetings be amended to provide for an additional Governance & Finance Committee meeting on 1 June 2020.

Voting was as follows:

For/Against 6/0

Councillor	Yes	No	Councillor	Yes	No
Gibson	Y		Barbour	Y	
Beregi	Abs	sent	Drummond	Y	
Keen	Y		Gunning	Abs	sent
Brodie	Y		Mutton		
Carr	Abs	sent	Baker	Abs	sent

CHAIRPERSON	GENERAL MANAGER
The meeting concluded at 7.36pm.	

8. Reports to Council

8.1. Matters Outstanding - May 2020

AUTHOR: Ian Curry, Manager Governance & Committee Services

ENDORSED BY: Carrie Chan, Executive Governance Manager

ATTACHMENTS:

1. Divisional reports on Matters Outstanding meeting 18 May 2020 [8.1.1 - 15 pages]

PURPOSE:

To report to Council on the status of Councillor resolutions.

EXECUTIVE SUMMARY:

Each month, a report is presented to Council on the status of those resolutions arising from Mayoral Minutes and Notices of Motion.

The attached table has been updated to include resolutions from the 27 April 2020 Ordinary Meeting of Council.

FINANCIAL IMPLICATIONS:

Nil.

RECOMMENDATION:

1. THAT the report be received.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 5. Our Civic Leadership
- 5.2 Council is well governed, and customer focused

BACKGROUND

The previous report to Council on this matter was on 27 April 2020.

CONSULTATION REQUIREMENTS

Community engagement is not required.

DETAIL

Attached is the updated table of outstanding resolutions for the information of Council.

ITEM REPORTS 18/05/20



REPORT TO THE COUNCIL MEETING OF 18 MAY 2020 ON MATTERS OUTSTANDING FROM NOTICES OF MOTION OR COUNCILLOR REQUESTS FOR ACTION, UP TO THE 27 APRIL 2020 MEETING

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
1.	27/08/18 (Min. No.295)	NoM04: Notice of Motion No. 20/18 by Crs Keen and Mutton – 22/08/18, Re: The Return of Hockey to North Sydney	THAT Council in principle support a hockey field in the North Sydney LGA. THAT Council report on the options available for hockey.	OSE	Jun 2020	Matter raised at Sports and Recreation Reference Group for discussion. A report was presented to the Sport & Recreation Reference Group meeting on 18 November 2019, which will be presented to Council with comments from the Reference Group.
2.	19/11/18 (Min. No.426)	NoM03: Notice of Motion No. 30/18 by Cr Mutton - 14/11/18, Re: Wendy's Secret Garden	1. THAT Council resolve to call for a report on the options that are available to bridge or enclose the water course and estimate the costs of delivering each of the options.	OSE EPS Assessment Complete	Jun 2020	Engineers are investigating a diversion of the watercourse. Council staff met with Cr Mutton on site 4 March to discuss what is required to address the issue in the NoM. EPS & OSE staff will prepare a concept plan to address the issues raised at the site meeting. Consultant has prepared costings and they have been distributed to all stakeholders

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
						Site meeting held on 1 May to discuss design options with stakeholders. A new design is being prepared based on stakeholder feedback. Site meeting held with garden's volunteer coordinator 29 October 2019. Option for
						embankment stabilisation selected, budget bid will be prepared for the 2020/21 financial year considerations and a report prepared for Council.
3.	27/05/19 (Min. No. 126)	MM02: Floating Dry Dock Development Application for 6 John Street, McMahons Point	1. THAT Council writes to the Chair of the Sydney North Planning Panel (SNPP), Mr Peter Debnam, to request that, given the significant community interest in the development application for the installation of a floating dry dock facility at 6 John Street (Noakes Boat Yard) in McMahons Point, a public briefing meeting be held in addition to the formal determination meeting; 2. THAT Council offer the Hutley Hall as the venue for the requested meeting; 3. THAT the DA applicant and members of the community who have made submissions be invited to attend; 4. THAT Council invites the local members, Ms Felicity Wilson and Mr	CiS	Jun 2020	Following on from legal correspondence between the parties, a meeting was held with representatives from Stannards, Noakes and Council on 12 September 2019. The primary undertaking from that meeting was the formation of a working group to works towards the construction of the subject jetty. The Working Party is yet to be called as both parties, along with the EPA, are heavily involved in resolving the current Development Application and in investigating and responding to various compliance investigations.

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
			Trent Zimmerman to attend the meeting; and 5. THAT Council provides the required administrative support as provided for in the Planning Panels Operational Procedures September 2016.			
4.	27/05/19 (Min. No. 131)	NoM01:Notice of Motion No. 5/19 - Crs Baker, Carr and Beregi – 22/05/19, Re: Compliance with DA 1164/90 – Noakes: 6 John Street, McMahons Point	1. THAT Council take all necessary action to ensure compliance with the conditions of consent of DA 1164/90 for redevelopment of the existing boat maintenance facility at 6 John Street, McMahons Point. 2. THAT, in particular, such action include all steps necessary to ensure the enforcement of the public benefit condition D51 which required the developer to "provide a public jetty extending from land below John Street, approximately in the location of the former sea baths, subject to the design and position being acceptable to Council".	CiS	Ongoing	Following on from legal correspondence between the parties, a meeting was held with representatives from Stannards, Noakes and Council on 12 September 2019. The primary undertaking from that meeting was the formation of a working group to works towards the construction of the subject jetty.
5.	23/09/19 (Min. No. 254)	MM01: Trees at the Coal Loader	THAT a report be prepared on ways to provide additional shade at the Coal Loader Platform in time for the 2020/21 summer. The report should: a) provide a range of options for Council to consider, including planting additional trees along	OSE	Jun 2020	Investigation into appropriate options is being undertaken.

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
			the periphery of the Platform as well as options for shade using the existing colonnade; b) include a review of essential facilities on the Platform and recommendations for providing additional facilities, such as toilets and a drinking fountain, if required; c) include the cost of installing and maintaining each option; and d) be provided to Council in time to incorporate a project in the 2020/21 Operational Plan if required.			
6.	28/10/19	NoM01: Notice of Motion No. 10/19 – Cr Brodie – 23/10/19, Re: More Basketball Hoops and Netball Goal Rings for Informal Play in North Sydney	1. THAT Council requests a brief report to evaluate installing basketball hoops on existing hard surfaces and netball rings on grass and hard surfaces in North Sydney. The report to include cost, suggested locations where hard surfaces already exist that are suitable for basketball and grass areas suitable for netball goal rings in areas that are not in close proximity to residential areas. Suggested areas to include: Anderson Park, area under the Sydney Harbour Bridge and the Coal Loader.	OSE	Jun 2020	Report provided to March Sport and Recreation Reference Group (Minutes to be reported to 27 April Council meeting).

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
7.	25/11/19 (Min. No. 325)	MM01: RE: Jacaranda Season	1. THAT Council staff prepare a report on options for managing the impact of visitors in McDougall Street, Kirribilli; 2. THAT the options include traffic management, the provision of additional amenities or services, multilingual communications and the feasibility of holding a full day festival; and 3. THAT an estimate of costs for each option be provided.	EPS	Jul 2020	Matter deferred to July 2020 at the 23 March Council meeting.
8.	25/11/19 (Min. No. 363)	NoM01: Notice of Motion No. 11/19 – Crs Baker, Beregi and Carr – 13/11/19, Re: Review of the Neutral Bay Heritage Conservation Area	1. THAT the matter be deferred to the Legal and Planning Committee to clarify potential conflicts of interest and discuss the concerns raised by the Director City Strategy.	CiS	Aug 2020	A report was submitted to the 9 March Legal and Planning Committee meeting. Council on 6 April resolved that the matter be deferred until the 2020/21 financial year.
9.	24/02/20 (Min. No. 6)	MM02: Bring Back the Roundabout	1. THAT Council staff prepare a report to identify the costs associated with reinstating the roundabout with a flat top like the one on Alexander Street and identify a funding source. 2. THAT consideration be given to improving pedestrian access in the new design.	EPS	Jun 2020	A report addressing the actions related to associated costs with reinstating the roundabout and improving pedestrian access in the new design is being prepared for the Council meeting on 18 May.

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
10.	24/02/20 (Min. No. 7)	MM03: Diverting Food Waste from Landfill	THAT staff prepare a report on the options for conducting a food waste diversion trial that: includes information on costs and likely community interest to participate in a trial; and b. includes details on any smart phone applications that could be used to allow residents to express an interest in food waste collection in their street.	OSE	Dec 2020	This report cannot be developed until such time as we can arrange meetings with other Councils and associated contactors. None are available at this present time. Project and report delayed until December 2020.
11.	24/02/20 (Min. No. 8)	MM04: Prioritising Pedestrians	1. THAT staff prepare a report on options for improving our focus on pedestrian needs and that the report considers how to ensure that all voices are equally heard when balancing the needs of motorists, cyclists, pedestrians and public transport.	EPS	Jun 2020	A Report is being prepared by Council's Traffic Engineer and will be submitted to the 18 May 2020 Council Meeting.
12.	24/02/20 (Min. No. 10)	MM06: Save Our Recycling Campaign	1. THAT Council acknowledges the growing imperative to manage waste and recycling within NSW and calls for urgent action from the State Government to help build a circular economy in NSW. 2. THAT Council recognises initiatives and projects taken within the North Sydney local government area to help achieve this goal, including: the establishment and operation of a Community Recycling	OSE	May 2020	Letters have been sent out, per resolutions 4 and 5. Report to 18 May 2020 Council meeting. COMPLETED

Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
		Centre in Artarmon; provision of workshops and waste education programs that avoid waste generation and best practice recycling behaviours; provision of kerbside collection of garden waste and recyclables; ongoing review of options for resource recovery and alternative waste treatment; implemented Single Use Plastics Policy. 3. THAT Council endorses Local Government NSW's sector-wide			
		Save our Recycling campaign, and asks the State Government to reinvest the Waste Levy in: a. Funding councils to collaboratively develop regional-			
		scale plans for the future of waste and recycling in their regions; b. The delivery of the priority infrastructure and other local government projects needed to deliver regional-scale plans, particularly where a market failure has been identified;			
		 c. Support for the purchase of recycled content by all levels of government, to help create new markets; and d. Funding and delivery of a statewide education campaign on the importance of recycling, 			

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
			including the right way to recycle, the purchase of products with recycled content and the importance of waste avoidance. 4. THAT Council write to the Member for North Shore, Ms Felicity Wilson MP, Minister for Energy and Environment the Hon Matthew Kean MP, Local Government Minister the Hon Shelley Hancock MP, NSW Treasurer the Hon Dominic Perrottet MP, Premier the Hon Gladys Berejiklian MP, Opposition Leader Jodi McKay MP, Shadow Minister for Environment and Heritage Kate Washington MP and Shadow Minister for Local Government Greg Warren MP to confirm their support for recycling and outline the urgent need to educate, innovate and invest in local waste and recycling services via the Waste Levy. 5. THAT Council advise LGNSW President Linda Scott of the passage of this Mayoral Minute. 6. THAT Council shares and promotes the Save Our Recycling campaign via its digital and social media channels and via its networks.			
13.	24/02/20 (Min. No. 42)	Notice of Motion No. 1/20 – Cr Mutton– 24/11/19	THAT Council resolve to: i. Seek to lease on terms to be approved by Council from the Government of NSW the	OSE	Jun 2020	

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
		Re: Extension of Land into Sawmillers Reserve	"unused land" and incorporate that land in to Sawmillers Reserve. ii. Seek to reach an agreement on terms to be approved by Council with the Government of NSW and the Scout Association of Australia, North Sydney Region pursuant to which the toilets in that hall are made available for public use.			
14.	23/03/20 (Min. No 52)	MM01: Western Harbour Tunnel & Warringah Freeway Upgrade Environmental Impact - It's Time to Get Political	1. THAT Council immediately begin a public campaign seeking widespread community support for changes to the Western Harbour Tunnel and Warringah Freeway Upgrade project to ameliorate the impact on the North Sydney LGA. 2. THAT the General Manager report back to Council on an appropriate budget to fund the campaign and an appropriate commencement date. 3. THAT the campaign direct people to contact the NSW Premier, in both her capacity as Premier and Member for Willoughby, seeking an agreement to fund the cost of measures to address and mitigate the impacts that have been identified. 4. THAT Council write to the NSW Premier seeking financial and/or other compensation for the	CiS	Apr 2020	Letter to Premier sent 23/4/20 Report prepared for 27 April Council meeting COMPLETED

	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
			temporary and permanent loss of amenity created by the Western Harbour Tunnel project.			
15.	23/03/20 (Min. No 53)	MM02: Western Harbour Tunnel & Warringah Freeway Upgrade Environmental Impact - Protecting our CBD	1. THAT Council write to the NSW Premier asking her to attend a meeting with the Mayor, Deputy Mayor, General Manager, Georgina Taylor, Paul Walter, Brian Emanuel and Ian Grey to discuss alternative options for the tunnel entrance in North Sydney. 2. THAT Council write to the NSW Premier, Member for North Shore and Minister for Transport, urging them to promptly redesign the access points to the tunnel to ameliorate the identified negative impacts in our North Sydney CBD.	GM		Letter to Premier sent 23/4/20. Letter to Minister for Transport and Member for North Shore sent 12/5/20 COMPLETED
16.	23/03/20 (Min. No 54)	MM03: Western Harbour Tunnel & Warringah Freeway Upgrade Environmental Impact - Protecting our Open Space	1. THAT Council write to the NSW Premier asking her to attend a meeting with the Mayor, Deputy Mayor, General Manager, Georgina Taylor, Paul Walter, Brian Emanuel and Ian Grey to discuss how the open space permanently alienated by this project can be replaced. 2. THAT Council write to the NSW Premier asking her to guarantee that there will be no permanent structures on any of the land returned to public space.	GM		Letter to Premier sent 23/4/20 COMPLETED

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	Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
17.	23/03/20 (Min. No 55)	MM04: Western Harbour Tunnel & Warringah Freeway Upgrade Environmental Impact - Air Filtration	1. THAT Council write to the NSW Premier asking her to attend a meeting with the Mayor, Deputy Mayor, General Manager, Georgina Taylor, Paul Walter, Brian Emanuel and Ian Grey to discuss the location of the proposed ventilation stacks and the need for appropriate filtration. 2. THAT in the event the NSW Government will not commit to air filtration, the NSW Premier be asked to guarantee that the infrastructure is built in a way that allows it to be easily adapted/retrofitted if a future need is identified.	GM		Letter to Premier sent 23/4/20 COMPLETED
18.	23/03/20 (Min. No 56)	MM05: Free 15 Minute Parking at all Meters	1. THAT Council receive a report from the General Manager (Director Engineering and Property Services) outlining the practicality of running a 12-month trial for free 15 minute parking at all North Sydney Council parking meters. 2. THAT the report be submitted to the 27 April 2020 Council meeting, if practicable.	EPS	Feb 2021	At the 27 April 2020 Council Meeting, this matter was deferred to January 2021.
19.	23/03/20 (Min. No 74)	Item 18: Notice of Motion No. 3/20 –	1. THAT Council prepare a report investigating the temporary closing of specific streets in the North Sydney LGA for a few hours on	EPS	Jun 2020	This Item will be further investigated, and a Report will be prepared to be submitted to

Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
	Crs Beregi, Baker and Carr – 15/03/20, Re: Street Play - Pilot Program for North Sydney LGA	some Sunday afternoons to allow children and members of the community to participate in Street Play. 2. THAT the Council report be modelled on the initiative suggested by Waverley Mayor Paula Masselos and the report by Waverley Council as well as the report and any findings from the Inner West Council relating to a similar Play Street initiative, 3. THAT the Council report identify possible streets in the North Sydney LGA for a Pilot Program of "Street Play" in North Sydney 4. THAT the Council report consider following the Waverley model of Street Play, by defining the pilot program of Street Play as an event thereby allowing Council to provide the appropriate insurance cover. 5. THAT the Council report identify financial implications and sources of funding, where needed. 6. THAT should the Council adopt the report to proceed with the Pilot Program, then Council refer the report to the Traffic Committee for its support. 7. THAT should the Pilot Program be adopted, that Council apply to Sport Australia to provide funding for Street Play.			Council within the next 2-3 months.

Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
27/04/20 (Min. No. 79)	MM01: COVID-19 Financial Support	 THAT Council calls on the Federal Government and NSW Government to urgently deliver comprehensive and multifaceted financial support and stimulus packages to local government to enable them to continue to operate effectively and provide essential services during the COVID-19 pandemic. THAT Council calls for the packages to include the following measures: Increasing Financial Assistance Grants payments to 1% to help councils maintain essential functions and services, acknowledging the increased costs and mounting revenue losses arising from COVID-19 as well as giving councils capacity to provide hardship assistance to businesses and residents. Immediate financial assistance to support council employees, especially in early education and care. Providing stimulus funding to councils for projects that will help sustain council operations and boost local economies. This could be achieved through increasing or bringing forward funding under existing funding 			

Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
		programs or introducing new			
		programs.			
		• Increased access to TAFE, VET			
		and other apprenticeship			
		opportunities that council staff			
		can undertake to address skill			
		shortages, especially for staff in			
		non-essential services who are			
		unable to be redeployed.			
		3. THAT Council commends the			
		NSW and Federal Governments on			
		their stewardship during this crisis			
		and commits to working in			
		partnership to protect community			
		health and sustain local economies			
		through this crisis.			
		4. THAT Council write to the local			
		Federal Member for North Sydney,			
		Mr Trent Zimmerman MP; the local			
		State Member for North Shore, Ms			
		Felicity Wilson MP; Prime Minister,			
		the Hon Scott Morrison MP; NSW			
		Premier, the Hon Gladys Berejiklian			
		MP; Federal Treasurer, the Hon Josh			
		Frydenberg MP; NSW Treasurer, the			
		Hon Dominic Perrottet MP; NSW			
		Local Government Minister, the Hon			
		Shelley Hancock MP; Federal			
		Minister for Local Government, the			
		Hon Mark Coulton MP; Federal			
		Opposition Leader, the Hon Anthony			
		Albanese MP; NSW Opposition			
		Leader, Ms Jodi McKay MP; Federal			
		Shadow Minister for Local			

Report on Matters Outstanding - Meeting of 18/05/20

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Meeting Date and Min. No.	Item Reference	Agreed Action	Responsible Director	Target Date	Current Status
		Government, Mr Jason Clare MP; and NSW Shadow Minister for Local Government, Mr Greg Warren MP, to confirm their support for increased financial assistance and stimulus funding for local government to help councils maintain essential services and employment during the COVID-19 pandemic. 5. THAT Council endorses Local Government NSW's sector-wide campaign to obtain financial assistance, employment support and stimulus funding for the local government sector. 6. THAT Council advise LGNSW President Linda Scott of the passage of this Mayoral Minute. 7. THAT Council commend Councillor Linda Scott, President of LGNSW, for her successful action on this matter.			

KEN GOULDTHORP, General Manager

Legend:

GMO - General Manager's Office

CIS - City Strategy Division

CLS - Community and Library Services Division

COS - Corporate Services Division

EPS - Engineering and Property Services Division

OSE - Open Space and Environmental Services Division

MM - Mayoral Minute

MoU - Matter of Urgency

NoM - Notice of Motion

NoRM - Notice of Rescission Motion

Q - Question with/without Notice

8.2. Investments and Loan Borrowings Held as at 30 April 2020

AUTHOR: Garry Ross, Manager Financial Services

ENDORSED BY: Margaret Palmer, Director Corporate Services

ATTACHMENTS:

1. Monthly Investment Report - April 2020 [8.2.1 - 17 pages]

PURPOSE:

This report provides details of the performance of Council's investment portfolio and borrowing limits for the period ending 30 April 2020.

EXECUTIVE SUMMARY:

Investment Portfolio:

The Investment portfolio provided an annualised return of 2.27% for the year to date as at 30 April 2020, 1.12% above the reportable BBSW Bank Bill Index.

COVID-19 has adversely impacted financial markets, which in turn, has also affected Council's investment portfolio. The biggest risk that Council faces over the medium-longer term in this environment is not the potential loss of capital (given all the banks are well capitalised and regulated by APRA), but the rapid loss of interest income as interest rates are at an all-time low.

Borrowings:

Council entered into a fixed interest loan of \$9.5 million with quarterly interest and principal payments on 31 July 2018. The principal outstanding as at 30 April 2020 is \$8,099,526.94

FINANCIAL IMPLICATIONS:

The 2019/20 budgeted returns on investments is estimated to be \$1,529,055.00. This is significantly less than previous returns due to the declining cash reserve balances and continued low interest rates. Any surplus funds generated in excess of adopted estimates will be transferred to Council's internally restricted reserves.

RECOMMENDATION:

1. THAT the report on Investments and Loan Borrowings held as at 30 April 2020 be received.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 5. Our Civic Leadership
- 5.1 Council leads the strategic direction of North Sydney

BACKGROUND

The Responsible Accounting Officer must provide Council with a monthly report detailing all funds:

- a. Invested under Section 625 of the Local Government Act 1993. This report must include certification that the investments have been made in accordance with the Act and the Regulations made thereunder, the revised Investment Order issued by the Minister for Local Government and Council's Financial Investment Policy.
- b. Borrowed under Section 624 of the Local Government Act. This report must comply with the borrowings Order issued by the Minister for Local Government and Council's Debt Management Policy.

CONSULTATION REQUIREMENTS

Community engagement is not required.

DETAIL

Investment Portfolio

The following table provides details of the performance of Council's investment portfolio (excluding cash deposits) against the benchmark for the month of April 2020 and annualised for the year to date as at 30 April 2020 (including investments which have matured prior to 30 April 2020).

	April 2020	Annualised YTD as at 30 April 2020
Actual Return	0.16%	2.27%
Benchmark	0.05%	1.12%
Variance	0.11%	1.15%

Investment Performance

Investment returns continue to exceed the indicative benchmark (BBSW Bank Bill Index). All funds have been invested in accordance with the Act and the Regulations made thereunder and with Council's Financial Investment Policy. Further, Council's investment portfolio complies with the revised Investment Order issued by the Minister for Local Government, which places restrictions on the type of investments permitted. These restrictions have placed greater emphasis on obtaining competitive investment options and the need for sound investment advice from Council's independent advisor.

Council continues to seek independent advice for all investments and is actively managing the portfolio to ensure that returns are maximised taking into account diversification and risk. A complete analysis of the performance is covered in the Monthly Investment Report prepared by Council's Investment advisor. (refer attachment 1.)

The actual investment returns for the year to date as at 30 April 2020 have been reviewed and are \$315,094.00 greater than the revised budget.

Summary of Returns from Investments:

Year	Original Annual Budget	Revised Annual Budget	YTD Budget (April)	YTD/Annual Actual (April)	YTD Budget to Actual Variance (April)
2019/20	\$1,500,000	\$1,529,055	\$1,309,055	\$1,624,149	\$315,094
2018/19	\$1,590,000	\$1,730,000		\$2,253,497	\$523,497
2017/18	\$2,300,000	\$2,490,000		\$2,589,813	\$99,813
2016/17	\$2,000,000	\$3,000,000		\$3,276,518	\$276,518
2015/16	\$2,320,000	\$3,640,000		\$4,105,146	\$465,146
2014/15	\$3,400,000	\$4,150,000		\$4,414,692	\$264,692
2013/14	\$2,700,000	\$3,400,000		\$3,983,515	\$583,515
2012/13	\$2,000,000	\$2,887,751		\$4,238,785	\$1,353,069
2011/12	\$2,000,000	\$3,400,000		\$3,728,080	\$328,080

Financial Investment Policy

The maximum holding limit in each rating category and the target credit quality weighting for Council's portfolio shall be:

Long Term Rating Range	Maximum Policy Holding	Distribution as at 30 April 2020	
AA Category	100.00%	53.87%	
A Category	60.00%	11.52%	
BBB Category	35.00%	30.85%	
Unrated ADIs (NR)	10.00%	3.76%	

Loan Borrowings

Council's Debt Management Policy provides the framework for Council's borrowing activities and defines key responsibilities and the operating parameters within which borrowing and related risk management activities are to be carried out.

The Policy's objective is to control Council's exposure to movements in interest rates through the application of fixed, floating or a combination of both in order to maintain its risk averse strategy.

Loan borrowing will be undertaken in line with the following principles:

a) That the capital cost of infrastructure be recognised over the period during which the benefits will be enjoyed.

- b) That loan funds are a resource to fund the replacement and upgrading of existing infrastructure and fund the creation of new infrastructure.
- c) That loan funds will be limited to:
 - acquisition or enhancement of income producing assets;
 - construction and/or upgrading of buildings; and
 - infrastructure assets that have a life expectancy of greater than 10 years.

The current Debt Facility is as follows:

The current facility is a fixed loan financing option, fully amortising the drawn down amount of \$9,500,000.00 over 10 years, fixed interest rate with quarterly repayments of interest and principal.

The current loan details are as follows:

Loan amount:	\$ 9,500,000.00			
Loan term:	10 years			
From:	31/07/2018			
To:	31/07/2028			
Interest rate:	4.02%p.a.(fixed)			
Repayment:	Quarterly			
Dates	Principal Outstanding	Interest	Principal	Payment
1/07/2019	\$ 8,910,951.91			
31/7/2019	\$ 8,711,603.51	\$ 90,291.14	\$ 199,348.40	\$ 289,639.54
31/10/2019	\$8,510,235.19	\$88,271.22	\$201,368.32	\$ 289,639.54
31/01/2020	\$8,306,826.48	\$86,230.83	\$203,408.71	\$289,639.54
30/04/2020	\$8,099,526.94	\$82,340.00	\$207,299.54	\$289,639.54

The next loan instalment is due on 31 July 2020.

Loan Funded Capital Projects as at 30 April 2020:

Project 1: Upgrading the Car Park in Alexander Street, Crows Nest

A \$5 million loan has been sourced to fund this project.

Current length of Loan as per LTFP: 10 years to 2028

Project 2: Upgrading of On-Street Parking Management System

A \$4.5 million loan has been sourced to fund this project.

Current length of Loan as per LTFP: 10 years to 2028



Monthly Investment Report April 2020



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Impact of COVID-19 to Council's Portfolio

COVID-19 has adversely impacted financial markets, which in turn, has also affected Council's investment portfolio. We provide a quick summary in this section.

With regards to financial markets, of importance was the RBA cutting interest rates twice in March 2020, taking the official cash rate down to a record low of 0.25%. Shares (equities) experienced a significant correction, down over -20% in Australian and around -13% globally for the month of March. April saw shares rebound strongly, offsetting some of the losses in March, with equities returning around +9% in Australia and +10% abroad.

With regards to the medium-longer term outlook for financial markets, of importance is the RBA's outlook and stance on the current situation:

- 1. The RBA's official cash rate will remain unchanged at its emergency level of 0.25% until its objectives of full employment and inflation are reached (note, we are unlikely to see the unemployment rate down to 4.5% and inflation within their 2-3% target band any time soon);
- 2. RBA Governor Lowe has commented that he has not been any signs of stress in the financial system from this crisis because unlike the GFC, the banks have cash and are well capitalised;
- 3. The RBA Board expects rates would be low "for a very long period of time".

The biggest impact to Council's investment portfolio is with regards to its largest exposure being assets held in bank term deposits, which accounts for around ~73% of Council's total investment, and cash (~11% of the total investment portfolio). The biggest risk that Council faces over the medium-longer term in this environment is not the potential loss of capital (given all the banks are well capitalised and regulated by APRA), but the rapid loss of interest income as interest rates have plummeted.

Council's term deposit portfolio was yielding 1.97% p.a. at month-end, with a weighted average duration of around 525 days or $1\frac{1}{2}$ years. The long average duration will provide some income protection against the low interest rate environment for the next $1 - \frac{1}{2}$ years (short-term). As existing deposits mature however, they will inevitably be reinvested at much lower prevailing rates.

Given official rates have fallen to all-time lows, Council is likely to see a rapid decline in interest income over future financial years. Its budgeted income over the <u>medium-longer term</u> needs to be revised to reflect the low interest rate environment. Returns of 1%-1½% p.a. may potentially be the "norm" over the next few financial years.

Monthly Investment Report: April 2020



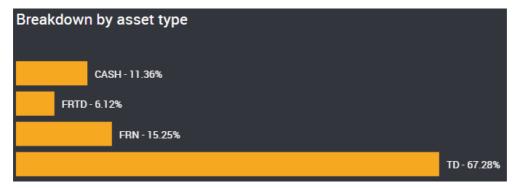
Council's Portfolio & Compliance

Asset Allocation

As at the end of April 2020, the portfolio was mainly directed to fixed and floating rate term deposits (73.40%). The remaining portfolio is directed to FRNs (15.25%) and overnight cash accounts (11.36%).

With credit securities widening over recent months, we would consider increasing the allocation to liquid senior floating rate notes (FRNs), if there are any remaining attractive securities in the secondary or primary market. This will not only offer additional upside with regards to the portfolio's investment returns, but also provide additional liquidity (FRNs are saleable – generally accessible within 2 business days). FRNs are also dominated by the higher rated ADIs which allows Council to maintain a bias towards the higher rated banks.

With official interest rates now at the RBA's effective zero bound, the priority should be to lock in any attractive medium-longer dated fixed deposits that may still be available to address reinvestment risk.



Term to Maturity

Overall, the portfolio remains lightly diversified from a maturity perspective with around 27% of assets directed to medium-term assets (2-5 years). There is still high capacity to invest in the medium-term horizon, with approximately \$22m at month-end.

All minimum and maximum limits comply with the Policy:



Monthly Investment Report: April 2020



Where liquidity permits, we recommend new surplus funds be directed to 2-5 year horizons given this is where the most attractive value can be found. We suggest this be allocated to fixed term deposits to address reinvestment risk.

Compliant	Horizon	Invested (\$)	Invested (%)	Min. Limit (%)	Max. Limit (%)	Available (\$)
✓	0 - 90 days	\$13,139,781	13.39%	10%	100%	\$84,960,503
✓	91 - 365 days	\$23,000,000	23.45%	20%	100%	\$75,100,284
✓	1 - 2 years	\$35,003,346	35.68%	0%	70%	\$33,666,853
✓	2 - 5 years	\$26,957,157	27.48%	0%	50%	\$22,092,985
✓	5 - 10 years	\$0	0.00%	0%	25%	\$24,525,071
		\$98,100,284	100.00%			

Counterparty

As at the end of April, ING Bank (A), Auswide (BBB) and BoQ (BBB+) counterparties were marginally overweight to the Policy limits due to the portfolio's shrinkage by around \$5m during the month. There are no concerns with this temporary overweight position given this was purely due to the overall portfolio's balance shrinking over April (capital expenditure).

Effective 1st March 2020, Nexus Mutual (unrated ADI) merged with Beyond Bank Australia (BBB/A-2). As a result, all investors holding Nexus Mutual deposits (including Council) received a ratings upgrade effective 1st March 2020.

Compliant	Issuer	Rating	Invested (\$)	Invested (%)	Max. Limit (%)	Available (\$)
✓	ANZ	AA-	\$2,022,004	2.06%	30.00%	\$27,408,081
✓	CBA (BankWest)	AA-	\$10,021,989	10.22%	30.00%	\$19,408,096
✓	NAB	AA-	\$7,038,390	7.17%	30.00%	\$22,391,695
✓	Westpac	AA-	\$26,000,000	26.50%	30.00%	\$3,430,085
✓	Suncorp	A+	\$1,238,654	1.26%	15.00%	\$13,476,389
X	ING Bank	Α	\$15,000,000	15.29%	15.00%	-\$284,957
✓	AMP Bank	BBB+	\$8,150,682	8.31%	10.00%	\$1,659,347
X	BOQ	BBB+	\$10,000,000	10.19%	10.00%	-\$189,972
X	Auswide Bank	BBB	\$10,000,000	10.19%	10.00%	-\$189,972
✓	Beyond (Nexus)	BBB	\$3,000,000	3.06%	10.00%	\$6,810,028
✓	ME Bank	BBB	\$3,248,004	3.31%	10.00%	\$6,562,025
✓	Newcastle PBS	BBB	\$1,380,561	1.41%	10.00%	\$8,429,467
✓	WAW CU	Unrated	\$1,000,000	1.02%	5.00%	\$3,905,014
			\$98,100,284	100.00%		

Overall, the portfolio is well diversified across the entire credit spectrum, including some exposure to the unrated ADIs.

We remain supportive of the regional and unrated ADI sector (and have been even throughout the GFC period). They continue to remain solid, incorporate strong balance sheets, while exhibiting high

Monthly Investment Report: April 2020



levels of capital – typically, much higher compared to the higher rated ADIs. Some unrated ADIs have up to 25-40% more capital than the domestic major banks, and well above the Basel III requirements.

APRA's Chairman affirmed that the banks had satisfactorily moved towards an 'unquestionably strong' capital position and that bank's stress testing contingency plans were now far better positioned that was previously the case years ago. RBA Governor Lowe has recently commented that they have not seen any signs of stress in the financial system and that unlike during the GFC, the banks now have cash, are well capitalised and are acting as "shock absorbers" in the current crisis.

Overall, the lower rated ADIs (BBB and unrated) are generally now in a better financial position then they have been historically (see the Capital Ratio figure below). We believe that deposit investments with the lower rated ADIs should be continued going forward, particularly when they offer 'above market' specials. Not only would it diversify the investment portfolio and reduce credit risk, it would also improve the portfolio's overall returns.

In the current environment of high regulation and scrutiny, all domestic ADIs continue to carry high levels of capital, particularly amongst the lower ("BBB") and unrated ADIs. There is minimal (if any) probability of any ADI defaulting on their deposits going forward – this was stress tested during the GFC. APRA's mandate is to "protect depositors" and provide "financial stability".

The biggest single risk that depositors face in the current low interest rate environment is not capital or credit risk, but reinvestment risk. <u>Interest rates are now at their effective lower bound of 0.25%.</u>

Capital Ratios*



 Per cent of risk-weighted assets; break in March 2008 due to the introduction of Basel II for most ADIs; break in March 2013 due to the introduction of Basel III for all ADIs

Source: APRA

Monthly Investment Report: April 2020



Credit Quality

The portfolio remains well diversified from a credit ratings perspective. As at the end of April 2020, the BBB category is slightly above the credit limit by around \$1.4m. This was attributed to the portfolio's balance shrinking by \$5m over the month as well as Nexus Mutual's merger with Beyond Bank on 1st March 2020 (\$3m of deposits that were previously 'unrated' formally received a credit rating of 'BBB').

From a ratings perspective, the BBB rated entities now generally dominate the number of ADIs issuing deposits within the investment grade space. We anticipate more investors will naturally allocate a higher proportion of their assets into this sector (on a historical basis), considering the most attractive assets from senior debt securities are generally offered by these ADIs.

Council may consider lifting the aggregate limit with the BBB and unrated ADI category at its next policy review.

Compliant	Credit Rating	Invested (\$)	Invested (%)	Max. Limit (%)	Available (\$)
✓	AA Category	\$45,082,383	45.96%	100%	\$53,017,900
✓	A Category	\$16,238,654	16.55%	60%	\$42,621,516
x	BBB Category	\$35,779,247	36.47%	35%	-\$1,444,147
✓	Unrated ADIs	\$1,000,000	1.02%	10%	\$8,810,028
		\$98,100,284	100.00%		

Monthly Investment Report: April 2020



Performance

Council's performance for the month ending 30 April 2020 is summarised as follows:

Performance	1 month	3 months	6 months	FYTD	1 year
Official Cash Rate	0.02%	0.10%	0.29%	0.61%	0.84%
AusBond Bank Bill Index	0.05%	0.22%	0.46%	0.83%	1.12%
Council's T/D Portfolio	0.17%	0.49%	1.02%	1.84%	2.30%
Council's FRN Portfolio	0.14%	0.45%	0.94%	1.68%	2.15%
Council's Portfolio^	0.16%	0.48%	1.01%	1.81%	2.27%
Outperformance	0.11%	0.26%	0.55%	0.98%	1.15%

 $[\]verb|^ATotal| portfolio| performance excludes Council's cash account holdings. Overall returns would be lower if cash was included.$

For the month of April, the total investment portfolio (excluding cash) provided a strong return of +0.16% (actual), outperforming the benchmark AusBond Bank Bill Index return of +0.11% (actual). The strong performance continues to be driven by the handful of the longer-dated deposits that were locked-in prior to the RBA's rate cuts, as well as the FRNs locked in at attractive margins.

However, the majority of the high-yielding deposits are fast maturing and will be reinvested at lower prevailing rates. With deposit margins tightening over the past few years, the FRN portfolio's performance has narrowed the gap compared to term deposits as shown in the past 12 months returns, although this is likely to reverse following the multiple interest rate cuts over the past year.

Over the past 12 months, the total portfolio (excluding cash) returned an outstanding +2.27% p.a., outperforming bank bills by 1.15% p.a. This has been very strong given deposit rates reached their all-time lows and credit margins have generally contracted over the past 3 years.

We are pleased that North Sydney Council remains amongst the best performing Councils in the state of NSW where deposits are concerned (as per our February 2020 Council Rankings), earning on average, around \$17,000 in additional interest income compared to its peers. We have been pro-active in our advice about protecting interest income and addressing reinvestment risk for many years and encouraged to maintain a long duration position. This is now reflected by the high performance of the investment portfolio. Of the 27 individual deposits North Sydney Council held, 16 are still yielding higher than 1.80% p.a. That is, around 59% of outstanding deposits held are currently earning more than the highest rate available in the market from any rated bank.

Monthly Investment Report: April 2020



Council's Term Deposit Portfolio & Recommendation

As at the end of April 2020, Council's deposit portfolio was yielding an **attractive 1.97% p.a.** (flat compared to the previous month), with an average duration of around 525 days (~1.4 years).

As the past decade has highlighted (post-GFC era), we have seen too many portfolios' roll a high proportion of their deposits between 3-6 months, resulting in their deposits being reinvested at lower prevailing rates. That is, depositors have overpaid for liquidity and generally not insured themselves against the low interest rate environment by diversify their funding across various tenors (out to 5 years) but rather placed all their 'eggs in one basket' and kept all their deposits short. Reinvestment risk has collectively been and continues to be the biggest detriment to depositors' interest income over the post-GFC period.

At the time of writing (early May), we see value in:

ADI	LT Credit Rating	Term	T/D Rate
Judo Bank	Unrated ADI	2 years	1.80% p.a.
BoQ	BBB+	5 years	1.80% p.a.
AMP Bank	BBB+	18 months	^1.75% p.a.
BoQ	BBB+	4 years	1.70% p.a.
Auswide Bank	BBB	2-4 years	1.60% p.a.
AMP Bank	BBB+	2 years	^1.60% p.a.
BoQ	BBB+	3 years	1.55% p.a.

[^] AMP T/Ds – these are grossed up rates which includes a 0.20% p.a. rebated commission from Imperium Markets

The above deposits are suitable for investors looking to provide some income protection and mitigate reinvestment/rollover risk in the low interest rate environment.

For terms under 12 months, we believe the strongest value is currently being offered by a number of lower and unrated ADIs offering above-market specials (dependent on daily funding requirements):

Monthly Investment Report: April 2020



ADI	LT Credit Rating	Term	T/D Rate
AMP	BBB+	6, 7 months	^1.85% p.a.
AMP	BBB+	8, 9, 10 months	^1.80% p.a.
BoQ	BBB+	6 months	1.55% p.a.
Auswide Bank	BBB	6-12 months	1.50% p.a.
BoQ	BBB+	5 months	1.50% p.a.
Auswide Bank	BBB	3 months	1.45% p.a.
Gateway Bank	Unrated ADI	3 months	1.45% p.a.
Judo Bank	Unrated ADI	6, 12 months	1.40% p.a.

[^] AMP T/Ds – these are grossed up rates which includes a 0.20% p.a. rebated commission from Imperium Markets

Amongst the higher rated ADIs ("A" rated or higher), the following deposits remain attractive for terms under 12 months:

ADI	LT Credit Rating	Term	T/D Rate
Macquarie Bank	A+	12 months	1.34% p.a.
Macquarie Bank	A+	6 months	1.24% p.a.
Macquarie Bank	A+	3 months	1.19% p.a.
NAB	AA-	12 months	1.05% p.a.



Senior FRNs & Recommendations

Over April, amongst the senior major bank FRNs, physical credit securities rebounded significantly, tightening around 40bp at the long-end of the curve. Liquidity returned to the secondary market and the bid-ask spreads tightened considerably as investors sitting on the sidelines (holding cash) looked to take advantage of any attractive offers available. Those investors that require liquidity with a domestic major bank (highly rated) and can roll down the curve should invest in 5 year terms over 3 year terms (or shorter), given the ability to lock in capital gains in subsequent years.

Any 5 year senior major bank FRNs (primary or secondary market) offered above +90bp appears to be fair value in our view, back to levels experienced in mid-2019. Any secondary market major bank FRNs offered around the same levels (or higher) for 4-5 year terms appear attractive and should be considered. We expect little primary issuance going forward given the RBA's \$90bn term funding facility (TFF) to the ADIs, offering a rate of 0.25% for 3 years. The anticipated lack of supply from new (primary) issuances going forward will favour those investors with the ability to take advantage of the discounted securities that remain in the secondary market.

Amongst the "A" rated sector, credit securities (senior level) were marked around 25-35bp tighter across the 3 and 5 year part of the curve. During the month, Suncorp issued a 5 year Covered FRN (AAA) at +112bp, printing \$750m, which we thought was at an attractive level, given it was priced at around a 15-20bp premium to where senior major bank FRNs (AA-) were marked in the secondary market. It has since tightened to around 100bp in the secondary market.

Meanwhile, amongst the 3 year "BBB" rated senior sector, they were marked around 15bp tighter over the month. There remains little turnover in the secondary market amongst the regional bank sector (turnover dominated by the major banks).

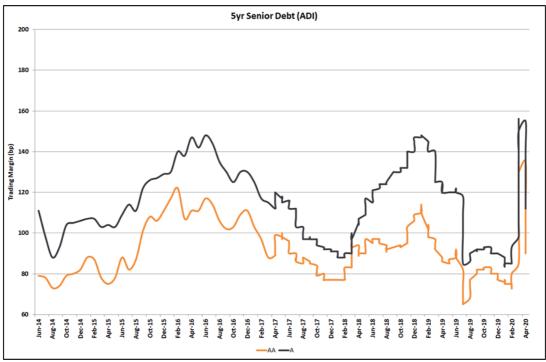
Credit margins remain at relatively attractive levels, although we expect further bouts of volatility. FRNs will continue to play a role in investor's portfolios mainly on the basis of their liquidity and the ability to roll down the curve and gross up returns over ensuing years (in a relatively stable credit environment). Investors should be ready to take advantage of any discounted securities amongst the senior ranked assets, particularly amongst the domestic major banks.

Senior FRNs (ADIs)	30/04/2020	31/03/2020
"AA" rated – 5yrs	+90bp	+130bp
"AA" rated – 3yrs	+65bp	+104bp
"A" rated – 5yrs	+112bp	+150bp
"A" rated – 3yrs	+88bp	+116bp
"BBB" rated – 3yrs	+120bp	+135bp

Source: IBS Capital

Monthly Investment Report: April 2020





Source: IBS Capital

We now generally **recommend switches** ('benchmark' issues only) into new primary issues, out of the following senior FRNs that are maturing:

- > On or before mid-2022 for the "AA" rated ADIs (domestic major banks);
- > On or before early-mid 2021 for the "A" rated ADIs; and
- Within 12 months for the "BBB" rated ADIs (consider case by case).

Investors holding onto the above senior FRNs ('benchmark' issues only) in their last 1-2 years are now generally holding sub-optimal investments and are not maximising returns by foregoing realised capital gains. In the current low interest rate environment, any boost in overall returns should be locked in when it is advantageous to do so.

With spreads widening significantly over the past few months, we recommend that Council retains its FRNs at this stage. We stress that Council's FRNs are senior ranking assets and high in the bank capital structure. We expect that, if held to maturity, the FRNs will pay back its original face value (\$100.00), along with its quarterly coupons throughout the life of the security. That is, we do not expect Council to lose any capital or interest payments from its current holding in its senior FRNs given all banks continue to maintain high capital buffers as required by APRA.

Monthly Investment Report: April 2020



Economic Commentary

International Market

Financial markets rebounded strongly in April, mainly on the back of fiscal and monetary stimulus provided by government and central banks. There were some early signs that various countries have "flattened the curve", tentatively looking to relax some of their lockdown measures. Late in the month, a positive drug trial in the US that would be an effective treatment against COVID-19 also boosted investor sentiment. This may lead to a quicker rollback of containment measures globally.

In the US, equity markets recovered a significant portion of their losses in March. The S&P 500 Index gained +12.68% (best month since January 1987), while the NASDAQ surged +15.45%. Across the main European markets, Germany's DAX gained +9.32%, UK's FTSE up +4.04% and France's CAC by +4.00%.

US Fed Chair Powell said rates would remain unchanged at their emergency levels until the economy was on track to achieve their targets of maximum employment and price stability.

The Fed announced it was willing to buy not only investment grade credit (as part of its unfolding QE bond buying programme), but also high yield or sub-investment grade paper.

US Q1 GDP contracted at -4.8% annualised, against the -4.0% consensus. The worst is yet to come with expectations that Q2 GDP might fall by around 30-40% annualised.

Jobless claims in the US reached over 30 million over a 6 week period to the end of April. The unemployment rate in the US is anticipated to skyrocket to as high as 20% in April once official figures are released. US retail sales collapsed by a record -8.7% m/m in March, driven by autos.

The Eurozone rescue deal was agreed by EU Finance Ministers totalling €540bn. The Eurozone economy contracted by -3.8% in Q1. The ECB made no changes to its bond buying programme, worth over €1000bn through to the end of the year.

China's trade figures beat expectations, with imports coming in at -0.9% y/y (expectations of -9.8%). Exports also beat consensus at -6.6% y/y against -13.9% expected. However, its economy (Q1 GDP) shrank by -9.8% q/q and -6.8% on an annual basis, the first time since at least 1992.

The IMF downgraded global growth forecasts to -3.0% for 2020, the weakest since the Great Depression. It is however projecting a v-shape recovery, forecasting growth at +5.8% in 2021, aided by policy support.

The MSCI World ex-Aus Index rebounded by +10.71% for the month of April:

Index	1m	3m	1yr	3yr	5yr	10yr
S&P 500 Index	+12.68%	-9.71%	-1.13%	+6.90%	+6.91%	+9.39%
MSCI World ex-AUS	+10.71%	-12.09%	-5.45%	+3.27%	+3.12%	+5.77%
S&P ASX 200 Accum. Index	+8.78%	-20.32%	-9.06%	+1.92%	+3.46%	+5.96%

Source: S&P, MSCI

Monthly Investment Report: April 2020



Domestic Market

The RBA kept its conventional and unconventional monetary policy unchanged at its April meeting. The bank reaffirmed its 0.25% targets for both the cash rate and the 3-year government bond yield.

RBA Governor Lowe forecasted for GDP to fall by -10% over H1 2020, marking the largest fall since the 1930s. Unemployment is expected rise to around 10% by June and likely to be above 6% over the next few years. Annual headline inflation is also expected to turn negative in Q2, marking the first negative annual inflation print since the early 1960s, reflecting falling oil prices and free childcare. However, core inflation is anticipated to still be positive.

Governor Lowe commented that they were not seeing any signs of stress at all in the financial system and that, unlike during the GFC, banks had cash and were well capitalised.

The IMF forecasts Australia's economy to shrink by -6.7% this year, entering its first recession in 30 years.

Retail sales rose by +0.5% in February, marginally higher than the preliminary estimate of a +0.4% increase. Strength in February was driven by food retailing (+0.8%), department stores (+3.1%) and households goods (+0.7%), reflecting the recent panic buying.

The Federal government passed its wage subsidy scheme ("JobKeeper") programme at a cost of up to \$130bn or 6.5% of GDP, paying a subsidy of \$1,500 per fortnight per employee. This stimulus compares with the two earlier packages totalling \$68bn or 3.4% of GDP.

Rating's agency S&P revised Australia's ratings outlook to negative from stable. The domestic major banks were also placed on negative watch (from stable outlook). Meanwhile, **Fitch Ratings downgraded the domestic major banks (and their NZ subsidiaries) by one notch from AA- to A+**, citing the significant economic shock in the first half of 2020 due to measures taken to halt the spread of COVID-19.

The unemployment rate edged higher to 5.2% in March, against expectations of a coronavirus-driven jump to 5.4%.

PM Morrison hinted strongly that some rolling back is likely to occur by mid-May, laying out three necessary criteria (more testing, robust health system, and a contact tracing system).

The headline CPI rose by +0.3% in Q1, mainly on the back of strong food prices, which more than offset falls in petrol prices and travel costs. The trimmed mean CPI rose by +0.5%, lifting annual inflation to +1.8%, the fastest annual increase since Q4 2018. However, the boost to inflation will be short-lived as the CPI should fall at a rapid rate in Q2.

The AUD surged in April by +6.33%, finishing at US65.66 cents, from US61.75 cents the previous month.

Monthly Investment Report: April 2020



Credit Market

The main global credit indices tightened significantly over April as risk assets rebounded. Spreads were marked up to 30% tighter across the main indices. The indices now trade at levels last experienced in early 2016:

Index	April 2020	March 2020
CDX North American 5yr CDS	87bp	113bp
iTraxx Europe 5yr CDS	81bp	97bp
iTraxx Australia 5yr CDS	118bp	175bp

Source: Markit

Fixed Interest Review

Benchmark Index Returns

Index	April 2020	March 2020
Bloomberg AusBond Bank Bill Index (0+YR)	+0.05%	+0.10%
Bloomberg AusBond Composite Bond Index (0+YR)	-0.07%	-0.21%
Bloomberg AusBond Credit FRN Index (0+YR)	+0.83%	-0.93%
Bloomberg AusBond Credit Index (0+YR)	+0.12%	-1.46%
Bloomberg AusBond Treasury Index (0+YR)	-0.43%	+0.23%
Bloomberg AusBond Inflation Gov't Index (0+YR)	-0.12%	-5.52%

Source: Bloomberg

Other Key Rates

Index	April 2020	March 2020
RBA Official Cash Rate	0.25%	0.25%
90 Day (3 month) BBSW Rate	0.10%	0.37%
3yr Australian Government Bonds	0.25%	0.25%
10yr Australian Government Bonds	0.89%	0.77%
US Fed Funds Rate	0.00%-0.25%	0.00%-0.25%
10yr US Treasury Bonds	0.64%	0.70%

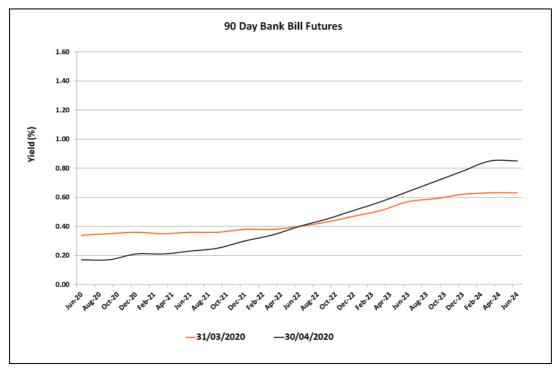
Source: RBA, AFMA, US Department of Treasury

Monthly Investment Report: April 2020



90 Day Bill Futures

Over April, bill futures fell at the short-end of the curve on a flight to quality, as investors and banks rushed to shore up capital in the dislocated and volatile environment. With the RBA suggesting they will keep rates unchanged for the foreseeable future, bill futures are likely to trade in a relatively narrow range, particularly for terms out to 3 years given the RBA's target to keep the 3 year bond rate at 0.25%.



Source: ASX



Fixed Interest Outlook

Policymakers continue to provide unprecedented support through various stimulus measures to mitigate the impact of COVID-19 across the global economy. Further stimulus measures may be required to spur global growth once the lockdowns have been relaxed.

After the RBA cut rates to their effective lower bound of 0.25%, their forward guidance is to commit keeping the official cash rate unchanged until there is a sustainable recovery and its economic objectives of full employment (unemployment rate of 4.5%) and target inflation (2-3%) are on track.

The RBA has also provided a set of objectives through some of the unconventional policies implemented, including:

- Setting a 0.25% target for the 3-year bond yield;
- Buying Commonwealth/semi-government bonds to meet this target and across the curve to address market dislocations; and
- Establishing a term funding facility (TFF) for bank loans (\$90bn at 0.25% for 3 years), especially to support new loans to small and medium-sized businesses.

RBA Governor Lowe has commented that unlike during GFC, the banks have cash and are well capitalised, and that he was encouraged as the banks are acting as "shock absorbers" during this crisis. Importantly, he also suggested that they are not seeing any signs of stress in the financial system.

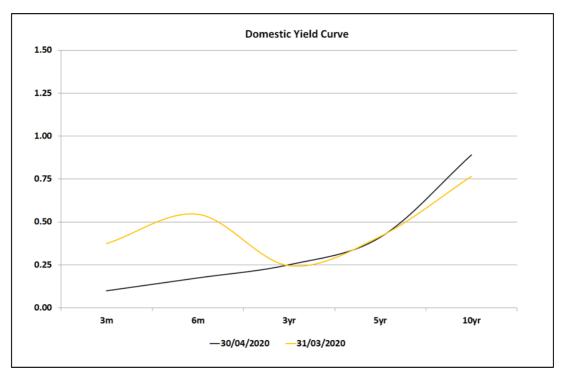
With official rates at 0.25% and the RBA undertaking quantitative easing (QE), interest rates are not expected to move from their current policy setting, although there is the possibility for the RBA to cut in smaller increments (less than 25bp) or adopting negative rates if the global economy continues to deteriorate (the RBA has reiterated that negative interest rates are not on their agenda).

Governor Lowe said rates would be low "for a very long period of time" as the RBA needed to be confident inflation would be back in the 2-3% target range before considering lifting official rates.

Over the longer-term, the domestic bond market continues to suggest a depressed period of interest rates. Over the month, yields rose around 13bp at the longer end of the curve, with 10-year government bond yields trading around 0.9%. In contrast, yields fell significantly at the short-end after the RBA cut rates to emergency levels, while injecting liquidity into the market and undertaking quantitative easing.

Monthly Investment Report: April 2020





Source: AFMA, ASX, RBA

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Monthly Investment Report: April 2020

8.3. Briefing Report - 128 Miller Street, North Sydney - State Significant Development Proposal

AUTHOR: Lara Huckstepp, Executive Assessment Planner

ENDORSED BY: Marcelo Occhiuzzi, Acting Director City Strategy

ATTACHMENTS:

1. Architectural Drawings [8.3.1 - 17 pages]

PURPOSE:

The purpose of this report is to provide Councillors with details of the proposed development and to seek Council's support to prepare a submission to the Department of Planning, Industry and Environment on the application identifying issues addressed in this report.

EXECUTIVE SUMMARY:

State Significant Development Application (SSDA) SSD_10393 has been submitted to the NSW Department of Planning, Industry and Environment, seeking to carry out alterations and additions to an educational establishment at No.128 Miller Street, North Sydney (Monte Sant' Angelo Mercy College). An Environmental Impact Statement has been prepared to accompany the Application.

The project is deemed to be State Significant as it involves alterations and additions to an educational establishment and the capital investment value is greater than \$20 million. The Minister for Planning is the consent authority for State Significant Development, unless the determination is referred to the Independent Planning Commission which occurs where more than 25 objections are received, or the proposed development is not supported by a local authority.

The proposal involves demolition of two existing sports courts and associated undercroft staff parking and the construction of a new Sports and Science Building. The new Sports and Science building will comprise a three-level building with a basement staff car park, underground dual sports court facility, three levels of new teaching spaces (innovation / science labs) and a rooftop sports court.

A preliminary review of the proposed development has raised issues regarding building height, design, setbacks, landscaping and heritage impacts including concerns raised with the partial demolition of the brick boundary wall on Miller Street.

FINANCIAL IMPLICATIONS:

Nil.

1. TH	OMMEND (AT Councation SSD)	cil prepare	e a submis	ssion in r	esponse ssed in th	to State is report.	Significant	Development

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 2. Our Built Infrastructure
- 2.1 Infrastructure and assets meet community needs
- 2.2 Vibrant centres, public domain, villages and streetscapes
- 3. Our Future Planning
- 3.4 North Sydney is distinctive with a sense of place and quality design
- 4. Our Social Vitality
- 4.4 North Sydney's history is preserved and recognised

CONSULTATION REQUIREMENTS

The NSW Department of Planning, Industry and Environment placed the State Significant Development Application on exhibition from 23/4/2020 - 20/5/2020.

DETAIL

Subject Site

The site in its entirety is referred to as 128 Miller Street, Monte Sant' Angelo Mercy College. The site occupies the majority of the street block bounded by McLaren Street to the north, Berry Street to the south, Miller Street to the east and Angelo Lane to the west. The Monte campus is approximately 2.52 hectares comprising 18 allotments. The proposed works are related to the following lots outlined below:

Legal Description	Street Address	Type	Zoned
LOT: 2 DP: 262534	128 Miller aka 15 Angelo	Registered	NSLEP 2013 - SP2
	Street North Sydney		Infrastructure
LOT: 3 DP: 262534	128 Miller aka 15 Angelo	Registered	NSLEP 2013 - SP2
	Street North Sydney	_	Infrastructure
LOT: 4 DP: 262534	128 Miller aka 15 Angelo	Registered	NSLEP 2013 - SP2
	Street North Sydney		Infrastructure
LOT: 5 DP: 262534	128 Miller aka 15 Angelo	Registered	NSLEP 2013 - SP2
	Street North Sydney		Infrastructure

The site is listed as an item of heritage (Lots 1–8, DP 262534) in Council's LEP of local significance. The following heritage items are listed below:

- I0894 Monte Sant Angelo Group
- I0895 Monte Sant Angelo Group
- I0896 Monte Sant Angelo Group
- I0897 Monte Sant Angelo Group

The northern part of the site is located within the McLaren Street Conservation area.

The comprises educational facilities that include a chapel, sports courts, administration buildings/ ancillary functions, gym and classrooms.

Site Plan



Subject Site – Miller Street, North Sydney



Proposed Development

The proposed development will require demolition of two existing sports courts and associated undercroft staff car park. The courts and carpark were constructed on the College in the mid-1990s and include vehicular access to the carpark at grade from Miller Street.

The demolition works will include demolition of all structures between the main 'Circular Drive' entry from Miller Street, and refurbishment of the connection of the McQuoin/Aquatic Centre at the corner of Miller and Berry streets.

The proposed new Sports and Science Building (the building) is a three storey building with a basement staff carpark, underground dual sports court facility, three levels of new teaching spaces (innovation / science labs) and a rooftop sports court. The new education and sports facilities will include:

- Two basement sports courts and a gym;
- 13 Science laboratory teaching spaces spread over three levels;
- Innovation hub;
- Social enterprise kitchen area; and
- Staff facilities and meeting rooms.

The levels within the proposed Building are proposed to align with the existing levels within the McQuoin Hall / Aquatic Centre, to provide a level connection between the two buildings at the existing Hall level within the McQuoin Hall / Aquatic Centre building and the Innovation / Café level of the Building.

The new underground dual sports court facility has been designed to have multiple modes of use:

- Training mode operating as two courts side-by-side.
- Stadium mode operating as a single court with surrounding stadium seating.
- Assembly mode To accommodate College assemblies and graduations.

The proposed new basement staff carpark will provide 55 parking spaces. If additional staff parking is required, dedicated staff car park spaces can be provided within the under-utilised hard stand area to the south of Stormanston building (by agreement with 'Sister of Mercy'), accessed from McLaren Street. The proposed basement car park and the hard stand parking area is able to replace the existing under-croft carpark onsite. The proposal includes the relocation of the existing vehicle entry into the Site approximately 10 metres south of its current location.

The proposal includes landscaping works surrounding the new Building. A new landscaped area is proposed to the west of the Building, known as the 'Wildflower Garden'. The Wildflower Garden including a circular lawn which will reflect the form and scale of the Holy Grass. Circular tiered stairs are proposed surrounding the new Wildflower Garden providing a path connecting the Circular Drive and northern part of the College campus with the new Building and McQuoin Hall / Aquatic Centre. This space will be used as a social gathering space for students, as well as provide a potential informal outdoor teaching space.

It is noted that there are a number of established trees running along the southern side of the 'Circular Drive', and the proposed works have been designed to ensure these trees of high significance within this area will be retained.

A total of 14 existing trees are proposed for removal, with the landscape plan detailing the provision of an additional 40 trees to be planted on the site.

The proposed new teaching facilities and classrooms within the new Building are not proposed to increase the student or staff capacity of the College.

The application outlines that community uses will be maintained within the proposed Science and Sports Building which will include:

- Use of the underground carparking for existing community use;
- Use of the basement Level 2 courts for team sports; and
- Occasional functions such as Rugby lunch for 700 persons.

The application outlines that uses are expected to operate no later than 10.30pm on any day.

Full details of the application can be viewed at www.planningportal.nsw.gov.au/major-projects/project/25866.





Montage of Proposed Development – from Miller Street



VIEW FROM MILLER STREET LOOKING WEST

Statutory Framework

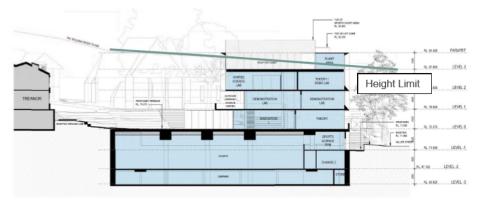
The project is deemed to be State Significant pursuant to State Environmental Planning Policy (State and Regional Development) 2011 (SEPP SRD) as it involves alterations and additions to an educational establishment and the capital investment value is greater than \$20 million. The Minister for Planning is the consent authority for State Significant Development, unless the determination is referred to the Independent Planning Commission which occurs where more than 25 objections are received, or the proposed development is not supported by a local authority.

The site is zoned part SP2 Educational Establishment and part R3 Medium Density Residential under the North Sydney Local Environmental Plan (NSLEP) 2013. The proposed new building will be located wholly within the SP2 Educational Establishment wherein alterations and additions to an educational establishment are permissible with development consent under the NSLEP 2013.

Building Height pursuant to Clause 4.3 NSLEP 2013

Clause 4.3 NSLEP sets a maximum building height of 16m on that part of the site. The proposed Science and Sports building has the following maximum height at various elements of the building measured from the existing ground level:

- Maximum height to plant room/lift overrun: 19.2m
- Maximum height to façade screen: 19.9m
- Maximum height to the rooftop court mesh fence: 21.7m



Source: Hayball Architecture

Whilst there is a breach of the Building Height control, the applicant outlines that Clause 42 of the Education SEPP states that:

"Development consent may be granted for development for the purpose of a school that is State significant development even though the development would contravene a development standard imposed by this or any other environmental planning instrument under which the consent is granted."

Notwithstanding this, concern is raised that the proposed building height non-compliance results in development that will result in excessive bulk and scale on Miller Street. Refer to the Design Excellence Panel comments.

No Cl 4.6 objection has been lodged. The Education SEPP while providing for the acceptance of a non-compliant height does not appear to waive the requirement for a Cl 4.6 objection to address the breech of the building height control as imposed by the NSLEP. Whether this is necessary is ultimately a matter for the consent authority.

Referrals

The Department of Planning, Industry and Environment have requested comments be submitted on the proposed development by 20 May 2020. A number of internal referrals have been requested including Environmental Health (including issues of acoustics and lighting); Engineering (public domain, stormwater, vehicular access) and landscaping that are still being assessed.

Comments provided from Council's Heritage and Traffic Sections are provided below. Prior to lodgement the plans were referred to the Design Excellence Panel and the plans have not changed since this time. Accordingly, the comments of the Design Excellence Panel remain relevant and are provided below.

Heritage

Council's Heritage Planner provided the following comments:

1. Heritage Status and Significance

The site includes several heritage significant built elements being Masalou, Mercy Hall and O'Reagan House that have been sited around the Holy Grass (circular lawn area) and are aligned to create an open-sided square. There is also the heritage significant Chapel which, by contrast, breaks away from these alignments with a traditional Christian north/south - east/west alignment. It opens the space out from the Holy Grass towards the driveway and entry on Miller Street creating a formal entry. As a group, these buildings create the character of the site and define its cultural heritage.

The large trees also contribute to the landscape setting of the buildings, as does the historic brick wall that wraps around Berry and Miller Streets enclosing the site. Together the landscape and built elements create a low scale historic group of heritage items that are viewed by the public from Miller Street as a borrowed landscape.

2. Heritage Impact Assessment

a) North Sydney LEP 2013 Clause 5.10 and Part B Section 13 of North Sydney DCP 2013

Demolition of Existing Miller Street Historic Wall – The existing wall contributes to the Catholic cultural history of the enclosed school site and also contributes to the Miller streetscape. It is noted on the site's Statement of Significance as being an item of heritage significance. Demolition of the wall is therefore not supported.

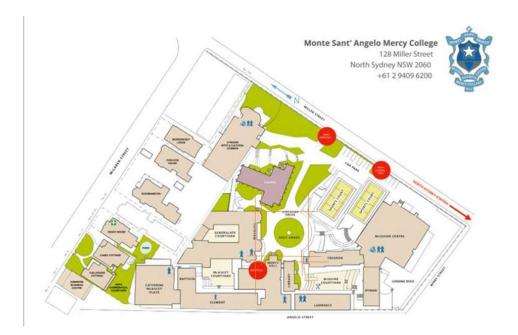
It is recommended that the proposal be re-designed to retain the existing wall.

Curtilage and Setting – The visual curtilage and setting of the heritage significant buildings should be retained. The facades of Mercy Hall, the Library, Masalou and Treanor are all aligned either perpendicular or parallel to each other (see image below) and assist in creating a spatial relationship with each other in a square formation as well as creating a sense of enclosure to the Holy Grass. The Chapel as noted above, has a traditional east/west alignment, opening up to the driveway and gate. The McQuoin Centre on the corner of Berry/Miller Streets is set far enough downslope and away from the Holy Grass and the heritage-significant buildings that it reads as a separate built element on the site and has little relationship with them.

The proposed development, however, relates to none of the historic existing alignments and established curtilage but instead relates to the alignment of the McQuoin Centre and the Miller Street frontage. The new development should acknowledge the alignment of the heritage buildings as it will be seen in the context of the historic buildings due to their proximity.

Because of the proposal's bold design and monolithic massing, it is likely that it will visually overwhelm the traditional setting of the group of heritage significant buildings and their visual curtilage over the Holy Grass. Although the northwest corner has been 'eroded' at Ground Level on the North Elevation of the design, the curved frame on the North Elevation takes the eye to the right-angled corner of the building on the upper levels giving it visual dominance. The new development will therefore confuse the setting of the heritage items by its corner conflicting with the established building alignments.

It is therefore recommended that to comply with Part B Section 13.4 Objective O1, Provisions P1 and P3, that the façade on the north elevation be parallel to the Chapel or that it turn approximately halfway along its North Elevation to be parallel with the Chapel. This will acknowledge the alignment of the Chapel and the established building alignments at the cultural core of the site rather than creating an incursion. It will also allow the formality of the driveway to the highly significant Holy Grass to be retained and potentially enhanced.



Plan showing the square alignment of the heritage buildings around the Holy Lawn except for the Chapel.

Source of Base Map: https://www.ahigs.nsw.edu.au/Schools/Monte-Sant-Angelo

Character - North Sydney Local Government Area is characterised by heritage buildings and conservation areas. Where infill buildings are proposed, Part B Section 13.4 of NSDCP 2013 requires the design to be respectful of the significance of the heritage items. The existing heritage buildings have a strong vertical emphasis with their spires, posts, columns and vertically proportioned windows as well as fine-grained detail. The proposed new building by contrast, will have a strong horizontal massing that will conflict with the established character.

It is recommended that the form of the proposed building be further articulated and that a greater vertical emphasis be placed on the building's massing, not just the façade treatment. It is also recommended that some fine-grained detail be introduced such that the character of the new building does not overwhelm the character of the historic buildings.

Setback- Miller Street currently enjoys a borrowed landscape from the school with its tall trees, shrubberies and historic facades. The proposed new building will remove a large portion of these existing views from the public domain but does not provide adequate compensation in the form of a landscape buffer along the Miller Street frontage. The proposed setback of 5m of the new building from the heritage wall on Miller St in addition to its scale will harden off the streetscape.

It is recommended that the setback along Miller Street match that of the Mc Quoin Centre and that a deep soil planted buffer zone of trees is provided behind the Miller Street heritage wall.

Views- Views to and from the heritage buildings should be retained where possible from the public domain. The design proposal will limit views to the driveway entry.

As noted above, it is recommended that this impact be ameliorated by providing a landscaped buffer of trees along the Miller Street frontage and that views up the driveway be improved by re-alignment of the new building's North Elevation.

Materials- New buildings may be constructed using contemporary materials and with a contemporary character, subject to the infill building being visually submissive to the heritage items. The proposed reinforced glass concrete blades and frame will be a visually strong element and will visually dominate the fine-grained detail of the historic buildings.

It is recommended that there be greater variety in the material selected and the materials respond to the existing palette of materials.

Demolition of Existing Building- The existing building does not appear to have any heritage significance however; Council must have a submission from the applicant that addresses NSDCP 20-13 Part B Section 13.8 in order to adequately assess the proposal.

Demolition of heritage items

- **P1** Heritage items must not be demolished, and demolition will not be supported by Council.
- **P2** Despite P1 above, Council may consider the demolition of a heritage item, but only where an applicant can satisfactorily demonstrate:
- (a) why it is not reasonable to conserve the heritage item taking into consideration:
- (i) The heritage significance of the property; and
- (ii) the structural condition of the building; and
- (iii) pest inspection reports; and
- (iv) whether the building constitutes a danger to the public. Note: A report from a qualified quantity surveyor is required where the costs of retention are part of the justification for the proposed demolition.
- (b) that alternative options to demolition have been considered with reasons provided as to why the alternatives are not acceptable.

It is therefore recommended that an Addendum to the Heritage Impact Statement be provided that addresses this planning control in the DCP.

Traffic

Council's Traffic and Traffic Engineer provided the following comments:

Traffic Generation

The proposed development will not have additional traffic to the roads network therefore no traffic implications in terms of road network capacity.

Parking Provision

Based on the Council's DCP 2013 (amended on 5 November 2015 Section 10) the following maximum parking provisions for this development are required:

Type of Parking	North Sydney Council DCP	
	2013	
	Requirement per dwellings	
Educational establishments	1 space per 6 staff	29
(171)		
Total car parking		29

The development proposes to retain the existing number of parking spaces on the basement car park totalling 55 spaces for staff and eight (8) at-grade parking spaces in the under-utilised 'Sisters of Mercy' car park for emergency vehicle and accessible parking.

Even the proposed number of parking spaces exceeds Council's maximum parking rate, but it is acceptable to permit the school to retain the existing number of parking spaces within the site to avoid unnecessary pressure on the available on-street parking.

Bicycle Parking

Council's DCP does not make any provision for parking for bicycles for Educational Establishments. However, the proposed development provides eight (8) additional bicycle parking spaces in the basement car park.

Conclusion

It is recommended that the proposed development be supported with the following conditions be imposed:

- 1. That a Construction Management Plan be prepared and submitted to Council for approval by the North Sydney Traffic Committee prior to the issue of the Construction Certificate. Any use of Council property shall require appropriate separate permits/approvals.
- 2. That all aspects of the carpark comply with the Australian Standard AS2890.1 Off-Street Parking.

Design Excellence Panel

Prior to lodgement, the proposed development was considered by the Design Excellence Panel with the following comments provided:

The Panel provided the following comments in relation to key issues and concerns of the proposed development.

The Monte Sant' Angelo Mercy College site is an important site with respect to its prominent civic presence to Miller Street in the North Sydney centre, as well as in the context of the significant groupings of heritage buildings and places within the school campus. The Panel commends the school for holding a design competition to facilitate achieving design excellence on this important site. The Panel comments are made in this context. The Panel raised concerns with the excessive visual bulk and scale of the building on Miller Street. There is also potentially a bulk and scale issue with respect to nearby heritage buildings within the site. An independent heritage assessment and additional renders will assist in determining this. Height and scale are important considerations on this heritage site. Overall, the building design does not satisfactorily respond to the site context. There is a need to reduce the visual bulk and the perceived bulk. It is noted that the upper recreational roof area screening can be amended to lower the apparent bulk and scale of the building. Along with some setbacks to the mechanical plant, service riser and garden areas along the eastern frontage, this potentially could resolve the presently excessive visual bulk issue.

This part of Miller Street is one of the highest amenity sections of Miller Street, with civic building frontages augmented by strong tree canopy and 'borrowed' landscapes within the site visible from the street. As an important element of the civic spine, this site will contribute to the vision for the North Sydney CBD. The Panel felt this building must complement the character of Miller Street and needs to continue to contribute with respect to scale and landscape. The Panel believes that the proposal in its current form will detract from both Miller Street and potentially the heritage qualities of the site.

Consideration should be given to the termination of the building top to generally comply with the building height control. This proposed building, including its setback to Miller Street, is too assertive and dominates the streetscape.

With regards to the detail design, the blades on building elevations are proposed for solar control, and considered to be appropriate in principle in relation to their function and appearance However due to their application to all three upper levels and consequential vertical emphasis, the building facades will be perceived as a singular bulk, and the visual impact of the height exacerbated. If they were to terminate one storey lower, and the top level were to be set back, the outcome could well be acceptable. The unprotected large glass panels on the north-western façade are also questioned.

The building setback on Miller Street should provide a setback consistent with the school building (McQuoin Centre) to the immediate south and retain a strong landscape. The proposed pathways should be reconfigured to achieve this. The plans are unclear whether this setback will align with this building.

Part of the streetscape on Miller Street is the strength of the continuous brick wall, tying together the different buildings behind, and with the landscape backdrop creates the strong civic setting and this brick wall should be retained. Its partial removal is not supported as it will significantly detract from the street, increasing the apparent scale of the proposed building further.

The proponent advised that part removal and modification to the brick enclosing boundary wall would benefit the building users in one space with some additional solar access and outlook within the site. The panel agreed that the design must balance the broader public domain with the user experience. Retaining the wall and greening a usable setback will contribute to the Miller Street streetscape and provide user amenity.

There should be a landscaped barrier between the driveway entrance and the adjoining entrance area to the gymnasium and new northern pathway, which could attract considerable pedestrian traffic, -for both safety and aesthetic reasons.

The Panel felt that the greenery between the wall and the building to be a critical element within the streetscape. The setback between the building and the wall should be provided with landscaping including canopy trees, to contribute to the character of Miller Street as the wall immediately to the south currently does. Miller Street is a street of canopies and the design should reflect and incorporate this greenery. Greening Miller Street and contributing to the canopy edge should be the priority.

With regards to the context of the new building internally within the site, it is necessary to understand the relationship between the new building and existing buildings within the heritage site, including the Chapel. Additional drawings detailing the internal relationships with the new and old buildings are critical to understanding the impact of the new works within the site. As part of this analysis, further demonstration that the new building fulfills the transitioning role (as outlined as key design principle by the proponent) from fine grain to large format is required.

The use of the curved undercroft building edge is questioned and the design is not considered to be convincing given the deep south facing undercroft space created adjacent to the Staff room Level 0, and the very rectilinear form of the other buildings that define spaces. These curved elements will be the only curves amongst a set of buildings with square hard edges. The Panel noted the curves feature as part of the garden / landscaped design however questioned whether curves related to building structure were appropriate.

Clarification is requested as to over shadowing impacts as the shadow diagrams do not appear to take account of the tennis court fencing. The proposal must ensure it does not adversely impact on surrounding special areas within the CBD.

Sustainability measures should be incorporated into the building, for example rainwater recycling for irrigation, and solar collectors. Consideration should be given to whether the building will produce less carbon or other appropriate initiatives. This is considered to be of particular importance given the science theme of the building. The theme of individuals taking action would be very powerful. The design should genuinely contribute to resilience including the introduction of landscaping.

The kiosk substation located at the gate entry, should be discreetly integrated within a building footprint or other structure. Booster values and utilities should also be carefully integrated and made discreet to positively contribute to the heritage setting and public domain.

Consideration could be given to providing public access at certain times to the proposed heritage trail.

Conclusion

The Panel is supportive in principle of the application and commends the applicant on the quality of the submission. However, the design could not be endorsed unless the issues raised above, particularly those in relation to the alignment along Miller Street, and the visual bulk are addressed.

CONCLUSION

As Council is not the consent authority for the subject application, this report has been prepared to provide Councillors with information on the subject application and provide a summary of those issues and concerns raised to date. A number of referrals are outstanding at the time of finalising this report, and additional issues may be raised.

As set out in this report, a preliminary review of the proposed development has raised concerns regarding building height, design, setbacks, landscaping and heritage impacts including concerns raised with the partial demolition of the brick boundary wall on Miller Street.

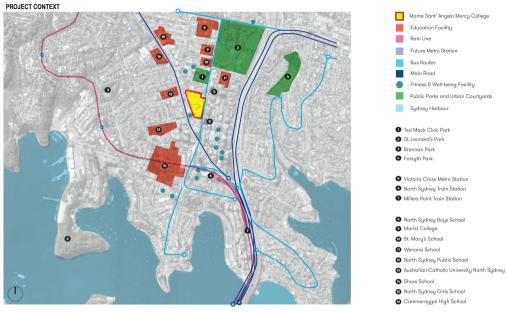
It is recommended that Councillors note the issues raised within this report and provide support to prepare a submission to the Department of Planning, Industry and Environment on the application identifying issues addressed in this report.

Attachment 8.3.1

MONTE SANT ANGELO COLLEGE SCIENTIA PROJECT

NO SHEET NAME A00.00 COVER SHEET A01.01 A01.02 EXISTING & DEMOLITION PLAN PROPOSED SITE PLAN A02.00 A02.01 A02.02 PROPOSED LEVEL -3 PLAN PROPOSED LEVEL -2 PLAN PROPOSED LEVEL -1 PLAN A02.03 A02.04 A02.05 PROPOSED LEVEL 0 PLAN PROPOSED LEVEL 1 PLAN PROPOSED LEVEL 2 PLAN A02.06 A03.01 A03.03 PROPOSED LEVEL 3 PLAN STREET ELEVATIONS A03.11 A04.03 SECTIONS SHADOW DIAGRAMS - EQUINOX A04.04 SHADOW DIAGRAMS - WINTER A06.04 PERSPECTIVES x03.15 egress stairs

SURVEY SET AS REFERENCE: PLAN SHOWING DETAIL & LEVELS OVER LOT 1 DP104072 LOT A & B DP339358 LOT 4-9 & LOT 11 DP 5030 LOT 1-8 DP 262534



EXTERNAL OVERALL 3D









MCLAREN ST EXISTING PARKING & BERRY ST SITE EXISTING & DEMOLITION

3731st Council Meeting - 18 May 2020 Agenda

Attachment 8.3.1

LEGEND



TREES TO BE DEMOLISH

TREE 4: CHEESE TREE TREE 7: JACARANDA TREE 8: CAMELIA TREE 12: TURPENTINE

TREE 12: TURPENTINE
TREE 13: MAGENTA CHERRY
TREE 14-16: CAMPHOR LAUREL
TREE 17-16: BANGALOW PALM
TREE 19: KENTIA PALM
TREE 19: KENTIA PALM

REFER TO ARBORICULTURAL IMPAG

5 SSDA 1600,2020
4 SSDA 2100,2020
3 SSDA 1800,2020
2 SSDA 1400,2020
1 SSDA 064PT 7700,2020
Rev Descriptor Use

ABN: 84006394261 NSW Nominated Architects:Tom Jordan 75 Richard Leonard 7522, David Tordoff 8028

Monte Sant Angelo Mercy College Scientia Project

128 Miller St, North Sydney

Drawing Title

EXISTING & DEMOLITION PLAN

Status

SSDA

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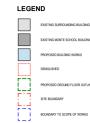
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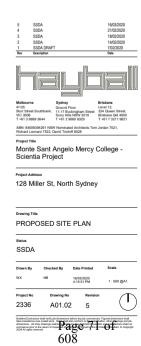
3731st Council Meeting - 18 May 2020 Agenda

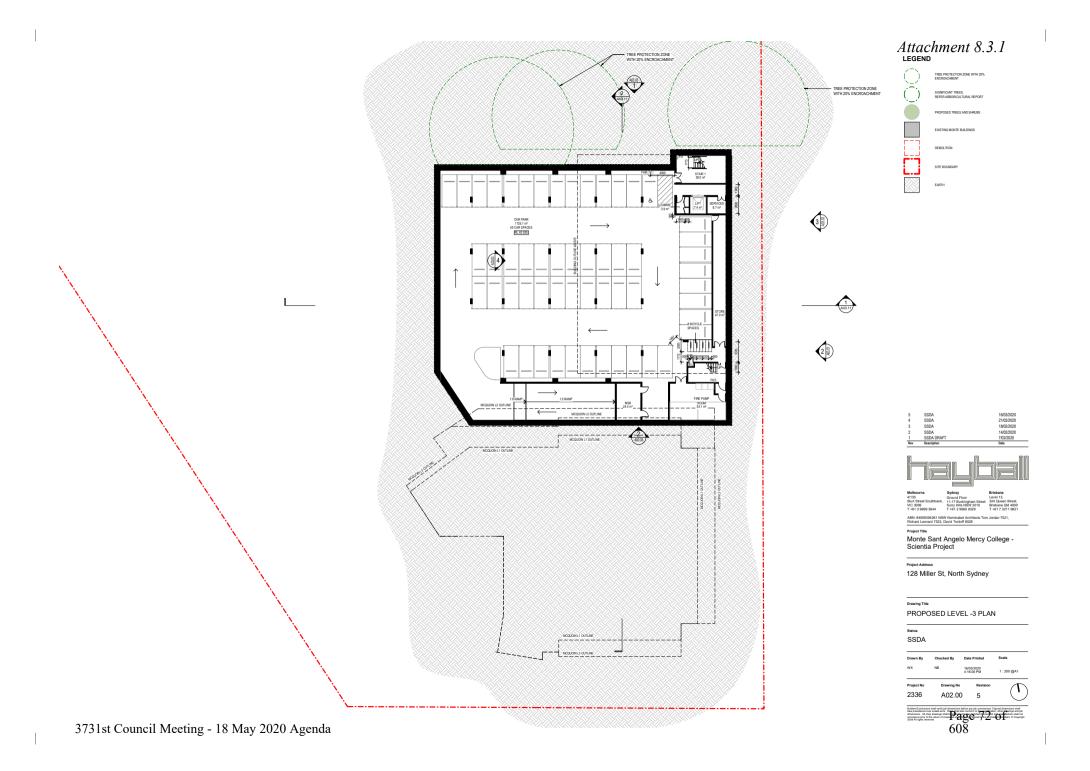
PROPOSED SITE PLAN
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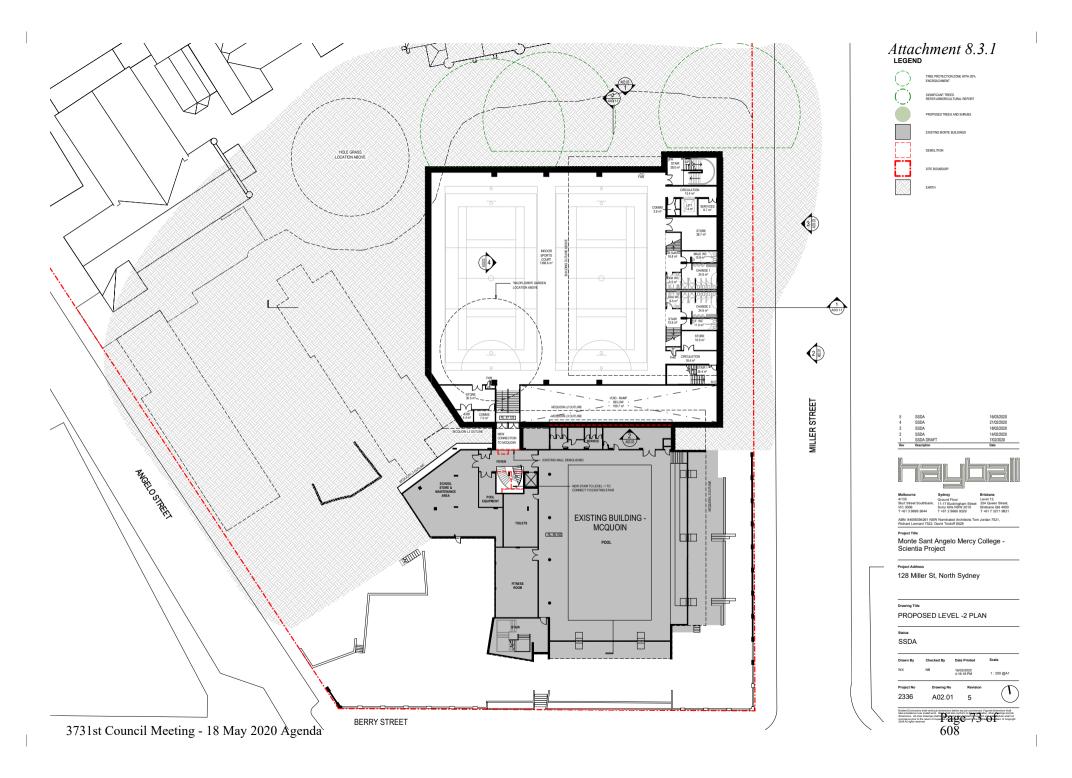
Attachment 8.3.1

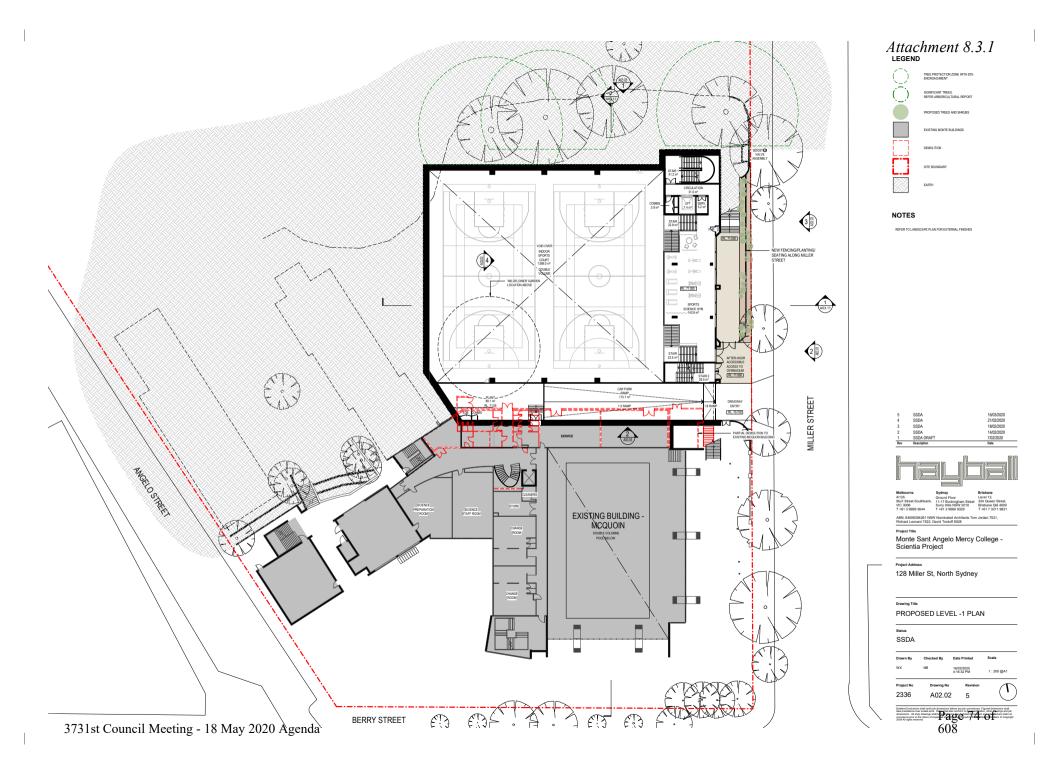


NOTES: 1. REFER TO FLOOR PLANS FOR SITE AND FLOOR RLS.









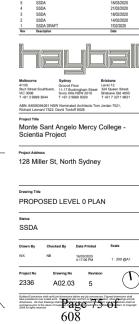


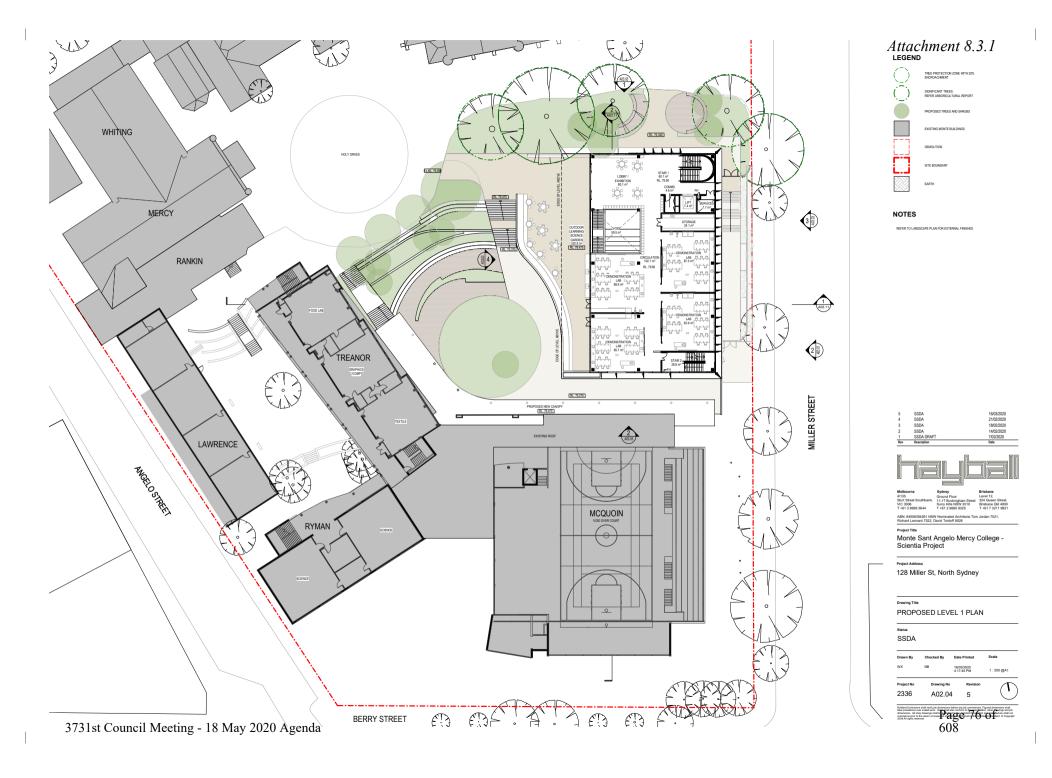
Attachment 8.3.1

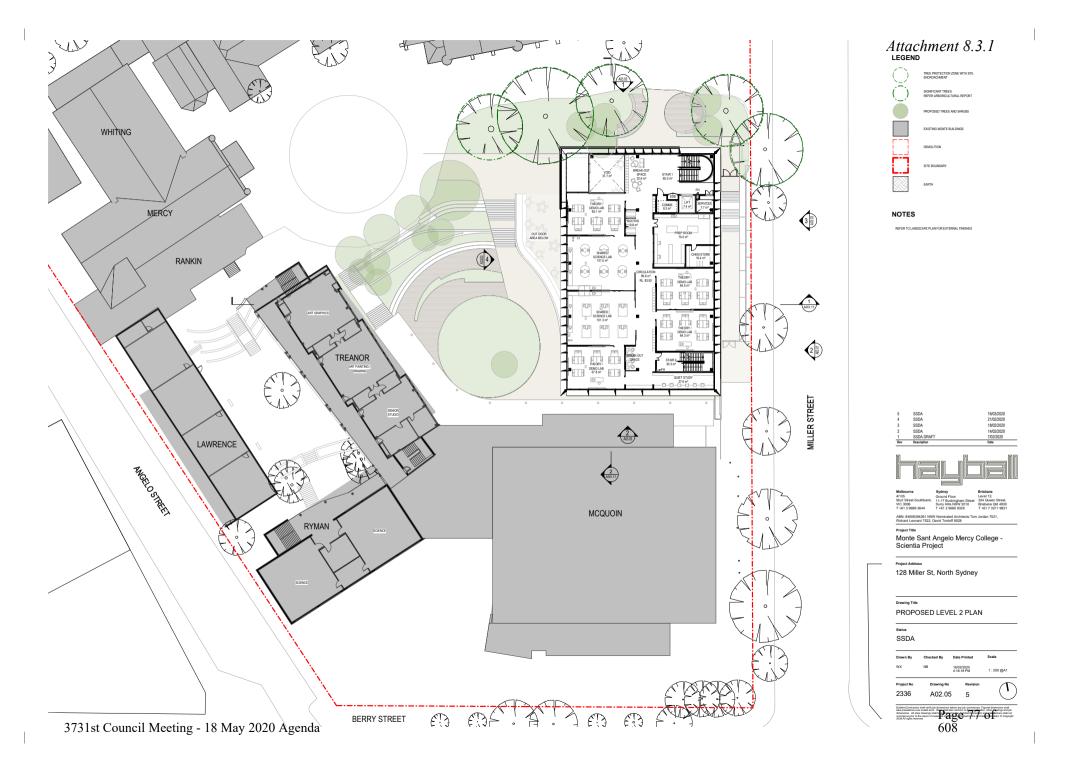


NOTES

REFER TO LANDSCAPE PLAN FOR EXTERNAL FINIS

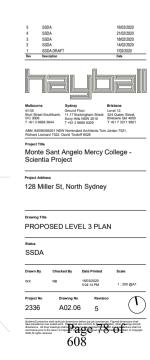


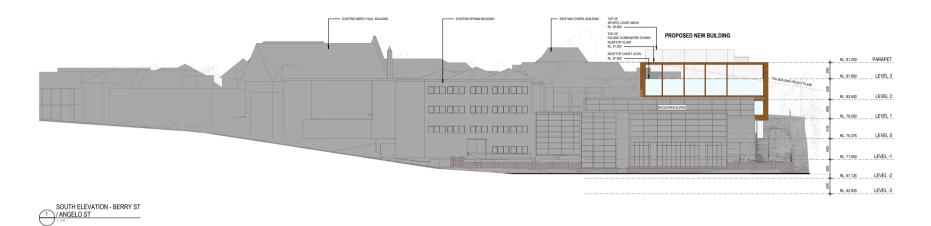


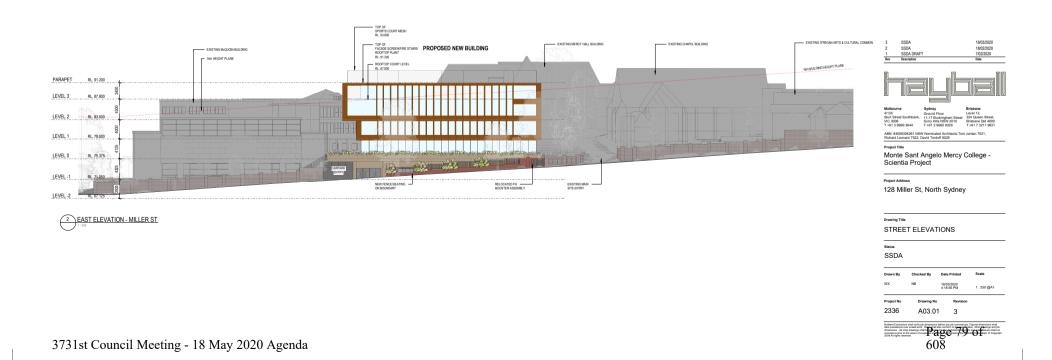


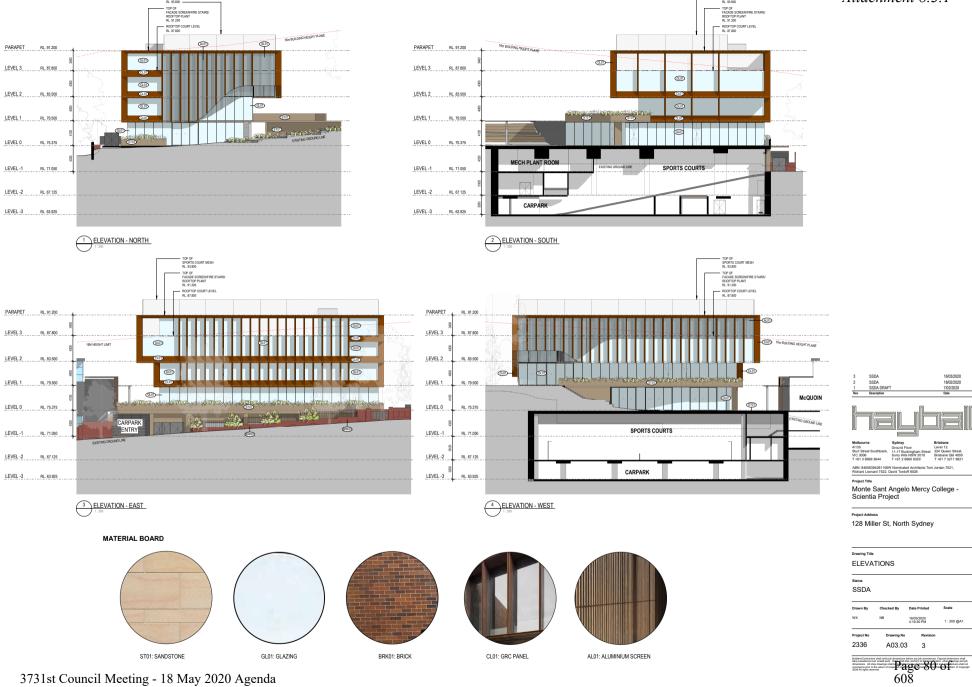
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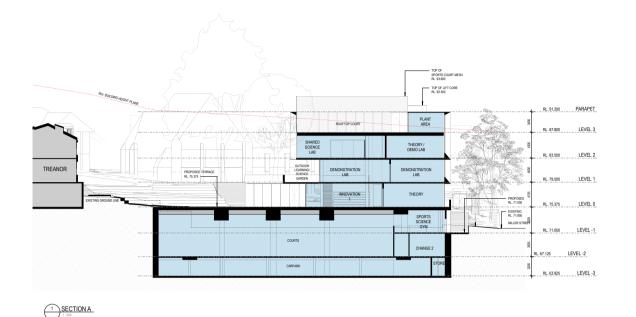


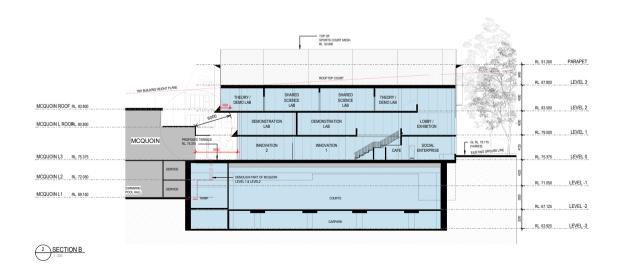


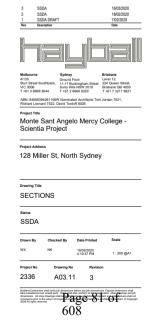




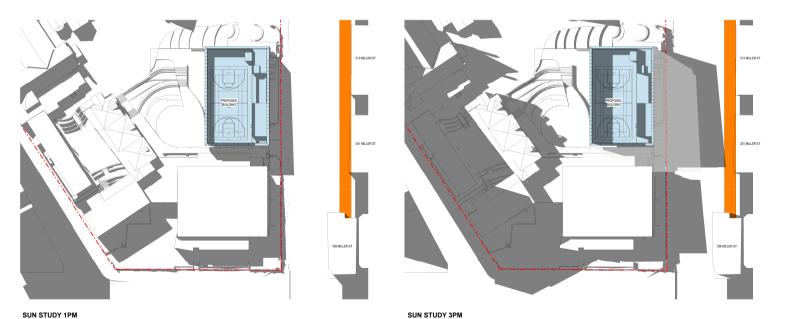




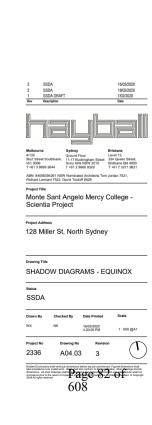




SUN STUDY 9AM SUN STUDY 11AM



3731st Council Meeting - 18 May 2020 Agenda



Attachment 8.3.1

MILLER STREET SPECIAL AREA, AS IDENTIFIED IN THE NORTH SYDNEY LEP

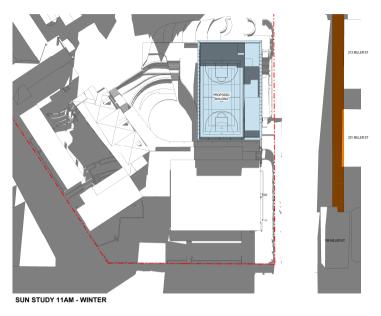
SITE BOUNDARY
PROPOSED BUILDING

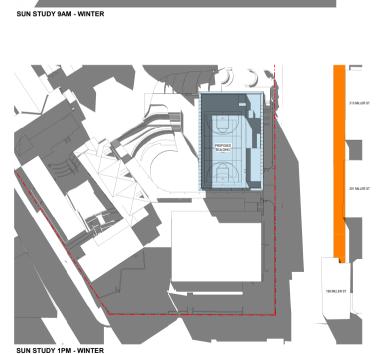
SHADOW

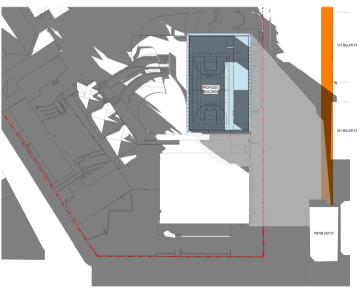
2PM NEW BUILT CAST SHADOW

EXTENT OF SHADOW RELATING TO LEP CLAUSE 6.3(2), CONFIRMING NO NET INCREASE IN OVERSHADOWING TO THE SPECIAL AREA BETWEEN 12 PM AND 2 PM FROM THE MARCH EQUINOX TO THE SEPTEMBER EQUINOX (INCLUSIVE)









Monte Sant Angelo Mercy College -Scientia Project 128 Miller St, North Sydney SHADOW DIAGRAMS - WINTER SSDA 16/03/2020 4:20:41 PM 2336 A04.04 4 SUN STUDY 3PM - WINTER y job dimensional belion any job commercias. Figured dimensiona shall be a commercial for the commercial dimensional shall be a commercial for the commercial for the

3731st Council Meeting - 18 May 2020 Agenda

Attachment 8.3.1





VIEW FROM THE MAIN DRIVE LOOKING SOUTH

VIEW FROM MILLER STREET LOOKING WEST



VIEW FROM THE LOWER COURTYARD LOOKING TOWARDS THE CHAPEL



VIEW FROM THE UPPER COURTYARD LOOKING EAST

Note: Trees is foreground have been removed from this view to show the full extent of the building. Please refer to the Landscape Drawings for location and species of proposed trees.



8.4. Draft Civic Precinct Planning Study

AUTHOR: Pedro Garcia, Senior Strategic Planner Urban Design

ENDORSED BY: Marcelo Occhiuzzi, Acting Director City Strategy

ATTACHMENTS:

- 1. Civic Precinct and Surrounds Planning Study [8.4.1 46 pages]
- 2. Civic Precinct and Surrounds Background Report [8.4.2 281 pages]

PURPOSE:

This report provides an overview of the draft Civic Precinct Planning Study prepared by Council in collaboration with an external team of consultants and recommends placing the study (Attachment 1) on public exhibition.

EXECUTIVE SUMMARY:

In October 2018, North Sydney Council accepted Accelerated LEP Review Grant Funding from the NSW Department of Planning, Industry and Environment. The funding aims to assist Council align the North Sydney LEP 2013 with the Greater Sydney Commission's North District Plan (2018). As part of this effort, the Civic Precinct Planning Study was identified as one of the studies to be funded from the state grant.

The study focuses on the area directly north of North Sydney CBD. The objectives of the study are to plan for the arrival of the new metro station in 2024 and to establish the role of the Civic Precinct within the wider LGA context, developing a future vision for it.

The study has been undertaken in two stages. Stage 1 was carried out in mid-2019 to understand community's aspirations for the area and was reported to Council in October 2019. This report presents Stage 2 of the Planning Study. Stage 2 has been developed in collaboration with an external team of consultants led by Conybeare Morrison (CM+), a leading urban design and landscape architecture firm.

Based on extensive analysis of the area, and further public consultation workshops, the consultant team has developed a suite of technical studies that underpin the draft Civic Precinct Planning Study. The draft study is at Attachment 1. The technical studies are at Attachment 2.

The draft planning study:

- 1. Outlines a vision and an urban structure for the Civic Precinct.
- 2. Establishes the desired character and feel for the precinct that is supported by a series of urban design principles to guide future growth.
- 3. Outlines a series of strategies and actions to achieve the vision for the precinct.
- 4. Identifies opportunities to deliver additional public domain, improve transport networks and provide jobs and housing in appropriate locations.

The study envisions the area becoming a stronger and more attractive civic and educational campus with a low-scale and leafy character and feel. A low-scale civic campus that complements the high-density commercial cores of North Sydney CBD and St Leonards.

Actions under the planning study are:

- Create more jobs and housing opportunities near the new northern Metro portal;
- Increase amenity and activation along the Pacific Highway;
- Support small to medium sized business growth;
- Preserve heritage;
- Provide urban design guidelines for education facilities;
- Improve public open space;
- Identify access and active transport network improvement opportunities;
- Undertake further review of Falcon Street; and
- Undertake further review of Civic Core.

Public benefits that may be negotiated as part of any planning proposal to amend the North Sydney LEP 2013 are a new location for Kelly's Place Day Care, community centre, public domain upgrades, affordable housing and public art.

FINANCIAL IMPLICATIONS:

On 29 October 2018, North Sydney Council accepted Accelerated LEP Review Grant Funding from the NSW Department of Planning, Industry and Environment to align the North Sydney LEP 2013 with the North District Plan (GSC 2018). The Civic Precinct Planning Study (formerly Northern CBD Planning Study) was identified as one study to be funded from the Accelerated LEP Review Grant. The study is fully grant-funded and approximately \$205K has been expended to date with a small amount outstanding for exhibition and minor adjustments that may be required from the consultant team.

RECOMMENDATION:

- **1. THAT** the draft Civic Precinct Planning Study (Attachment 1) be adopted for the purposes of public exhibition.
- **2. THAT** the draft study be placed on public exhibition for a period of 6 weeks.
- **3. THAT** relevant state authorities such as Transport for NSW (TfNSW), Department of Planning, Industry and Environment (DPIE) and Greater Sydney Commission (GSC) be notified of the exhibition.
- **4. THAT** following exhibition, a report be prepared accounting for submissions made and any resulting amendments to the final Civic Precinct Planning Study.
- **5. THAT** Council note the significant risks posed by the current alignment outlined in the Western Harbour Tunnel and Warringah Freeway EIS to the successful pursuit of the some of the actions recommended in this planning study.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

1. Our Living Environment

- 1.1 Protected and enhanced natural environment and biodiversity
- 1.2 North Sydney is sustainable and resilient
- 1.3 Quality urban greenspaces
- 1.4 Public open space and recreation facilities and services meet community needs
- 2. Our Built Infrastructure
- 2.1 Infrastructure and assets meet community needs
- 2.2 Vibrant centres, public domain, villages and streetscapes
- 2.3 Sustainable transport is encouraged
- 3. Our Future Planning
- 3.1 Prosperous and vibrant economy
- 3.3 North Sydney is smart and innovative
- 3.4 North Sydney is distinctive with a sense of place and quality design
- 3.5 North Sydney is regulatory compliant
- 4. Our Social Vitality
- 4.1 North Sydney is connected, inclusive, healthy and safe
- 4.2 North Sydney is creative and home to popular events
- 4.4 North Sydney's history is preserved and recognised
- 5. Our Civic Leadership
- 5.1 Council leads the strategic direction of North Sydney
- 5.3 Community is informed and consulted

BACKGROUND

On 29 October 2018, the Council accepted Accelerated LEP Review Grant Funding and subsequently agreed with the NSW Department of Planning, Industry and Environment to prepare specific projects to bring the North Sydney LEP & other planning instruments and policies into closer alignment with the Greater Sydney Commission's North District Plan (2018). The Civic Precinct Planning Study is one of these projects.

Following a public procurement process, Council appointed a team of consultants led by Conybeare Morrison in October 2019 to undertake this work.

In the meantime, Council also carried out Stage 1 of the study with preliminary consultation taking place between 11 July and 8 August 2019. The outcomes of Stage 1 were reported to Council in October 2019 and the outcomes were noted. Further, more focused consultation occurred in November 2019.

CONSULTATION REQUIREMENTS

Community engagement will be undertaken in accordance with Council's Community Engagement Protocol.

Due to the current Covid-19 restrictions in place, it is recommended that the Draft Civic Precinct Planning Study be placed on exhibition for a period of 6 weeks with mostly an online presence.

DETAIL

1. Purpose & Background

The purpose of the Civic Precinct Planning Study is to establish the role of the Civic Precinct within the wider LGA context and develop a vision for it.

The aim of the study is to understand the impacts resulting from the arrival of new public transport infrastructure to the area, as well as from the evolving changes occurring in the surrounding areas and to formalise an urban design framework for the future of the precinct.

This vision and framework will then help determine any necessary changes to the area's built form and public domain and establish criteria for any new developments that may be identified as being appropriate in the precinct.

The study will also ensure that the Civic Precinct is aligned with the objectives and targets of the Greater Sydney Commission's North District Plan (2018) and Council's Local Strategic Planning Statement (2020).

1.1 The Study Area

The study area is directly north of North Sydney CBD. It is bounded by McLaren Street, Pacific Highway, Falcon Street and the Warringah Freeway with an additional area east bounded by Walker Street and Berry Street at the southeast corner.

The Study responds to the construction of the Victoria Cross Metro Station northern portal, located at the intersection of McLaren and Miller Streets which will provide greater access to public transport to the area. This metro portal has generated development interest (with four planning proposals already lodged within the study area) and prompted a closer look at any further implications of the new public transport infrastructure.

Additionally, there have been major changes to areas surrounding the Civic Precinct including the St Leonards / Crows Nest Planned Precinct and the Ward Street Masterplan proposing changes to the urban structure around the study area.

The study is part of a Council effort to align the statutory framework with the objectives and directions identified in the Greater Sydney Commission's Regional Plan and North District Plan as required by the Department of Planning, Industry and Environment (DPIE). The work has funded by a grant from the NSW Government.

1.2 Drivers of the Study

Strategic Context

North Sydney is part of the wider Harbour CBD within the Sydney metropolitan region. As the main urban node in Sydney, the Harbour CBD is expected to grow and contribute to the city's economic, civic and social life.

The Metropolitan Plan by the Greater Sydney Commission, and in particular the North District Plan, have set employment and residential targets for North Sydney. These targets include the increase of 21,000 jobs in the CBD and 16,000 jobs in St. Leonards over the next 16 years. Residential targets estimate the need for 3,000 additional dwellings by 2021 (from 2016) and approximately another 11,000 additional dwellings by 2036. These targets have recently been revised by the DPIE, with a current estimate of 10,250 new dwellings by 2036.

Major transformations are occurring in North Sydney CBD and St. Leonards including the introduction of the Sydney Metro. Due to these changes, it is necessary to look at the role, character and future vision of surrounding areas, including the Civic Precinct.

Alignment of Council's Planning Controls with the North District Plan

On October 2018, North Sydney Council accepted Accelerated LEP Review Grant Funding and subsequently agreed with the DPIE to prepare specific projects to bring North Sydney's planning instruments and policies into closer alignment with the Greater Sydney Commission's North District Plan. The Civic Precinct Planning Study is one of these projects.

The Proposed Metro Northern Entry Portal

The new Northern Metro entry will be a 'lift only' portal that provides greater accessibility to the north of the CBD Precinct. Sydney Metro estimate the new northern portal will increase patronage by approximately 5%.

Both the North Sydney Transport Strategy and Sydney Metro prioritise walking and cycling above private vehicle traffic, and the Civic Precinct Planning Study will need to consider this in the study. One of the stated objectives of Sydney Metro is to 'serve and stimulate urban development' and this study seeks to provide a framework to guide, control and facilitate any development stimulated by the proposed northern Metro station portal.

Development Interest

Council has received Planning Proposals for several sites within the study area, and the significant individual increase in density and height has the potential to undermine the character of the area in the absence of a more holistic vision for the precinct. The Civic Precinct Planning Study is to provide that holistic vision and guide any development.

2. Objectives

The objectives of the planning study are:

- To understand the community and stakeholders' visions and needs for future planning of the study area;
- To develop a considered urban planning vision and strategy for the Civic Precinct, establishing a series of urban planning principles for the study area;
- Achieve the objectives and actions identified in the GSC North District Plan;
- Identify, and retain the urban values of the precinct including its heritage and fine-grained structure and 'leafy' character;

- Identify possible improvements in the public domain, including pedestrian linkages, wayfinding, new public open space and improved amenity consistent with the objectives and directions identified in the draft North Sydney CBD Public Domain Strategy (2020);
- Identify the existing and future 'high significance civic places' within the precinct and define their desired character;
- Identify the appropriate density across the study area considering the existing opportunities and constraints, its location within the North Sydney LGA, and current and future access to transport and public infrastructure;
- Explore appropriate opportunities to review building height and density within the precinct in light of the introduction of improved access to public transport; and
- Explore opportunities to deliver public benefit within, or on the periphery of, the study area as a result of redevelopment proposals.

3. Process & Methodology

Stage 1

Council conducted a Stage 1 preliminary community consultation in mid-2019 to understand community's aspirations for the Civic Precinct area. An internal analysis of public facilities was also conducted. This stage yielded a set of community priorities that have informed the design brief for stage 2.

A report compiling community input and reflecting the demographics and character of the area was reported to Council in October 2019.

Stage 2

In October 2019, Council appointed Conybeare Morrison (CM+) and a team of sub-consultants to carry out stage 2.

Based on the brief, extensive analysis of the area and further public consultation workshops, the team developed a vision for the Civic Precinct together with a series of urban design principles and objectives. Their work resulted in a series of proposed actions and strategies to be implemented across the precinct in order to achieve the vision for the area, together with proposed changes to the planning controls in selected locations. This work underpins the final planning study subject of this report (Attachment 1)

The full documentation developed by the consultants' team is at Attachment 2 to this report. These consultant reports underpin the ideas put forward in this study.

3.2 Project Control Group

At the start of the project, Council established a Project Control Group (PCG) to provide direction and feedback to the consultant team. The PCG comprised staff from each Division of Council. The ideas of the study were presented to Council's PCG at several points during the process as well as to the North Sydney Design Excellence Panel.

3.3 Project Methodology

The methodology for Stage 2 of the study is outlined below. Part 1. Analysis and Opportunities

- Extensive urban design analysis of the current environment and planning framework;
- Identifying opportunities and constrains of the study area;
- Testing of the existing planning controls;
- Identifying public domain and public benefits opportunities; and
- Establishing preliminary urban design principles.

Part 2. Further Community Consultation

Further consultation with the community, including representatives from local schools, was undertaken via two workshops. At the workshops, the opportunities and principles were discussed with the participants through a series of activities.

Part 3. Urban vision and suggested actions

Incorporating the feedback of the workshops and based on the findings or Part 1, the design team subsequently undertook:

- Alternative scenarios for the precinct;
- Internal design workshop with Council;
- Scenario refinement and vision for the precinct;
- Strategies and actions; and
- Documentation of the study.

The consultant team then prepared a suite of technical studies for the precinct. These reports, which included traffic, heritage and sustainability advice, landscape, public domain, and urban design, underpin the draft Planning Study prepared by Council.

4. Strategic Context

The attached Study identifies its consistency and relationship to the Greater Sydney Commission's Regional Plan (A Metropolis of Three Cities - 2018) and North District Plan (2018). The Study is specifically identified in Council's strategic response to these documents, being the North Sydney Local Strategic Planning Statement and is consistent and supportive of the North Sydney Community Strategic Plan. These latter documents supporting a strong place based, strategic led planning basis for managing change.

5. Proposed Planning Strategy Overview

This Planning Study sets up the vision and the framework to deliver a modern civic campus for North Sydney that complements and balances the high density commercial cores in the vicinity (North Sydney CBD and St. Leonards) and provides the community with a generous, active and attractive social open space.

The Civic Precinct holds a dual role for the North Sydney LGA, as both a civic centre and as a social and recreational node. The current residential and civic buildings, as well as existing open space, are characteristic of the area. The area is the location of the Council Chambers as well as the setting for community markets, exercise classes and social gatherings.

5.1 Vision

Through small improvements and selected interventions, the Civic Precinct will become a modern, connected, and attractive Civic & Education Campus with a strong network of pedestrian links and open spaces, a low-scale village feel, and a leafy appearance.

The area will continue to be characterised by older educational institutions, heritage-listed Victorian terraced houses, the greenery of St Leonards Park and a cluster of civic buildings and public open space at the precinct's centre.

The village feel of the Civic Precinct will be preserved and enhanced to strengthen its anchor role within North Sydney's green-civic corridor that connects all the parks and reserves from Port Jackson to Middle Harbour and provides a lower density green break between the two high-density areas of North Sydney CBD and St Leonards.

This distinct Civic and Educational campus will be complemented by vibrant mixed-used zones at the NW and SE of the precinct that will act as soft transition zones towards the CBD and Crows Nest (Figure 1).



Figure 1. Diagrammatic skyline along the Pacific Highway

5.2 Principles

Based on community consultation, analysis of the planning study area, and the vision developed for the Civic Precinct, a series of principles have been developed to guide the future planning of the precinct.

These principles have informed and guided the proposed structure for the precinct as well as the implementation strategies and proposed actions:

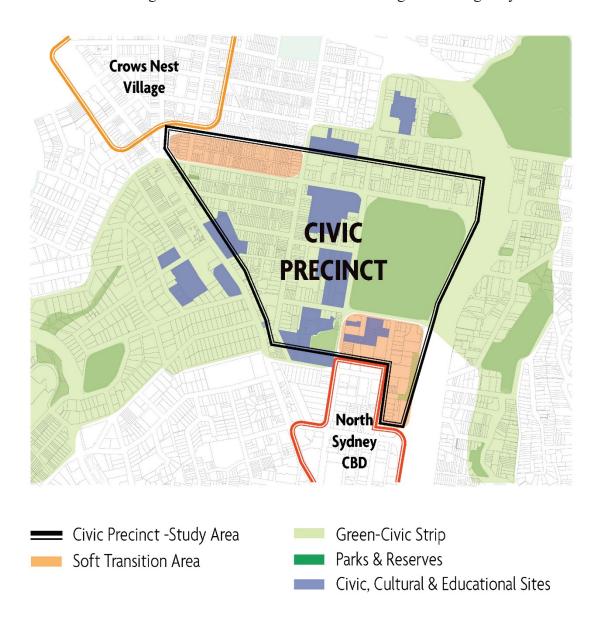
- Preserve, enhance, and strengthen the rich heritage of the precinct;
- Maintain the small-scale character and village feel of the precinct's core;
- Define the edges of Crows Nest Village and North Sydney CBD;
- Manage growth and development which is of appropriate scale and character;
- Encourage the implementation of an education campus;
- Increase the public amenity within the precinct;
- Integrate open spaces into the green-civic spine network;
- Encourage active transport and improved connectivity across the precinct;
- Promote a varied network of small business and commerce;
- Promote housing diversity and affordability; and
- Advocate design excellence and sustainability in new buildings and the public domain.

5.3 Proposed Design Concept

The Civic Precinct is the main social and civic gathering place for the community of North Sydney. The area will keep evolving as a civic and educational campus at its core with two mixed-use transition zones towards the CBD and Crows Nest.

This Civic and Educational campus will continue to be defined by low-scale, tree-lined streets, and by a rich network of connected public open spaces with new pedestrian and cycling routes through the precinct that will link to the new metro station.

At the fringes of the study, mixed-used areas will form soft transition zones towards the CBD and Crows Nest along Walker and McLaren Street and along Pacific Highway.



6. Actions under the Draft Planning Study

The planning study includes the following actions / interventions.

Action 1: Create more jobs and housing opportunities near the Metro

- Consolidate the northern end of the CBD with a Mixed-Use zone that makes the most of the Metro Northern Portal, transitions towards the heritage conservation area and connects to St. Leonards Park.
- Provide a height transition between the commercial core and the lower density conservation area.
- Increase the permeability, connectivity and public amenity of the area. In particular, complete and strengthen the N-S link from North Sydney train station to St Leonards Park through Elliott Street.
- Preserve the Ridge Street Heritage Conservation Area.

Action 2: Develop the Pacific Highway frontage into a medium-scale environment with increased pedestrian amenity

- Create a low-scale streetscape area between North Sydney CBD and St. Leonards that provides for a more human centric and pedestrian friendly environment.
- Bookend either side of the Civic Precinct along the Pacific Highway through increased building heights to allow for a transition into the higher-density areas on either side.
- Widen the footpath by 1.5 metres to improve pedestrian amenity planting and activation.
- Provide active frontages to Pacific Highway where possible.
- Continue the implementation of cycleways along the Pacific Highway.
- Explore ways to improve the Five Ways intersection by diverting traffic if possible.

Action 3: Support small to medium sized business growth through targeted increases to the non-residential FSRs

- Increase the non-residential FSR in the area to provide for smaller commercial tenancy spaces within mixed use buildings.
- Encourage mixed-use buildings with retail and commercial tenancies to increase foot traffic and improve street activation along the main thoroughfares and nodes.
- Build on the existing presence of educational and medical facilities in the area as well as the adjacent CBD and encourage companies and initiatives that support these sectors.

Action 4: Preserve heritage, add value

The Civic Precinct has 93 Local and State Heritage Listed Items and four Heritage Conservation Areas (HCA). Of these items, ninety-one are locally significant, and two are State significant.

It is important to protect and respect these heritage buildings and conservation areas for future generations as they define the local identity and unique character of the area. At the same time we must ensure that the area is well maintained, remains relevant, activated and used, so it is equally important to allow for careful and considered additions that add value to the area and provide updated spaces for modern needs.

Action 5: Undertake further review of Falcon Street

There is some degree of uncertainty around the Falcon Street corridor due to the different infrastructure projects currently ongoing, in particular the DPIE's Planned Precinct, Crows Nest Metro Station and the Western Harbour Tunnel Motorway project. There are varying levels of uncertainty surrounding these projects.

It is recommended that once the outcomes of these projects are known and better understood, a detailed Urban Design Study of Falcon Street is undertaken, focusing mostly on the area between Five Ways and West Street.

The corridor is a major traffic thoroughfare connecting the Military Road Corridor with Pacific Highway, and at the same time, it is the transition between the Civic Precinct and Crows Nest Village. Being able to combine both roles will be key in order to achieve a successful outcome.

Action 6: Undertake further review of Civic Core

The Civic Core is bounded by McLaren, Ridge, Miller and West Streets and includes a high amount of publicly owned assets. Greater connectivity and legibility between the different buildings and a more attractive and pleasant civic environment is desirable.

It is desirable to develop a detailed masterplan for the block.

Action 7: Provide urban design guidelines for education facilities

There is a strong presence of educational facilities within the precinct. Most of them present a very insular layout with poor interfaces with the surroundings.

A series of guidelines promoting more integration and openness with the local context and the community have been developed to guide any future development of the educational institutions. These guidelines include the following recommendations as:

- In principle height should be located at the core of the site with a lower scale perimeter.
- Higher building should not have any presence or impact onto the public domain.
- Ensure that school amenity (overshadowing / privacy) is not affected by any new development.
- Ensure future development does not impact the visual amenity of the surrounding area.
- Integrate educational facilities into the neighbourhood by improving their interfaces with adjoining streets and the public domain.
- Improve the area's permeability by encouraging pedestrian through site links during day light hours and after-hours public access to schools' facilities, subject to security considerations.

Action 8: Improve public open space

The study has identified 4 main opportunities for public domain improvement.

- 1. Implementation of the St Leonards Park Landscape Masterplan.
- 2. Implementation of the Civic Spine / Miller Walk up to Falcon Street.
- 3. Provide additional open spaces.
- 4. Improve walking, cycling and public transport safety and amenity on major urban corridors.

Action 9: Identify access and active transport network improvements

With the arrival of the metro to the corner of Miller and McLaren streets there is an opportunity to improve the access to the precinct and its connection to the wider metropolitan context. In particular it is critical to connect cycle and walking routes to the new metro station.

The following key improvements have been identified that would maximise the impact and reach of the metro station.

- A connection between the metro portal and Ridge Street through Elliot Street and 52 McLaren Street.
- Better East-West routes through Council's civic block would make Pacific Highway and Mater Hospital more accessible from the metro.
- Improved E-W permeability between Pacific Highway and the heritage conservation areas to the east. In particular upgrade Hazelbank Place and a new connection from Eden Lane to West Street.
- Completion of Bernard Lane up to Myrtle Street as a pedestrian only space.
- A connection between Myrtle and Emmett Streets through the back of North Sydney Girls High School.
- Completion of the Ridge Street cycleway and pedestrian priority footpath as essential parts of the wider active transport regional network.
- Implementation of the West Street unidirectional cycleways.

7. Potential Public Benefits

The study has identified a series of potential public benefits that address the community's aspirations as well as existing needs for the area. Some of these benefits could be delivered via planning agreements with private landowners to redevelop some opportunity sites. Others might be delivered through the Council's applicable Contributions Plan.

It is worth noting that there are not many sites that are expected to undergo significant changes. This limits the capacity to deliver on the desired public benefits. Notwithstanding, public benefits that may be negotiated as part of any planning proposal to amend the North Sydney LEP 2013 are a new community centre, public domain upgrades, affordable housing and public art and potentially, a child care centre.

8. Risks

8.1 Western Harbour Tunnel Project

The design and alignment of the Western Harbour Tunnel (WHT), as outlined in the EIS currently on public exhibition, poses a significant risk to the realisation of some of the key actions put forward by the Civic Precinct Planning Study.

Council's North Sydney Transport Strategy and the subsequent CBD Transport Masterplan identified and modelled various interventions to the overall traffic network to respond to the significant increases in pedestrian movements and activity as a result of Metro and maximise its benefits to the city.

This was designed to essentially redirect "through traffic" from Pacific Highway to Falcon Street and the Warringah Freeway, thus improving pedestrian amenity across the precinct, specifically along Pacific Highway south of the five ways intersection and along Miller Street. This would enable more fundamental placemaking and public domain improvements to be explored.

The WHT project would see a significant increase in through traffic via Miller Street and potentially Pacific Highway. This would make the proposed pedestrian and cycling improvements across the precinct and their connections to the new metro portal unviable.

Further, the Draft North Sydney CBD Public Domain Strategy seeks to capitalise on reducing all the streets within the precinct to local traffic only having redirected "through traffic" from Pacific Highway to Falcon Street and on to the Warringah Freeway.

Council's broad placemaking and city building objectives were an important input into the previous ongoing collaboration with the NSW Government in its preparation of the North Sydney Integrated Transport Plan. Another element was the WHT and its role and impacts upon Council's broad objectives and aspirations. Unfortunately, this process may have stalled and the future of its good work is now very much uncertain.

In the context of these risks, and the draft nature of the WHT project, the Draft Civic Precinct Planning Study should continue to be pursued as an aspirational strategy with acknowledgement that the NSW Government's approval and ultimate pursuit of the WHT project, will mean the Planning Study may need to be revisited.

8.2 Planning Proposals and State Government Overwrite

The Planning Study presents a consultative, evidence based strategic assessment of the needs and the role of the area as a Civic Precinct and a lower density environment to complement the nearby North Sydney CBD and St. Leonards Hub. Notwithstanding this, there is a tendency to assign strategic merit for more density to any site within 400 metres of a metro station by the DPIE and the various planning panels.

This tendency is based on the general principles that underpin the Greater Sydney regional plan. Among them, the idea of a 30-minute city and a well-connected city.

However, even though these principles are valuable and serve as strategic underpinnings of our planning work, we have to ensure that holistic local strategic planning is also considered when looking at any given site. In some instances, being close to a metro or a train station is not enough strategic merit to consider changing the controls or the character of the area.

Council as the depositary of local knowledge and as the planning authority for the area, must develop a holistic strategic framework for the area in alignment with the general objectives and principles of the regional and district plan and then must ensure that this local strategic framework is upheld by the planning panels and the DPIE.

Recent planning proposals in the area have been recognised as having strategic merit by the planning panels disregarding local strategic context. This study is a step to providing a level of certainty to both the general community as well as the various levels of decisionmakers to ensure that individual proposals and initiatives are considered with the benefit and in the context of a holistic and consultative framework.

9. Next Steps

Pending Council's endorsement, the draft Study will be publicly exhibited to seek community and stakeholder input and feedback. Once feedback is received, the study will be amended as necessary and reported back to Council for adoption.

The final adopted planning study will then guide the planning for the area and provide council with criteria to manage any future proposals for the precinct as well as serve as a tool for advocacy with state and federal agencies and inform local investment decisions.

10. Conclusion

The draft Civic Precinct Planning Study puts forward a vision for North Sydney's Civic and Social core, identifies a series of public domain, transport and built form opportunities arising from the metro arrival, and provides a series of actions and strategies to deliver the vision.

The study envisions the area becoming a stronger and more attractive civic and educational campus with a low-scale and leafy character and feel.

The Civic Precinct will act as the green buffer between North Sydney CBD and St. Leonards, and as the central civic hub that ties together the surrounding commercial and mixed-use nodes.

The study provides a holistic planning framework for the area in alignment with other planning studies for adjacent areas developed by Council and also in alignment with the Greater Sydney Commission's North District Plan.

It is worth noting that the WHT project remains a significant threat to the realisation of some of the actions put forward by the planning study.

NORTH SYDNEY

CIVIC PRECINCT

PLANNING STUDY

North Sydney Council May 2020





▽ Acknowledgment of Country

North Sydney Council acknowledges the Guringai Tribe and Cammeraygal Clan as the Traditional Owners of this land. The authors of this report pay our respects to the Elders, past and present and recognise the inherent connection of the Indigenous people of Australia to Country.

▽ Message of Appreciation

We gratefully acknowledge and appreciate the extensive input, feedback and support provided by key stakeholders, industry and community representatives as well as the staff and Councillors of North Sydney Council in the preparation of this planning study

We also thank Conybeare Morrison and their team of consultants for their extensive and thoughtful work that underpins this planning study, as well as their involvement and assistance with the community consultation workshops.

A full set of reports by the consultant team has been included as a background study to this document.

▽ Abbreviations

ABS	Australian Bureau of Statistics	NSDCP	North Sydney Development Control Plan
AHO	Aboriginal Housing Office	NSITP	North Sydney Integrated Transport Program
CBD	Central Business District	NSLEP	North Sydney Local Environmental Plan
CSP	Community Strategic Plan	NSTS	North Sydney Transport Strategy
DCP	Development Control Plan	NSROC	Northern Sydney Regional Organisation of Councils
DPIE	Department of Planning, Industry & Environment	NSW	New South Wales
EP&A	Environmental Planning & Assessment Act 1979	PDS	Public Domain Strategy
FACS	Family and Community Services	PPH	Persons per hectare
GFC	Global Financial Crisis	REIA	Real Estate Institute of Australia
GSC	Greater Sydney Commission	SEPP	State Environmental Planning Policy
LALC	Local Aboriginal Land Council	TfNSW	Transport for New South Wales
LEP	Local Environmental Plan	WSMP	Ward Street Masterplan
LGA	Local Government Area		
LHS	Local Housing Strategy		
LSPS	Local Strategic Planning Statement		
NDP	North District Plan		
NSC	North Sydney Council		

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▽ Intent

The Civic Precinct is located directly north of North Sydney CBD and is bounded by McLaren Street, Pacific Highway, Falcon Street and the Warringah Freeway with an additional area bounded by Walker Street and Berry Street at the southeast corner.

The Civic Precinct Planning Study focuses on this area and is a response to the construction of the Victoria Cross Metro Station northern portal, located at the intersection of McLaren and Miller Streets. This portal will provide greater access to public transport in the area. The metro portal has generated development interest (with several planning proposals already lodged within the study area) and prompted a closer look at any further implications of the new public transport infrastructure.

Additionally there has been significant transformation to the areas surrounding the Civic Precinct. The proposed St Leonards and Crows Nest Planned Precinct and the Ward Street Masterplan have both changed the urban structure around the study area and with it the potential role of the Civic Precinct within the LGA.

The study is part of a Council effort to align the statutory framework with the objectives and targets of the Greater Sydney Commission's Regional Plan and North District Plan as required by the Department of Planning, Industry and Environment (DPIE). The work has funded by a grant from the NSW Government.

▼ Aim

The aim of the Civic Precinct Planning Study is to understand the impacts of new public transport infrastructure and transformations in surrounding areas and formalise an urban design framework for the future of the precinct.

The study aims to establish the role of the Civic Precinct within the wider LGA context and develop a future vision.

This vision and framework will then help determine any necessary changes to the area's built form and public domain and establish a criteria for any new developments.

The study would also ensure that the Civic Precinct is aligned with the objectives and targets of the Greater Sydney Commission's Regional plan for Sydney.

▽ Background

The Metropolitan Plan by the Greater Sydney Commission, and in particular the North District Plan have set employment and residential targets for North Sydney. These targets include an increase of 21,000 jobs in the CBD and 16,000 jobs in St Leonards over the next 16 years. Residential targets estimate the need for 3,000 additional dwellings by 2020 and approximately another 10,250 additional dwellings by 2041 (DPIE revised figures from late 2019).

Alignment of Council's planning controls with the North District Plan

On October 2018 North Sydney Council accepted Accelerated LEP Review Grant Funding and subsequently agreed with the DPIE to prepare specific projects to bring North Sydney's Local Environmental Plan (LEP) and other planning instruments and policies into closer alignment with the Greater Sydney Commission's North District Plan. The Civic Precinct Planning Study is one of these projects.

The Proposed Metro Northern Entry Portal

The NSW Government announced the Sydney Metro City and South West station locations in late 2016 and initially proposed a single entry portal for the North Sydney Victoria Cross Metro Station located on the corner of Miller Street and Berry Street.

Following further investigation, a planning modification was exhibited in June 2017 proposing an additional station entry portal and the relocation of the northern portal from 194-196A Miller Street to 50 McLaren Street. The new entry is a 'lift only' portal and provides greater accessibility to the north of the CBD Precinct. Sydney Metro estimate that the new northern portal will increase patronage by 5%.

Both the North Sydney Transport Strategy and Sydney Metro prioritise walking and cycling above private vehicle traffic, and the Civic Precinct Planning Study will need to consider this in the study. One of the stated objectives of Sydney Metro is to 'serve and stimulate urban development' and this study seeks to provide a framework to guide, control and facilitate any development stimulated by the proposed northern Metro station portal.

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Council has received Planning Proposals for several sites within the study area, and the significant individual increase in density and height has the potential to undermine the character of the area in the absence of a more holistic vision for the precinct. The Civic Precinct Planning Study is to provide that holistic vision and guide any development.

Planning Context

▽ Strategic Planning Context

The Greater Sydney Regional Plan: A Metropolis of Three Cities (2018)

A Metropolis of Three Cities establishes the aspirations for the Greater Sydney region for the next 40 years. It envisages the creation of three distinct cities that will be the focus of economic, social and cultural activity for Sydney's population through to 2056.

Among the three cities the Eastern Harbour City, where North Sydney is located, will include:

- Population increases leading to urban renewal including increased infrastructure and services as well as open space and public places
- Investments in transport and services, jobs growth and business activity
- Improved access to foreshores, waterways and the coast for recreation, tourism, cultural events and water-based transport
- Health and education collaboration areas

The North District Plan (2018)

The North District Plan guides the implementation of the Greater Sydney Regional Plan at a district level and provide a bridge between regional and local planning.

The plan emphasises important links between the economy of the North District and that of the Harbour CBD, with North Sydney providing half a million jobs - by far the largest office market in the North District.

There is significant transport and infrastructure investment underway and planned for the North District, including the Sydney Metro and the St Leonards Health and Education Precinct, to support this economic hub.

The focus of growth in the North District will be around well-connected places that build on local strengths and focus on the quality of the public domain. The 20 year housing target for the North District is 92,000 additional dwellings. The North District will be home to 13% of Greater Sydney's new growth.

The North District Plan also provides a framework and guidelines for more sustainable urban development anchored on "The Greater Sydney Green Grid". The Green Grid is an effort by the regional and district plans to bring sustainable developments in various fronts, from more open spaces, to more energy efficient buildings, to smarter transport networks.

☐ Local Planning Context

North Sydney Council has developed a strategic planning framework based on 'place-based' planning studies for key areas within the local government area (LGA) that were subject to significant change.

This strategic planning framework is aligned with the goals and vision of the Local Community Strategic Plan, which is the overarching document for the North Sydney Council LGA, and is guided by the principles and priorities set out in the Local Strategic Planning Statement adopted in March 2020.

North Sydney Local Strategic Planning Statement (LSPS)

North Sydney's LSPS seeks to maintain and enhance the role of the North Sydney CBD and St Leonards/Crows Nest precinct as key job attractors. It also encourages housing growth in areas with good access to transport, jobs, services and amenity, whilst maintaining the special attributes of individual areas and neighbourhoods outside intensification zones.

Building on the themes of Collaboration, Liveability, Productivity and Sustainability set out in the North District Plan, the LSPS also identifies a series of Planning Priorities and actions for the LGA including:

- II 'Provide infrastructure and assets that support growth and change'
- 12 'Collaborate with State Government Agencies and the community to deliver new housing, jobs, infrastructure and great places'
- L1 'Diverse housing options that meet the needs of the North Sydney community'
- L2 'Provide a range of community facilities and services to support a healthy, creative, diverse and socially connected North Sydney community.'
- L3 'Create great places that recognise and preserve North Sydney's distinct local character and heritage' and P4 'Develop a smart, innovative and prosperous North Sydney economy'
- P6 'Support walkable centres and a connected, vibrant and sustainable North Sydney'
- S2 'Delivering a high quality, well-connected and integrated green space system'
- S3 'Reduce greenhouse gas emissions, energy, water and waste'

A more sustainable, green and connected North District

The Greater Sydney Green Grid is expected to build on existing natural features and parklands of the North District to create green links which support and encourage walking, cycling and community access to open space.

Landscape and scenic features including views from the Harbour and views to the Harbour and foreshores are to be preserved and enhanced.

▽ Process

The Planning Study has been done in two stages, a preliminary consultation and research phase and a design phase carried out in collaboration with an extensive team of urban design, planning, heritage, transport, sustainability and landscape experts.

Stage 1

Council carried out a Stage 1 preliminary community consultation in mid-2019 to understand the community's aspirations for the Civic Precinct area. An internal analysis of public facilities was also conducted.

This stage yielded a set of community priorities that have informed the design brief for stage 2.

A report compiling community input and reflecting on the demographics and character of the area was reported to Council in October 2019.

Stage 2

In October 2019, Council appointed Conybeare Morrison (CM+) and a team of sub-consultants to assist with the development of stage 2.

Based on the brief, extensive analysis of the area and further public consultation workshops the team developed a vision for the Civic Precinct together with a series of urban design principles and objectives.

Their work has resulted in an extensive and thorough background report with a series of proposed actions and strategies to be implemented across the precinct in order to achieve the vision for the area, together with proposed changes to the planning controls in selected locations.

The ideas of the study were presented to the North Sydney Design Excellence Panel and Council's Project Control Group.

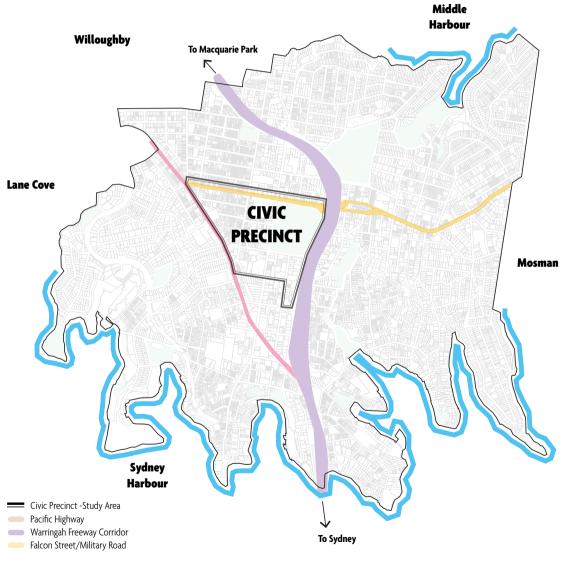
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▽ Objectives

The objectives of the planning study are:

- To understand the community and stakeholders' visions and needs for future planning of the study area
- To develop a considered urban planning vision and strategy for the Civic Precinct, establishing a series of urban planning principles for the study area
- Achieve the objectives and actions identified in the GSC North District Plan
- Identify and retain the urban values of the precinct including its heritage, finegrained structure and 'leafy' character
- Identify possible improvements in the public domain including pedestrian linkages, wayfinding, new public open space and improved amenity. These improvements will be consistent with the objectives and directions identified in the North Sydney CBD Public Domain Strategy (2020)
- Identify the existing and future 'high significance civic places' within the precinct and define their desired character
- Identify the appropriate density across the study area considering the existing opportunities and constraints, the location within the North Sydney LGA and current and future access to transport and public infrastructure
- Explore appropriate opportunities to review building heights and density within the precinct in light of the introduction of improved access to public transport
- Explore opportunities to deliver public benefit within, or on the periphery of, the study area as a result of redevelopment proposals

Location



☐ North Sydney Local Government Area (LGA)

The Civic Precinct is located at the centre of the North Sydney LGA. It sits north of North Sydney CBD and is approximately 2km south-east of St Leonards. The precinct is also surrounded by the medium-density villages of Crows Nest, Cammeray and Neutral Bay and an educational zone to the south-west which contains the Australian Catholic University (ACU).

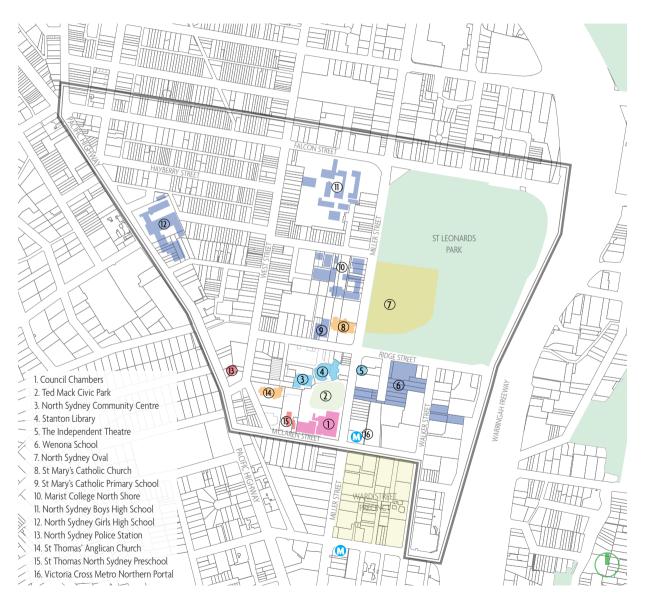
The Civic Precinct occupies approximately 65 hectares and is a mixed-use area providing retail, commercial, educational and residential opportunities. It also includes several community facilities and civic buildings including St Leonards Park, Ted Mack Civic Park, the North Sydney Council Chambers and Stanton Library.

The Civic Precinct is bounded by three major vehicular routes - the Pacific Highway, Falcon Street and the Warringah Freeway. The area is currently serviced by a range of buses and is in walking distance to the North Sydney train station. It is the site of the northern portal of the Victoria Cross Metro Station which is due to open in 2024. This portal is located at the south edge of the precinct across from Council Chambers.

The new metro station will link the Civic Precinct to employment centres to the north and south including Macquarie Park, Barangaroo and Sydney CBD. This connection could create employment and housing opportunities in the Civic Precinct.

⁴ North Sydney Council Civic Precinct Planning Study

Location



▽ The Civic Precinct

The Civic Precinct is bounded by Falcon Street to the north, Pacific Highway to the west, the Warringah Freeway to the east and McLaren Street and Berry Street to the south.

The study area borders the north-east corner of North Sydney CBD creating a mixed-use transition area between McLaren Street and Ridge Street, and Miller Street and the Warringah Freeway.

Uses in this area currently include a hotel, some residential buildings, an old commercial tower soon to be replaced by a residential apartment building and educational facilities. It also includes several heritage items and a heritage conservation area along the southern side of Ridge Street.

St Leonards Park and North Sydney Oval are north of Ridge Street. The park is lined by older residential houses as well as schools and churches along Miller Street.

At the centre of the Civic Precinct is the North Sydney Council Chambers and other civic buildings and facilities including Stanton Library, a community centre, and early education centre, several schools and Ted Mac Civic Park.

In the north-west corner of the precinct, on the edge of Crows Nest village, is the Hayberry quarter, a low-rise, heritage-rich, leafy residential area characterised by terraced houses and local cafes or shops, usually located at quiet street corners.

Together these areas form a rich precinct with a range of building types and uses as well as varying building heights. In spite of the variation, the precinct presents as a cohesive environment with a clear urban structure.

The fringes of the precinct are denser and more active, creating soft transition zones towards the adjacent medium and high-density areas, while the core is characterised by a low-scale, leafy, and pedestrian friendly nighbourhood, filled with civic and educational facilities.

The new metro northern portal will be located within the Civic Precinct on the edge of the North Sydney CBD providing easier access to some of North Sydney's more significant public open spaces and civic buildings.

Site History - The Origins of North Sydney

The township of St Leonards was laid out by 1838 bounded by what is now Berry Street. Layender Street. Miller Street and Walker Street.

St Leonards Park opened in 1838 and the area bordering the park developed as an upper-middle-class neighbourhood. Surviving examples of Victorian and Federation houses still remain on Ridge Street as well as in the school grounds of Monte Sant' Angelo Mercy College and Wenona.

Religious observance was assisted by the opening of an Anglican church in 1845 and a Catholic church in 1856. That the former was a stone structure and the latter a tent is evidence of the respective wealth of the two parishes. Both churches are still on their original locations on McLaren and Ridge Streets.

The move of Monte Sant' Angelo Mercy College to a former grand residence in 1879 begins a pattern followed by Shore School, Wenona School, Loreto Convent and others. North Sydney is a prestigious education hub by 1910, with a strong

educational presence already concentrating around St. Leonards Park.

North Sydney Girls and North Sydney Boys are established by 1930 to cater for the growing population, forming the current educational network in and around the Civic Preinct.

North Sydney Council was established on 29 July 1890 after ratepayers in three neighbouring Boroughs (East St Leonards, St Leonards and Victoria) voted for union. The name North Sydney was chosen explicitly to link the new Municipality to the metropolis on the south side of the Harbour.

North Sydney Council moved to a former doctor's surgery on the corner of Miller and McLaren Streets, then the medical heart of the lower north shore. Chambers opened in 1926 and were enlarged in 1938. The former Town Hall was at Milsons Point became a theatre, and no replacement town hall was built, despite plans to do so. In the 1940s North Sydney Council began acquiring land adjacent to its

Chambers with a view to developing a civic centre that was never realised.

The opening of the Sydney Harbour Bridge in 1932 and consequent redevelopment of Lane Cove Road as the Pacific Highway created a major traffic corridor for the area. Crows Nest grew as a commercial hub. After the effects of the Great Depression and World War Two had subsided the fixed link to the south side facilitated the long-awaited development of a 'twin city'. The MLC Building completed in 1957 marks the beginning of that transformation.

High-rise residential and office development from the 1950s to the 1970s resulted in the election of councillors who favoured curtailing the unfettered development and protecting existing precincts. The NSW Heritage Act is passed in 1977 and the State's first Heritage Study completed in 1981. Mayor Ted Mack ensured that a Civic Park was created next to Council Chambers rather than the high-rise civic precinct planned in the 1970s; this remains the last major public intervention in the area until this day.



1854
First post office in
North Sydney opened,
trading under the name
of St Leonards. The
current North Sydney
Post Office was opened
in 1889



Monte Sant'Angelo College moves to former grand residence 'Masalou'



1890
The Borough of North Sydney was formed through the amalgamation of three local borough councils - East St Leonards, St Leonards and Victoria

1926

North Sydney Council Chambers opens on the corner of Miller and McLaren Street



1957

MLC building opens in North Sydney CBD, the first high-rise office block in the area and Australia's largest office building

1982

Civic Park opens incorporating a number of existing buildings including the Council Chambers and the Stanton Library









1838 is ee
The township of St dist
Leondards is gazetted and the grid pattern centred on Miller and Walker Streets proposed by 1828 is and confirmed The

1856

St Mary's Catholic Parish is established, a large district extending from Port Jackson to Pittwater. The original church was a tent placed at the corner of Miller and Ridge Street in 1856. The current church building dates from 1938



1888

Marist Brothers High School North Sydney opens on 2 July 1888, catering for boys in primary to intermediate years (Years 1-10). The school would go on to teach secondary years (Years 7-12) in 1965



The population climbs 34,000, twice the figure of 1890 and nearly 20 times that in 1861









First stage of the Warringah Expressway is completed



2019

Civic park renamed the Ted Mack Civic Park in 2019 after the politician and former North Sydney mayor

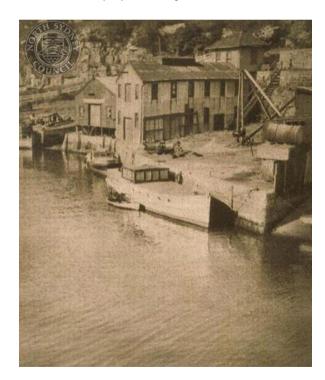
6 North Sydney Council Civic Precinct Planning Study

Site History - North Sydney's Waterways

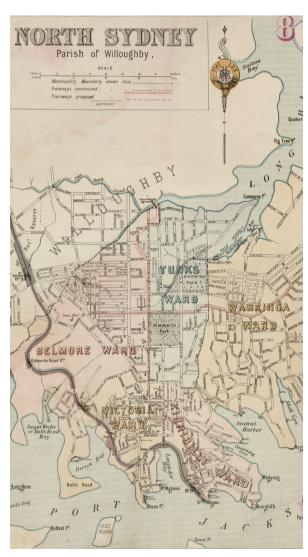
North Sydney LGA is bounded in equal parts by waterfront and land boundaries, yet its foreshore along Port Jackson, the main harbour, and Middle Harbour is the defining feature.

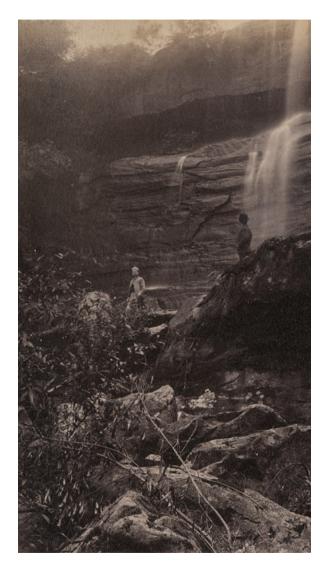
In turn bays, coves and headlands define the waterfront. Each bay and cove once had a freshwater creek feeding into it. These were still present in the late 19th century as indicated by maps but most were built over by the mid-20th century. The waterways rising on the ridges ran south to Port Jackson and north to Middle Harbour, along them, parks and recreational areas full of vegetation and trees were developed for the use of the growing population.

Tidal areas, such as Berrys Bay and Careening Cove were also 'reclaimed'.

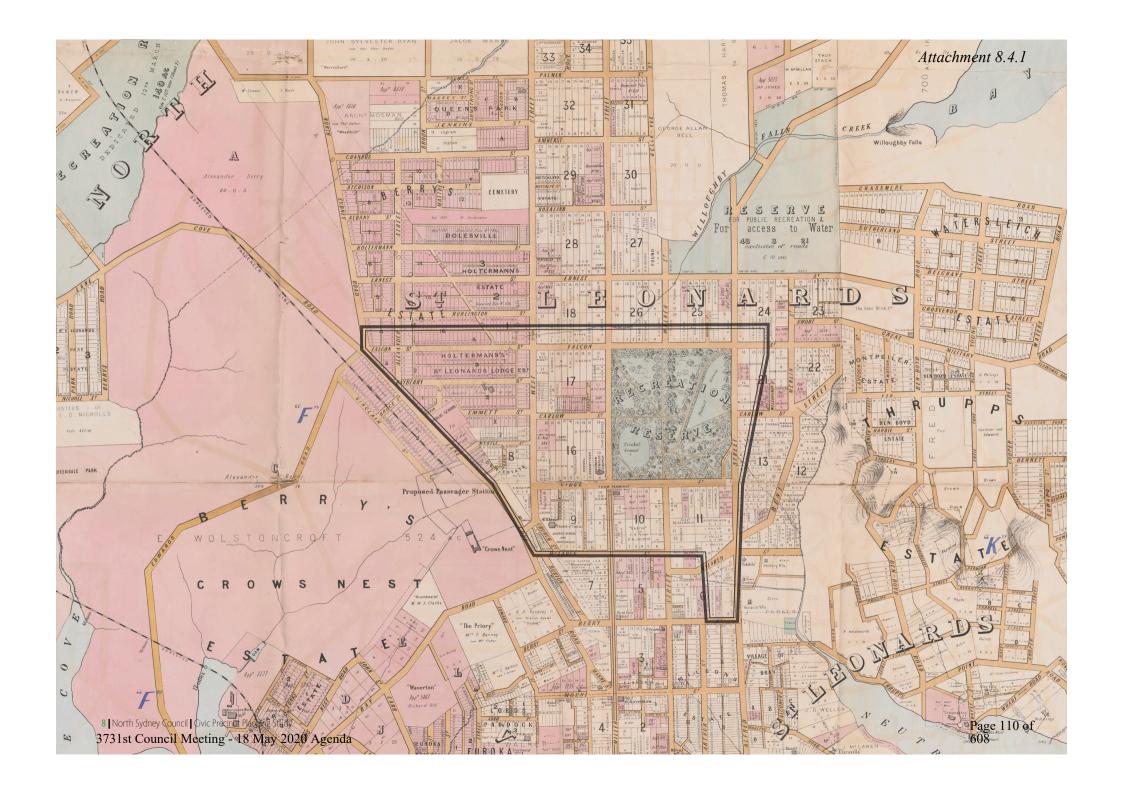


Top: Wharf at Berrys Bay (c.1910), Right: North Sydney [cartographic map], Parish of Willoughby (1890s), Far Right: Willoughby Falls (1886).



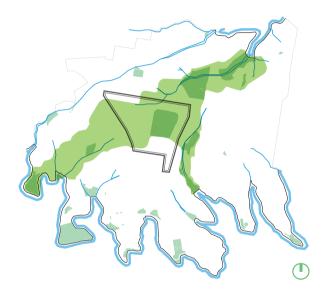


North Sydney Council | Civic Precinct Plange Stand 70f



Site Context - A Layered Precinct

The urban structure of the North Sydney LGA is defined by 3 main overlapping urban elements - a green link, a strong civic and educational centre influenced by surrounding urban nodes, and a multi-layer metropolitan transport network. Together they generate a rich mosaic of distinct urban areas and environments. Located at the centre of these three urban elements is the Civic Precinct. Future development within the Civic Precinct should be able to combine the green link with a strong civic presence, protect and strengthen the heritage character of the area, and at the same time provide employment and educational opportunities that complement the high-density nodes of North Sydney and St Leonards.



Precinct North Sydney CBD



Following old creek lines within the LGA, North Sydney's green link connects Sydney Harbour to Middle Harbour by linking parks, reserves, heritage areas and civic buildings.

The Civic Precinct is located at the centre of this link and includes two key open spaces in the LGA - St Leonards Park and Ted Mack Civic Park.

The Civic Precinct also contains key civic buildings including Council Chambers, Stanton Library and the Independent Theatre, as well as several churches and schools.

The green link framework provides the area with an underlying character and an important social and civic role, as well as a distinctive leafy and green outlook.

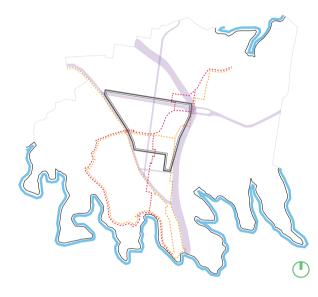
A modern 'traditional core'

Older European cities can consist of a traditional centre surrounded by more dense, modern areas, which reflects the evolution of the city over time. The centre is usually filled with civic and administrative institutions as well as plazas and social gathering spaces, with higher density residential and commercial environments on the outskirts.

Military Road

North Sydney's Civic Precinct performs the same functions in present day and is located at the centre of the LGA with major commercial and high-density areas surrounding it.

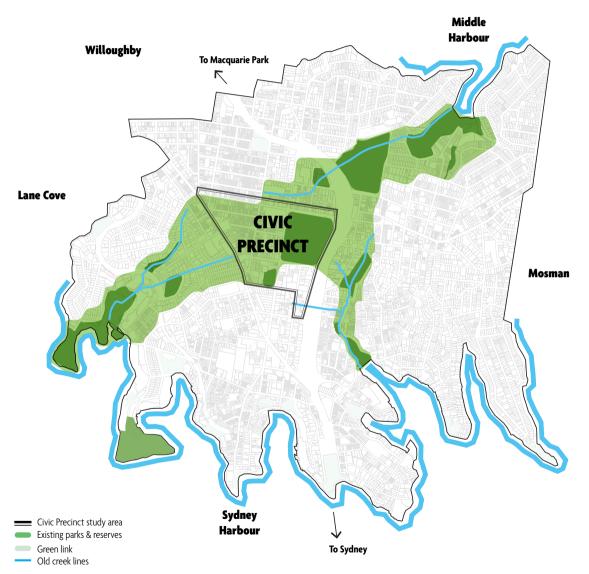
The area retains a low-rise, human-scaled, heritage character and has become over time a true civic heart for the LGA with a strong presence of civic institutions and public open spaces.



✓ A multitude of existing and potential transport connections

Walking and cycling links, designed to improve local access and regional connectivity between North District and the Eastern Harbour City, are identified under the North Sydney CBD Transport Masterplan, the North Sydney Public Domain Strategy, and TfNSW's Greater Sydney Services & Infrastructure Strategy. Most of these links converge in or on the fringes of the Civic Precinct, which acts as a fulcrum for regional walking, cycling, bus and private vehicle travel.

Proposed treatments for major bus and traffic corridors on the fringes of the precinct are also identified in the North Sydney CBD Transport Masterplan, with the Falcon Street and Warringah Freeway corridors continuing to perform the major regional traffic function while the layout of the Pacific Highway, south of Falcon Street, has the potential to be amended to deliver more of a local distributor role.



☐ Highlighting the underlying natural structure of the LGA

North Sydney LSPS, Priority S2: 'Delivering a high quality, well-connected and integrated green space system'

North Sydney, like the rest of metropolitan Sydney, has been developed and defined over time by its topography.

Main roads throughout Sydney including Parramatta Road and the Pacific Highway were designed along ridges to avoid having to cross the multitude of existing small creeks and wetlands, where reserves and green spaces developed instead.

Over time these creeks and valleys receded due to urban growth, with some drying up, being filled in or turning into stormwater drainage canals. Despite this topography being less prevalent today, its imprint is still there. Three main creek lines shaped the centre of what is today North Sydney LGA, and their presence can be seen on the network of parks and reserves that were created along them. Furthermore, many residential, civic and cultural buildings were developed along these three paths in the past, creating areas rich in heritage and culture.

The Civic Precinct Planning Study is an opportunity to bring this latent topography to the foreground as an influence for future development in the area. This green link will provide higher density areas with a breathing zone while at the same time weaving together the different urban centres of the LGA, creating a cohesive and integrated city.

As shown on the transport diagram, the identified creeks and reserves corridor also provides an opportunity to improve local walking and cycling connections along the new east-west green link, providing a route between Middle Harbour and Berry's Bay with a potential future connection to Bradfield Park and the Harbour Bridge via the proposed North Sydney Highline.

▽ From Port Jackson to the Middle Harbour

The green link is based on Willoughby Creek, an unnamed Neutral Bay Creek and an unnamed Bandagi Reserve Creek. These three streams create a strip of reserves that connect Sydney Harbour at Oyster Cove with the Middle Harbour at Willoughby Bay.

The corridor crosses the Pacific Highway and creates a well-needed break along the different town centres. Whilst Pacific Highway can be seen as a corridor of regional commercial centres, the green link is a locally and socially focused piece of urban infrastructure with the Civic Precinct at its centre.

The Sydney Green Grid - Government Architect NSW



▽ Sydney Green Grid - Government Architect NSW

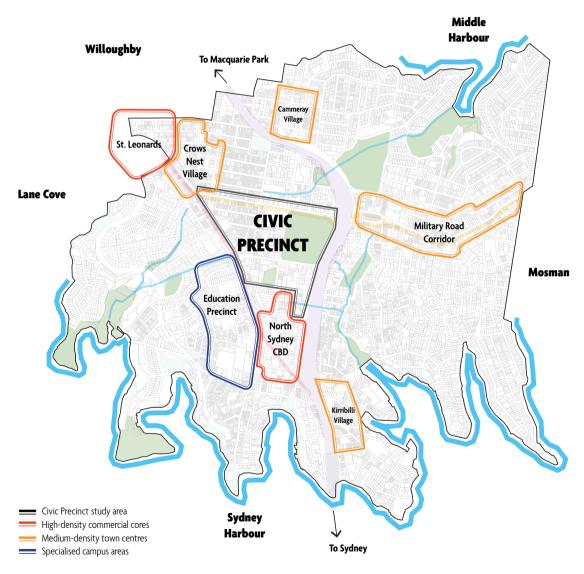
The Sydney Green Grid by the Government Architect NSW (GANSW) identifies a network of high-quality spaces throughout Greater Sydney that connects town centres, public transport hubs and major residential areas. Currently lacking an overall schema, these hydrological, recreational and ecological fragments of the city have been mapped into a proposition for a cohesive blue-green infrastructure network.

The North District Sydney Green Grid also highlights regional open space opportunities in ten project clusters. These clusters are formed around key landscape features and act as a starting point for ensuring more generalised district strategies and future project delivery responds to the local strategic context and landscape conditions.

Some of the key principles of the Sydney Green Grid that apply to the Civic Precinct Planning Study include:

- ☐ Improve connectivity to key regional destinations, foreshores, beaches and bays and continue to invest in the improvements of major parks and infrastructure
- Promote and improve the pedestrian environment to increase the enjoyment of traveling on foot and by bike
- ☐ Align the open space network with longer term transport plans
- Protect priority green corridors and create a network of walking trails, cycle paths and open spaces along the river and creek corridors
- □ Integrate key civic spaces or destinations with public transport opportunities and existing development
- ∇ Create green corridors that provide habitat and biodiversity connections along major and minor watercourses
- Use Sydney's water systems as an interconnected network by linking public open space, active transport and pathway systems along waterways
- ☐ Enhance the particular character of Sydney's waterways to define the landscape character and urban structure of the districts of the city

Land Use



▽ North Sydney's civic and social centre

North Sydney's Civic Precinct is at the centre of the LGA and it is surrounded by major urban nodes in the area.

These nodes include two major high-density commercial cores (St. Leonards and North Sydney CBD), four medium-density villages (Crows Nest, Neutral Bay, Cammeray and Kirribilli) and one educational and health zone which contains the Australia Catholic University and the Mater Hospital.

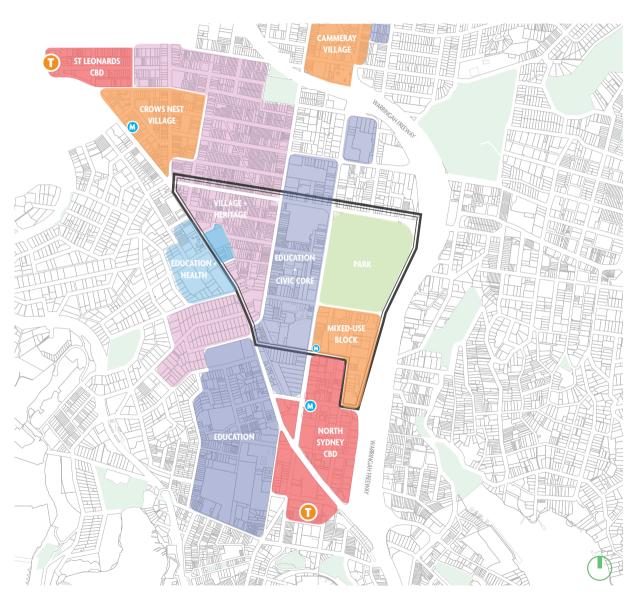
The Civic Precinct is the central element among these cores promoting a social, community-focused and heritage-rich precinct that celebrates the history of the LGA and its strong sense of community.

Addressing its surroundings, each edge of the precinct should have a different character and interface, and then transition into a common core. This core should be defined by its open spaces and civic buildings

As well as providing green space and civic facilities to complement the high-rise areas of North Sydney LGA, the Civic Precinct also provides complementary development typologies and spaces to support the surrounding major commercial areas

From small scale local shops, to a wider range of housing options and commercial opportunities, the area can provide interesting spaces at affordable prices and in well-connected locations.

Land Use



☐ North-South Land Use banding

The Land uses and urban character of the Civic Precinct and surrounding areas can be grouped in 3 main categories. A CBD core between the freeway and Miller Street, an Education and Civic band between Miller and West Streets, and a heritage and "village" band to the west of West Street.

Together they form three distinctive north to south bands, aligned from the freeway towards the west edge of the Civic Precinct, each with a distinct urban environment.

A heritage rich area

There is a strong heritage presence in the precinct with a number of heritage listed items and heritage conservation areas. These areas are characterised by their leafy outlook, quiet streets, pedestrian-friendly environment and local cafes and shops located mostly at social street intersections.

One and two-storey buildings form a low-scale streetscape of mostly terraced houses with fenced front gardens, generous footpaths and abundant street trees.

A Strong civic & education core

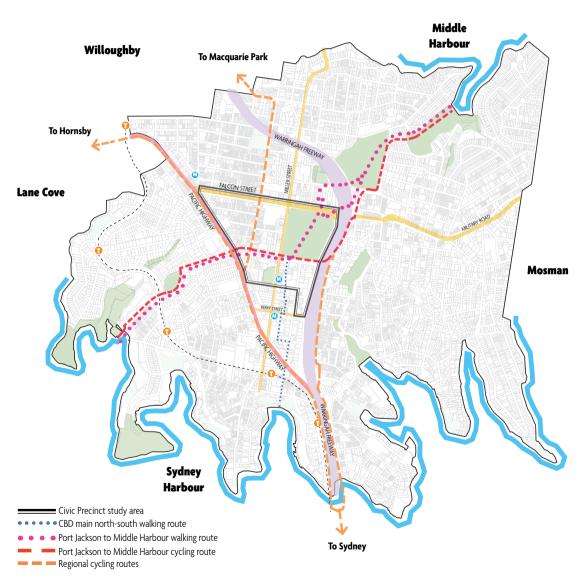
The precinct's civic and education buildings are largely concentrated along Miller Street. These include Council Chambers, Stanton Library, a theatre, a community centre, a youth centre, several churches and schools, North Sydney Oval and St Leonards Park.

These civic spaces strengthen the Civic Spine along Miller Street and help enhance the connection between the Civic Precinct and North Sydney CBD. The study provides a series of urban design guidelines that should be put in place to manage future interventions in the area and create a well-defined civic and educational campus that supports schools and addresses future growth.

A mixed use CBD fringe

McLaren and Walker Streets adjoin the fringe of the North Sydney CBD. Along these streets, and also along Pacific Highway, a medium to high-density, mixedused environment has developed over the years providing a mix of residential apartments, office buildings, hospitality venues, a hotel and other typologies.

This study aims to consolidate and regularise these areas to create a vibrant mixeduse transition zone between the CBD and the centre of the Civic Precinct. This transition zone could increase commercial offerings and job opportunities to support both the CBD and nearby educational and medical sectors.



☐ A keystone for North District transport

The Civic Precinct is the area where active transport regional routes, east-west walking and cycling routes and CBD routes converge. It is also the centrepiece that connects many of the cycling and pedestrian routes within the LGA and the wider North District. Additionally, major traffic routes also cross the precinct or run along its edges.

East-west active transport connections

In order to strengthen and consolidate the green link and formalise its presence within the North Sydney LGA, a series of walking and cycling routes are proposed to facilitate the connection between the different parks and reserves, and ultimately between Sydney Harbour and Middle Harbour.

These routes cross the Civic Precinct along Ridge Street, which becomes an important cycling and pedestrian street, and through St Leonards Park. The intersection of these routes with the Civic Spine and the pedestrian north-south CBD link becomes a key node of the wider active transport network.

Completing the north-South pedestrian route across the CBD

Following the strategies and projects put forward by the North Sydney CBD Public Domain Strategy (endorsed by Council in April 2020), the completion of the proposed north-south pedestrian route from North Sydney train station to St Leonards park is a critical intervention for the wider CBD strategy. This requires a pedestrian link between McLaren Street and Ridge Street via Elliott Street. The junction of Elliott and Ridge Streets will become an important node for the LGA as it connects the north-south CBD pedestrian route with the green link.

Implementing the regional cycle network

All cycling access between the Harbour Bridge and the North District is channeled through North Sydney's Civic Precinct. The North Sydney CBD Transport Masterplan identified the provision of cycling facilities along Pacific Highway, West Street, Ridge Street and the eastern edge of the Warringah Freeway as key elements of the North District cycle network and one of the final stretches yet to be completed.

Regional traffic routes

The study area is bounded by the Warringah Freeway, Falcon Street, Pacific Highway and a short stretch of Berry Street, all of which maintain a high volume of traffic. The study will work in alignment with other traffic endorsed policies to minimise the impact of these regional routes on the Civic Precinct.

Transport

▽ Overview

Based on the key transport priorities established in the North Sydney Transport Strategy (NSTS) and the objectives of the CBD Transport Masterplan, the focus within the Civic Precinct will continue to be prioritising walking and cycling and encouraging regional traffic to use Falcon Street and Warringah Freeway.

Victoria Cross Northern Portal

The arrival of the northern Metro portal on the corner of McLaren and Miller Streets presents a unique opportunity to promote sustainable transport options instead of using the car. The new metro portal will provide mass transit access to the precinct, and to major educational facilities and sporting grounds.

Walking

An overlap of an 800m radius circumference with the actual 800m walking catchment from the metro northern portal shows several areas within the precinct that are outside walking reach of the station. These areas could get into the walking catchment if additional pedestrian routes and links were created to maximise the reach and impact of the metro. The strategies and actions proposed by the study focus on identifying opportunities to create these additional routes and connections and increase the walkability of the Precinct.

Cycling

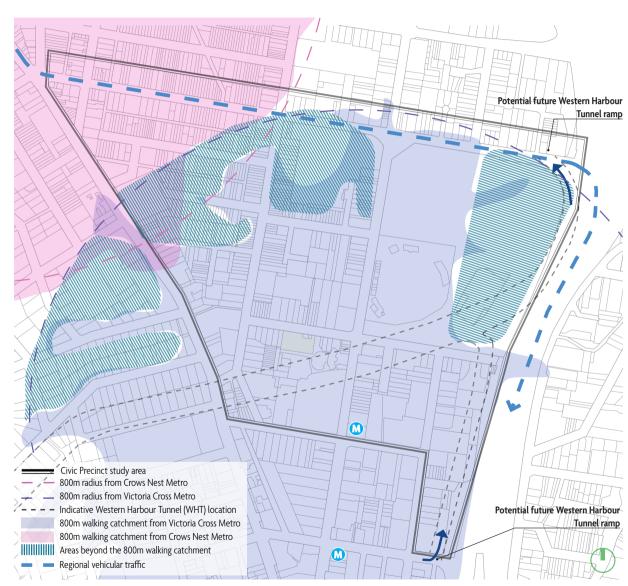
There are several interventions within the precinct that have already been identified in the North Sydney CBD Transport Masterplan and in the Sydney Services and Infrastructure Strategy (TfNSW). These interventions include the completion of the Ridge Street cycleway and the implementation of new cycleways along West Street and Pacific Highway.

Vehicular Traffic

The main traffic priority is to keep regional traffic limited to Falcon Street and the Warringah Freeway, thus allowing the rest of the precinct to be a low speed (40Km/h maximum), low volume (local traffic only) environment.

Western Harbour Tunnel

If it proceeds, the proposed Western Harbour Tunnel (WHT) will attract more vehicular traffic to the study area and will increase conflicts between walking, cycling and local traffic, in particular along Miller Street. The WHT will undermine the positive impacts of the metro arrival. The proposed off-ramp on Falcon Street will channel all northbound Pacific Highway traffic through the CBD and Civic Precinct, and will take over a the north eastern corner of St Leonards Park, substantially reducing the amenity and usability of this vital State Heritage listed green space.



Community Input

▽ Stage 1 Consultation

Preliminary community consultation was undertaken in Stage 1 following the Community Engagement Plan. This first stage of consultation aimed to promote awareness of the planning study and understand what the community values in the study area.

A short survey and interactive map gave the community an opportunity to define what they value. Interviews were held with key stakeholders. Community awareness was promoted in the area using posters as well as online and through social media.

Council received 217 submissions via the online survey plus an additional 23 written submissions. A total of 5 submissions outlining development aspirations were received from landowners.

Community's Valued Places

The community were asked to locate on an online map, their 'top-three' most valued geographic locations. A total of 73 pins were located by 27 contributors.

The majority of the respondents voted Civic Park and St Leonards Park as the most valued places.

▼ Community Priorities

Community Priority #1 The precinct's heritage value is conserved

Community Priority #2 The village feel and civic character of the precinct is respected

Community Priority #3 Parks and public spaces are highly valued for both active and passive recreation and should be protected

Community Priority #4 The access, safety and amenity of local streets is improved

Community Priority #5 The scale and design of new development is carefully managed

Community Priority #6 More opportunities for social interaction and activity are created.



Community Input

▽ Stage 2 Consultation

Two community workshops were held in late November 2019. The workshops were advertised through the Your Say website. Council also invited key stakeholders, initial consultation participants and representatives from each of the Precinct Committees to attend the workshops.

Objectives of the workshop included to confirm the purpose of the study, build on consultation from Stage 1, get new input to the planning principles for the study area, look at potential locations that might be acceptable for density, determine the types of public benefits needed in the community and to provide information on future steps for the project.

During the workshops, participants were asked to take part in 4 activities.

Activity 1 - Identify 'Considerations' and 'Initiatives' under 'Community Priority' areas identified in Stage 1 consultation

Comments raised by participants:

- Controlling development type including height, location and character
- Traffic congestion (now and in the future)
- Protection and management of existing parkland and the need for more smaller parks

Activity 2 - Comment on the preliminary Urban Design Principles

Comments raised by participants:

- Controlling development type including height, location and character
- Desire for the promotion of housing diversity
- Ensuring any future development provides public benefits

Activity 3 - Discuss and identify the types of development that might be acceptable based on precedent images provided

The consolidated dots (see map) indicate the locations, types and scales of development that might be acceptable to attendees in the study area.

Activity 4 - Identify the three most desired public benefits

Comments raised by participants related to:

- Increased and improved community facilities (childcare, theatre)
- Upgrades to the public domain (footpaths, trees, streetscapes, parks)
- The need for more affordable housing

A detailed description and outcomes of the workshops can be found in the appendixes as a separate report.







The Civic Precinct will be a modern, connected, and attractive Civic & Educational Campus with a strong network of pedestrian links and open spaces, a low-scale village feel, and a leafy appearance.

A unique civic centre

The Civic Precinct is a unique pocket of the North Sydney LGA. Located on the northern fringe of the CBD and bordered by three high-use roadways (the Pacific Highway, Falcon Street and the Warringah Freeway), it is characterised by older educational institutions, heritage-listed Victorian terraced houses, the greenery of St Leonards Park and a cluster of civic buildings and public open spaces at the centre of the precinct.

The area holds a dual role for the North Sydney LGA as both a civic centre and a social and recreational node. It is the location of the Council Chambers, a public library and a theatre and at the same time the setting for community markets, exercise classes, social gatherings and sport events.

Any future interventions within the precinct will be guided and framed by these local conditions and will focus on contributing to the civic and social characters of the precinct.

A central point in North Sydney's green link

The Civic Precinct is at the centre of a network of public open spaces, parks and reserves that run across the North Sydney LGA from Berrys Bay, located to the west of McMahons Point, to Willoughby Creek and onto Middle Harbour.

This green link is characterised by open green space, educational institutions, heritage items and recreational and civic facilities, and forms an almost uninterrupted route from one harbour to the other. The green link can be further strengthened through achievable pedestrian and cyclist routes from Berrys Bay to Willoughby Creek which can be extended to other public spaces and recreational areas within the LGA.

As the centre of the green link the leafy, heritage rich, village feel of the Civic Precinct will be preserved and enhanced. This will ensure a much needed respite between the two high-density areas of North Sydney CBD and St Leonards nearby. The Precinct's open spaces and civic facilities will also strengthen the offerings of the green link.

An accessible and connected precinct

In alignment with North Sydney Council's overall approach to transport, initiatives within the Civic Precinct will focus on prioritising pedestrian and cycling modes by improving existing links and creating new ones, specifically connecting the pedestrian and cycling networks to the new metro station portal.

The precinct will provide a network of pedestrian and cycle friendly routes that will connect schools, civic facilities and open spaces to each other and to the metro station.

At the same time the precinct will link to routes along the green link, facilitating new connections between different open spaces and public facilities along the link from Berrys Bay to Middle Harbour.

Vehicular traffic will be kept to local traffic only through the precinct with regional traffic located along Falcon Street and the Warringah Freeway.

Principles

Based on community consultation, analysis of the planning study area and the defined vision for the Civic Precinct, a series of principles have been developed to guide the future planning of the precinct.

These principles have informed and guided the proposed structure for the precinct as well as the implementation strategies and proposed interventions.

1. Preserve, enhance and strengthen the rich heritage of the precinct

Heritage buildings, Heritage Conservation Areas, streetscapes and contributory buildings will be protected and enhanced by restoration, adaptive reuse or, where appropriate, contemporary additions or infill development that respects the heritage character and scale.

2. Maintain the small scale character and village feel of the precinct's core

The Civic Precinct's low-scale urbanity and leafy streets will be retained. North Sydney Council Chambers and Offices, Civic Park, Stanton Library and nearby community facilities will remain at the precinct's core.

3. Define the edges of Crows Nest Village and North Sydney CBD

Clearly define the limits of the CBD and Crows Nest and provide a transition zone between the Precinct's core and the adjacent areas. Develop a mixed-used higher-density area around the new Victoria Cross metro station northern portal and limit any future growth and development to a very clearly defined area.

4. Manage growth and ensure development is of an appropriate scale and character

Limit any potential significant development to the transport corridors and the transition zones (if and where appropriate) and ensure built form and scale transitions are sympathetic to the heritage items and conservation areas.

5. Encourage an education campus

Educational institutions are encouraged to work together and share knowledge, facilities and resources. Each educational institution should be designed to integrate with their surroundings, to be welcoming and outward looking, contributing to the urban streetscapes along their frontages.

6. Increase public amenity within the precinct

Public amenity within the precinct should be maximised. Existing trees and landscaped areas should be protected and additional landscaping and tree canopy promoted where possible.

Capitalise on any potential public domain improvements and opportunities arising from the Victoria Cross Metro Station or any other future development.

7. Integrate open spaces into the Green link network

Develop a network of connected open spaces within the green link. Maximise the potential of existing open space and explore the potential for new, attractive and high-quality public and community places

8. Encourage active transport and improved connectivity across the precinct

Walking and cycling will be prioritised, placing residential and employment areas, recreational and entertainment destinations, and community and social hubs, within an easy walk or cycle of each other.

9. Promote a varied network of small business next to the CBD

Attract smaller scale businesses and employment to complement the North Sydney CBD by providing opportunities for innovative small-to-medium scale businesses, shops and cafes in predominantly mixed-use developments.

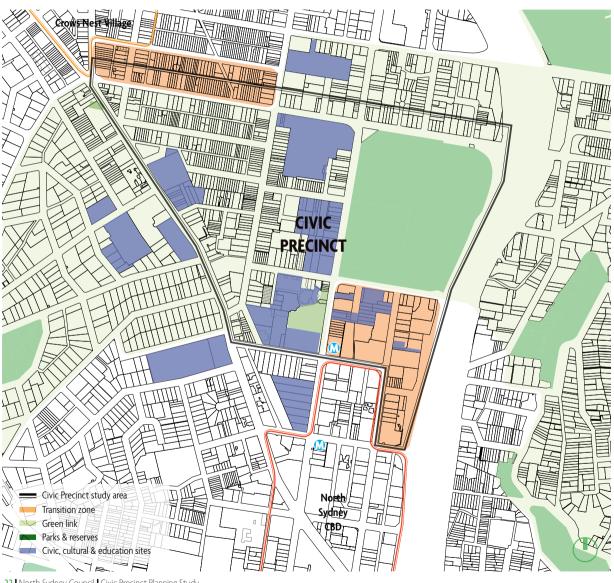
10. Promote housing diversity and affordability

Encourage different residential typologies appropriate to each of the different areas within the precinct that can complement the existing offering.

11. Advocate design excellence and sustainability in new buildings and the public domain

Ensure that future development and public open spaces are of high quality, offer good amenity to the final user and provide a sustainable environment.

Design Concept



▽ A distinct Civic Precinct

The Civic Precinct is the main social and civic gathering place for the community of North Sydney. The area will continue to evolve with a civic and education campus at its core and two mixed-use transition zones towards the CBD and Crows Nest.

This civic and education precinct will continue to be defined by low-scale, treelined streets and as part of a rich network of connected public open spaces. New pedestrian and cycling routes through the precinct will link these spaces to the new metro station.

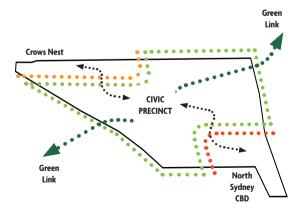
At the edges of the precinct, mixed-used areas will create transition zones between the CBD and the precinct, particularly along Walker and McLaren Streets, and at Crows Nest around the Five Ways intersection.

Civic & education precinct with a leafy & green character



Mixed-use transition zones

This framework strengthens the east-west green link through the precinct as a central element within the LGA, connecting the precinct to other open spaces along the link. It also creates two clear mixed-use transition zones between the centre of the precinct and nearby, higher-density nodes. These transition zones will complement both the core of the Civic Precinct and the surrounding nodes.



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Design Concept

□ A complementary approach

The Pacific Highway is one of Sydney's key economic corridors, connecting a series of town centres and urban nodes from North Sydney, through St Leonards and Chatswood, all the way to Gordon and Hornsby.

In between these nodes are lower-density areas that provide breathing space between the larger centres. North Sydney and St Leonards are comparatively close to each other, however they still benefit from a clear break in between them. The Civic Precinct provides that break.

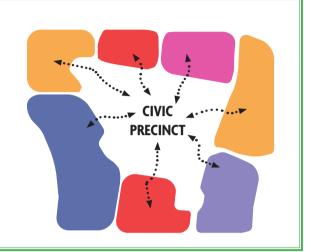
In addition to a lower built form and a greener environment, the Civic Precinct also provides a contrast in uses and activity.

Beyond the main civic and education functions at the core of the precinct, the mixed-use transition zones of the study area have the potential to provide development typologies and spaces that are complementary to those of the surrounding major commercial cores.

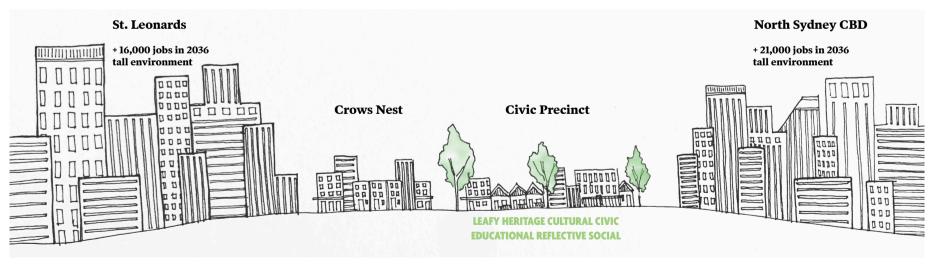
The proposed urban framework for the Civic Precinct provides the opportunity for uses and typologies that will support, and benefit from, the adjacent North Sydney CBD, St Leonards and nearby medical precincts.

These opportunities include:

- Promoting housing diversity and affordability
- Providing smaller commercial spaces and tenancies for small to medium scale businesses that can complement the nearby major commercial entities
- Providing spaces and facilities to support nearby educational and medical facilities
- Facilitating the adaptive reuse of underutilised heritage items to provide for new creative contemporary spaces.



∇ A leafy break in the journey

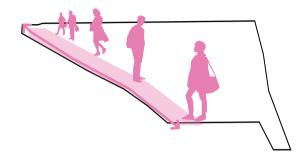


Skyline is diagrammatic only and does not reflect actual heights or tower distribution

Actions overview

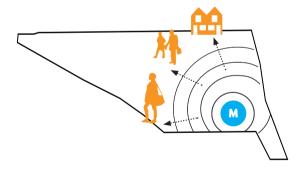
▽ Key actions

The study has identified 9 actions to develop the vision and the design concept for the Civic Precinct. Some of these actions are specific for a certain area, whereas others are more general and apply to the whole of the study area. Some propose concise changes to the existing built form or transport network, while others provide broader guidelines for future interventions.



2. Increase amenity and activation along Pacific Highway

Create a medium-scale streetscape area between North Sydney CBD and St Leonards that provides alternative employment opportunities and a pedestrian friendly environment. This area would transition into the higher-density areas on Crows Nest / St Leonards and the CBD



1. Create more jobs and housing opportunities near the Metro

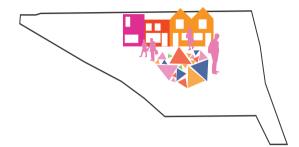
Develop the northern end of the CBD into a mixed-use zone near the Metro Northern Portal with transitioning building heights from the CBD towards the low-rise, heritage conservation area and increased public amenity through the completion of the N-S link from North Sydney train station to St Leonards Park.

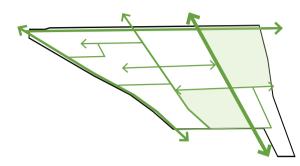


3. Support small to medium sized business growth

Increase the non-residential FSR in the area to provide for smaller commercial tenancy spaces predominantly within mixed-use buildings, encouraging retail and commercial tenancies on the ground floor to improve street activation and increase foot traffic in the area.







4. Preserve heritage; add value

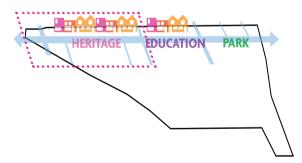
It is important to protect and respect the heritage buildings and conservation areas of the Precinct for future generations as they define the local identity and unique character of the area. At the same time we must ensure that the area is well maintained, remains relevant, activated and used.

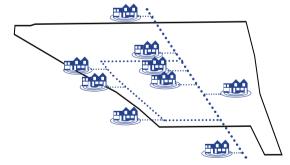
6. Undertake further review of Civic Core

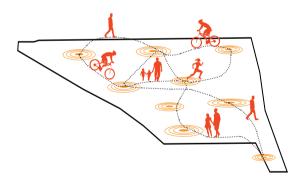
The Civic Core is central to the Civic Precinct and includes a number of publicly owned assets. The current overall layout is not cohesive. Further work on this layout and public space within the core would allow for greater connectivity and legibility and a more attractive and pleasant civic environment.

8. Improve public open space

Four key public domain opportunities have been identified for the Civic Precinct, those being the implementation of the St Leonards Park Landscape Masterplan and the Civic Spine/Miller Walk, additional open spaces and improved pedestrian amenity on major urban corridors.







5. Undertake further review of Falcon Street

After the outcome of state government infrastructure projects (including the Western Harbour Tunnel) are known, a detailed Urban Design Study of Falcon Street should be undertaken, with specific focus on the area between Five Ways and West Street

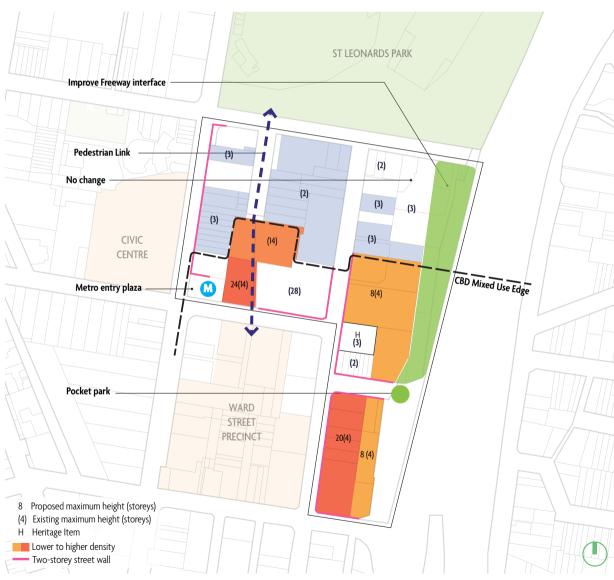
7. Provide guidelines for education facilities

A series of guidelines promoting integration and openness between the many educational facilities in the area and the Civic Precinct has been developed. Recommendations include staggered building heights, pedestrian amenity, street activation and the exploration of pedestrian through-site-links.

9. Identify access and active transport network improvements

To maximise the impact and reach of the new metro station a number of new pedestrian connections have been identified throughout the precinct. These connections would improve walkability to both the Victoria Cross Metro Station and the nearby Crows Nest Metro Station.

Action 1: Create more jobs and housing opportunities near the Metro



□ A vibrant mixed-use CBD fringe

Desired Character

The arrival of the metro on the corner of Miller and McLaren Streets and the implementation of the Ward Street Precinct Masterplan will transform the character of McLaren and Walker Streets. The area will be a vibrant and active, mixed-use fringe of the CBD with a variety of commercial, community and residential uses. Building heights will transition from the CBD and Ward Street Precinct down towards the low-scale residential area along Ridge Street. The area will present a variety of building typologies from medium-scale commercial buildings (6 to 10 storeys) to residential towers (around 20 storeys). The break between the buildings will allow for intimate public open spaces and gardens and filtered views towards the harbour.

Objectives

- Deliver jobs and housing growth on the fringe of the CBD in areas with additional transport infrastructure
- Provide a height transition between the commercial core (up to 40 storeys) and the lower conservation area
- Protect the amenity and sunlight of existing and proposed public open spaces
- Complete the N-S laneway link from the train station to St Leonards Park
- Preserve the Ridge Street HCA

Implementation *

- Increase the maximum height limit to 24 storeys to the southern half of 52 McLaren Street
- Maintain the current maximum height (14-storeys) to the northern half of 52 McLaren Street and 54 McLaren Street
- Rezone the current R4 zone along the east side of Walker Street to B4 and B3 zones, except for heritage listed sites, and require mixed-use towers and mid-rise standalone commercial buildings
- Increase the maximum height to 20-storeys along the eastern frontage of Walker Street (similar to some existing buildings on the street)
- Increase the maximum height to 8-storeys to the area fronting the Freeway (similar to some of the existing buildings)
- Do not allow any overshadowing to Doris Fitton Park
- Adopt view sharing principles for new developments
- Require a 6-metre setback to 52 McLaren Street along the eastern boundary
- Provide a 8-metre setback to Miller Street and a plaza for the Metro portal site
- Turn the end of Hampden Street into a pocket park
- Develop a public park along the Freeway edge

^{*} Refer to page 27, significant sites guidelines for further detail

Action 1: Create more jobs and housing opportunities near the Metro

Significant Sites Guidelines

50 & 52 McLaren Street | The Metro Sites

The northern portal of the Victoria Cross metro station will be located at 50 McLaren Street, on the NE corner of McLaren and Miller Streets. It is understood it will be a lift only access to the station and will take the shape of a 5 to 6 storey structure. Pedestrian entry will be at ground level with bicycle parking below ground and technical plant and service rooms above.

52 McLaren Street, adjacent to the metro site, is currently vacant. There is an opportunity to create a mixed-used development next to the metro that includes commercial activation, community and public uses at the lower levels, and residential uses above. The development will have a transitioning nature, with the height stepping down from the Ward Street Precinct towards the low-scale residential and educational uses fronting St Leonards Park.

Design Guidelines

- Provide a commercial podium fronting McLaren Street (6-storeys)
- Maximum height of 24 storeys on the southern side fronting McLaren Street, similar to the approved development at 168 Walker Street
- Maximum height of 14 storeys, similar to the existing hotel, on the northern side of the site
- Provide a 2-storey podium at the northern end of the site dedicated to community/public uses
- Provide a dedicated 6-metre wide setback for a through-site-link from McLaren Street to Elliott Street along the eastern boundary of the site
- Activate the through-site link with commercial, retail and public uses
- Vehicular access from McLaren Street

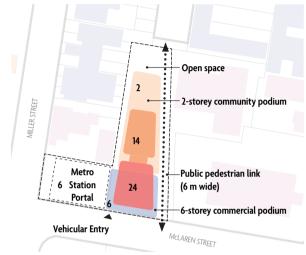
East Walker Street

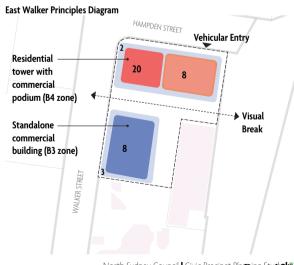
The area on the eastern side of Walker Street, specifically between Berry Street and Hampden Street presents an opportunity for mixed-use developments that transition from the future Ward Street Precinct and commercial core to the more residential areas along the Warringah Freeway. The area should be a mix of medium scale (6-8 storeys) commercial buildings and residential towers (up to 20 storeys). Both building height and use will relate to the existing surroundings. The mix of typologies will provide breaks between buildings and allow filtered views of gardens and trees.

Design Guidelines

- The area is to be rezoned B4 (Mixed-use) on the northern side of the site and B3 (Commercial core) on the southern side of the site
- Maximum height of 8-storeys along the Warringah Freeway frontage
- Maximum height of 20-storeys along Walker Street at specific locations for mixed-use towers.
- Elements above 8-storeys should not exceed 25m in facade length along the street
- Maximum height of 8-storeys for commercial buildings along Walker Street. This
 will allow for view sharing principles and also will provide smaller tenancies and
 floorplates to support small and medium businesses on the fringes of the CBD
- The step down approach from Ward Street to the Warringah Freeway is to provide more depth and breathing space to the CBD when seen from afar, and allow for view sharing principles to be adopted in relation to existing developments on the western side of Walker Street
- A 2-3 storey streetwall along Walker Street is required with upper levels setback to emphasis the street wall. A minimum 3m upper level setback is to be provided
- Developments should provide a break between buildings allowing filtered views from the street to the rear landscaped areas of the lot and the harbour
- New development is not to cast any additional shadows on Doris Fitton Park
- New developments are to adopt view sharing principles with existing developments on the western side of Walker Street
- New buildings should be designed to be respectful of heritage items and HCAs in regard to bulk, scale and aesthetics
- New development within or adjacent to a heritage item or HCA should contribute positively to the heritage value and character of the area

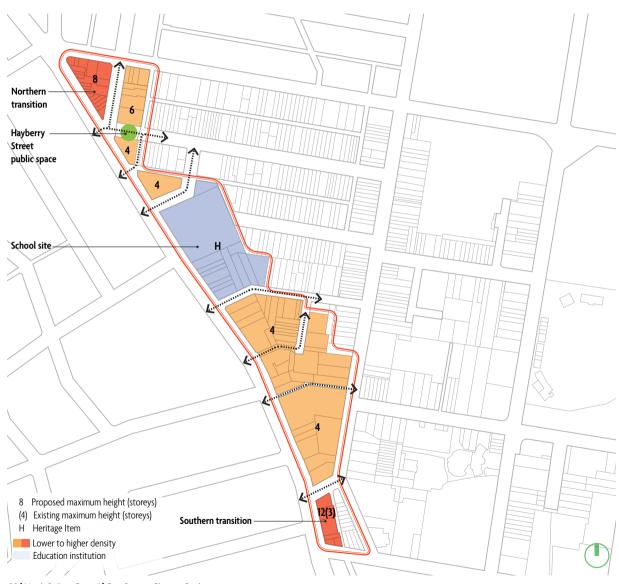
Metro Sites Principles Diagram





North Sydney Council | Civic Precinct Plaping & up 267 of

Action 2: Develop the Pacific Highway frontage into a medium-scale environment with increased amenity



☐ Increased amenity and friendly streetscapes

Desired Character

The Pacific Highway is a metropolitan corridor that connects a series of town centres or urban nodes. Some of these nodes are significantly dense such as North Sydney CBD or St Leonards.

Between these high-density nodes there is a need for a well-defined, low-scale edge that still provides enclosure and legibility and offers a more human friendly streetscape.

The stretch of Pacific Highway along the Civic Precinct sits between North Sydney CBD and St Leonards and it is at the core of North Sydney's green link. It needs to be medium-scale with a maximum of 4 storeys to be in keeping with the rest of the precinct. An increased building height at each end of the stretch will transition towards the CBD and St Leonards.

Objectives

- Create a medium-scale streetscape area between North Sydney CBD and St Leonards that is more human-scale and pedestrian friendly
- Transition either side of the Civic Precinct along the Pacific Highway through increased building heights into the higher-density areas
- Increase amenity and activation along the Pacific Highway

Implementation*

- Maintain a height limit to 4 storeys within the Civic Precinct with a 3-storey streetwall and a recessed 4th storey
- Provide higher buildings on the corner sites to transition between areas
- Provide a 1.5-metre whole building setback along Pacific Highway to improve pedestrian amenity and street tree planting
- Provide active frontages to the Pacific Highway where possible
- Continue the implementation of cycleways along Pacific Highway (see Action 9)
- Investigate ways to improve the Five Ways intersection by diverting traffic if possible (see action 5)

^{*} Refer to page 29, significant sites guidelines for further detail

Action 2: Develop the Pacific Highway frontage into a medium-scale environment with increased amenity

▽ Significant Sites Guidelines

Northern Transition Area

The northern transition site of the Civic Precinct along Pacific Highway is a triangular block on the southern side of the Five Ways intersection.

This area and the site should be included in the recommended detailed study of Falcon Street once the outcomes of the DPIE Planned Precinct Investigation Area (2036 Plan) and the impacts of the Western Harbour Tunnel are known.

Notwithstanding, the site will become the transition between the Civic Precinct and Crows Nest and as such, a medium scale marker that relates to both is appropriate. The future building should be of a similar scale to Crows Nest and relate to the low-scale environment of the Civic Precinct.

Due to the site's layout and shape it can only be through the amalgamation of all the sites within the block that future development could happen.

The site is in a prominent location and would have the potential to provide a well-designed urban node.

Design Guidelines

27st 🕶

Proposed Crows Nest

OSD (DPIE)

17st -

- Consolidation of all the sites within the block into one development
- Provide a 3-storey commercial podium with commercial or residential uses above
- Minimum 3-metre setback for the floors above the podium
- Potential for an internal, publicly accessible arcade through the ground floor to avoid deep tenancies and unusable areas
- The footprint above the podium should comply with the ADG requirements in regards to building depth, separation, solar access and articulation
- Any future development will have a maximum height of 8-storeys in response to the Crows Nest Village context
- Provide an additional 1.5 metre whole building setback along Pacific Highway

Southern Transition Area

The southern transition site of the Civic Precinct along Pacific Highway is highly constrained with a limited footprint and it is immediately adjacent to a conservation area. Furthermore, there are two heritage items within the block. However, due to its location, it still has the potential to provide a good transition from the Civic Precinct towards North Sydney CBD with a medium scale marker that bridges the gap between the mid-rise residential buildings along Pacific Highway and the low-scale environment of the Civic Precinct.

Design Guidelines

- The site should be developed in two separate buildings, one on each side of the heritage item.
- The northern building should relate to the built form north of the site and have a maximum high of 5 storeys with a 3-storey streetwall
- The southern building should relate to the built form south of the site towards the CBD and transition from 10 to 12 storeys with a 3-storey podium
- The heritage item will be preserved and adequate separation around it provided.
 This separation should allow for a pedestrian passageway either site of the heritage item with space for activation. Adaptive reuse of the heritage item is encouraged
- Future development should provide a three storey podium in alignment with the rest of the streetscape and also in alignment with the height of the heritage item
- Podiums should be fully commercial with commercial or residential uses above
- Future development cannot overshadow the open spaces of the education facilities located on the western side of Pacific Highway
- An adequate transition to the conservation area to the east should be provided in the form of a podium with significant setbacks above.

NORTH SYDNEY CBD

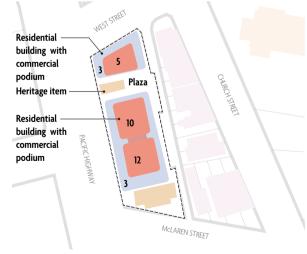
Relates to CBD

(12 storevs)

• Provide an additional 1.5 metre whole building setback along Pacific Highway

Separation to comply with ADG Potential Ground Level Break / Arcade 8 Separation to comply with ADG Potential Ground Level Break / Arcade





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CROWS NEST VILLAGE CIVIC PRECINCT

NORTHERN

TRANSITION AREA

Relates to Crows Nest

(8 storevs)

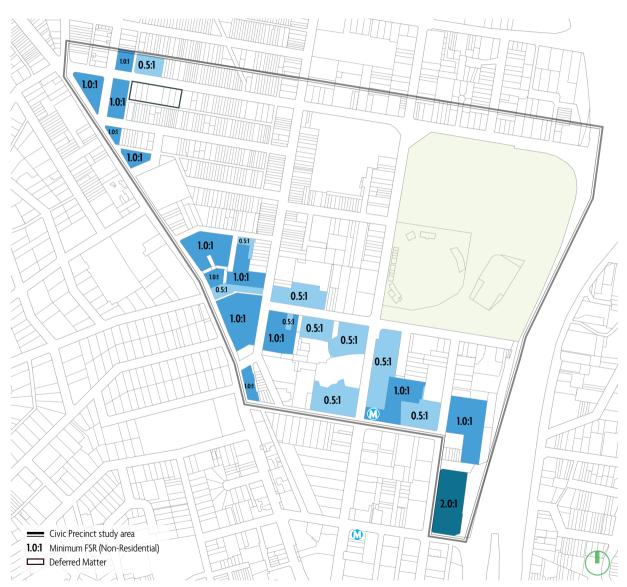
24st —

10st —

LOW-SCALE STREETSCAPE (3 +1 STOREYS)

SOUTHERN
TRANSITION AREA

Action 3: Support small to medium sized business growth



▽ Supporting North Sydney's business ecosystem

The Civic Precinct is an attractive location for both the residential and commercial market. It is an alternative for companies who want to be close to the CBD and not pay the premium rents charged. Both residential and commercial demand is high.

In order to achieve the regional job targets set for North Sydney by the Greater Sydney Commission and support the employment growth of the CBD, it is important to maintain and increase commercial floorspace within the Civic Precinct.

Surrounding established development within the North Sydney CBD and pipeline stock are aimed at providing large amounts of floor space, particularly offices. Therefore, it is in the best interest for the precinct to provide smaller commercial tenancy space to accommodate businesses that service the area such as medical consultation or accounting. Employment projections identify that the Professional, Scientific and Technical Services industries, which already total 40% of the area's employment, will increase by 33% in 2036, continuing to be the highest employer category.

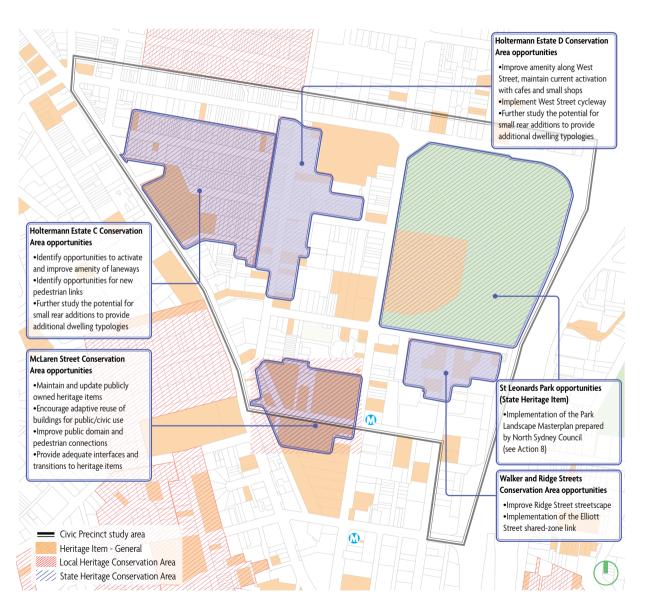
Objectives

- \bullet Provide over 3,000 additional jobs within the precinct (estimated as 1 job per $15m^2$ of commercial area)
- Build on the existing presence of education and medical facilities in the area encourage companies and initiatives that support and complement these sectors to settle in the area
- Develop unique and distinct employment opportunities that capitalise on the metro station and benefit being located adjacent to the CBD
- Provide a range of smaller commercial tenancy spaces to accommodate businesses
 that service the CBD and surrounding residential areas such as accounting, medical
 consultation, information technology or conferences and seminars

Implementation

- Increase the non-residential FSR across the precinct as shown on the map
- Maintain B4 mixed-use zone to provide for smaller commercial tenancy spaces within mixed use buildings.
- Require full commercial podiums (2 to 3 storeys) along Pacific Highway
- Encourage ground floor retail and commercial tenancies to increase foot traffic and improve street activation along the main thoroughfares and nodes
- Require fully dedicated, small to mid-size commercial buildings along the southern end of Walker Street to complement the CBD's premium office towers.

Action 4: Preserve Heritage, add value



☐ Maintaining, Updating & Adding Value to Our Heritage

The Civic Precinct has 93 Local and State Heritage Listed Items and four Heritage Conservation Areas (HCA). Of these items, ninety-one are locally significant, and two are State significant.

It is important to protect these heritage buildings and conservation areas for future generations as they define the local identity and unique character of the area.

At the same time, we must ensure that the area is well maintained, remains relevant, activated and used, so it is equally important to allow for careful and considered additions that add value to the area and provide updated spaces for modern needs.

Adaptive Reuse of Buildings

Adaptive reuse of a building can help to conserve and celebrate the heritage significance of a place while allowing it to have a new use and an extended lifespan. Adaptation of a building to allow for a new use should be underpinned by an understanding of its significance and should result in a design which creates a relationship between existing and new work.

Alterations and Additions to Heritage Items

The North Sydney DCP outlines several provisions for altering and adding to listed heritage items. Underpinning these provisions is the objective that any changes to the fabric should be based on an understanding of the heritage significance of the specific item.

Development within Heritage Conservation Areas

The following general principles apply for any work done within a Heritage Conservation Area.

- New development will complement and retain the significance and character of the place
- Development in these areas should recognise and complement the predominant architectural scale, form, bulk, setbacks and materiality of the HCA to create a unified character.
- Development of new buildings within a HCA (known as 'infill development') will need to positively respond to the setting and character of the area

Further study the potential for additional secondary dwelling types

As a response to the need for more affordable housing options and with the idea of liven up the heritage conservation areas and maintain their character, further studies should be carried out to explore the potential for additional secondary dwellings facing the laneways in the form of granny flats and over garage studios.

Action 5: Undertake further review of Falcon Street



The current development controls for Falcon Street are retained, whilst there is uncertainty around the corridor due to the different state government infrastructure projects currently ongoing, in particular the DPIE's Planned Precinct Investigation Area and the Western Harbour Tunnel.

It is recommended that once the outcomes of these two projects are known, a detailed Urban Design Study is undertaken of Falcon Street, focusing on the area between Pacific Highway and West Street.

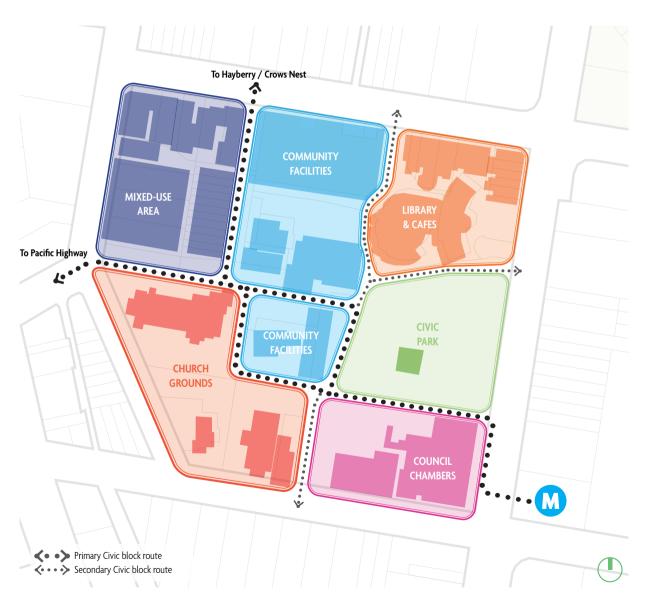
The corridor is a major traffic thoroughfare connecting the Military Road Corridor with Pacific Highway. It is also the transition between the Civic Precinct and Crows Nest Village.

Being able to combine both locational functions will be key on order to achieve a successful outcome.

Proposed objectives of the future urban design study

- Manage and mitigate the future traffic conditions
- Explore potential reconfiguration of the Five Ways intersection to increase pedestrian amenity and quality and quantity of open space
- Explore the potential of diverting regional traffic from Pacific Highway to Falcon Street / Warrignah Freeway to reduce traffic volumes in the CBD and increase the amenity of Pacific Highway
- Provide a more pleasant pedestrian environment
- · Protect existing trees
- Study the potential for additional landscape and planting buffers
- Explore opportunities to upgrade the footpaths
- Capitalise on the proximity to Crows Nest Village and the Health Precinct
- Successfully connect Crows Nest to the Civic Precinct
- Consider employment and activation opportunities along the spine

Action 6: Undertake further review of Civic Core



▽ A revamp of the Civic Core

The Civic Core is bounded by McLaren, Ridge, Miller and West Streets and includes a high amount of publicly owned assets.

There is an opportunity to improve pedestrian connections through the block and to examine the relationship between the different buildings to make them more open, collaborative and efficient. At the same time, the potential for future expansion of community uses as the population of the area grows should be explored.

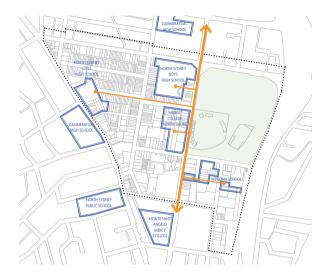
The area should be guided by a masterplan able to connect all the existing public buildings and civic institutions within the block. This will create a seamless environment that fosters collaboration, sharing of resources and a welcoming space for visitors and workers alike. Further work to the existing layout and public space within the core would allow for greater connectivity and legibility between the different buildings.

The core is currently separated into 6 different areas, each of them with its own land use. Between these areas are a series of pathways, some formally defined others more informal. Developing and formalising these pathways will help with connections through the Civic core. It will also improve the existing public domain and the relationship between the different buildings.

Proposed objectives of the future urban design study

- Formalise and improve pedestrian connections through the block
- Study the potential redesign and redistribution of the child early education centre, the community centre, the basketball courts and the library.
- Facilitate the sharing of resources and spaces between the different public and civic institutions
- Explore potential improvements to the edges and surroundings of Ted Mack Civic Park and implement Miller Walk along Miller Street
- Integrate the church's open space into the public domain
- Consideration of underground public car parking
- Rationalise the vehicular circulation within the block, and consolidate and redesign the existing car parks.

Action 7: Urban design guidelines for education facilities



∇ Overview

Schools in the Civic Precinct are generally walled and isolated and do not interact with the surrounding environment. Students are largely driven to and from school. There are few comfortable pedestrian connections or areas to gather and play.

The Civic Precinct could evolve into an civic and education campus with a strong relationship between school grounds and the wider precinct area.

Schools have the opportunity to open up (within reason and maintaining the required safety and privacy) and activate the streets. To be more efficient, facilities could be shared between the community and the schools. A network of pedestrian links should be implemented between the schools and the public transport nodes, in particular the new metro station, to further these opportunities.

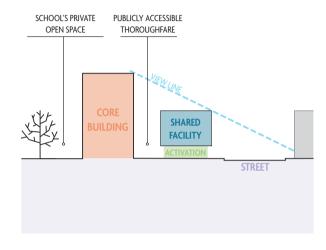
¬ A precinct-wide civic and educational campus

Similar to other metropolitan areas that have been earmarked for education and health collaboration areas, like St Leonards or Randwick, North Sydney's Civic Precinct should be seen as a local, more intimate version of such collaboration areas.

The precinct already has the institutions, the open spaces and the civic infrastructure in place. There is the opportunity to develop a strong, singular educational identity and an environment that encourages and fosters interaction, knowledge sharing and community interaction.

Currently lacking, is a solid public domain presence, the legibility and connectivity of a campus, the ancillary supporting uses and businesses, and above all a shared approach.

The first step is to improve the public domain in a way that promotes students' presence in the area. Facilities for study groups, places for after school activities and play time, lunchtime offerings, a library space for students and residents to interact, are all initiatives that would help to improve the educational identity of the area.



Design principles

Additional building height may be requested on education campuses in the long term to accommodate growth in student population. This should be subject to a case by-case detailed masterplan that addresses the following principles:

- Reinforce the area's educational function
- Any additional height should, in principle, be located at the centre of the school's ground with a lower scale perimeter so that the character and feel of the streetscape are maintained
- New development should not alter the presentation of the school to the street
- Amenity impacts to the public domain should be minimised. Above all, no additional overshadowing of open spaces and plazas will be allowed
- Privacy and visual amenity of surrounding properties should not be affected by any new development
- Integrate educational facilities into the neighbourhood by improving their interfaces with adjoining streets and the public domain
- Improve the area's permeability by encouraging pedestrian through-site-links (see Actions 8 & 9)

Action 8: Improve public open space



Main public domain opportunities within the study area are centered around 5 main elements.

1. Implementation of the St Leonards Park Landscape Masterplan

- Improve and enhance the park landscape
- Improve and enhance recreation opportunities for a range of park users
- Develop a range of options for additional facilities that could appropriately be provided in the park to meet community needs
- Ensure the park is accessible and appealing to all members of the community
- Resolve existing circulation and access issues for pedestrians, cyclists, and vehicles

2. Implementation of the Civic Spine / Miller Walk up to Falcon Street

- Developed as part of the CBD Public Domain Strategy
- Improve link between civic and public buildings and areas along Miller Street from Blue Street to Falcon Street
- Consider widening pedestrian footpath along Miller Street to create "Miller Walk" with areas for landscaping, seating and a wide range of activities.

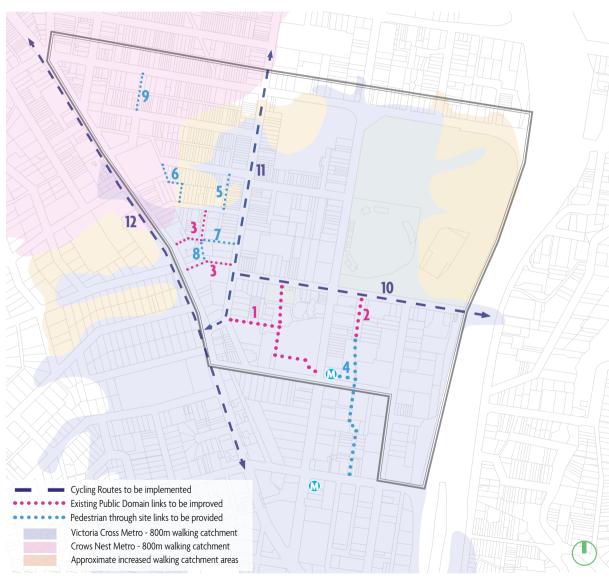
3. Provide additional open spaces

- Provide smaller, more localised open spaces that would improve the local amenity for residents and workers
- A. New pocket park at the eastern end of Hampden Street
- B. Upgrade to public open space at the western end of Hayberry Street
- C. New entry plaza to the future Metro Station entry portal.
- D. Explore options to turn on grade carparks into public open spaces along Miller Street (St. Mary's Church)
- E. Investigate opportunity to develop a green edge to the Warringah Freeway

4. Improve walking, cycling and public transport safety and amenity on major urban corridors

- Improve the amenity of Falcon Street and Pacific Highway through improvements to the tree canopy, footpath width, setbacks, and increasing building setbacks
- Implement cycleways along Pacific Highway

Action 9: Identify access and active transport network improvements



With the arrival of the metro to the corner of Miller and McLaren streets there is an opportunity to improve the access to the precinct and its connection to the wider metropolitan context.

In particular it is critical to connect cycle and walking routes to the new metro station, complete the N-S CBD pedestrian link coming from the Ward Street Precinct, through Elliott Street and arriving at St Leonards Park and implement further E-W connections that will link to the overall walking and cycling network of the green link

The following key improvements and potential new links have been identified that would maximise the impact and reach of the metro station.

Existing Public Domain links to be improved

- 1. Better East-West routes through Council's civic block would make Pacific Highway and Mater Hospital more accessible from the metro (see Action 6)
- 2. Elliot Street to become a shared zone
- 3.Improved E-W permeability between Pacific Highway and the heritage conservation areas to the east. In particular upgrade Hazelbank Place and Eden Street / Eden Lane

Pedestrian through site links to be provided

- 4.A connection between the metro portal and Elliott Street through and 52 McLaren Street (see Action 1)
- 5. Completion of Bernard Lane up to Myrtle Street as a pedestrian only link (desirable)
- 6.A connection between Myrtle and Emmett Streets through the back of North Sydney Girls High School.
- 7. A connection between Eden Street and West Street (desirable)
- 8. A connection between Hazelbank Place and Eden Lane (desirable)
- 9. A through-site link between Hayberry and Falcon Streets

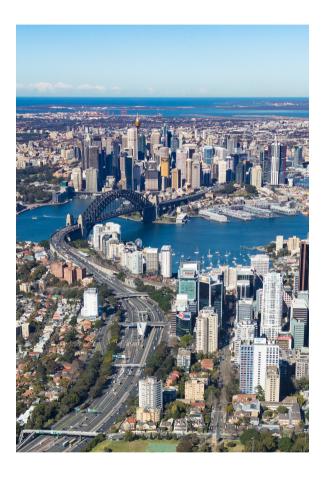
Cycling Routes to be implemented

- 10. Completion of the Ridge Street cycleway
- 11. Implementation of the West Street cycleway
- 12. Implementation of the Pacific Highway cycleway

With a collaborative approach between council, the state government, private owners and the education and religious institutions in the area, these connections could be completed, creating a network of pedestrian routes serving the schools, the community facilities and the public in general. The result would be a safe, attractive and rich pedestrian environment connected to the new metro portal .



Public Benefits Overview



☐ Public benefits and the VPA process

A driving principle of the study is that development opportunities beyond those available under existing controls should only be pursued if much needed public benefits are provided to meet the community's needs. These benefits are in addition to what would normally be required by a new development, such as design excellence and section 7.11 developer contributions.

Accordingly, for the Civic Precinct a planning proposal seeking to amend the NSLEP 2013 should be accompanied by a draft voluntary planning agreement (VPA) which sets out any public benefits proposed to be delivered in accordance with Council's 'Voluntary Planning Agreements Policy'.

A planning proposal (PP) is a submission to Council to modify existing planning controls, allowing a developer to achieve increases to a building's maximum floor space ratio (FSR), building height or other planning controls.

When a planning proposal is submitted, there is opportunity for the developer and Council to negotiate a voluntary planning agreement (VPA) whereby the developer delivers public benefits in exchange for amendments to the planning controls. The public benefit offered could be a monetary contribution, dedication of land or an inkind contribution (a provision of a material public benefit specified by the Council).

• public amenities and public services

Under a VPA, a developer agrees to provide or fund:

- affordable housing
- transport or other public infrastructure

These contributions can be made through:

- dedication of land
- · monetary contributions
- construction of infrastructure
- provision of materials for public benefit and/or use

Planning Proposal Process

Council's City Strategy Team will hold a pre-lodgement meeting with property owners and their advisors to discuss specific requirements for a planning proposal.

This will generally include discussion on:

- Existing planning controls
- Specific requirements for the site identified in this study
- Potential public benefits the scheme may offer, and
- Planning process, including information to be submitted with a planning proposal

A draft planning proposal and any draft voluntary planning agreement will then be accepted by the City Strategy Team for comment.

To ensure agreed design outcomes are identified early in the process, the draft planning proposal may be referred to the North Sydney Design Excellence Panel for comment

Once agreed, a planning proposal should be formally lodged with Council.

For further details on how to prepare and lodge a planning proposal, please refer to the Department of Planning & Environment's 'Guide to Preparing Planning Proposals'.

Civic Precinct Public Benefits List

☐ Civic Precinct - Potential Public Benefits List

The study has identified a series of potential public benefits that address the community's aspirations as well as existing needs for the area. Some of these benefits could be delivered via agreements with private landowners to redevelop opportunity sites. Others may be delivered through the existing Council's Section 7.11 Contributions plan.

It is worth noting that there are not many sites expected to undergo significant changes under this planning study. This limits the capacity to deliver on the desired public benefits through the VPA process.

Notwithstanding, the list of identified public benefits for the Precinct are:

- Potential new location for the Kelly's Place Children's Centre
- Community Centre & Community meeting rooms
- Public domain upgrades (as per Action 8)
- Affordable housing provision
- Public art and sculpture

Kelly's Place Children's Centre

Kelly's Place is a not-for-profit, community-based, long day care centre currently located in Hume Street Park, Crows Nest. Kelly's Place Children's Centre is an independent organisation located in a council owned site.

Should the centre had to be relocated as part of the changes to the area due to the new Crows Nest metro station, North Sydney Council believes that the Civic Precinct could potentially be a good location for Kelly's Place that would enable the centre to keep its roots and its community.

Kelly's Place location and spatial requirements are as follows:

- A ground floor location (single-storey)
- 400m² of outdoor space with sunlight and trees (deep soil)
- 400m² of indoor space.
- Not located on a main road but with an easy drop off / pick up area
- To maintain the relationship with Crows Nest and its community

Further study of the potential move will be carried out in consultation with the centre to ensure that any future location is able to provide the necessary requirements.

Community Centre & Community Meeting Rooms

The current community centre and the Early Education facility next to it, located near Ted Mack Civic Park, both need to expand. Currently they share a location and more space is needed for expansion.

Therefore, a new location for either a community centre and/or community meeting rooms would allow for the North Sydney Community Centre to grow and at the same time allow for the Early Education facility to also expand.

Public Domain Upgrades (as per Action 8)

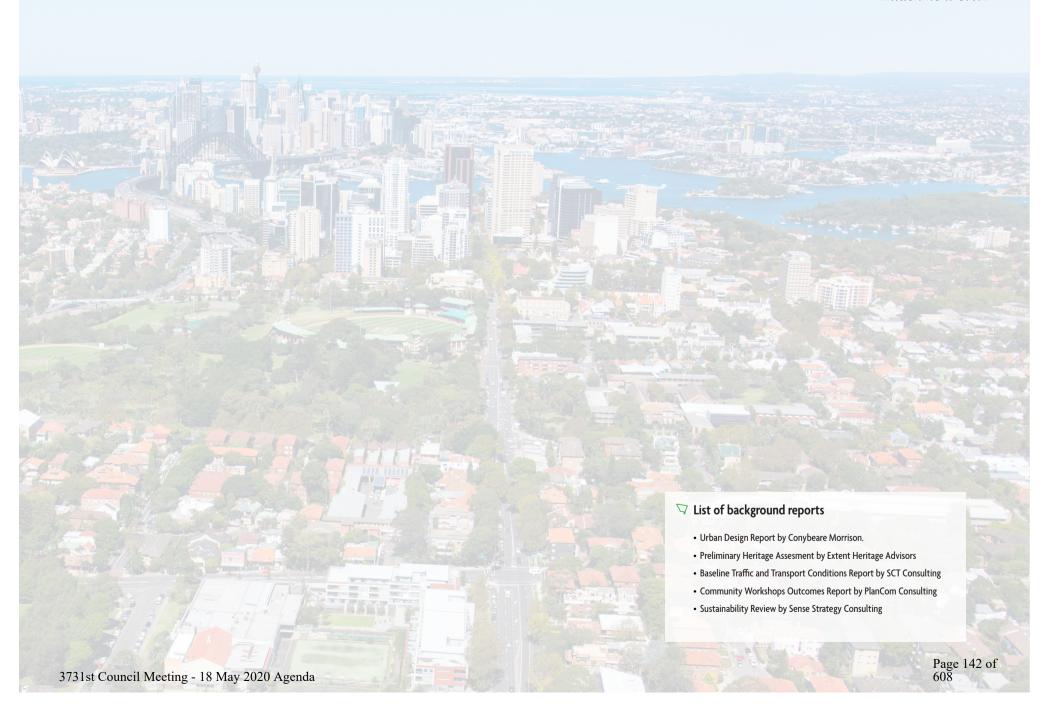
Improving the public domain within the precinct is one of the main priorities for Council and also for the community. The Public Domain opportunities (described in Action 8) include the implementation of the St Leonards Park Landscape Masterplan and the North Sydney Oval improvements, the development of cycleways along West and Ridge Streets as well as Pacific Highway and further advancement of Miller Walk.

Affordable Housing Provision

North Sydney's Local Housing Strategy (LHS) has identified a gap in the provision of affordable housing across the LGA. Affordable social housing currently accounts for only 1.8% of the total dwellings in North Sydney, below the 5.5% average across Greater Sydney. Council is committed to find ways to add more affordable housing across the LGA

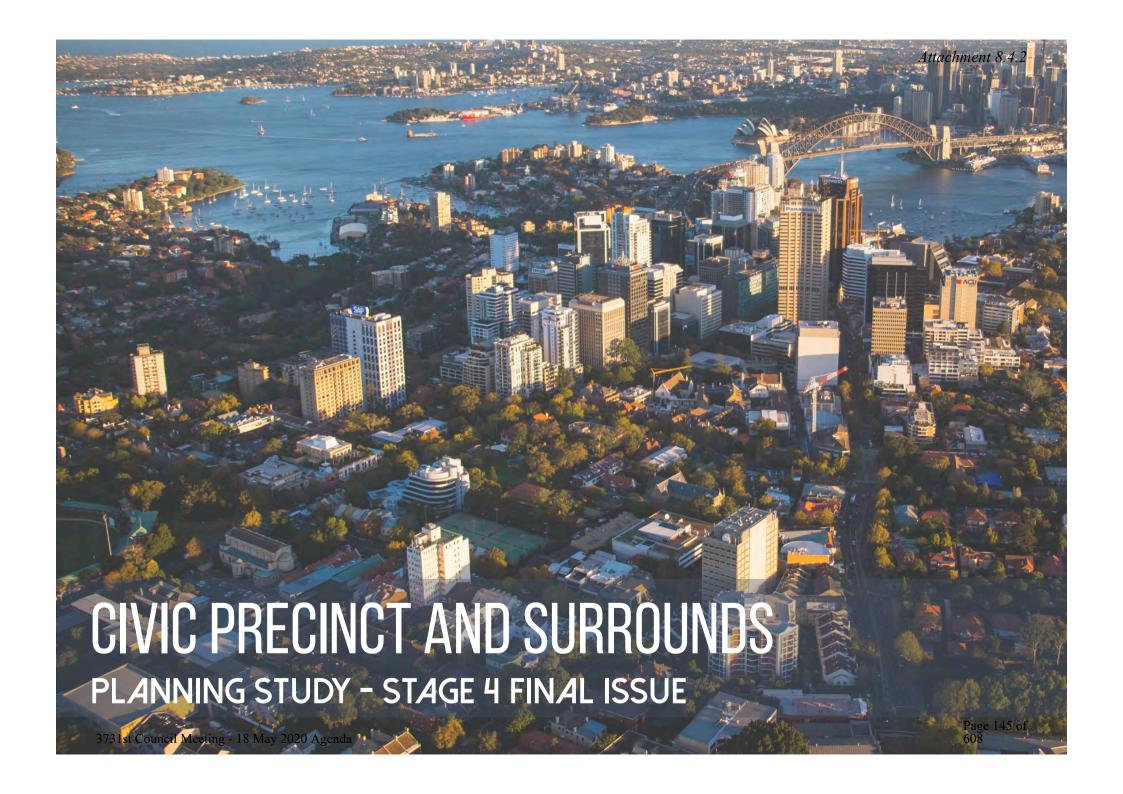
Public Art & Sculpture

The North Sydney CBD Public Domain Strategy identifies key locations along Miller Street and the N-S CBD Pedestrian link for new public art and sculptures. These art interventions at key nodes along the route help reinforce the public domain and add interest to the different space through the journey from North Sydney train station to St Leonards Park.











By: CM+

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Revision	Date	Description	Ву	Chk	Арр
01	15/11/2019	Draft Stage 2 Report	WW/MK	WW	-014
02	06/02/2020	Stage 3 Report Issue	WW/MK	WW	-014
03	20/03/2020	Stage 4 Report Issue	WW/MK	WW	-DA
04	08/04/2020	Final Stage 4 Issue	WW/MK	WW	-04
05	22/04/2020	Final Stage 4 Issue - Revised	WW/MK	MK	WW

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4.0	Urban Design Analysis	25	Assessment
5.0	Current Controls Analysis	43	Appendix 03 North Sydney Civic Precinct Planning Study Baseline
6.0	Workshop Outcomes	65	Traffic and Transport Conditions Report
7.0	Urban Design Vision and Principles	71	Appendix 04 Civic Precinct and Surrounds Planning Study Workshop Report
8.0	Preferred Planning Strategy	73	Appendix 05 North Sydney Civic Precinct and Surrounds Planning
9.0	Public Benefits	111	Study, Sustainability Review
10.0	Public Exhibition Feedback		
11.0) Final Plan		

12.0 Implementation

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Figure 01: North Sydney Civic Precinct and Surrounds aerial view from the west (Courtesy of North Sydney Council)

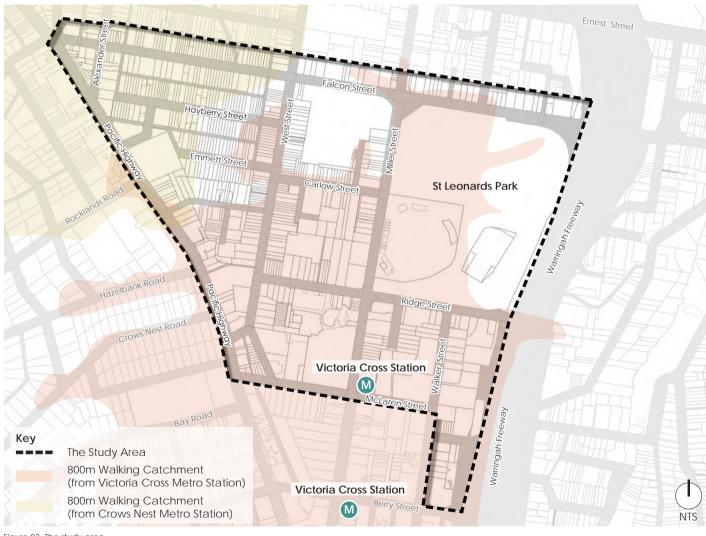
CONYBEARE MORRISON

I.I INTRODUCTION

Conybeare Morrison (CM+) and the consultant team were engaged by North Sydney Council (NSC) to conduct a planning study of the North Sydney Civic Precinct and Surrounds. The purpose of the study is to review the current development controls and urban design of the Study Area in light of the new access to public transport that will be provided by the Victoria Cross Metro Station northern portal planned to open in 2024, and the release of the North District Plan. The planning study seeks to strengthen the elements of the precinct that the community value and identify any public benefit that may result from new development. The study will be a part of the Department of Planning, Industry and Environment's (DPIE) initiative to align the Local Environmental Plans (LEP) with the Greater Sydney Commission's (GSC) District Plans.

The North Sydney CBD provides the third largest office floor spaces in Greater Sydney and is projected to grow by up to 21,000 jobs over the next 17 years. The North District Plan provides an overarching vision and guidance for the future growth of North Sydney.

This Study will provide recommendations for the orderly growth of the area which aligns with the visions and goals set in the North District Plan, and which reflects community expectations.



1.2 THE STUDY AREA

The North Sydney Civic Precinct and Surrounds (the study area) consists of a mix of land uses and of urban character. The study area will be within walking distance of two future Metro stations -Crows Nest Station and Victoria Cross Station (both the South and North Portals).

The study area has an area of approximately 65ha. It is bounded by the Pacific Highway to the west, the Warringah Freeway to the east, McLaren Street and Berry Street to the south, and Falcon Street to the north (including St Leonards Park and the north side of the street). Refer to Figure 02.

The study area comprises significant civic, community, education, sporting, open space and recreational facilities.

Figure 02: The study area

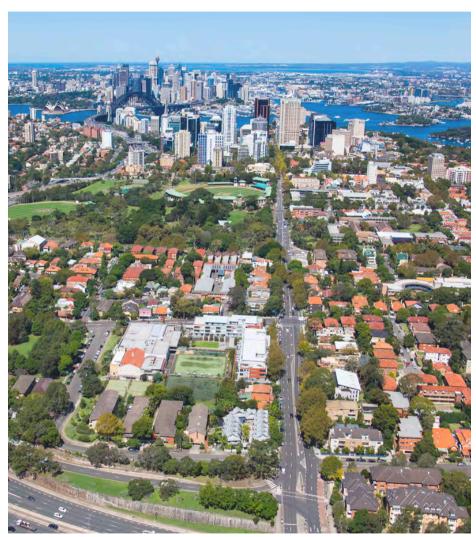


Figure 03: North Sydney aerial view from the north

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1.3 PROJECT OBJECTIVES

The primary project objectives for the study are to:

- Explore appropriate opportunities to review building height and density within the precinct in light of the introduction of improved access to public transport.
- Develop a considered urban planning vision and strategy to guide Council's response to several Planning Proposals lodged for sites within the study area.
- Identify, and retain, the urban values of the precinct including its heritage and its fine-grained structure and 'leafy' character, and to balance this with any identified opportunities for growth within the study area.
- Achieve the objectives and actions identified in the GSC District Plan.
- Identify possible improvements in the public domain, including pedestrian linkages, wayfinding, new public open space and improved public domain amenity - consistent with the objectives and directions identified in the draft North Sydney CBD Public Domain Strategy (2018).

- Explore opportunities to deliver public benefit within, or on the periphery of the study area, as a result of redevelopment proposals.
- Establish urban planning principles for the study area.
- Identify the appropriate density across the study area considering the existing opportunities and constraints, its location within the North Sydney Local Government Area (LGA) and current and future access to transport and public infrastructure.
- Identify the existing and future 'high significance civic places' within the precinct and define their desired future character.
- Understand the community and stakeholders' visions and needs on the future planning of the study area.

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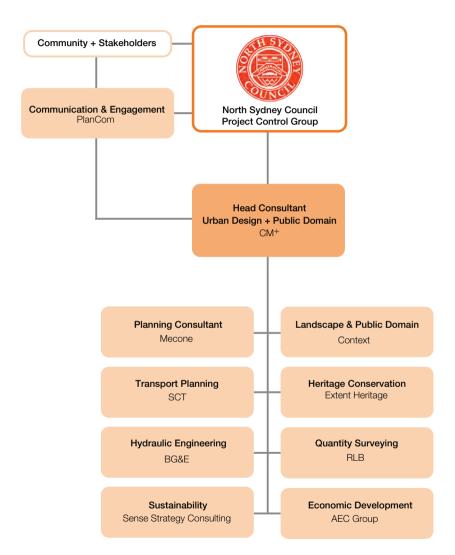


Figure 04: Team structure

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1.4 THE PROJECT TEAM

The CM⁺ led Consultant Team included the following expertise:

- Project Management CM⁺
- Urban Design CM⁺
- Strategic and Statutory Planning -Mecone
- Community / Stakeholder
 Engagement and Consultation PlanCom
- Landscape + Public Domain Design -Context Landscape Design
- Economic Development AEC Group
- Transport Planning SCT Consulting
- Heritage Extent Heritage
- Flood Management BG&E
- Sustainability Sense Strategy Consulting
- Quantity Surveying RLB

The Consultant Team worked closely with North Sydney Council, and engaged with key stakeholders and the community to deliver the project.

The team structure is illustrated in Figure 04.

1.5 PROJECT METHODOLOGY

The planning study is undertaken with the following methodology:

- Conduct a site visit of the study area and its surroundings to familiarise with the study area and its context.
- Attend an Inception Meeting with Council to confirm the project objectives, program and Community Engagement Plan.
- Undertake background information review, including review of the Stage 1 Report by NSC.
- Conduct Urban Design Analysis, and constraints and opportunities study.
- Consult with the project team with regard to transport issues and opportunities, public domain requirements, heritage conservation and economic feasibility.
- Establish the Design Principles for the precinct.
- Conduct high-level built form testing of the study area based on the current controls.
- Consult with the community.
- Develop an alternative built form scenario for the less constrained sites, informed by the analysis and principles, and with input from the consultant team, as well as community feedback.
- Document the preferred built form scenario in the draft Planning Study.
- Public exhibition of the draft Planning Study and Community Engagement Events.
- Collate the feedback received during the exhibition period and amend the Planning Study, if necessary addressing the comments received.
- Finalise the Planning Study.

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I.6 PROJECT PROGRAM

The project has a one-year program, running from July 2019 to June 2020. Initial Consultation work (Stage 1) has been completed and reported by North Sydney Council. The full Stage 1 Report can be accessed via the link below:

https://yoursay.northsydney.nsw.gov.au/ civicprecinct

Stage 2 - Research and Analysis commenced in October 2019 and culminated in January 2020.

Workshops with the community were organised in November and the first week of December 2019 as a part of Stage 3 - Research Analysis and Consultation.

Stage 4 Documentation of the Planning Study commenced in late January 2020, following the Stage 3 consultation with the community.

Public exhibition of the draft Planning Study (Stage 5) commenced in May 2020. Community consultation events were held during the exhibition period. The draft Planning Study was available for comments / submissions during this period. All the comments / submissions received were reviewed and informed the final Planning Study.

The study was finalised late June and early July 2020. Please refer to the program below in Figure 05.

I.7 COMMUNITY ENGAGEMENT STRATEGY

The Community Engagement Strategy (the Strategy) has been prepared in accordance with Local Government Act 1993 and Council's Community Engagement Protocol. It will ensure that the stakeholders and the community can be involved in the decision-making process.

The Strategy identifies the key community stakeholders, key communication messages and the timetable for engagement events.

The Strategy lays the foundation for a high-quality engagement process, that will inform the preparation of the Planning Study.

The Community Engagement Strategy is provided in Appendix 01.



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	October				November			December					January				February				March					April				May				June						Ju	лly	
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Figure 05: Program
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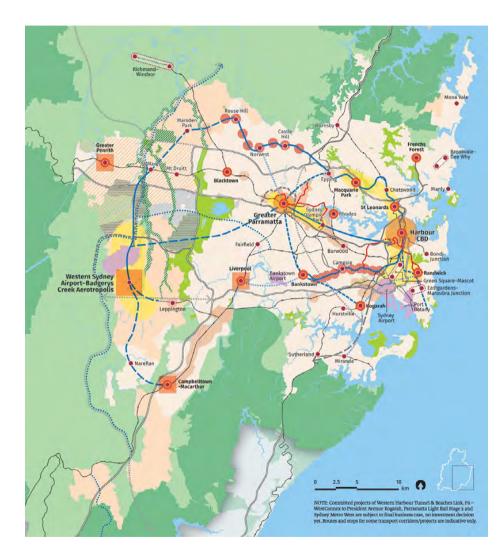


Figure 06: A Metropolis of Three Cities (Courtesy of the Greater Sydney Commission) CONYBEARE MORRISON

2.1 STRATEGIC CONTEXT

A Metropolis of Three Cities

The Greater Sydney Regional Plan; A Metropolis of Three Cities, establishes the aspirations for the Greater Sydney region for the next 40 years. It envisages the creation of three distinct cities that will be the focus of economic, social and cultural activity for Sydney's population through to 2056, including:

- The Eastern Harbour City an established city with a strong transport network to which the Sydney and North Sydney CBDs are its hub and economic heart
- The Central River City a developing city with Parramatta as its economic and cultural centre
- The Western Parkland City a planned city focused on Western Sydney Airport and supported by several key strategic centres

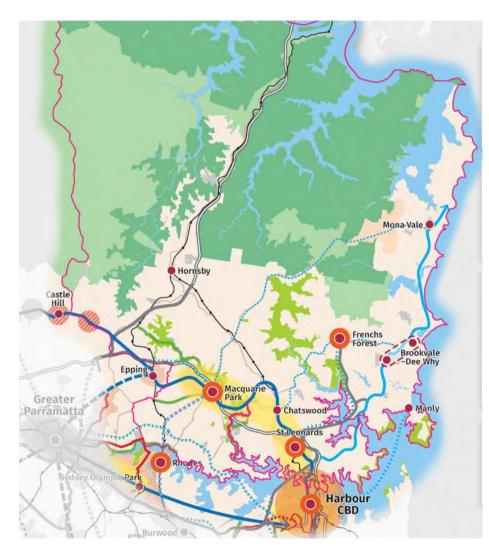
The Eastern Harbour City will include:

- Health and education collaboration areas
- Population increases leading to urban renewal and the associated need for increased infrastructure and services, open space and public places
- Investments in transport and services, jobs growth and business activity – being the catalyst for innovation and global competitiveness in the Harbour CRD
- Improved access to foreshores, waterways and the coast for recreation, tourism, cultural events and water-based transport

North Sydney CBD is located within the Eastern Harbour City. Along with the Sydney CBD, it is part of the 'Harbour CBD' which is Australia's global gateway and financial capital.

The Eastern Harbour City is a focus for innovation and global competitiveness. The Greater Sydney Metropolitan Plan prioritises infrastructure investments in and around the Harbour CBD to enhance public transport, walking and cycling accessibly, as well as developing plans which strengthen the international competitiveness and vibrancy of this key asset.

The Greater Sydney Metropolitan Plan emphasises the role of collaboration, especially in health and education precincts such as those located in St Leonards/Crows Nest.



North District Plan

The North District Plan (the Plan) is developed to guide the implementation of the Greater Sydney Regional Plan at a district level and provide a bridge between regional and local planning. The 20 year housing target for the district is 92,000 additional dwellings. The North District will be home to 13% of Greater Sydney's new growth.

The Plan emphasises the important links between the economy of the North District and that of the Harbour CBD. North Sydney provides half a million jobs and is the largest office market in the North District.

There is significant transport and infrastructure investment underway and planned for the North District, including the Sydney Metro City & Southwest and the St Leonards Health and Education Precinct. The focus of growth in the North District will be around well-connected places that build local strengths and focus on the quality of the public domain.

The Greater Sydney Green Grid is expected to build on existing natural features and parklands of the North District to create green links which support and encourage walking, cycling and community access to open space.

Landscape and scenic features, including views from the Harbour and views to the Harbour and foreshores are to be preserved and enhanced.

North Sydney LGA

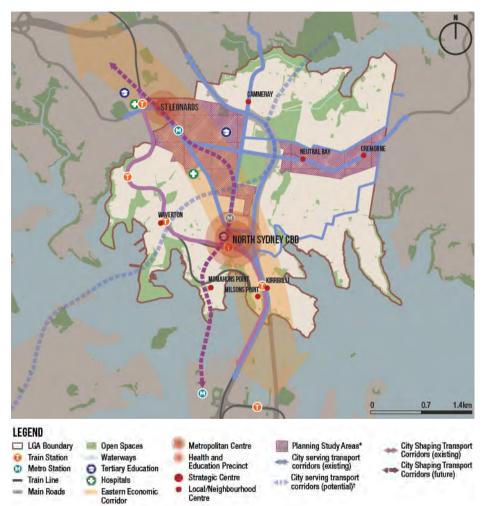
The North District Plan sets a five-year dwelling target of 3,000 additional dwellings for the North Sydney LGA, as well as requiring that a Local Housing Strategy (LHS) be provided for the period up to 2036.

In addition to the dwelling target, the Plan identifies that 15,600 – 21,000 additional jobs are to be created within the North Sydney CBD, and another 6,900-16,400 jobs are expected within the St Leonards precinct by 2036.

The 2019 NSW Population Projections by the DPIE estimates a total population of 89,800 and a total number of 48,250 dwellings by 2041 within the North Sydney LGA.

Figure 07: The North District Plan (Courtesy of the Greater Sydney Commission) 8 | CIVIC PRECINCT AND SURROUNDS PLANNING STUDY

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North Sydney Local Strategic Planning Statement (LSPS)

A LSPS sets a 20-year vision for land use in an LGA and is a bridge between strategic and statutory planning and local government legislation.

Each LSPS will implement regional and district plan actions, and the council priorities as identified in the Community Strategic Plan prepared under the Local Government Act 1993.

Built upon the existing plans, policies and community views, the LSPS sets a framework for an orderly growth of North Sydney LGA.

The LSPS vision seeks to maintain and enhance the role of the North Sydney CBD and St Leonards / Crows Nest precinct as key job attractors. It also encourages housing growth in areas with good access to transport, jobs, services and amenity, whilst maintaining the special attributes of individual areas and neighbourhoods outside intensification zones.

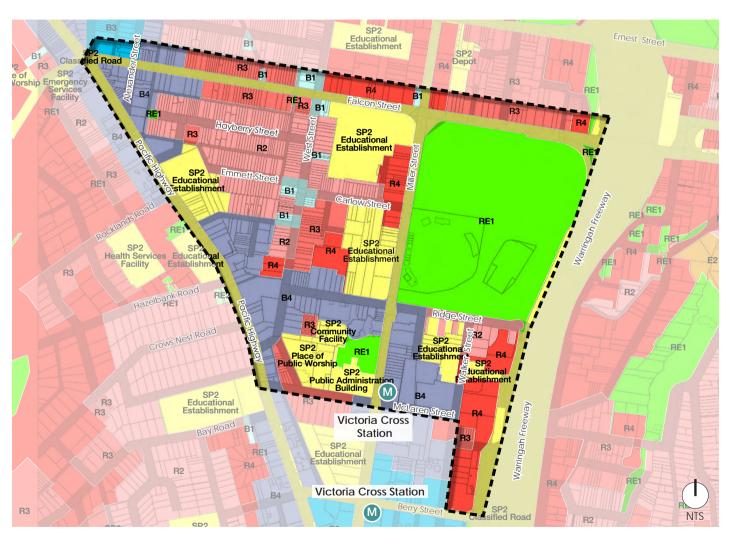
The LSPS structure plan highlights the relative importance placed on the North Sydney CBD and the St Leonards / Crows Nest precinct in comparison to other metropolitan, strategic, and local centres both within and in close proximity to the LGA (Figure 08).

The LSPS shows Council's desired future directions for housing, employment, transport, recreation, environment and infrastructure across the LGA; guiding Council's review of the LEP and Development Control Plan (DCP).

Building on the themes of Infrastructure and Collaboration, Liveability, Productivity and Sustainability set out in the North District Plan, the LSPS also identifies a series of Planning Priorities and actions for the LGA. The Planning Priorities that are relevant to the study area include:

- I1 'Provide infrastructure and assets that support growth and change' and I2 'Collaborate with State Government Agencies and the community to deliver new housing, jobs, infrastructure and great places'
- L1 'Diverse housing options that meet the needs of the North Sydney community' and L2 'Provide a range of community facilities and services to support a healthy, creative, diverse and socially connected North Sydney community.'
- L3 'Create great places that recognise and preserve North Sydney's distinct local character and heritage' and P4 'Develop a smart, innovative and prosperous North Sydney economy'
- P6 'Support walkable centres and a connected, vibrant and sustainable North Sydney'
- S2 'Delivering a high quality, well-connected and integrated greenspace system'; S3 'Reduce greenhouse gas emissions, energy, water and waste' and S4 'Increase North Sydney's resilience against natural and urban hazards'

Figure 08: North Sydney Structure Plan (Courtesy of North Sydney Council) CONYBEARE MORRISON



2.2 CURRENT PLANNING CONTROLS - ZONING

The key zoning provisions under North Sydney Local Environmental Plan 2013 (NSLEP 2013) which apply to the study area are illustrated in Figure 09.

The zoning mix includes large green spaces (St Leonards Park, Ted Mack Civic Park), several Educational Establishments (SP2), Mixed Used Areas (B4), and residential zoning from Low Density (R2) to High Density (R4).

Key

--- The Study Area

B1 Neighbourhood Centre

B3 Commercial Core

B4 Mixed Use

Environmental Conservation

R2 Low Density Residential

R3 Medium Density Residential

R4 High Density Residential

RE1 Public Recreation

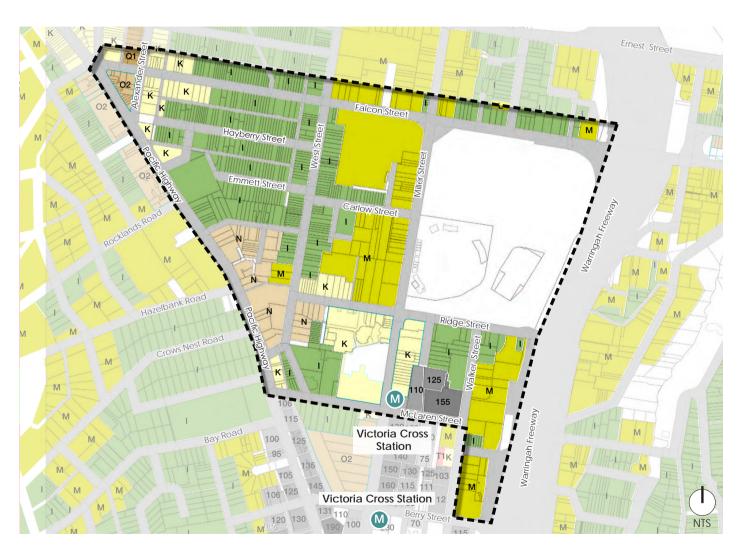
RE2 Private Recreation

SP2 Infrastructure

Figure 09: Land zoning map

10 | CIVIC PRECINCT AND SURROUNDS PLANNING STUDY

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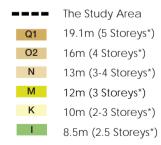


2.2 CURRENT PLANNING CONTROLS - HEIGHT OF BUILDING

The height of buildings within the study area generally range from 8.5m (2.5 Storeys*) to 13m (3-4 storeys*). Notable exceptions include a 16m and 19.1m (5 storeys*) height zone at the prominent 'Five Ways' intersection; on the corner of Falcon Street and Pacific Highway.

A further exception is the group of RL100m plus buildings on McLaren Street, at the north entrance to Victoria Cross Metro Station (currently under construction).

Key



^{*} Please note that the number of storeys listed is based on 3.1m residential floor to floor height and 4.1m non-residential floor to floor height.

Figure 10: Height of building map CONYBEARE MORRISON

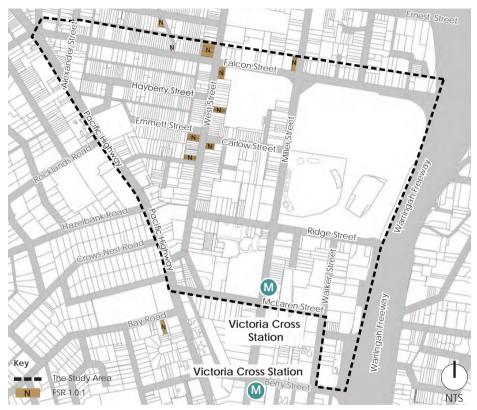


Figure 11: Floor Space Ratio

Falcon Street Hayberry Street Age Hazelbank Road Crows Nest Road Say Ro

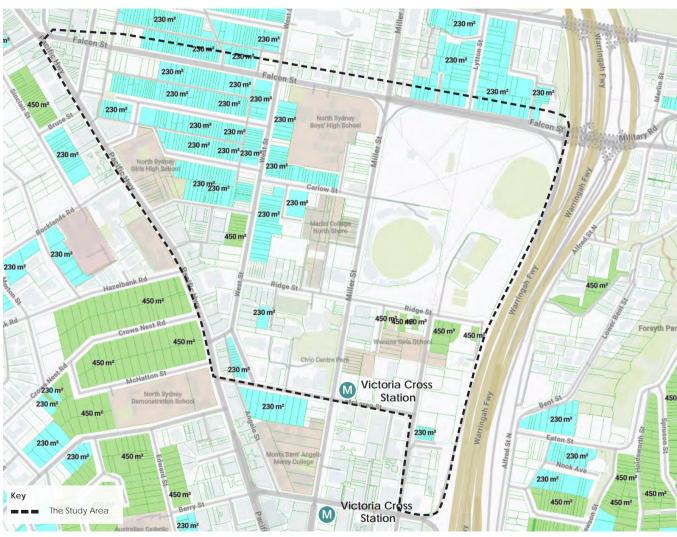
Figure 12: Floor Space Ratio - Non-Residential (Courtesy of Mecone)

2.2 CURRENT PLANNING CONTROLS - FLOOR SPACE RATIO

Figure 11 illustrate the current applicable Floor Space Ratio (FSR) controls.

Most properties within the study area do not have a general FSR control.

North Sydney also utilises a minimum nonresidential Floor Space Ratio. Outlined in Clause 4.4A of the LEP, the non-residential Floor Space Ratio control is unique to the North Sydney LGA. The purpose of the control is to ensure a minimum amount of floor space for community, employment or retail uses within the B1 Neighbourhood Centre, B4 Mixed Use, and SP2 Infrastructure zones.



2.2 CURRENT PLANNING CONTROLS - MINIMUM LOT SIZE

The minimum lot size controls ensure that subdivisions and associated developments promote the desired future character of the neighbourhood through consistent lot size, shape, orientation and housing density.

Minimum lot sizes within the investigation area are set at 230m² within R2 and R3 zones in Heritage Conservation Areas and at 450m² for a number of key large sites on Ridge and West Streets. Figure 13 indicates the existing minimum lot size control in the North Sydney LEP.

Figure 13: Minimum Lot Size (Courtesy of Mecone)



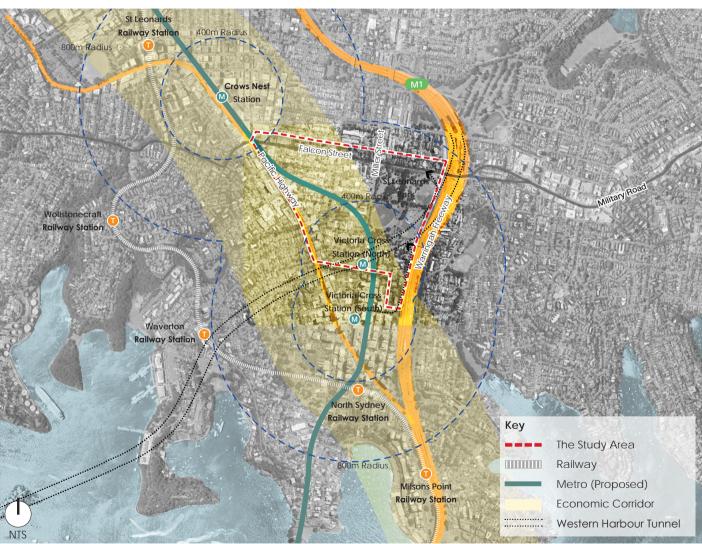


Figure 14: The study area and Sydney Harbour and North Sydney context CONYBEARE MORRISON

3.1 LOCAL CONTEXT

The study area is at the northern fringe of the Eastern Harbour City, identified in A Metropolis of Three Cities and is located immediately to the north of the North Sydney CBD.

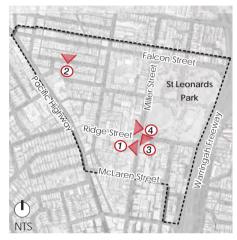
The Eastern Economic Corridor, stretches from Macquarie Park to the Sydney CBD, and the study area is situated in this corridor.

The study area is well connected to surrounding suburbs, parks, recreational facilities and waterfronts and is well served by major vehicular thoroughfares, local roads and public transport.

There is a mixture of built form and urban character within the study area. The study area is mostly comprised of low scale buildings from one to four storeys in height. This is in scale with the predominant tree canopy. Apart from the major thoroughfares at the edges of the precinct, generally speaking most streetscapes are tree-lined and contribute to a village character. Exceptions include a few taller tower buildings at the fringe of the North Sydney CBD and buildings proposed at the Victoria Cross Metro Station North Portal, which is currently under construction.

St Leonards Park is a major recreational space servicing local residents and also a broader catchment.

Key Map



Site Photos



Figure 15: View 1 - Traditional theatre and shop front within the study area



Figure 17: View 3 - A multi storey residential flat building in the study area

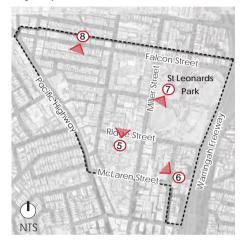


Figure 16: View 2 - A single storey dual occupancy dwelling along a leafy stree



Figure 18: View 4 - A place of worship in the study area

Key Map



Site Photos



Figure 19: View 5 - A renovated commercial building



Figure 21: View 7 - St Leonards Park (Source: Google Maps)



Figure 20: View 6 - A hotel tower building in the study area



Figure 22: View 8 - Falcon Street houses (Source: Google Maps)

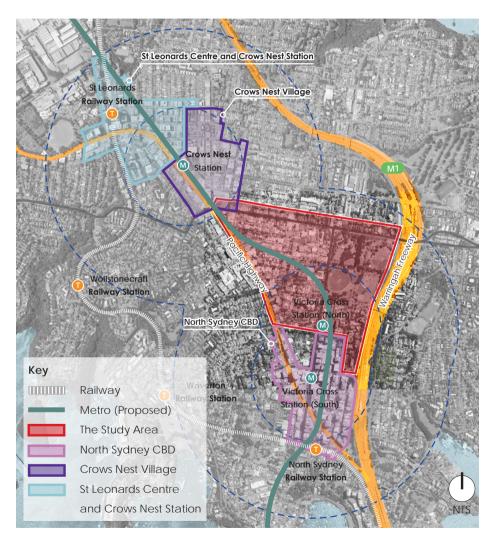


Figure 23: The study area - view southeast

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Figure 24: The study area - view northwest CONYBEARE MORRISON



3.2 THE STUDY AREA AND THE SURROUNDING CENTRES

The study area is in close proximity to three urban centres:

- St Leonards Centre and Crows Nest Station
- Crows Nest Village
- North Sydney CBD

St Leonards Centre and Crows Nest Station

is situated to the northwest of the study area, and extends from St Leonards Station to the future Crows Nest Metro Station. St Leonards Centre is in a process of urban transformation, particularly evident along the Pacific Highway where low to medium rise buildings are progressively being replaced by towers of up to 40 storeys. The height steps down to proposed 17 storey buildings in the Crows Nest Station area.

Crows Nest Village is immediately to the north of the study area, and is generally low in scale and has a pedestrian friendly environment. The future Crows Nest Metro Station is along the Pacific Highway and will provide the area with increased access to public transport. Relatively tall buildings (17-27 storey) are planned over the future Crows Nest Metro Station along the Pacific Highway (the plan is still in the proposal stage; no final decision has been made).

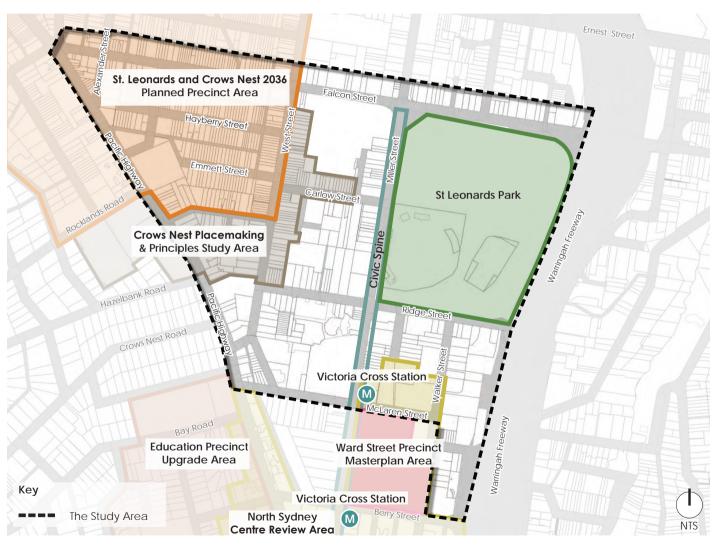
Willoughby Road forms a high street with one to two storey shop fronts defining the street. The village atmosphere is highly valued by the community. The southern end of Crows Nest Village extends into the study area (refer to Figure 25).

North Sydney CBD is to the south of the study area and currently provides the third largest office floor space in metropolitan Sydney. The CBD includes a mix of uses. It is a sought offer address for some of Australia's top companies, and accommodates a mix of uses including commercial, mixed use and residential, which co-exist. Towers of up to 40 storeys occur in the North Sydney CBD, and together with St Leonards Centre they form two height peaks along the Pacific Highway.

The study area is situated between Crows Nest Village and North Sydney CBD. The study area provides a transition in height and scale from the two urban centres to lower scale residential communities and Heritage Conservation Areas.

Figure 25: The study area and the surrounding centres 20 | CIVIC PRECINCT AND SURROUNDS PLANNING STUDY

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3.3 THE STUDY AREA AND THE SURROUNDING PRECINCTS

A number of state and local level studies coincide with the study area, including:

- St Leonards and Crows Nest 2036 Planned Precinct Area (Department of Planning, Industry and Environment)
- Crows Nest Placemaking and Principles Study Area (NSC)
- Ward Street Precinct Masterplan Area (NSC)
- North Sydney Centre Review Area (NSC)
- Education Precinct Upgrade Area (NSC)
- St Leonards Park Master Plan Area (NSC)
- Civic Spine (NSC)

The St Leonards & Crows Nest 2036
Planned Precinct Area is situated to
the northwest of the study area. To the
south, the study area adjoins the Ward
Street Precinct, the North Sydney Center
Review Area and the Education Precinct
Upgrade Area. The Miller Street Civic Spine
connects the study area to the North
Sydney CBD.

Figure 26: The study area and the surrounding precincts CONYBEARE MORRISON

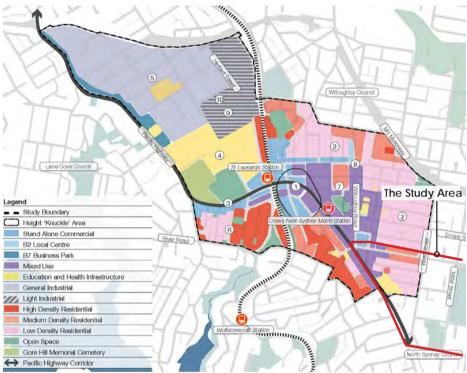


Figure 27: St Leonards and Crows Nest 2036 Framework Plan (Courtesy of SJB)



Figure 28: St Leonards and Crows Nest 2036 potential built form massing (Courtesy of SJB)

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St Leonards and Crows Nest 2036 Planned Precinct Area

The vision of the Precinct Plan prepared by the DPIE, is for St Leonards and Crows Nest to develop as major centres for employment and residential accommodation, supported by high quality, open spaces and infrastructure.

The Precinct Plan envisages the area to be an attractive place to walk and cycle. A green leafy place with well designed buildings and places. The desired future character objectives for Crows Nest Village that are relevant to the study area:

- Retain the existing fine grain character of the Village/Willoughby Road area.
- Sensitively transition development near Willoughby Road to ensure appropriate solar access and visual character is maintained.
- Support the continuous growth of active shop fronts and quality public domain.

The Framework Plan (Figure 27) sets out a spacial structure for the future growth of the area. It encourages high-density mixed-use development between St Leonards Station and the future Crows Nest Metro Station. This built form aims to transition to adjacent lower density areas (Figure 28). Heritage Conservation Areas are preserved to enhance the local character.

Implications for the study: The importance of preserving and enhancing the local character is emphasised by the plan. It establishes the future built form for the two urban centres that will guide future development along the Pacific Highway.

This Planning Study will preserve and enhance the heritage items and Heritage Conservation Areas to strengthen the cultural heritage of the community. The Precinct Plan specifically identifies the importance of maintaining the heritage look and feel of the five way intersection, which is located at the northwest tip of the study area.

This Planning Study presents an opportunity to develop the Precinct Plan's vision for better connected, activated and high amenity places to live, work and visit.

This Planning Study can dovetail with the Precinct Plan to provide a coordinated and consistent approach to the Urban Design of key streetscapes, including the Pacific Highway and Falcon Street.

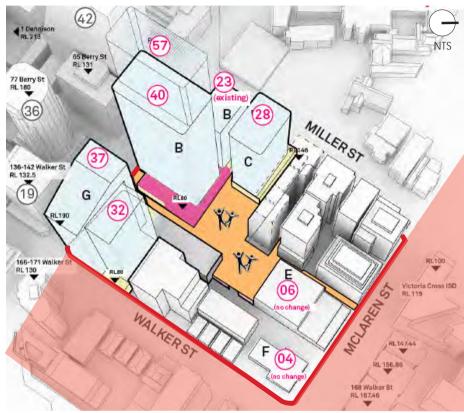


Figure 29: Ward Street Precinct masterplan massing (Adapted from NSC)

Ward Street Precinct boundary Retail The Study Area Open space Community use Number of storeys Commercial use

Ward Street Precinct Masterplan

The Ward Steet Precinct is situated to the south of McLaren Street and adjoins the study area. It has an area of about 3.2 ha. A masterplan has been approved and endorsed by North Sydney Council to guide future development. The key planning principles are to:

- Integrate with the Metro
- Complete the pedestrian core
- Enhance the public space journey
- Establish a hub for public life
- Strengthen the commercial centre

The masterplan provides a 2,080 m² mid block Central Square which links McLaren Street to Berry Street and has several direct connections to Miller Street (Figure 29).

A maximum building height of 40 storeys with a potential to achieve further height subject to design and public benefits is proposed near the future Victoria Cross Metro Station with a transition in height to McLaren Street to mitigate the potential overshadowing of the proposed Central Square.

The Miller Street footpath will be upgraded including the street setback. The Pedestrian Right of Way (ROW) linking McLaren Street to the Central Square also adds to the Ward Street Precinct's permeability.

Implications for the study: Future development along the north side of McLaren Street, within the study area, should not result in any additional overshadowing of public open space in Ward St Precinct.

The Planning Study area presents an opportunity to continue the Miller Street footpath improvements north; connecting the North Sydney CBD to St Leonards Park.

The north-south mid-block connections through the Ward Street Precinct should be continued north as part of the Victoria Cross Metro Station redevelopment, linking through to Elliot Street and St Leonards Park.

CONYBEARE MORRISON



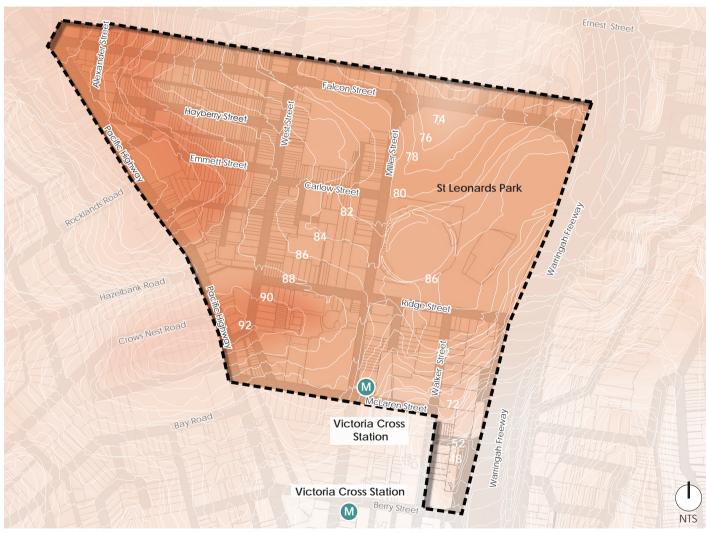


Figure 30: Topography

CONYBEARE MORRISON

4.1 INTRODUCTION

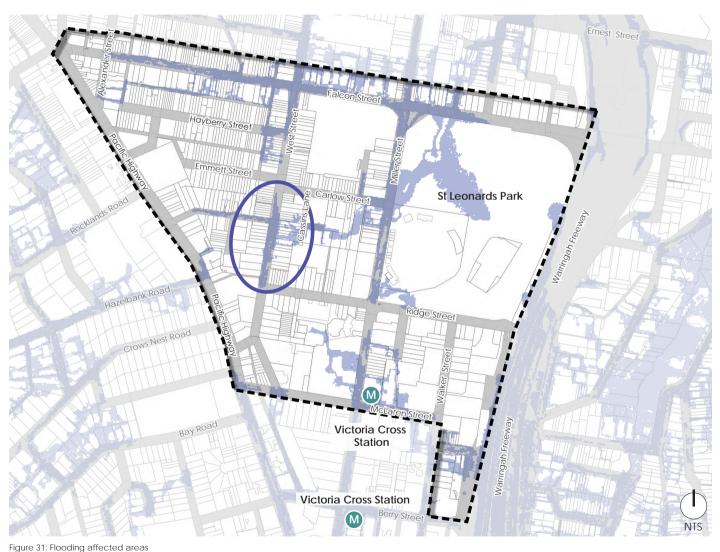
Informed by several site visits and a review of background information, the Urban Design Analysis assesses the existing conditions of the study area, identifies constraints and opportunities; and thereby informs the future visioning and the preparation of a set of Urban Design Principles for the study area. This will assist Council respond to new development proposals in the study area.

4.2 TOPOGRAPHY

The Warringah Freeway to the east is generally set down below the study area. The topography varies from high points along the Pacific Highway in the west (RL 92), to a low point near the Warringah Freeway in the southeast (RL 48).

Key

The Study Area



4.3 FLOODING

Flooding affects a number of properties within the study area. The 1% Annual Exceedance Probability(AEP) map, refer to Figure 31, indicates the predicted 1 in 100 year occurrence of a flood event.

West Street and Cassins Lane are identified as an area of flood concern in Council's North Sydney LGA Flood Study. It is stated in the report that the current drainage at this location is insufficient, resulting in overland flow and pooled water around properties. Future development on or adjacent to the flood prone lands should be carefully considered to avoid any flood related issues.

The other areas identified in Figure 31 as subject to a 1 in 100 flood event are not considered to be as high risk, however site specific flood studies should be considered when assessing development applications.

Key

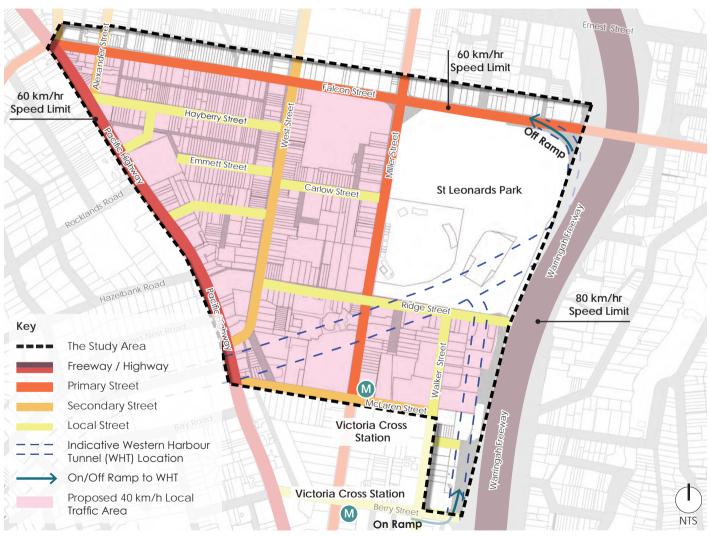
The Study Area

1% AEP Area

Area of Flood Concern

on Hooding andoted areas

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4.4 CONNECTIVITY

Vehicular Hierarchy

The study area is well connected to adjacent strategic and local centres via the road network. The road hierarchy is illustrated in Figure 32 and includes Freeway/Highways, Primary Streets, Secondary Streets, Local Streets and laneways.

Two major roads - Warringah Freeway and the Pacific Highway, connect the study area to other centres and destinations in Metropolitan Sydney. Falcon Street is the main east-west connector serving the study area. Miller Street provides north-south access and is North Sydney's 'Civic Spine', linking the study area to the North Sydney CBD, North Sydney Station, Sydney Harbour and to other residential neighbourhoods.

The proposed Western Harbour Tunnel will require on and off ramps within the study area. This proposal is currently on exhibition. A brief summary of its impact on the study area can be found overleaf.

Figure 32: Road hierarchy

Western Harbour Tunnel and Beaches Link (WHTBL)

The Western Harbour Tunnel aims to deliver a new crossing of Sydney Harbour to take pressure off the Sydney Harbour Bridge and Tunnel, and creates a western bypass of the Sydney CBD. The project also includes upgrading four kilometres of the Warringah Freeway.

In the vicinity of the study area, an onramp is proposed from Berry Street to connect to the southbound carriageways. An off-ramp is proposed to connect to Falcon Street westbound, as shown in Figure 32. Work on the Warringah Freeway will include a new, uninterrupted bus lane from Cammeray to the Sydney Harbour Bridge, which will feature an access link to North Sydney and allow express buses from the Northern Beaches and North Shore to have direct access to Sydney Trains at North Sydney Station and the new Sydney Metro at Victoria Cross. The project will also create the potential to introduce direct express bus access between the Inner West and Lower North Shore

New cycleway infrastructure and improvements will contribute to the North Shore Link Cycleway. The Ridge Street Bridge will be rebuilt to provide more room for cyclists and pedestrians. A dedicated cycleway between Miller Street, Cammeray Park and Falcon Street will be implemented, and the Ernest Street Bridge across the Warringah Freeway will be upgraded to link Cammeray Golf Course with Anzac Park, better connecting local communities and schools.

Implications for the Civic Precinct and Surrounds Planning Study

Current condition: Berry Street and Falcon Street provide on-ramp and offramp access for Warringah Freeway. respectively, and play an important role for vehicular access in the Study Area. Berry Street is the entry point for both northbound and southbound vehicles from the Warringah Freeway. The Freeway forms the eastern part of the orbital motorway system. The Falcon Street exit is the second exit after entering the North Shore, following the exit at North Sydney / Pacific Highway and mainly services Crow Nest. Based on desktop review and typical traffic conditions provided by Google Maps, road sections in the vicinity of the existing ramps show slow speed performances during peak hours. This is probably due to the heavy traffic volume associated with vehicles traveling to / from the North Sydney CBD, and Crows Nest, and the long waiting times at signalised intersections for pedestrians and opposing vehicles.

Condition with the opening of WHTBL: The travel time to North Sydney would be reduced by about 10 minutes, especially from Sydney's Inner West. The proposed on / off-ramps of the Western Harbour Tunnel at Berry Street and Falcon Street could attract more vehicular traffic in and through the study area. This may increase conflicts between walking, cycling and car traffic and result in a less walkable environment for the precinct, in particular along Miller Street. The impacts on Miller Street could be minimised if Pacific Highway becomes the main feeder route to the WHTBL on-ramp via Berry Street.

Benefits of the new tunnel and the associated freeway upgrade also include improved cycleways and potential new bus connections, which further encourage sustainable transport use to / from the precinct. Further quantitative assessment is required to estimate the additional car trips on the road network due to WHTBL and assess intersection performance and the impact on pedestrian / cyclist access.



Western Harbour Tunnel and the study area (Courtesy of SCT)

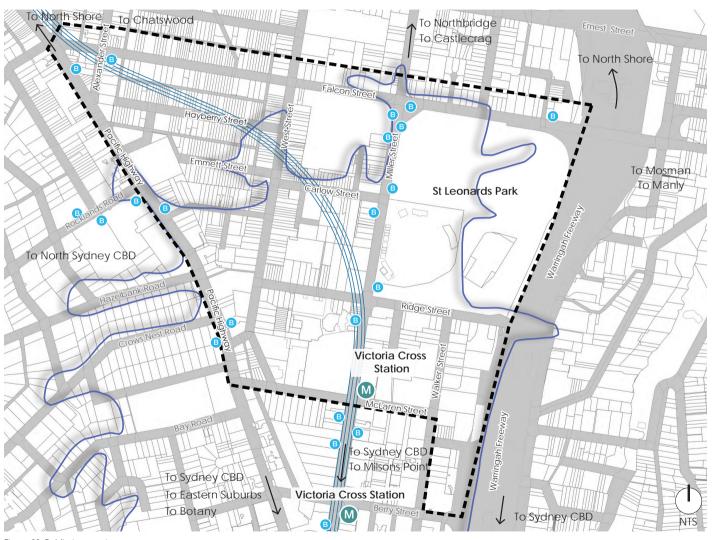


Figure 33: Public transport

CONYBEARE MORRISON

Public Transport

North Sydney is generally well-served by public transport and the service will be further enhanced once the Metro is completed. North Sydney Station is approximately 800m (approx. 10 min walk) to the south of the study area, providing frequent services to Metropolitan Sydney and other cities in NSW. Several bus services also provide easy access to the surrounding suburbs and neighbourhood.

The future Victoria Cross and Crows Nest Metro Stations will further improve the study area's accessibility, as most of the study area is within 800m radius of the future stations.

The Victoria Cross Station North Portal is expected to cater for 10% of the total Victoria Cross passenger volumes, with the Berry Street - South Portal accommodating the remaining 90%. The North Portal is accessed by a lift only, as the station platform is located deep below ground level.

Key

The Study Area

Metro Tunnel

Future Metro Station

Bus Stop

800m Walking Catchment from Victoria Cross Metro Station

CIVIC PRECINCT AND SURROUNDS PLANNING STUDY | 29

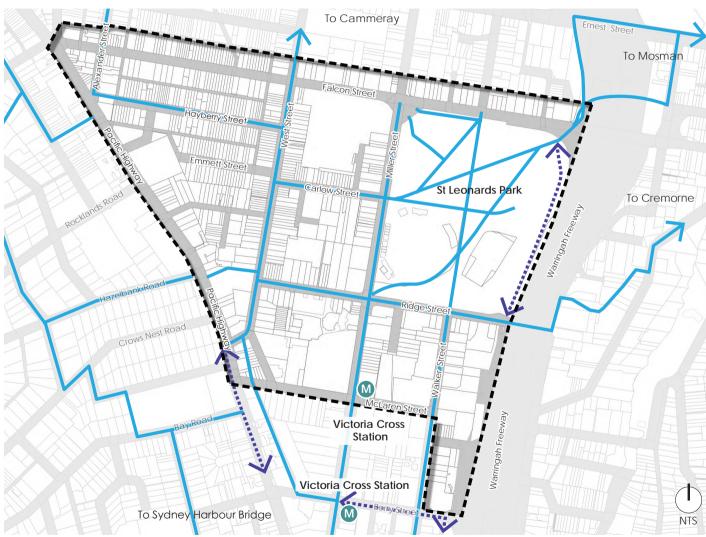


Figure 34: Cycleway networks

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The existing cycle network in the study area, generally uses on-road facilities. Off-road cycling routes are provided within St Leonards Park; however, these routes are subject to changes as a result of the proposed WHTBL project. This may include the removal of the commuter cycleways within St Leonards Park, according to the current exhibited WHTBL documents (dated January 2020). A designated bicycle lane is provided on Ridge Street; between Miller Street and West Street.

The North Sydney Integrated Cycling Strategy and North Sydney Public Spaces Vision, both promote active transport within the North Sydney LGA. Proposed improvements to the existing cycling network and additional cycle routes are identified in the Strategy, and are illustrated in Figure 34.

North-south cycleways along West Street, Miller Street, and St Leonards Park - Walker Street are planned to be upgraded (subject to the final plans the WHTBL project). As will east-west cycleway connections over the Warringah Freeway via Ridge Street. New cycleways are proposed along the Pacific Highway to connect the study area to the North Sydney CBD and to the Harbour Bridge.

Key

The Study Area

Existing Cycle Route

Existing Cycle Route

Future Cycle Route

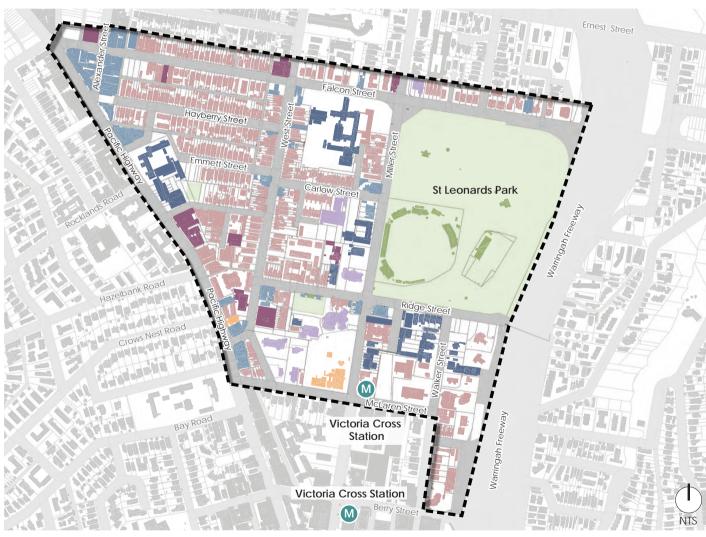


Figure 35: Existing land use

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4.6 LAND USE

There is a broad range of existing land uses within the study area.

Residential uses are spread throughout the study area, especially within the Holtermann Estate Heritage Conservation Area and along Walker Street.

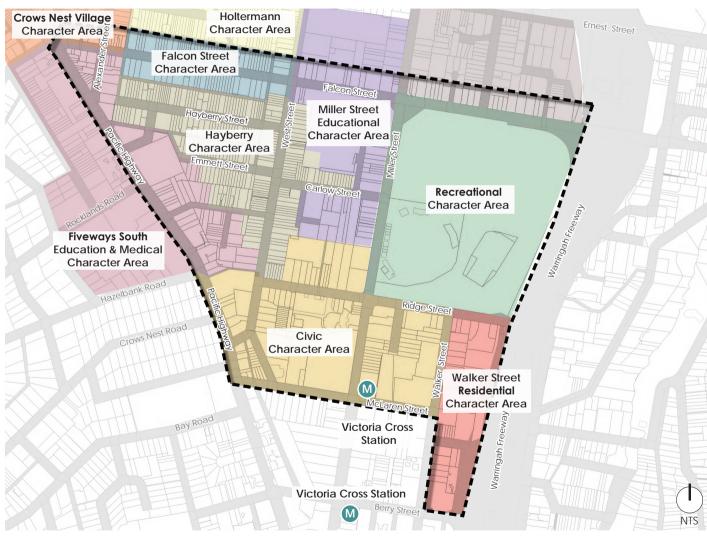
Mixed use and retail / commercial areas are mainly concentrated along Miller Street, Ridge Street, West Street and the Pacific Highway.

Civic buildings and community facilities are mostly clustered around the North Sydney Council Chambers, and within St Leonards Park.

The key public open spaces and recreational facilities within the study area are St Leonards Park, Ted Mack Civic Park and the Ridge Street tennis courts.

There are several private and public schools and places of worship distributed across the study area.





4.7 CHARACTER AREAS

The study area is characterised by a diverse mix of lot patterns, land uses, built forms, urban streetscapes and parks.

The Crows Nest Placemaking and Principles Study identified a series of character areas that overlap the study area, including Crows Nest Village, Falcon Street, Hayberry and Fiveways South Education and Medical Character Areas.

Along with these four character areas, a further four have been identified within the study area:

- Miller Street Educational Character Area
- Civic Character Area
- Recreational Character Area
- Walker Street Residential Character Area

The essential characteristics of each character area are summarised on the following pages.

Key

The Study Area

Figure 36: Character zone

MILLER STREET EDUCATIONAL CHARACTER AREA



CIVIC CHARACTER AREA



RECREATIONAL CHARACTER AREA



WALKER STREET RESIDENTIAL CHARACTER AREA





Public and private schools are the primary land use of this precinct. Low and medium density residential uses are also a characteristic of the precinct. Retail or commercial uses are present along West Street and Carlow Street.



This precinct is characterised by civic facilities, including North Sydney Council Chambers, Ted Mack Civic Park, Stanton Library and the Community Centre. The area is characterised by a mix of uses, that may potentially broaden with the opening of Victoria Cross Metro Station. Generally, the area is low in scale, with some taller buildings along McLaren Street rising to 22 storeys.



This precinct consists of the St Leonards Park green spaces and associated recreational facilities. The precinct accommodates formal and informal sports and recreational activities, including open spaces, North Sydney Oval, and a children's playground. Large mature trees provide important shade and cooling benefits to the precinct.



The precinct mostly accommodates residential uses. A mix of building typologies exist in the area, ranging from low scale houses and terraces to residential flat buildings (up to 21 storeys).

CROWS NEST VILLAGE CHARACTER AREA



FALCON STREET CHARACTER AREA



HAYBERRY CHARACTER AREA



FIVEWAYS SOUTH EDUCATION & MEDICAL CHARACTER AREA





The precinct contains the Crows Nest village centre. It is a vibrant precinct with traditional shop fronts and a recently constructed shopping centre with an upper level carpark.



This section of Falcon Street is tree lined and has low scale houses fronting the street. A mix of new and old buildings co-exist.

Many houses or terraces employ front garden walls or landscaping to mitigate the impact of significant traffic volumes along Falcon Street.



The precinct is characterised by Heritage Conservation Areas and by its low scale residential buildings, including attached / detached houses and terraces. The area has attractive tree-lined streets, fine-grain urban development and small scale buildings.



This precinct contains a mix of low scale residential buildings and taller mixed-use towers. Educational and health facilities in this precinct spread along both sides of the Pacific Highway.



Figure 37: Heritage Items and Heritage Conservation Areas

CONYBEARE MORRISON

4.8 HERITAGE ITEMS AND HERITAGE CONSERVATION AREAS (HCA)

The character of the study area benefits from many Local and State Heritage Listed Items and four Heritage Conservation Areas (HCA). Protecting and conserving the significant cultural built heritage of North Sydney is critical to preserving the local identity and unique character of the study area. Figure 37 illustrates the heritage listed items and HCAs within the study area.

The HCAs within the study area are:

- CA09: Holtermann Estate C Conservation Area
- CA18: Holtermann Estate D Conservation Area
- CA19: McLaren Street Conservation Area
- CA20: Walker and Ridge Streets Conservation Area
- St Leonards Park

Key

The Study Area

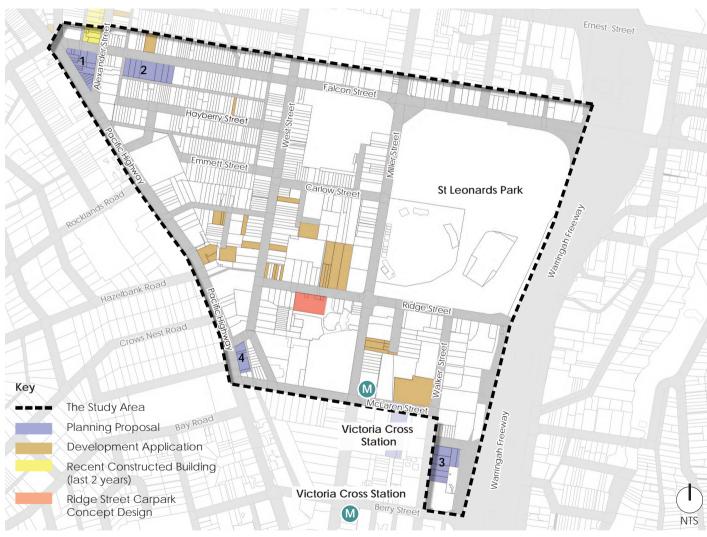
Heritage Item - General

Heritage Item - Landscape

Local Heritage Conservation Area

State Heritage Conservation Area

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4.9 RECENT DEVELOPMENT, CURRENT / APPROVED DEVELOPMENT APPLICATIONS AND PLANNING PROPOSALS

The study has identified sites within the study area that are subject to Planning Proposals, Approved Development Applications (DA) and Ridge Street Carpark Concept Design. Recent development completed in the last two years have also been mapped as these sites are unlikely to be redeveloped in the short-medium term.

The Planning Proposal sites are:

- 1. No. 423 Pacific Highway, Crows Nest (Application Withdrawn)
- 2. No. 27-57 Falcon Street, Crows Nest
- 3. No. 179 Walker Street, North Sydney
- 4. No. 257-267 Pacific Highway, North Sydney

The above Planning Proposals have not been determined and will be informed by the outcomes of this study.

All the DAs indicated on Figure 38 have been approved by the relevant planning authority. The Ridge Street public car park and tennis courts site is being reviewed by North Sydney Council with a view to incorporating further community facilities.

Figure 38: Recent development, DAs and Planning Proposals



Figure 39: Land ownership pattern

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4.10 LAND OWNERSHIP PATTERN

The current property ownership within the study area has been examined to identify public and private held land, and those properties that are constrained by strata title ownership. Three categories are identified:

- Public owned land; including Council and State Government properties
- Schools, hospitals and places of public worship
- Strata titled properties (residential and commercial uses)

Public owned land, such as parks and community facilities, are unlikely to change to other types of use overtime; whereas, private owned properties, which are not constrained by strata title, could potentially be redeveloped in the future.

Schools, hospitals and places of public worship are expected to retain their current use in the foreseeable future.

Strata title properties with six or more owners (particularly those with more than 12 owners), are unlikely to be redeveloped in the near future.

Key

--- The Study Area

Schools + University

Hospital

Place of Public Worship

Council Owned

State Government Owned

Strata Titled Property (12+ owners)

Strata Titled Property (6-12 owners)

CIVIC PRECINCT AND SURROUNDS PLANNING STUDY | 37



Figure 40: Constraints map

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4.11 CONSTRAINTS MAPPING

A Constraints Map has been prepared to understand the potential layers of constraints which apply to properties within the study area.

The following constraints information layers have been combined to identify land that is unlikely to redevelop:

- Heritage items and HCAs
- Recent developments, Council Facilities, and approved DAs
- Parks and open spaces
- Community / civic facilities
- Educational facilities
- Places of public worship
- Council and State Government owned land
- Strata titled properties

The sites which have less constraints, are in light colours. The more constrained sites are in darker colours.

Whilst educational facilities, Council and State owned properties are considered constrained in this mapping exercise, it is accepted that there is the potential for these sites to redevelop in the future to provide upgraded services to the community.

Key

The Study Area



The less constrained sites are illustrated in Figure 41, including the Planning Proposal sites which are currently being assessed by North Sydney Council.

The study considers strata titled properties with 6 to 12 owners as moderately constrained sites. There are examples in the North Sydney LGA of strata titled properties with 12 or more owners that have been successfully amalgamated and redeveloped. The properties with 6-12 owners are identified in light orange colour (refer to Figure 41).

Strata properties with greater than 12 owners are considered to be unlikely to redevelop, and with less than 6 owners to be likely to redevelop.

Key

The Study Area

Moderately Constrained Site (6-12 owners)

Less Constrained Site (strata not considered as a constraint)

Figure 41: Less constrained sties

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4.12 STUDY AREA CONSTRAINTS

The Urban Design Analysis has identified the following constraints to be addressed in the future planning of the study area:

- Sensitive interfaces with schools, open
 spaces, heritage items and HCAs, need to be considered when new development is proposed.
- 7)) The noise impacts of the Warringah Freeway, Pacific Highway and Falcon Street, reduces the acoustic amenity of adjoining properties and the public domain.
- The Warringah Freeway is a major barrier for pedestrians and cyclists travelling to the east.
- There are few dedicated cycleways within the study area discouraging cycling and limiting access to adjoining areas.
- Flooding issues could potentially impact the design of buildings and places along Cassins Lane and West Street.
- Pedestrian connections to adjacent precincts are not ideal. There is often poor pedestrian amenity along major road corridors.
- → The proposed on / off-ramps of the Western Harbour Tunnel at Berry Street and Falcon Street could attract more motorised traffic in and through the study area. This may increase conflicts between walking, cycling and vehicular traffic and result in a less walkable environment for the precinct, in particular along Miller Street.

- Impact of the future metro on the pedestrian volume around the station and the current footpath along Miller Street and McL aren Street.
- Potential change of traffic volume due to the proposed partial closure of Miller Street and the Western Harbour Tunnel project.

Please also refer to Figure 42.

4.13 STUDY AREA OPPORTUNITIES

The study area presents the following opportunities:

- To realise high quality buildings and complimentary social and green places to address future growth.
- To improve the connections within the study area, and to the area, east of the Warringah Freeway.
- To improve the footpath condition and capacity along Miller Street and McLaren Street to support the new demand generated by the opening of Victoria Cross Metro Station.
- To provide additional pedestrian links and the overall pedestrian amenity of road corridors.
- To improve pedestrian access to adjacent urban centres, residential neighbourhoods and to the Education and Health Precinct to the west of the Pacific Highway.
- → To strengthen Miller Street as the primary Civic Spine; linking the study area to the North Sydney CBD and Sydney Harbour.
- To upgrade existing cycleways and to construct new links.
- To implement the potential cycleway upgrades proposed by the WHTBL project.

- To mitigate the acoustic impacts of the peripheral major road corridors through building design and landscaping.
- Explore the opportunities to provide additional public domain and open space.

Please also refer to Figure 43.

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CONSTRAINTS St Leonards Park Hazelbank Road Crows Nest Road Victoria Cross Station Victoria Cross Station



Figure 42: Constraints

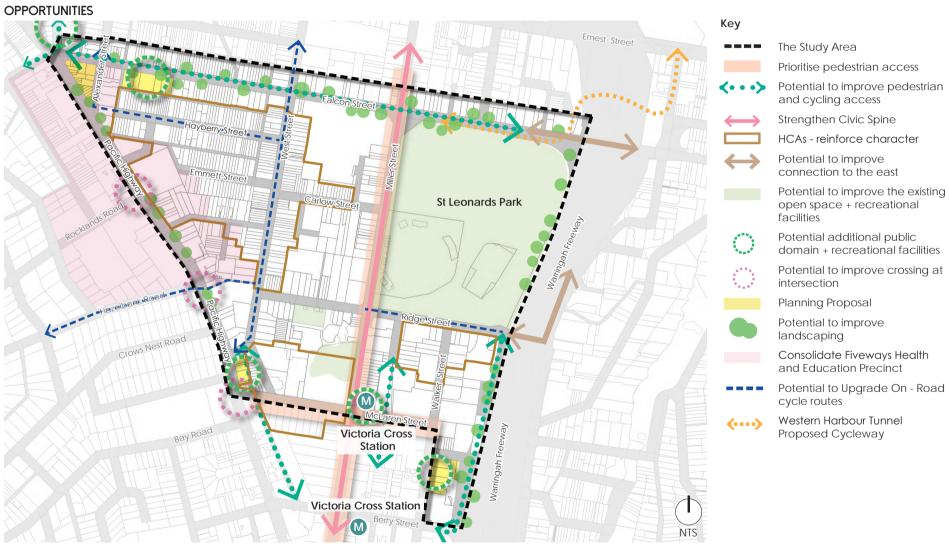


Figure 43: Opportunities

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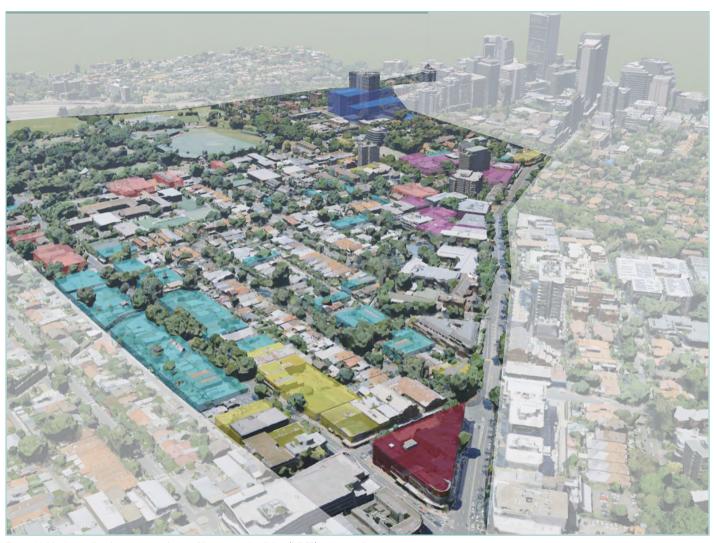


Figure 44: Maximum development envelopes of less constrained sites (NSLEP) - view southeast

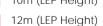
CONYBEARE MORRISON

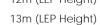
5.1 LESS CONSTRAINED SITES - CURRENT DEVELOPMENT CONTROLS

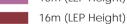
A study was prepared to understand the maximum building envelopes permitted under the current planning legislation, for the identified Less Constrained Sites. Figures 44, 45 and 46 illustrate the maximum height planes under the current LEP legislation, however does not apply DCP requirements or represent an architectural design.













CIVIC PRECINCT AND SURROUNDS PLANNING STUDY | 43



Figure 45: Maximum development envelopes of Less Constrained Sites (NSLEP) - view northeast



Figure 46: Maximum development envelopes of Less Constrained Sites (NSLEP) - view northwest

CONYBEARE MORRISON



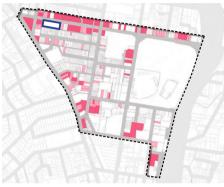


Figure 48: Key Plan

5.2 KEY SITE TESTING

This study tests individual (consolidated) sites to understand building envelopes that are permissible under the current planning controls (LEP and DCP).

5.2.1 27-57 FALCON STREET

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building 10m
 Floor Space Ratio (FSR) N/A
 Non-residential FSR 0.5:1

A two-storey built form is permitted under the current controls; with ground floor retail / commercial uses and residential above.

Key



The Study Area

Site boundary

Existing buildings (retained) Compliant building envelope

Existing buildings

Figure 47: Site Plan

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27-57 Falco	on Street								
		Residential				Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	-	-	0 m ²	2,460 m ²	85%	2,091 m ²		
	2	685 m²	75%	514 m²	1,230 m²	85%	1,046 m ²		
4,323 m²	Sub- total	685 m²	75%	514 m²	3,690 m²	85%	3,137 m²	3,650 m ²	0.8:1



Figure 49: Existing Development



Figure 50: Current Permitted Development

Note: Maximum building envelope is 2 storeys; however as the site slopes down to the south the building envelope is illustrated as stepped.

SWOT Analysis (27-57 Falcon Street) Strengths

- The site is consolidated
- The site is close to Crows Nest Village which provides good access to retail / commercial uses and facilities
- Falcon Street provides easy access to the surrounding areas

Weaknesses

- The acoustic amenity of the site is affected by heavy traffic flow along Falcon Street
- The redevelopment potential of the site is limited by the current height control
- The site has a sensitive frontage to the Heritage Conservation Area

Opportunities

- The site presents an opportunity to increase the area's permeability by providing through site link(s)
- The opportunity to improve the pedestrian amenity and streetscape along Falcon Street
- The opportunity to present a sensitive built form transition from Falcon Street to the heritage conservation area
- The opportunity to provide additional retail / commercial spaces to the locality

Threats

 The potential additional traffic along Falcon Street will affect the livability and pedestrian amenity



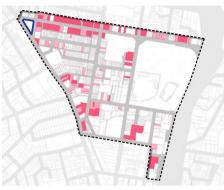


Figure 52: Key Plan

5.2.2 ALEXANDER STREET SITE

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building 16m
- Floor Space Ratio (FSR) N/A
- Non-residential FSR 0.5:1

A four-storey building containing two retail / commercial floors, providing small to large scale retail / commercial floor spaces, and two residential floors is currently permitted on the site. The two-storey podium matches the current street wall height. Two separate buildings are proposed above the podium to break down the overall built form scale.

The Ground Floor could potentially accommodate a through-site arcade linking Falcon Street, Pacific Highway and Alexander Street. It would also likely provide pedestrian / vehicular access, loading, residential foyers and service facilities.

Key

The Study Area
Site boundary

Compliant building envelope

Existing buildings

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			Residential			Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	-	-	-	3,196 m²	85%	2,717 m²		
	2	-	-	-	3,196 m²	85%	2,717 m²		
	3	2,022 m²	75%	1,517 m²	-	-	-		
	4	2,022 m²	75%	1,517 m²	-	-	-		
3,200 m²	Sub- total	4,044 m²	75%	3,033 m²	6,392 m²	85%	5,433 m²	8,466 m²	2.6 :1



Figure 53: Existing Development



Figure 54: Current Permitted Development

SWOT Analysis (Alexander Street Site)

Strengths

- · The site is consolidated
- The site is next to Crows Nest Village which provides good access to retail / commercial uses and facilities
- The Pacific Highway and Falcon Street provide easy access to the surrounding areas
- The streets around the site provide buffers to the surrounding heritage items and Heritage Conservation Area

Weaknesses

- The acoustic amenity of the site is affected by heavy traffic flow along Pacific Highway and Falcon Street
- The redevelopment potential of the site is limited by the current height control
- The existing retail / commercial uses are run down

Opportunities

- The site has a significant potential to become a landmark for the precinct
- The site presents an opportunity to improve pedestrian amenity
- The opportunity to reinforce the established street wall height
- To define the Five Ways urban intersection with an appropriate built form

Threats

 The potential additional traffic along Pacific Highway and Falcon Street will affect the livability and pedestrian amenity





Figure 56: Key Plan

5.2.3 173-179 WALKER STREET

The current controls applicable to the site:

- Zoning R4 High Density Residential
- Height of Building 12m
- Floor Space Ratio (FSR) N/A
- Non-residential FSR N/A

The DCP suggests a maximum two storey built form to be provided along Hampden Street. The existing 2-3 storey buildings along Hampden Street have either reached or exceeded the nominated height in the DCP, therefore, these properties are likely to be retained.

Therefore the developable area under the current controls is along Walker Street. A three storey residential building can be accommodated under the current controls. Setbacks to the boundaries must comply with the Apartment Design Guide (ADG).

Key

The Study Area

Site boundary

Existing buildings (retained) Compliant building envelope

Existing building

Figure 55: Site Plan

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173-179 Wa	alker Stree	t							
			Residential			Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	883 m²	75%	662 m²	-	-	-		
	2	883 m²	75%	662 m²	-	-	-		
	3	776 m²	75%	582 m²	-	-	-		
3,949 m²	Sub- Total	2,542 m²	75%	1,907 m²	-	-	-	1,907 m²	0.5:1



Figure 57: Existing Development



Figure 58: Current Permitted Development

SWOT Analysis (173-179 Walker Street)

Strengths

- The site has potential to be consolidated
- The site is within walking distance to the North Sydney CBD and the future Victoria Cross North Portal
- The site enjoys view to Sydney Harbour

Weaknesses

- The retaining wall (heritage) on Walker Street limits the potential for pedestrians to cross the street
- The redevelopment potential of the site is limited by the current height control
- The air quality and acoustic amenity of the site is affected by Warringah Freeway

Opportunities

- The site presents an opportunity to improve Warringah Freeway edge condition
- The opportunity to improve Walker Street and Hampden Street streetscapes

Threats

 The redevelopment of the site will be affected by view sharing with the adjacent properties and potential overshadowing issue to Doris Fitton Park



5.2.4. PACIFIC HIGHWAY AND WEST STREET SITES

Site 1 - 253-267 Pacific Highway

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building 10m
- Heritage Item No. 265 Pacific Highway(Local Heritage)
- Floor Space Ratio (FSR) N/A
- Non-residential FSR 0.5:1

A two-storey building is permitted under the current controls (10m height); with retail / commercial on the ground floor (min. 4.1m floor to floor height) and residential uses above (min. 3.1m floor to floor height). The heritage item is retained.

Site 1 - 253	-267 Pacifi	ìc Highway							
			Residential			Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	-	-	-	1,290 m²	85%	1,096 m²		
	2	1,290 m²	75%	967 m²	-	-	-		
1,468 m²	Sub- total	1,290 m²	75%	967 m²	1,290 m²	85%	1,096 m²	2,063 m²	1.4 :1



Figure 61: Existing Development



Figure 62: Current Permitted Development

SWOT Analysis (253-267 Pacific Highway)

Strengths

- The site is consolidated
- The site is within walking distance to the North Sydney CBD and the future Victoria Cross Metro Station North Portal

Weaknesses

- The site contains a heritage item and is adjacent to heritage items and a Heritage Conservation Area
- The pedestrian environment is poor along the Pacific Highway and Church Lane
- The redevelopment potential of the site is limited by the current height control

Opportunities

- The site presents an opportunity to improve the Pacific Highway and Church Lane streetscape and laneway access
- The opportunity to improve the heritage item conditions and setting within the site

Threats

- The heritage item within the site may limit the developable area
- The potential widening of Church Lane will further affect the developable area of the site



Figure 63: Site Plan

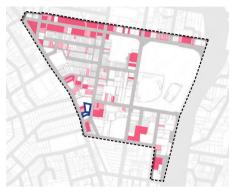


Figure 64: Key Plan

Key

The Study Area

Site boundary

Compliant building envelope

Existing buildings

Site 2 - 273 Pacific Highway

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building 13m
- Floor Space Ratio (FSR) N/A

A three-storey building is permitted under the current controls, with retail / commercial on the ground floor and residential uses above. The setbacks and building depths comply with the ADG requirements.

Site 3 - 8 West Street

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building 13m
- Floor Space Ratio (FSR) N/A

A three-storey building is permitted under the current controls, with retail / commercial on the ground floor and residential uses above. The setbacks and building depths comply with the ADG requirements.

			Residential			Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1				714 m²	85%	607 m²		
	2	541 m²	75%	406 m²	-	-	-		
	3	541 m²	75%	406 m²	-	-	-		
717 m²	Sub- total	1,082 m²	75%	812 m²	714 m²	85%	607 m²	1,418 m²	2.0:1



Figure 65: Existing Development



Figure 66: Current Permitted Development

SWOT Analysis (273 Pacific Highway)

Strengths

- The site is consolidated
- The Pacific Highway and West Street link the site to the surrounding areas and provide the site with access

Weaknesses

- The site has an irregular shape
- The site is adjacent to a heritage item
- The site is overshadowed by the existing 12 storey building at 8 West Street
- The redevelopment potential of the site is limited by the current height control

Opportunities

 The site presents an opportunity to improve its interface with West Street which currently is an access ramp

Threats

- The site's irregular shape will reduce the redevelopment efficiency
- The adjacent heritage item will further limit the developable area of the site

		Residential				Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	-	-	0 m²	1,814 m²	85%	1,542 m²		
	2	816 m²	75%	612 m²	-	-	-		
	3	816 m²	75%	612 m²	-	-	-		
1,814 m²	Sub- Total	1,632 m²	75%	1,224 m²	1,814 m²	85%	1,542 m²	2,766 m ²	1.5 :1



Figure 67: Existing Development



Figure 68: Current Permitted Development

SWOT Analysis (8 West Street)

Strengths

- A relatively large and well proportioned site
- The site is located on a relatively quiet street - West Street, which provides pedestrian and acoustic amenity

Weaknesses

 The redevelopment potential of the site is limited by the current height control

Opportunities

 The opportunity to redevelop the site and provide a more contextual fit built form, which responds to West Street streetscape

Threats

 The current building has already exceeded the allowable development potential, therefore it is likely the building will remain in its current form in short to medium term



Site 4 - 3-9 West Street

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building 13m
- Floor Space Ratio (FSR) N/A

A three-storey building is permitted under the current controls, with retail / commercial on the ground floor and residential uses above.

Site 5 - 2 Cunningham Street

The current controls applicable to the site:

- Zoning R3 Medium Density Residential
- Height of Building 8.5m
- Floor Space Ratio (FSR) N/A

A two-storey medium density dwelling is permitted under the current controls. The rear setback will enable backyards to be provided for the dwellings.

Site Area			Residential			Commercial			FSR
	LvI	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	
	1	-	-	-	1,601 m ²	85%	1,361 m ²		
	2	857 m²	75%	643 m²	-	-	-		
	3	857 m²	75%	643 m²	-	-	-		
1,884 m²	Sub- Total	1,714 m²	75%	1,286 m²	1,601 m²	85%	1,361 m²	2,646 m²	1.4:1



Figure 71: Existing Development



Figure 72: Current Permitted Development

Site 4 SWOT Analysis (3-9 West Street)

Strengths

- A relatively large site area and a regular rectangle shape
 The site is located on a relatively quiet
- The site is located on a relatively quie street - West Street, which provides pedestrian and acoustic amenity

Weaknesses

- The redevelopment potential of the site is limited by the current height control
- The site is adjacent to heritage items and a Heritage Conservation Area

Opportunities

 The site presents an opportunity to improve the interface with the surrounding buildings, especially 2 Cunningham Street

Threats

• The redevelopment of the site may result in a loss of commercial GFA

Site 5 - 2 C	unninghaı	n Street							
			Residential			Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	924 m²	75%	693 m²					
	2	924 m²	75%	693 m²					
1,270 m²	Sub- Total	1,848 m²		1,386 m²				1,386 m²	1.1 :1



Figure 73: Existing Development



Figure 74: Current Permitted Development

Site 5 SWOT Analysis (2 Cunningham Street)

Strengths

- The site is located on a quiet street, which provides pedestrian and acoustic amenity
- The site is close to community facilities and public spaces

Weaknesses

- The site has already reached its development potential under the current zoning and height controls
- The site is adjacent to heritage items and a heritage conservation area
- Cunningham Street is narrow, which provides limited vehicular access and a poor pedestrian environment

Opportunities

• The opportunity to improve Cunningham Street streetscape

Threats

 The residential amenity of the site is largely affected by the building along West Street, specifically 3-9 West Street. This will in turn limit the development potential of the site

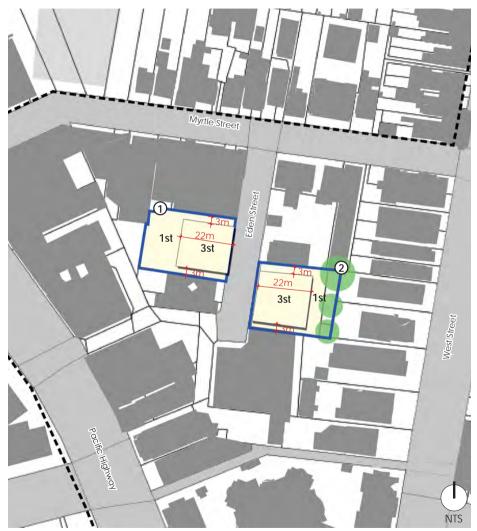


Figure 75: Site Plan

60 | CIVIC PRECINCT AND SURROUNDS PLANNING STUDY

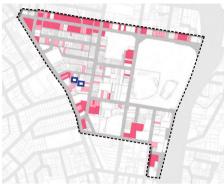


Figure 76: Key Plan



Figure 77: Existing Development



Figure 78: Current Permitted Development

5.2.5 EDEN STREET SITES

Site 1 - 8-16 Eden Street + Site 2 - 3 Eden Street

The current controls applicable to the

- Zoning B4 Mixed UseHeight of Building 13mFloor Space Ratio (FSR) N/A

A three-storey building is permitted under the current controls, with retail / commercial on the ground floor and residential uses above.

Key



The Study Area





Compliant building envelope

Existing buildings

Site 1 - 8-1	Site 1 - 8-16 Eden Street											
			Residential			Commercial						
	LvI	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR			
	1	-	-	-	897 m²	85%	762 m²					
	2	424 m²	75%	318 m²	-	-	-					
	3	424 m²	75%	318 m²	-	-	-					
897 m²	Sub- Total	848 m²	75%	636 m²	897 m²	85%	762 m²	1,398 m²	1.6:1			

			Residential			Commercial			
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	-	-	-	751 m²	85%	638 m²		
	2	472 m²	75%	354 m²	-	-	-		
	3	472 m²	75%	354 m²	-	-	-		
914 m²	Sub- Total	944 m²	75%	708 m²	751 m²	85%	638 m²	1,346 m²	1.5 :1

SWOT Analysis (Site 1+2)

Strengths

 Both sites are located on a quiet street, which provides acoustic amenity

Weaknesses

- Site 2 is adjacent to a heritage item and a Heritage Conservation Area. Its redevelopment potential is limited by the current height control
- Eden Street is a narrow street and provides poor pedestrian amenity

Opportunities

- The sites present an opportunity to complete a continuous streetscape along Eden Street
- The opportunity to improve the pedestrian environment along Eden Street

Threats

 The potential loss of commercial floor space



Figure 79: Site Plan

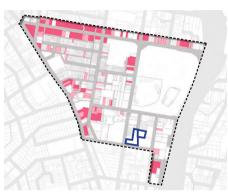


Figure 80: Key Plan



Figure 81: Existing Development



Figure 82: Current Permitted Development

5.2.6 MCLAREN STREET SITES

Site 1 - 50-52 McLaren Street

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building RL90-RL110
- Floor Space Ratio (FSR) N/A

The Metro is currently being constructed on the site. There is a Planning Approval permitting a maximum 14-storey building on the site. The site plan illustrates the previously approved built form.

Site 2 - 54 McLaren Street

The current controls applicable to the site:

- Zoning B4 Mixed Use
- Height of Building RL125
- Floor Space Ratio (FSR) N/A

A maximum 14-storey mixed-use building is permitted on the site under the current controls. A ground floor setback is provided to Site 1 to avoid any overlooking issue. The tower form is set back from the proposed two storey podium to respond to the adjacent lower scale buildings. Separation distances will ensure amenity for the adjacent buildings.

Key

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The Study Area

Site boundary

Compliant building envelope

Existing buildings

Site 1 - 50-52	2 McLaren Stree	et*				
Site Area	LvI	GBA per level	Efficiency	GFA	Total GFA	FSR
	Basement	N/A	-	2,550		
	LG	2,350	-	2,000		
	G	2,350	-	2,000		
	L1-2	2,350	-	4,000		
	L3-4	2,250	-	3,800		
	L5	2,050	-	1,750		
	L6	1,550	-	1,300		
	L7-10	1,375	-	4,675		
	L11	1,000	-	850		
	L12-13	800	-	1,360		
4,148 m²	Sub-Total	-	-	-	24,350	5.8:1

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Site 1 SWOT Analysis (50-52 McLaren Street)

Strengths

- The site will benefit from proximity to the Victoria Cross Metro North Portal
- The site is close to public places, community facilities and is within walking distance of the North Sydney CBD
- Taller buildings are permitted in this area under the current controls

Weaknesses

- The previously approved DA on the site will need to be revisited
- The site is adjacent to heritage items
- The pedestrian environment along McLaren Street and this section of Miller Street is poor
- The site north-south permeability is poor considering its length

Opportunities

- The site presents an opportunity to provide additional open space to the area in conjunction with the development of the Metro portal
- The site has the potential to provide a north-south link from Elliot Street to McLaren Street and the Ward Street Precinct

Threats

 The irregular shape of the site may prove a challenge to achieve its full development potential

^{*} Please note the GBA and GFA in this table is from the previously approved DA

Site 2 - 54	McLaren	Street							
			Residential			Commercia	al		
Site Area	Lvl	GBA	75% Efficiency	GFA	GBA	85% Efficiency	GFA	Total GFA	FSR
	1	-	-	-	1,195 m²	85%	1,016 m ²		
	2	-	-	-	1,195 m²	85%	1,016 m²		
	3	844 m²	75%	633 m²	-	-	-		
	4	844 m²	75%	633 m²	-	-	-		
	5	844 m²	75%	633 m²	-	-	-	_	
	6	844 m²	75%	633 m²	-	-	-		
	7	844 m²	75%	633 m²	-	-	-		
	8	844 m²	75%	633 m²	-	-	-		
	9	844 m²	75%	633 m²	-	-	-		
	10	844 m²	75%	633 m²	-	-	-		
	11	844 m²	75%	633 m²	-	-	-		
	12	844 m²	75%	633 m²	-	-	-		
	13	844 m²	75%	633 m ²	-	-	-		
	14	844 m²	75%	633 m ²	-	-	-		
2,535m²	Sub- Total	10,128 m²	75%	7,596 m²	2,390 m²	85%	2,032 m²	9,628 m²	3.8 :1

Site 2 SWOT Analysis (54 McLaren Street)

Strengths

- The site is in close proximity to the future Victoria Cross Metro North Portal
- Taller buildings are permitted in this area under the current controls

Weaknesses

- The site has sensitive frontages to the school and the Heritage Conservation Area
- The site is to the north of the approved development at 168 Walker Street, which it may potentially overshadow
- The site does not have a direct address to McLaren Street

Opportunities

- The site presents an opportunity to improve the existing interfaces with the surrounding properties
- The site together with 50-52 McLaren Street, presents an opportunity to improve the permeability of the whole block

Threats

- The potential transitions to the sensitive frontages will reduce the developable area of the site
- The future vehicular access may affect the location and the amenity of the potential through site link



•

Stage 1

6.1 WORKSHOP OVERVIEW

Stage 1 of the study consultation was conducted in July and August 2019. Two workshops were convened to seek community perspectives of the study. The Stage 1 consultation outcomes can be found in the document library which is accessible through the Your Say North Sydney website.

Stage 2

A second round of workshops were advertised through the Your Say North Sydney website. Council also invited the following parties to attend the workshops:

- Identified key stakeholders (total of 27)
- Those who submitted comments to the initial consultation phase (total of 89)
- Those providing email contact details via Your Say for the Stage 1 consultation
- Representatives of each of the Precinct Committees.

Two workshops were held at the Ros Crichton Pavilion at the following dates and times:

- Thursday 21 November 2019 from 4pm to 6.30pm 15 participants attended
- Thursday 28 November 2019 from 6pm to 8.30pm – 13 participants attended

The objectives of the workshops were to:

- Confirm the purpose of the study
- Build on the consultation conducted during July and August 2019 by confirming the themes heard through that consultation

- Get input to the planning principles for the Urban Design of the Study Area
- Look at potential locations that might be acceptable for density
- Determine the type of public benefit that the community hopes might result from future development
- Confirmation of the valued 'local places'
- Provide information on the future steps for the project, including timing for the exhibition and future discussions with the community

The room was arranged in tables to allow for participants to work in groups. Participants were invited to attend the following Activities:

- Identify 'Considerations' and 'Initiatives' under 'Community Priority' areas identified in Stage 1 consultation
- 2. Comment on the preliminary Urban Design Principles
- Discuss and identify the types of development that might be acceptable based on precedent images provided (refer to images on Page 68)
- 4. Identify the three most desired public benefits for each group

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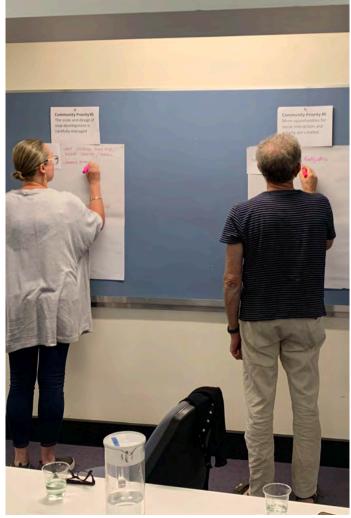




Figure 83: Activity 1 Work Clouds (Source: PlanCom)

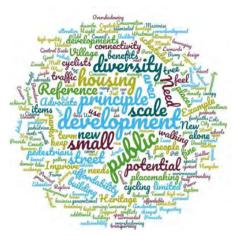


Figure 84: Activity 2 Work Clouds (Source: PlanCom)



Figure 85: Activity 4 Work Clouds (Source: PlanCom)

6.2 WORKSHOP OUTCOMES SUMMARY

Word Clouds have been developed for the Activities 1-3, in which participants contributed input as part of both workshops.

Activity 1 - Community Priorities

Comments raised by participants:

- Controlling development type including height, location and character
- Traffic congestion (now and in the future)
- Protection and management of existing parkland in terms of uses and sunlight and the need for more smaller parks

Activity 2 - Urban Design Principles

Comments raised by participants:

- Controlling development type including height, location and character. Note that this was the same theme as for the community priorities above
- Desire for the promotion of housing diversity
- Ensuring any future development provides public benefits

Activity 3 - Development Types

The consolidated results from the placement of adhesive dots on study area maps from each of the six groups (three groups per workshop) is summarised in Figure 87.

These dots indicate the locations that types and scales of development (refer to the precedent images on Page 68) might be acceptable to attendees in the study area.

The precedent images are selected to assist the attendees to visualise the different building typologies and scales. Most of the examples are chosen from metropolitan Sydney with one from Melbourne. The examples provide a range of typical development types, including alterations / additions to an existing building, infill development, heritage redevelopment, modern residential development and mixed-use development, to enable the attendees to choose the most suitable building typology for each part of the study area.

The height intervals are also provided in association with the building typologies.

Activity 4 - Public Benefits

Comments raised by participants:

- Increased and improved community facilities (childcare, theatre) – noting that there was also the suggestion that aged care should be added to childcare
- Upgrades to the public domain (footpaths, trees, streetscape, parks)
- The need for more affordable housing

The detailed description and outcomes of the workshops can be found in Appendix 04 Civic Precincts and Surrounds Planning Study Workshop Report by PlanCom.

Low Rise (1-3st) O







Medium Rise (4-6st)









High Rise (14st+)







6.0 WORKSHOP OUTCOMES



Figure 87: Community workshops result - Activity 3 - Consolidated hand mark-ups from six groups (from two workshops) CONYBEARE MORRISON

Activity 3 Summary

This diagram summarises the results collected from the two workshop sessions. The dots in the diagram indicate the locations that types and scales of development (refer to the precedent images on Page 68) might be acceptable to attendees in the study area.

The result shows that the attendees felt that low rise building typologies were preferred in the core of the study area; and that medium to high rise development was more acceptable at the fringe of the study area, especially along the Pacific Highway, near the future Victoria Cross Metro Station North Portal and in the existing North Sydney CBD (in and around the Ward Street Precinct).

Key

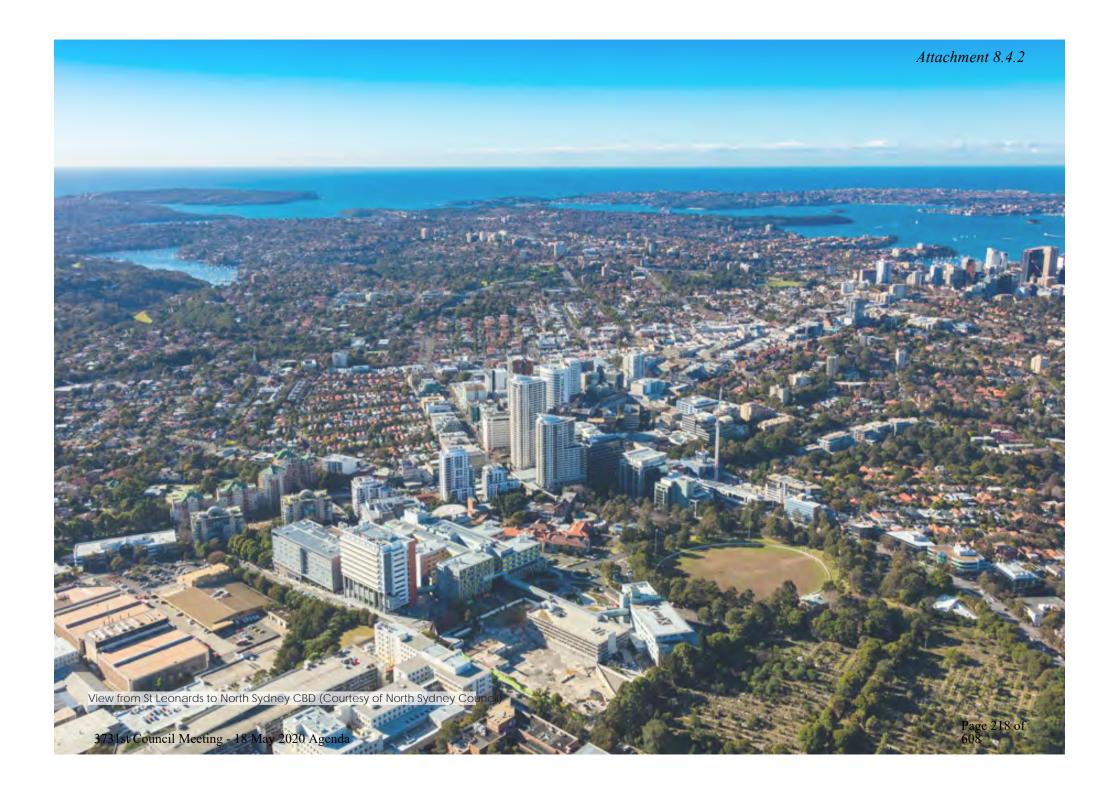
Study Area

High Rise (14st+)

Medium-High Rise (7-9st+)

Medium Rise (4-6st)

O Low Rise (1-3st)



7.0 URBAN DESIGN VISION AND PRINCIPLES

7.1 URBAN DESIGN VISION

The Civic Precinct and Surrounds will be an exemplary sustainable urban community that promotes local employment opportunities and embraces a positive, healthy and vibrant lifestyle. Its identity will continue to be defined by its rich cultural heritage, civic and educational institutions and a strong sense of community.

Lower scale, leafy streets and a village atmosphere will complement the higher density and more urban North Sydney CBD to the south and Crows Nest and St Leonards urban centres to the northwest. The 'heart' of the Civic Precinct will be the North Sydney Council Chambers and Offices, Ted Mack Civic Park, Stanton Library and Community Facilities. The Civic Precinct will continue to be characterised by a lower scale urban environment that gradually transitions in height up to the higher density North Sydney CBD, and the new Metro Victoria Cross North Portal, to Crows Nest and St Leonards to the northwest.

The many heritage buildings, Heritage Conservation Areas, streetscapes and contributory buildings will be protected and enhanced by restoration, adaptive reuse or where appropriate contemporary additions or infill development that respects the heritage character and scale.

Employment opportunities will be provided by innovative small-to-medium scale businesses, shops and cafes, in developments incorporating a vibrant and 'eday' mix of uses.

Educational campuses will be encouraged to work together to share knowledge and facilities. Campuses will be designed to integrate with their surroundings, to be welcoming and outward looking, contributing to the urban streetscapes along their frontages.

Walking and cycling will be prioritised, placing residential streets and apartments, employment areas, recreational and entertainment destinations, and community and social hubs within an easy walk or cycle of each other.

Public transport will be within easy reach, including Metro, Rail and bus services, providing connections to adjoining neighbourhoods and the greater metropolitan area.

The public domain within the precinct will contribute to an enhanced 'sense of place', will be 'fine grain', of good human scale and interconnected, comprised of generous tree-lined local streets, urban laneways and vibrant plazas. It will integrate sustainability initiatives, including generous footpaths, bicycle paths, and Water Sensitive Urban Design (WSUD).

Regional and local parks will continue to accommodate sports venues, and provide opportunities for exercise and relaxation. The precinct's streets and parks will contribute to an enhanced tree canopy for the LGA.



7.0 URBAN DESIGN VISIONS AND PRINCIPLES

7.2 URBAN DESIGN PRINCIPLES

Urban Design Principles have been identified and then refined following input from the community workshops:

- Preserve and respect the heritage, conservation areas and fine grain of the study area
- Provide built form and scale transitions that are sympathetic to the heritage items and conservation areas
- 3. Preserve the village feel and civic character of the precinct
- 4. Protect existing trees and promote tree planting where possible
- 5. Maximise the amenity of public places
- Manage growth and development which is of appropriate scale and character
- 7. Prioritise walking and cycling amenity and connectivity
- Capitalise on public domain improvements, local identity and land use opportunities resulting from the Victoria Cross Metro Station
- 9. Promote housing diversity and affordability
- Attract smaller scale businesses and employment to complement the North Sydney CBD
- 11. Maximise the potential of existing open space and explore the potential for new places
- Advocate design excellence and sustainability in new buildings and the public domain
- 13. Provide safe, attractive and highquality public and community places
- 14. Ensure transparency where the leveraging of public benefits is pursued in exchange for additional development potential

7.3 SUSTAIN ABILITY RECOMMENDATIONS

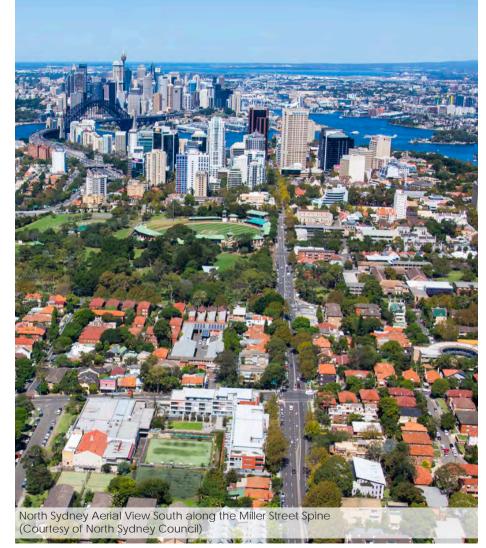
Transport and Access Infrastructure:

- Ensure adequate sidewalk space for pedestrian movement.
- Ensure adequate bike and other mobility paths are available,
- Ensure adequate pedestrian infrastructure for crossing roads is provided, particularly at key intersections
- Ensure adequate parking and storage facilities for bicycles and potentially other micro-mobility vehicles is available at the Station entry
- Ensure landscaping is undertaken to retain and enhance the "leafy" nature of the area
- Pedestrian and cycling infrastructure should also make it safe, efficient and pleasurable for Station patrons to exit

Other Recommendations:

- Ensure that adequate options for retail and other services are provided in the vicinity of the station entry,
- Use local indigenous planting for landscaping and retain the 'leafy' nature of the area.

Refer to Appendix 05 for more detailed information.



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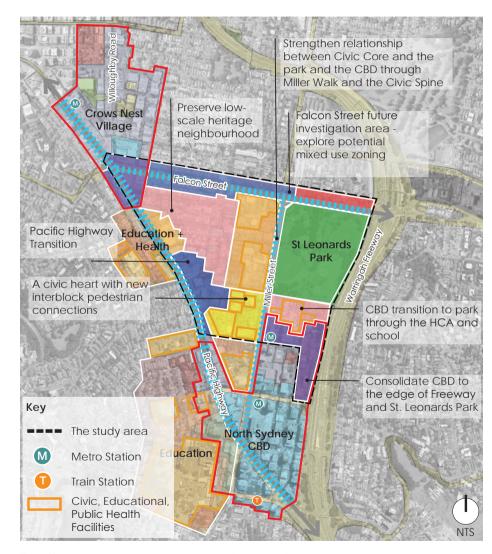


Figure 88: Urban structure - showing the study area, Crows Nest Village and North Sydney CBD CONYBEARE MORRISON

8.1 URBAN STRUCTURE

The study area adjoins two major centres - Crows Nest Village to the northwest and North Sydney CBD to the south. Understanding the structural relationships with the surrounding precincts is critical in establishing an appropriate strategic planning response.

The study area is not uniform in scale, urban typology or character. Rather it is comprised of a series of component areas; each with their own scale, character and typology.

The major streets form 'spines' which connect the different character areas.

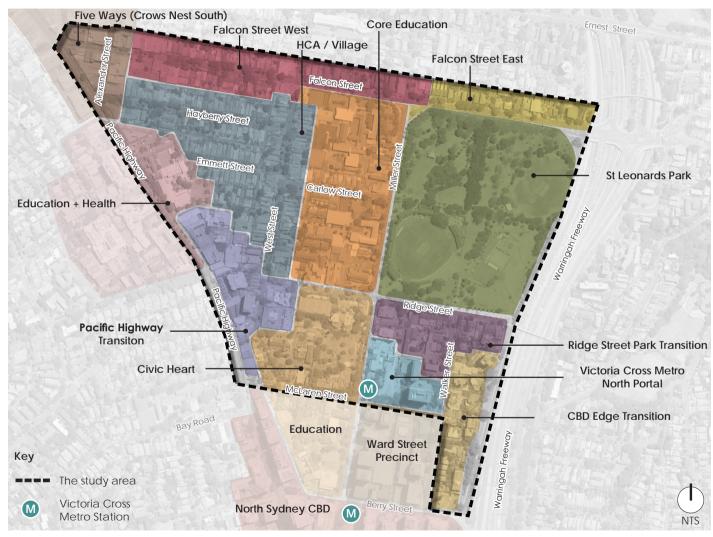
The Pacific Highway and Falcon Street 'spines' define the edges of the study area and are envisaged as grand urban avenues. Miller Street is an important pedestrian 'spine', connecting the CBD to the Civic Heart and north to St. Leonards Park. Transitions between each component area is seen to be progressive, rather than an abrupt or hard defined edge. The urban structure plan is comprised of the following strategies:

- The Pacific Highway connects Crows Nest through the study area to the North Sydney CBD.
- The Falcon Street spine links Crows Nest Village through the study area east to Neutral Bay and Cremonne.
- The Miller Street spine links the North Sydney CBD, Civic Heart and St. Leonards Park. The pedestrian oriented streetscape is known as Miller Walk.
- The Five Ways urban node is addressed as part of Crows Nest Village.

- Development of the land at the future Metro station presents an opportunity to enhance pedestrian connections from the Ward Street Precinct northwards through to St. Leonards Park.
- The existing civic, educational and health facilities provide employment and services enhancing the amenity and wellbeing of the local community. They play an important role in shaping the character of North Sydney. The planning strategy provides a framework for these functions to develop and grow.
- The low scale village character in the heart of the study area is preserved.

Falcon Street Spine

The study identified the potential for the Falcon Street spine to develop into a mixed-use area, where apartments with ground floor commercial and small scale retail uses could develop. However, due to a number of unresolved major changes to the planning and infrastructure along the spine, including the finalisation of the St Leonards and Crows Nest 2036 Plan by the DPIE and the recent proposal of the Western Harbour Tunnel and Beaches Link by Transport for NSW, it is recommended that the area in the short term retains its current zoning until the relevant studies are completed and the impacts to Falcon Street are clear. A separate detailed study is needed to plan for the transformation of the Falcon Street corridor.



8.2 FUTURE CHARACTER AREA

The Preferred Planning Strategy is comprised of a mosaic of Character Areas across the study area. The Character Areas have been established based on the Urban Design analysis, the overall project vision, Urban Design principles and the overarching Urban Structure Plan. The Character Areas provide a clear future direction for each distinct part of the study area. There are twelve future character areas:

- Five Ways (Crows Nest South)
- Falcon Street West
- Falcon Street East
- St Leonards Park
- Core Education
- HCA / Village
- Education + Health
- Pacific Highway Transition
- Civic Heart
- Victoria Cross Metro North Portal
- Ridge Street Park Transition
- CBD East Transition

Each area has a unique character and set of characteristics. The detail planning of the study area should contribute positively to the desired future character of each nominated area.

The local character, identity and typologies of each area should be reinforced; as it is this variety which makes the study area unique, rather than bland and homogeneous.

Figure 89: Future character areas

8.3 INDICATIVE LAYOUT PLAN

The identified desired future character for each area has informed the development of the spatial framework for the North Sydney Civic Precinct and Surrounds. The key principles that have informed the development of the Urban Design framework and the Indicative Layout Plan (Figure 90) for the study area are summarised below. The identified Urban Design framework principles reflect and enhance the Urban Design Principles identified in Chapter 7 of this report.

- Retain and enhance the village character of the study area by respecting the heritage items, heritage conservation areas and contributory buildings, and locating taller buildings at the future Victoria Cross Metro Station North Portal and along major road arteries. (Urban Design Principles 1, 3)
- Accommodate potential alterations and additions to heritage buildings, and buildings within Heritage Conservation Areas through ensuring additions contribute positively to the identified heritage character. (Urban Design Principle 1)
- 3. Focus potential uplift along the main roads at the fringe of the study area. This will maintain the low scale character at the heart of the study area. Improve the retail / commercial offering and public domain along the main roads. (Urban Design Principles 3, 6,9,10)

- Provide transitions in height to nearby lower scale properties and sensitive frontages (e.g. schools, heritage items and HCAs). (Urban Design Principle 2)
- Maximise the public benefits resulting from new development. (Urban Design Principles 8, 11, 14)
- 6. Ensure the growth of educational facilities will contribute positively to the surrounding area and will open up visually to the main frontage streets, respect the scale and amenity of adjoining properties and public realm, and improve the area's permeability. The potential growth should provide adequate community benefit (e.g. providing public access to school facilities). (Urban Design Principles 6, 7, 11)
- Continue to upgrade St Leonards
 Park and Ted Mack Civic Park. (Urban
 Design Principles 4, 5, 11)
- Explore opportunities to provide additional public places, micro plazas, parks, and community facilities where appropriate. (Urban Design Principles 8, 11, 13)
- Encourage mixed-use development that provides local employment opportunities and enables living and working within easy walking distance. (Urban Design Principles 9, 10, 12)
- Improve the vibrancy of the study area by promoting ground floor activities around the Metro entry, along Pacific Highway and at Five Ways junction. (Urban Design Principles 5, 13)

- 11. Support the nighttime economy in appropriate locations. (Urban Design Principle 10)
- 12. Improve the appearance of the Pacific Highway and Falcon Street streetscapes by promoting treeline street spines and active street frontages where appropriate. (Urban Design Principles 4, 5)
- 13. Encourage active transport (walking and cycling) within the study area, by improving the existing public domain conditions and providing new cycling routes, through site links and shared ways. (Urban Design Principles 7,11)
- 14. Maximise pedestrian crossing safety and amenity at the key intersections along the Pacific Highway and Falcon Street. (Urban Design Principles 5, 11, 13)



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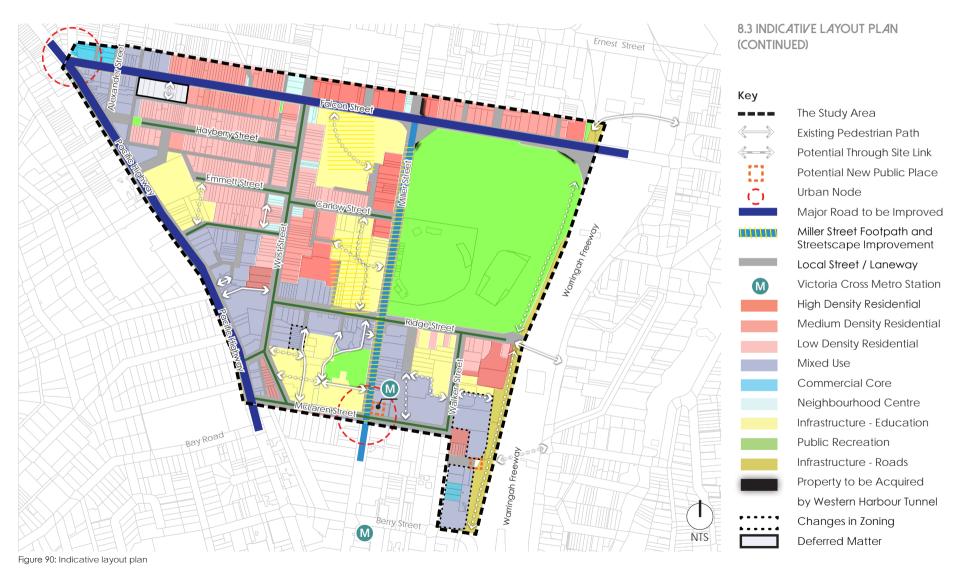




Figure 91: Active frontage

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8.4 ACTIVE FRONTAGE

Public streets form a major part of the public domain, and therefore play a very important role in contributing to the amenity and liveliness of our urban experience. Active street frontages are comprised of retail / commercial business frontages at ground level, that open up the internal building functions and activities, to contribute to the vibrancy of the street. This can include shop front windows, cafe's, community and civic buildings and the like. This will improve the attractiveness of key study area streets and create social destinations and meeting places. This will also provide employment opportunities within the local community. Please refer to Figure 91 for nominated active frontages within the study area. The key principles are to:

- Bring a concentration of vibrancy to the pedestrian environment for specific sections of street frontage, particularly along the Pacific Highway and Falcon Street. Footpath condition and width, tree planting and awnings all support this street activity.
- Facilitate Pacific Highway and Falcon Street footpath widening and promote ground floor active uses to activate the footpaths.
- Reduce long sections (i.e. greater than 40m) of blank walls, building services (i.e. substation) and minimise vehicular access points and width along active frontages to improve pedestrian safety and footpath continuity.
- Ensure future development contributes positively to streetscape amenity, liveliness and safety.

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Figure 92: Building height

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8.5 BUILDING HEIGHT

A modest increase in building height is proposed for properties within the study area that are in close proximity to public transport nodes, existing built up CBDs and along major road arteries. The height strategy preserves the study area's low scale village character and its significant heritage. The key principles of the height strategy are to:

- Maintain the low scale, fine grain built form in the heart of the study area to preserve the village feel and civic character.
- Focus height increases along the Pacific Highway and Falcon Street spines and in proximity to the Metro Station to ensure the public domain is appropriately scaled and to avoid any bulk and scale impacts to the core area
- Provide height transitions to lower scale areas, heritage items, HCAs and interfaces with school properties to minimise the scale and amenity impacts and to respect the scale and character of the heritage items and HCAs.

The recommended building height is illustrated in Figure 92. The maximum height will need to be reduced for parts of sites in the following situations:

- Where height transition is needed to adjoining low scale areas, heritage items, HCAs and school interfaces.
- Where the maximum height may result in impacts on either the public domain or adjacent properties, due to overshadowing, privacy or other loss of urban amenity.

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8.6 STREET WALL HEIGHT

An appropriately scaled and consistent street wall along street frontages plays an important role in shaping and defining the public domain and urban scale. It defines the perceived proportions of the streetscape and thereby the urban experience. A consistent street wall height will tie the urban area together and provide people with a well-scaled streetscape.

The proposed street wall heights are nominated in Figure 93. The key principles are to:

- Promote human scale through a wellproportioned, consistent street wall height.
- Require new developments to match the street wall height established by heritage items and contributory buildings.
- 3. Setback the upper levels, above the street wall height, by 3m to reduce the apparent scale of taller buildings.
- Include active and employment generating uses within the building podium levels to activate the street and to provide local employment.
- Utilise a contemporary architectural expression whilst complementing the scale and materiality of heritage and contributory items.

Figure 93: Street wall height

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8.7 NON-RESIDENTIAL FLOOR SPACE

The economic assessment undertaken by the study indicates that North Sydney Council needs to provide opportunities for further development of commercial space within the study area. This will facilitate employment growth and maintain the precinct's current market position as an alternative to Sydney's CBD.

The retention of retail and commercial uses as part of the mix of uses in the study area is encouraged, as this will maintain employment in the area, in proximity to residential accommodation. It will also provide opportunities for small to medium size office space for innovative and new start up businesses, to complement the North Sydney CBD to the south. A genuine mix of land uses in the study area will lead to a more interesting and vibrant public domain. This will facilitate the supply of additional jobs envisaged in the North District Plan. It will also promote local employment opportunities, contributing to the local economy, and facilitate new types of living and working arrangements.

The required minimum FSR (non-residential) is nominated in Figure 94. The key principles are to:

- Increase the proportion of nonresidential uses within the B4 Mixed Use zone
- Encourage multi-level non-residential uses, rather than on the ground floor only.
- Promote a diversity and mix of nonresidential uses in the study area, i.e. retail, creative commercial, spaces for start-up businesses, etc.

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8.8 DEVELOPMENT IN HERITAGE CONSERVATION AREAS

North Sydney's heritage buildings and streetscapes are a unique feature of the study area, providing a strong sense of identity and character for the community. The study area contains 93 heritage items and four HCAs. It is important to preserve and respect these heritage buildings and conservation areas for the current community and for future generations.

The adaptive reuse of buildings, and alteration and additions to heritage items are an opportunity to keep heritage 'alive' within the study area.

Adaptive Reuse of Buildings

If the function of a building becomes redundant or undesirable, it becomes vulnerable to damage through neglect and decay, or in some cases, demolition. Adaptive reuse can help to conserve and celebrate the heritage significance of a place while allowing it to have a new use and an extended lifespan. Renewing and using existing buildings also helps to prevent the unnecessary waste of building materials. While there are benefits to adaptive reuse, adaptation of a building to allow for a new use should be underpinned by an understanding of its significance and should result in a design which creates a relationship between existing and new work (Heritage Council 2008).

Within North Sydney, there is an opportunity to adaptively reuse public heritage listed buildings that have a redundant function and are currently unused or minimally used.

The North Sydney DCP outlines a series of provisions for the adaptive reuse of heritage items that are representative examples of their type (i.e. industrial, maritime and/or workshop buildings) Broadly, these provisions seek to:

- Ensure that changes to buildings are sympathetic to the existing fabric and building elements and do not compromise the heritage significance of the buildings
- Allow for interpretation of significant form and use of these buildings
- Conserve important built elements and any relevant equipment associated with the use of these buildings

Alterations and Additions to Heritage Items

The North Sydney DCP outlines several provisions for altering and adding to listed heritage items. Underpinning these provisions is the objective that any changes to fabric should be based on an understanding of the heritage significance of the specific item. Generally, there is a preference to locate changes away from primary intact elements of a place or highly visible areas, and to minimise the visual dominance of any new work on the place both as an individual item and on the streetscape.

The adjacent table outlines some broad provisions for making changes to heritage items.

Policy	Image
Locate changes away from primary intact areas of heritage items, with changes to the rear often being preferable.	Street
	Illustration identifying an addition to the rear of a heritage item (North Sydney DCP Figure B-13.2)
Additions should be smaller in scale and length than existing buildings.	Illustration identifying an addition to the rear of a heritage item that is an appropriate scale (North Sydney DCP Figure B-13.3)
New work should maintain the integrity of the building form, which would allow the original form of the building to be discerned.	Illustration identifying an addition to the rear of a building that still allows the original form to be discerned (North Sydney DCP Figure B-13.4)
Consideration should be given to using the existing roof spaces of buildings. The DCP does not generally support whole-storey additions to heritage items, with a preference for utilising the attic space with installation of dormer windows.	Front
	Illustration identifying changes within the roof space of a heritage building (North Sydney DCP Figure B-13.5)

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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA

Five Ways (Crows Nest South) Design Principles

- Maintain B3 Commercial Core and B4 Mixed Use zoning to promote commercial activities and offering.
- Enhance ground floor activities to further activate the area.
- Encourage nighttime economy to form a nighttime hub for local residents and visitors
- Reinforce the established three storey street wall height.
- Increase the height up to eight storeys with a six storey east transition.
- Improve the pedestrian connections and amenity from the area to Willoughby Road.
- Provide built form and scale transitions to the low scale areas, heritage items and conservation areas.
- Widen Pacific Highway and Falcon Street footpaths by introducing 1.5m setback to frontages.



Five Ways (Crows Nest South) Design Guidelines

- New development along Falcon Street and the Pacific Highway to provide 1.5m setback to accommodate tree planting and footpath widening.
- Provide active uses (retail / commercial) on the ground level to activate streets and laneways.
- New development to provide three storey street wall height with recessed upper levels to emphasis the street wall. A minimum 3m upper level setback is to be provided.
- The three-storey podium should provide non-residential uses in B4 zone to promote living and working arrangements. Token mixed-use with minimal retail spaces on ground level is not supported.
- Increase the maximum height to eightstorey on the triangular site bounded by Falcon Street, Pacific Highway and Alexander Street, with building height stepping down to six towards the lower scale area (to the east).
- New buildings should be designed with respect for heritage items and HCAs in regard to bulk, scale and design aesthetics.

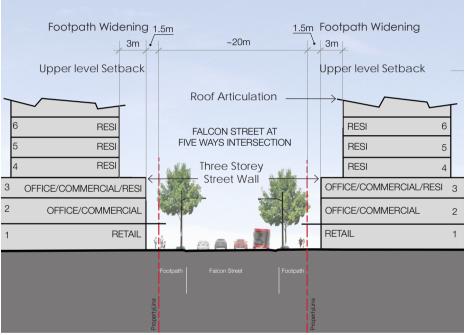


Figure 95: Indicative Falcon Street section



Key Map

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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

St Leonards Park Design Principles

- Provide a balance between active and passive recreation.
- Provide a clear definition between various spaces throughout the park.
- Minimise conflict between various users of the park, particularly cyclists / pedestrians, and active recreation / passive recreation.
- Provide a strategy for managing park use and develop appropriate maintenance strategies.
- Maintain views and open amenity in the south eastern lawn area (Harbour view lawn).
- Celebrate the strong historical layout of the park.
- Retain and manage historic trees
- Enhance the scale and prominence of War Memorial and Tunks Fountain.
- Redesign park furniture and paving to support the heritage character of the park
- Provide opportunities to highlight the park's history.



- Explore local history and stories past and present.
- Relocate lamp posts and reduce clutter on paths.
- Minimise the conflict between cyclists and pedestrians on internal park pathways.
- Investigate the possibility of a dedicated cycleway path on park boundaries.
- · Upgrade public toilets.
- Improve the amenity of the park by providing new park furniture.
- Provide new park lighting.
- Explore opportunities to expand the playground to provide a wider range of activities for older children.

St Leonards Park Design Guidelines

 The future development of St Leonards Park should be in accordance with the St Leonards Park Landscape Masterplan.



Key Map



Figure 96: St Leonards Park Landscape Masterplan by Gallagher Studio

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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

HCA / Low Scale Residential Village Design Principles

- Protect and enhance the heritage and HCA values and character of the area's buildings and streetscapes.
- Maintain the low scale and fine urban 'grain' of the built form and frontages.
- Encourage the development of lowscale secondary dwellings along the laneways.
- Maintain the retail / commercial uses within the area, especially along West Street
- Limit building height to generally two storeys in the area.

HCA / Low Scale Residential Village Design Guidelines

- Maintain the maximum two storey height limit.
- Development of a heritage item or within a HCA should seek heritage advice.
- New development within or adjacent to a heritage item or HCA should contribute positively to heritage value and character.
- New development within or adjacent to a heritage item or HCA should refer to Design in Context - Guidelines for Infill Development in the Historic Environment by the former NSW Heritage Office (now part of the DPIE).
- The existing B1 Neighbourhood Centre zone along West Street is to be kept and enhanced to provide local jobs and active uses along West Street.
- Improve the laneway appearance by allowing new additions above, such as garages, or the like. New development should contribute to the heritage value and character of the laneways.







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Figure 97: Laneway activation example - A property along Hayberry Lane (Source: Google)

8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

Core Education Design Principles

Whilst additional building height may be warranted on educational campuses; this would need to be subject to a case-by-case detailed masterplaning exercise to address the principles and guidelines identified.

- Reinforce the area's educational function.
- Ensure the area maintains a generally low scale character, with buildings progressively stepping up in height to a maximum of four storeys within the campuses, or along major road arteries.
- Ensure that school amenity (overshadowing / privacy) is not affected by any new development.
- Ensure future development does not impact the visual amenity of the surrounding area.
- Integrate educational facilities into the neighbourhood by improving their interfaces with adjoining streets and the public domain.
- Improve the area's permeability by encouraging pedestrian through site links during day light hours and after-hours public access to schools' facilities, subject to security parameters.

Core Education Design Guidelines

- The study envisages that school buildings could develop up to a maximum of four storeys in height, subject to further detailed Urban Design study.
- Protect the non-residential uses along Ridge Street. Any new development along Ridge Street within the B4 Mixed Use zone should provide active uses along the street.
- Surrounding development should avoid overlooking and overshadowing the educational facilities.
- Development of educational campuses should demonstrate improvement to the interfaces with the surrounding neighbourhood and provide community benefit including enabling after-hours public access to facilities.
- New residential development should be to a high level of design quality and residential amenity. Setbacks and separation distances, communal open space provision and deep soil zone requirements as per SEPP 65 and the Apartment Design Guide should be met.

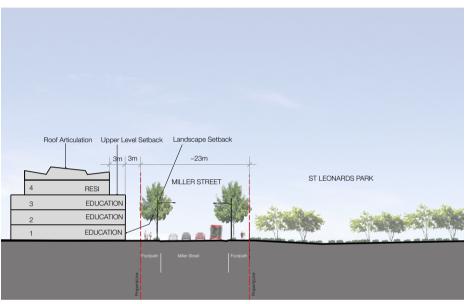


Figure 98: Indicative Miller Street section



Key Map



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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

Education + Health Design Principles

- Reinforce the area's education and health function and the heritage value of the buildings within and adjoining the area.
- Encourage retail / commercial uses at ground and first floor levels to activate the Pacific Highway frontage.
- Provide a three storey street wall with upper levels setback, to create a well scaled streetscape along the Pacific Highway.
- Limit the building height to four storeys.
- Provide built form and scale transitions to heritage items and Heritage Conservation Areas to the east.
- Improve the Pacific Highway streetscape with street tree planting to create a green boulevard.
- Widen footpaths by introducing a 1.5m setback to buildings along the Pacific Highway.



Education + Health Design Guidelines

- New development along the Pacific Highway to provide 1.5m setback to accommodate tree planting and footpath widening.
- Provide active uses (retail / commercial) at minimum to the ground level to activate streets and laneways within B4 Mixed Use zones.
- Increase the maximum height to fourstorey. New development to provide a three storey street wall along the Pacific Highway, with the fourth storey setback from the street wall. A minimum 3m upper level setback is to be provided.
- Non-residential uses should be maximised within the three-storey podium to promote living and working arrangements. Token mixed-use, with minimal retail spaces on ground level, is not supported.
- New buildings should be designed to respect heritage items and HCAs in regard to bulk, scale and aesthetics.
- New educational campuses should demonstrate an improvement to the interface with the surrounding neighbourhood and provide community benefit including enabling after-hours public access to facilities.



Figure 99: Precedent image - desired built form (Source: Google)



Key Map

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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

Pacific Highway Transition Design Principles

- Maintain the B4 Mixed Use zoning to maximise employment opportunities in the area and to support retail / commercial uses on the ground and first floor levels to activate the area.
- Provide a three storey street wall along the Pacific Highway frontage.
- Encourage medium scale development with a maximum height of four storeys in the majority of the area; and in the south of the character area provide a higher built form (12 storey) as a step up in scale to the taller buildings in the North Sydney CBD.
- Respect the scale and character of adjoining heritage items and Heritage Conservation Areas.
- Improve the Pacific Highway streetscape with street tree planting to create a green and pedestrian friendly boulevard.
- Widen footpaths by introducing a 1.5m setback to buildings along both sides of the Pacific Highway.

Pacific Highway Transition Design Guidelines

- New development along the Pacific Highway is to be setback 1.5m to accommodate tree planting and footpath widening.
- Provide active uses (retail / commercial) on at least the ground level of developments to activate streets, laneways and pedestrian / cycle links within the B4 Mixed Use zone.
- Increase the maximum height to generally four storeys, except for a 12-storey built form at the interface with the North Sydney CBD, on the site bounded by West Street, Church Lane, Pacific Highway and McLaren Street.
- New development is to provide a three storey street wall along the Pacific Highway with upper levels setback from the podium. A minimum of 3m upper level setback is to be provided.
- Non-residential uses should be maximised within the three-storey podium, to support living and working arrangements in proximity. Token mixed-use, with minimal retail spaces on the ground level is not supported.
- New buildings should be designed to respect heritage items and HCAs in regard to bulk, scale and aesthetics.
- Adjoining new development should avoid overlooking educational open space. Additional overshadowing of school playgrounds is not permitted.

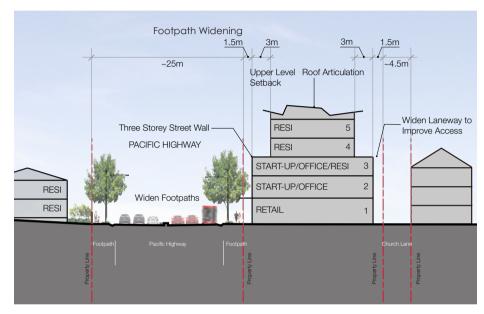


Figure 100: Indicative Pacific Highway - Church Lane section



Key Map





Figure 101: Site Plan

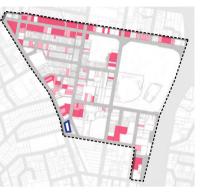


Figure 102: Key Plan

Key

The Study Area

Site boundary

Potential building envelope

Heritage item

HCA

Existing buildings

Site Specific Study -253-267 Pacific Highway

This is a consolidated site along the Pacific Highway, in the Pacific Highway Transition character area. A heritage listed building at 265 Pacific Highway is within the consolidated site.

A mixed use podium and tower built form is proposed with building heights that step up from five storeys at the north of the site to 12 storeys at the south. This will provide a height transition from the mid-scale Pacific Highway corridor, to the northern edge of the North Sydney CBD. The progressive stepping up in height on the site is also to manage the scale impact to the adjacent HCA to the east.

A three storey podium is proposed which matches the scale of the heritage item, that is framed by the retail / commercial podium. A setback in accordance with the DCP to the podium along Church Lane is proposed to improve pedestrian and vehicular access. A small plaza is created along Church Lane at the heritage building.

A 1.5m building setback along the Pacific Highway frontage is proposed for widening of the footpath to allow street tree planting, awnings and to enhance overall amenity.

Secondary upper level setbacks are introduced to provide further transitions in height and scale to adjoining heritage buildings and the HCA.



Figure 103: 3D view

Separation is proposed between the five storey built form and the tower form. The separation distance will provide a buffer between the tower and the heritage item within the site and will ensure adequate amenity can be achieved.

A larger upper level setback and the third podium level setback to the south boundary preserve the amenity of the adjacent heritage property.

Although this high level built form massing study presents an adequate bulk and scale in relation to the location and context, it should be further refined and amended addressing potential heritage issues, including:

 The form, pattern, and materiality surrounding the heritage item will need to be further considered. The lower façade is the perfect place to

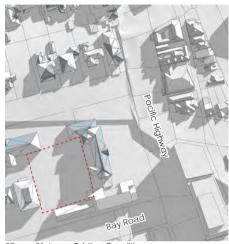
- pay homage to the former pattern of the streetscape.
- If a lift core is excavated beneath the heritage item is progressed, a historical archaeological assessment would be required to fully understand archaeological risks.
- Plantings will need to be considered in any green spaces so that they are appropriate for the setting of the streetscape.
- Heritage interpretation will need to be considered on the public and communal open spaces.
- The impact of overshadowing on heritage places will need to be better understood.
- Further view and vista assessments are needed to look at the development from neighbouring HCAs and heritage items.

253-267 Pacific Highway										
		Commercial			Residential					
		GBA	Efficiency	GFA	GBA	Efficiency	GFA			
Site Area	Lvl	(m²)		(m²)	(m²)		(m²)	Total GFA	FSR	
	1-3	3,144	85%	2,672	-	-	-			
1,468m²	4-12	-	-	-	3,597	75%	2,698	5,370 m²	3.6:1	
	Total GBA	6 7/11 m ²								

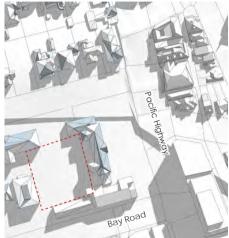
Site Specific Study - 253-267 Pacific Highway Shadow Testing

The shadow testing indicates that the proposed built form envelope on the site would not cast any additional shadows to the school playground (located in the middle of the school) in mid-winter.

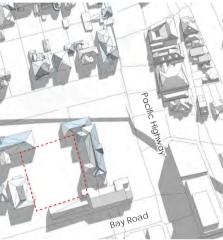
The proposed built form will cast additional shadow to the setback area along the Pacific Highway between 9am and 11am in mid-winter. However, the setback area is occupied by the on grad carpark and lawns, which are not used for recreational / sports activities for school students or children in the childcare centre at the corner of the Pacific Highway and Bay Road intersection. Therefore, the additional shadow is acceptable in this regard.



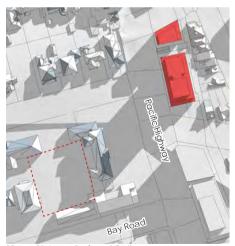
09 am, 21 June - Existing Condition



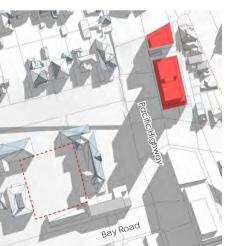
10 am, 21 June - Existing Condition



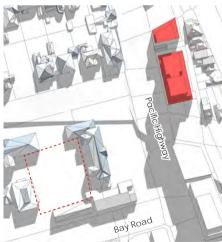
11 am, 21 June - Existing Condition



09 am, 21 June - Preferred Option



10 am, 21 June - Preferred Option

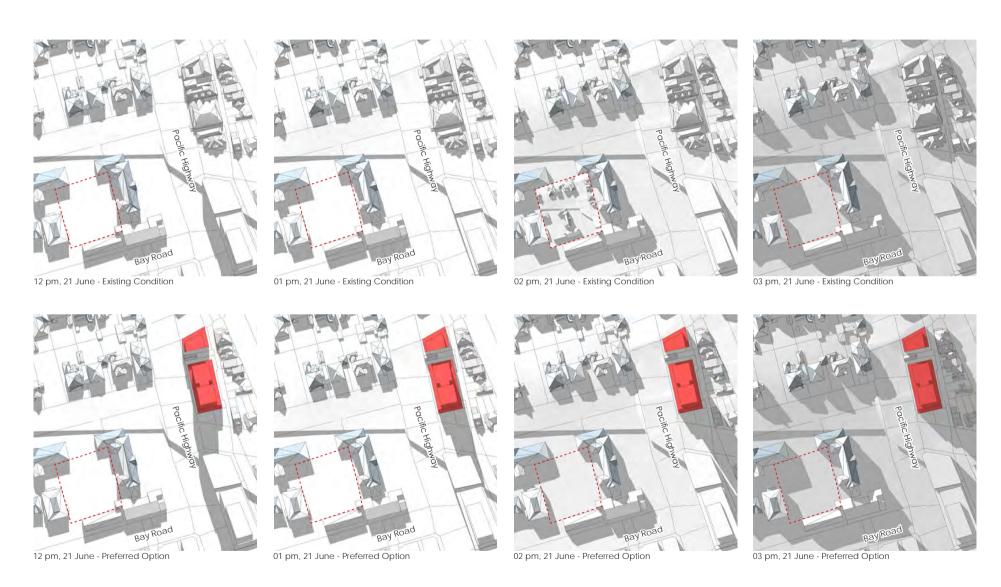


11 am, 21 June - Preferred Option

Proposed built form

[[[]]

School playground



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253-267 Pacific Highway - Preferred Built Form Indicative Photomontage

8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

Civic Heart Design Principles

- Reinforce the civic function of the character area.
- Enhance the low scale, fine urban grain by maintaining a three storey height limit, and reinforcing the pedestrian friendly intimate character of the area.
- Explore opportunities to refresh the community facilities and public domain of the area.
- Improve the permeability and connectivity of the block by introducing or improving pedestrian links.
- Upgrade the Miller Street streetscape to prioritise pedestrian and cycle access as part of Miller Walk Spine linking St Leonards Park to the North Sydney CBD.

Civic Heart Design Guidelines

- · Maintain the three storey height limit.
- Encourage retail / commercial uses along Ridge Street and Miller Street.
- Ridge Street, McLaren Street and Miller Street should be activated by providing appropriate uses at ground level and by improving pedestrian accessibility generally.
- New development should avoid additional overshadowing of Ted Mack Civic Park.
- The area's permeability should be enhanced by facilitating footpath upgrades along Miller Street and upgrading pedestrian links within Ted Mack Civic Park.
- Facilitate upgrades of the community facilities within the area through development contributions, etc.



Figure 104: Precedent image - active street frontage (Source: Google)





Key Map

8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

Victoria Cross Metro North Portal Design Principles

- Utilise B4 Mixed Use zoning to promote commercial offering and living and working opportunities.
- Strengthen the area's permeability by exploring new mid-block pedestrian links.
- Encourage the nighttime economy around the Victoria Cross Metro North Portal.
- Focus taller buildings at the future Victoria Cross Metro North Portal and step down to adjacent lower scale areas.
- Incorporate new approved developments of up to 28 storeys in height.
- Explore opportunity for public plaza space at the Metro portal entry.
- Avoid any additional overshadowing of the Ward Street precinct mid-block public square.



Victoria Cross Metro North Portal Design Guidelines

- The maximum building height in the area will be 28 storeys on the site at the corner McLaren Street and Walker Street. The tower above the Metro station at the corner of Miller Street and McLaren Street will be a maximum of 24 storeys.
- Buildings to the north of the character area will be limited to 2 or 3 storeys to transition to St Leonards Park.
- New development should not cast any additional shadow on the proposed Ward Street Precinct plazas.
- Provide a pedestrian link between Elliot Street and McLaren Street with a minimum width of 10m.
- Provide active uses (retail / commercial) on the ground level to activate Miller Street, McLaren Street, Walker Street, Elliot Street and the future pedestrian link.
- New development to provide a two storey street wall height with upper levels setback to emphasise the street wall and mitigate the scale of the tower residential. A minimum of 3m upper level setback is to be provided.
- Non-residential uses are to be maximised in the area to promote the proximity of living and working. Token mixed-use, with minimal retail spaces on ground level is not supported.
- Provide a new public plaza on the Metro site at the corner of Miller Street and McLaren Street.

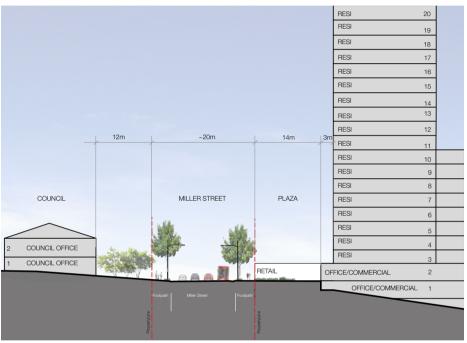


Figure 105: Indicative Miller Street Section



Key Map

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Figure 106: Site Plan

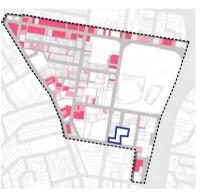


Figure 107: Key Plan

Key

The Study Area

Site boundary

Potential building envelope

Plaza / shared link

Existing buildings

Site Specific Testing - McLaren Street Sites

Concept designs for two adjoining sites have been developed. Site 1 is the Victoria Cross Metro Station North Portal site, and Site 2 is the 54 McLaren Street site. Both sites are in the Victoria Cross Metro North Portal character area.

A maximum 24 storey tower is proposed on Site 1. This is comparable to the recently approved development at 168 Walker Street and the building heights of the Ward Street Precinct. An 18 storey tower is proposed on Site 2 compared with the 15 storey (without parapet and plant room) existing building on the site. It provides a better contextual fit by providing adequate setback distances and separations. A through site link of minimum 10m width is proposed linking Elliot Street and St Leonards Park to McLaren Street and south to the network of pedestrian laneways and new plazas within the Ward Street Precinct. An urban plaza is proposed at the entry to the Metro to provide additional public space to ease pedestrian flows and to provide a meeting place for travellers.

A two storey podium and street wall will accommodate retail / commercial uses, activating the new entry plaza, the McLaren Street frontage and the through site link. Secondary setbacks above the podium are proposed to emphasis the street wall height and to mitigate the scale of the towers.

Setbacks to the common boundaries are proposed to provide a transition in scale and to introduce a landscape buffer to adjacent lower scale properties.

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Total GFA FSR

19,592 m² 4.6:1

8.0 PREFERRED PLANNING STRATEGY



	Site Area	l vil
100	Site Area	LvI
	4,241m²	1-2
	4,241111	3-24
		Tota GB/
	Site 2	
	Site Area	LvI
	2 5252	1-2
A SECTION OF THE PERSON OF THE	2.535m ²	-

Site 1

Site 2									
		Commercial			Residential				
		GBA	Efficiency	GFA	GBA	Efficiency	GFA	-	
Site Area	Lvl	(m²)		(m²)	(m²)		(m²)	Total GFA	FSR
	1-2	2,316	85%	1,969	-	-	-		
2,535m²	3-18	-	-	-	10,656	75%	7,992	9,961 m²	3.9:1
	Total GBA	12,972	m²						

Residential

Efficiency

75%

GFA

(m²)

15,607

Commercial

Efficiency

GBA (m²)

3-24

Total GBA

4,688 85%

25,498 m²

GFA

(m²)

3,985

GBA

(m²)

20,810

Figure 108: 3D view

Site Specific Study -McLaren Street Sites Shadow Testing

The shadow testing indicates that the proposed built form envelope on the site will not cast any additional shadows to the Ward Street Precinct mid-block plazas in mid-winter.

A high-level overshadowing study has indicated that the proposed built form massing will overshadow the properties along McLaren Street to the south of the sites. However, the majority of the units can still receive more than two hours solar access in their living spaces in midwinter, which complies with the ADG requirements.

This high level built form massing study presents an adequate bulk and scale in relation to the location and context. It also presents an acceptable amenity outcome to the surrounding properties; however, the built forms should be further refined and tested to ensure an optimised solar amenity of the adjacent residential properties can be achieved and no additional shadows to the Ward Street Precinct plazas are generated.

> Proposed built form Approved built form

Ward Street Precinct plazas



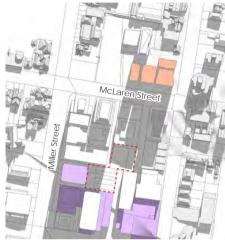




09 am, 21 June - Existing Condition



10 am, 21 June - Existing Condition



11 am, 21 June - Existing Condition



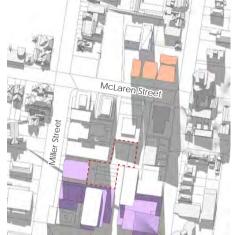
10 am, 21 June - Preferred Option

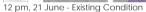


11 am, 21 June - Preferred Option

CONYBEARE MORRISON

Key







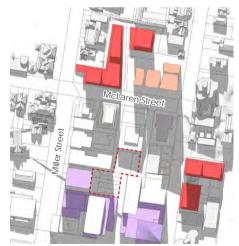
01 pm, 21 June - Existing Condition



02 pm, 21 June - Existing Condition



03 pm, 21 June - Existing Condition



12 pm, 21 June - Preferred Option



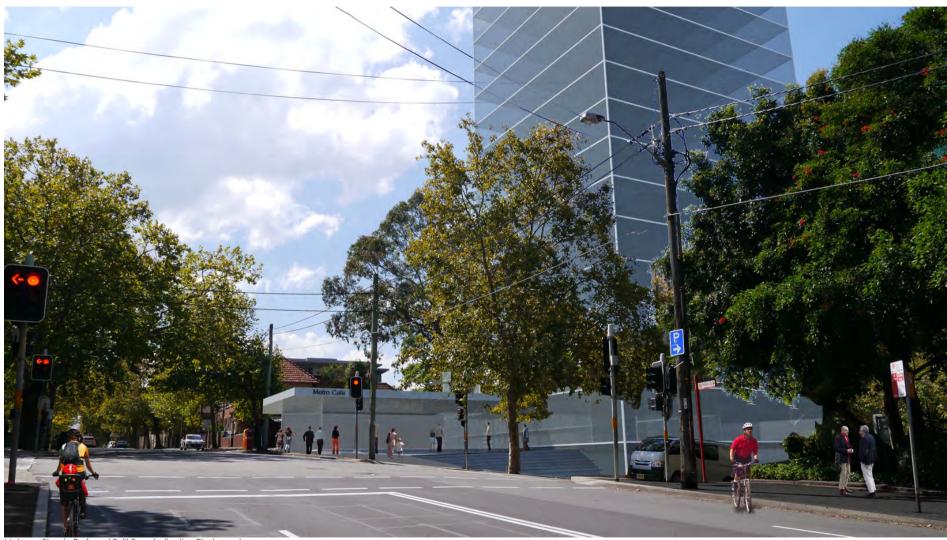
01 pm, 21 June - Preferred Option



02 pm, 21 June - Preferred Option



03 pm, 21 June - Preferred Option



McLaren Street - Preferred Built Form Indicative Photomontage

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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

Ridge Street Park Transition Design **Principles**

- Maintain the area's low scale and village character.
- Protect heritage items and enhance the streetscapes of Heritage Conservation Areas.
- · Limit building heights to a maximum of three storevs.

Ridge Street Park Transition Design Guidelines

- Maintain the three storey maximum building height control.
- Adaptive reuse or additions to a heritage item or new building within a HCA is subject to heritage assessment and expert advise.
- New development within or adjacent to a heritage item or HCA should contribute positively to the heritage values and character of the area.
- New development within or adjacent to a heritage item or HCA should refer to Design in Context - Guidelines for Infill Development in the Historic Environment by the former NSW Heritage Office (now part of the DPIE).
- · Development of educational campuses to accommodate future growth is supported. The extent of redevelopment should be assessed on a case by case basis.
- The development of educational campuses should demonstrate that proposals improve the interface with the surrounding neighbourhood and should provide community benefits including enabling public access to facilities.
- Views to the war memorial in St Leonards Park should not be obstructed and opportunities to improve the vista to and from the park, along Walker Street, should be souaht.
- Improve the interface of the Walker/ Ridge Street Heritage Conservation Area boundary with St Leonards Park.



Figure 110: View to the war memorial in St Leonards Park (Source: Google)



Key Map



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8.9 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

CBD Edge Transition Design Principles

- Encourage mixed use development in the area, that incorporates employment generating uses.
- New developments are to adopt view sharing principles; stepping down from 20 storeys to 8 storeys along the Warringah Freeway edge of the CBD.
- No additional overshadowing of Doris Fitton Park on the south side of Berry Street is permitted.
- Improve the Freeway edge condition by providing a pedestrian and cycle link and additional public places and landscaping.

CBD Edge Transition Design Guidelines

- Increase the maximum building height between 8 and 20 storeys across the character area, with buildings increasing in height towards the existing North Sydney CBD in the south.
- New development is not to cast any additional shadows on Doris Fitton Park in mid-winter.
- Provide active uses (retail / commercial) on the ground level to activate Walker Street.
- New development is to provide a two storey street wall height with upper levels setback to emphasis the street wall. A minimum 3m upper level setback is to be provided.
- Non-residential uses are to be maximised in the character area to promote living and working arrangements in proximity. Token mixed-use with minimal retail spaces on ground level is not supported.
- The Hampden Street houses are to be retained.
- New buildings should be designed to be respectful of heritage items and HCAs in regard to bulk, scale and aesthetics.
- New development within or adjacent to a heritage item or HCA should contribute positively to the heritage value and character of the area.
- New development within or adjacent to a heritage item or HCA should refer to Design in Context - Guidelines for Infill Development in the Historic Environment by the former NSW Heritage Office (now part of the DPIE).

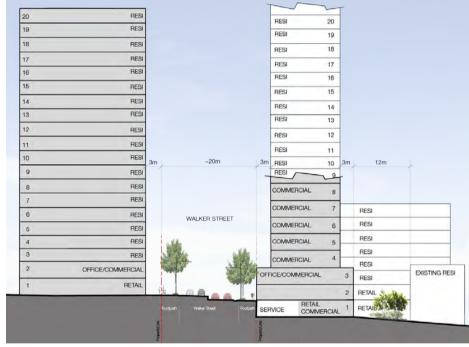


Figure 111: Indicative Walker Street Section



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Key Map



Figure 112: Site Plan

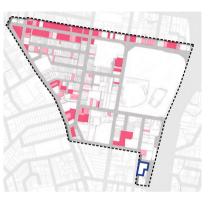


Figure 113: Key Plan



SITE SPECIFIC STUDY

173-179 Walker Street & 11-17 Hampden Street

The potential consolidated site is on the east side of Walker Street in the CBD Edge Transition character area. The site has two street frontages - Walker Street and Hampden Street. The site overlooks the Warringah Freeway.

A two storey retail / commercial podium is proposed to respect the scale of the heritage terraces along Hampden Street and the low scale buildings on the west side of Walker Street. The west part of the site has a maximum height of 20 storeys. whilst the east part is eight storeys. This step in height provides a transition from the taller buildings of the CBD in the Ward Street Precinct to the Warringah Freeway edge. A 20 storey tower is proposed on the Walker Street frontage, which is setback from the podium to reduce the scale impact to the heritage items. The height is comparable to other buildings along the street. The location and built form of the tower may need to be adjusted to maximise view sharing with other residential buildings on the west side to the street. An eight storey commercial building is proposed on the south part of the site. This will break down the built form and open up a visual corridor through the site to improve view sharing as well as streetscape views. The commercial building will provide jobs close to the future Metro station and contribute to the function of the North Sydney CBD. It will also activate Walker Street and provide living and working opportunities in convenient proximity.

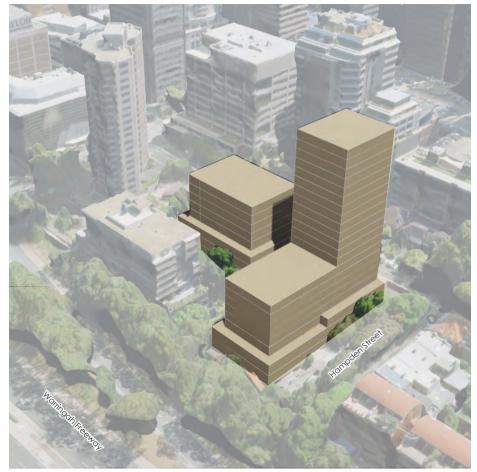


Figure 114: 3D view

Setbacks to adjacent properties are nominated to ensure landscaping and general amenity can be achieved. The setbacks to the common boundaries to the east will establish opportunity for deep soil planting and establish a buffer to the Freeway. A visual connection is made between the street and the landscaped courtyard in the mid-block.

Although this high level built form massing study presents an adequate contextual fit, it should be further refined and amended addressing potential heritage issues, including:

 The relationship with the Hampden Neighbourhood Locality Area character outlined in the North Sydney

- Develop Control Plan 2013.
- Plantings will need to be considered in any green spaces so that they are appropriate for the setting of the streetscape.
- The impact of overshadowing on heritage items and Whaling Road HCA will need to be better understood.
- Heritage interpretation will need to be considered on the public and communal open spaces.
- A comprehensive Visual Impact
 Assessment (VIA) is needed to look at
 the development from neighbouring
 HCAs and heritage items, including:
 - Vista along Walker Street to southern part of the North Sydney CBD
 - Views of Kirribilli and the Harbour from Walker Street. In particular, the heritage listed houses on Walker Street are identified as having 'harbour views'.

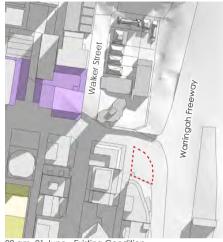
173-179 Walker Street & 11-17 Hampden Street Site										
		Commercial			Residential					
		GBA	Efficiency	GFA	GBA	Efficiency	GFA			
Site Area	Lvl	(m ²)		(m ²)	(m²)		(m²)	Total GFA	FSR	
3,949m²	1-8 (A)	6,292	85%	5,348	-	-	-	15,953 m²	4.0:1	
	1-2 (B)	2,880	85%	2,448	-	-	-			
	3-20 (B)	-	-	-	10,876	75%	8,157	13,733 111	1.0.1	
	Total GBA	20,048	3 m²	1	I		1			

Site Specific Study -173-179 Walker Street & 11-17 Hampden Street Shadow Testing

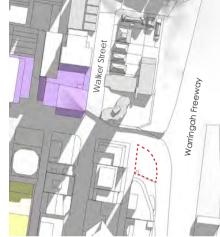
The shadow testing indicates that the proposed built form envelope on the site would not cast any additional shadows to Doris Fitton Park in mid-winter.

A high-level overshadowing study has indicated that the proposed built form massing will overshadow the properties to the south of the site. However, the majority of the units can still receive more than two hours solar access in their living spaces in mid-winter, which complies with the ADG requirements.

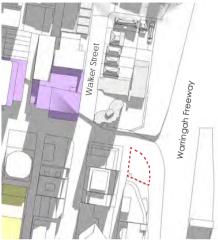
This high level built form massing study presents an acceptable amenity outcome to the surrounding properties; however, the built forms should be further refined and tested to ensure an optimised solar amenity of the adjacent residential properties can be achieved and view sharing principles are met.



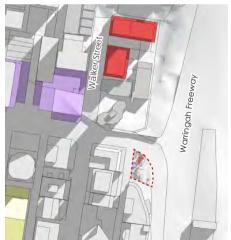
09 am, 21 June - Existing Condition



10 am, 21 June - Existing Condition



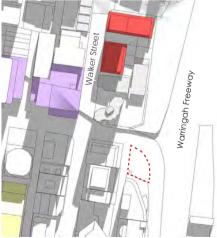
11 am, 21 June - Existing Condition



09 am, 21 June - Preferred Option



10 am, 21 June - Preferred Option



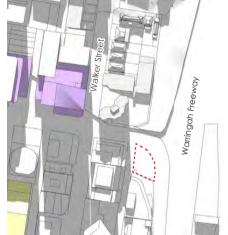
11 am. 21 June - Preferred Option

Proposed built form

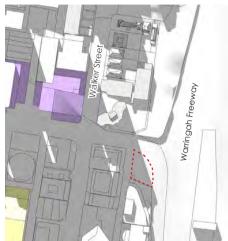
Approved built form

Ward Street Precinct built form

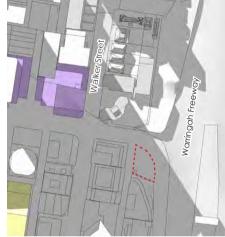
Doris Fitton Park







01 pm, 21 June - Existing Condition



02 pm, 21 June - Existing Condition



03 pm, 21 June - Existing Condition



12 pm, 21 June - Preferred Option



01 pm, 21 June - Preferred Option



02 pm, 21 June - Preferred Option



03 pm, 21 June - Preferred Option



173-179 Walker Street & 11-17 Hampden Street - Preferred Built Form Indicative Photomontag

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8.3 DESIRED FUTURE CHARACTER AREA-BY-AREA (CONTINUED)

The recommendation of this study is that the current development controls for Falcon Street are retained in the short term.

It is recommended that a detailed Urban Design Study is undertaken of the Falcon Street 'spine' to review the zoning, height and density of properties; in light of the potential to become a Mixed Use zone after the impacts and outcomes of the WHTBL and the St Leonards, Crows Nest Planned Precinct are known.

Falcon Street West Long Term Vision

- Rezone to B4 Mixed Use to provide local business and job opportunities, and to bring vitality to the streetscape.
- Enable six storey mixed use buildings along this section of the Falcon Street 'spine'.
- Transform Falcon Street to an urban street, with improved pedestrian amenity and streetscape.
- Widen footpaths by introducing a 1.5m setback to both sides of Falcon Street
- Provide built form and scale transitions to nearby low scale areas, heritage items and conservation areas.

Falcon Street East Long Term Vision

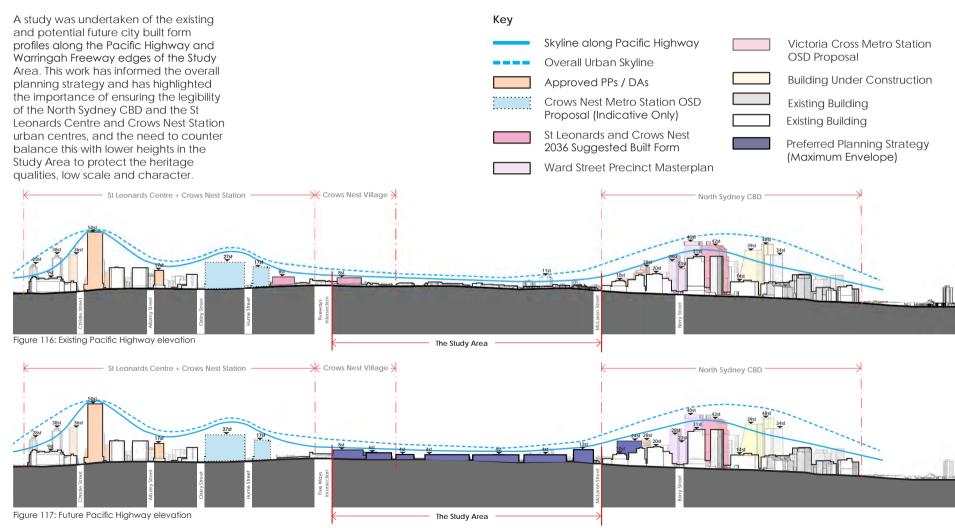
- Rezone this section of the 'spine' to R4 High Density Residential.
- Encourage residential development of four storey height along Falcon Street.
- Improve the school frontage along Falcon Street - consider student housing and / or incubator / research / commercial uses to activate and open up the frontage to the public domain.
- Protect the existing trees and explore opportunities to upgrade the footpaths and secondary landscaping along Falcon Street to a pedestrian friendly and tree lined avenue.



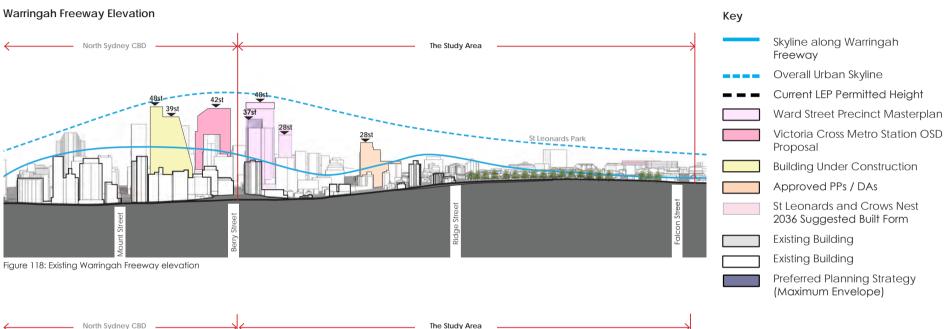


Figure 115: Precedent image - Passeig De St Joan Boulevard, Barcelona by Lola Domenech Architects

Pacific Highway Elevation



108 | CIVIC PRECINCT AND SURROUNDS PLANNING STUDY



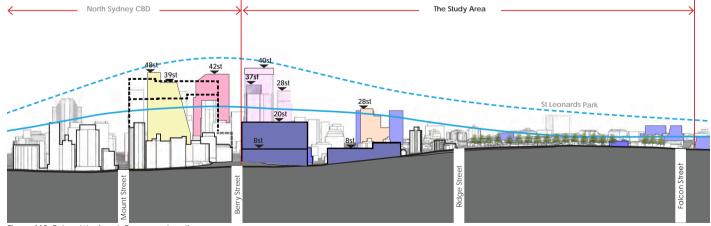
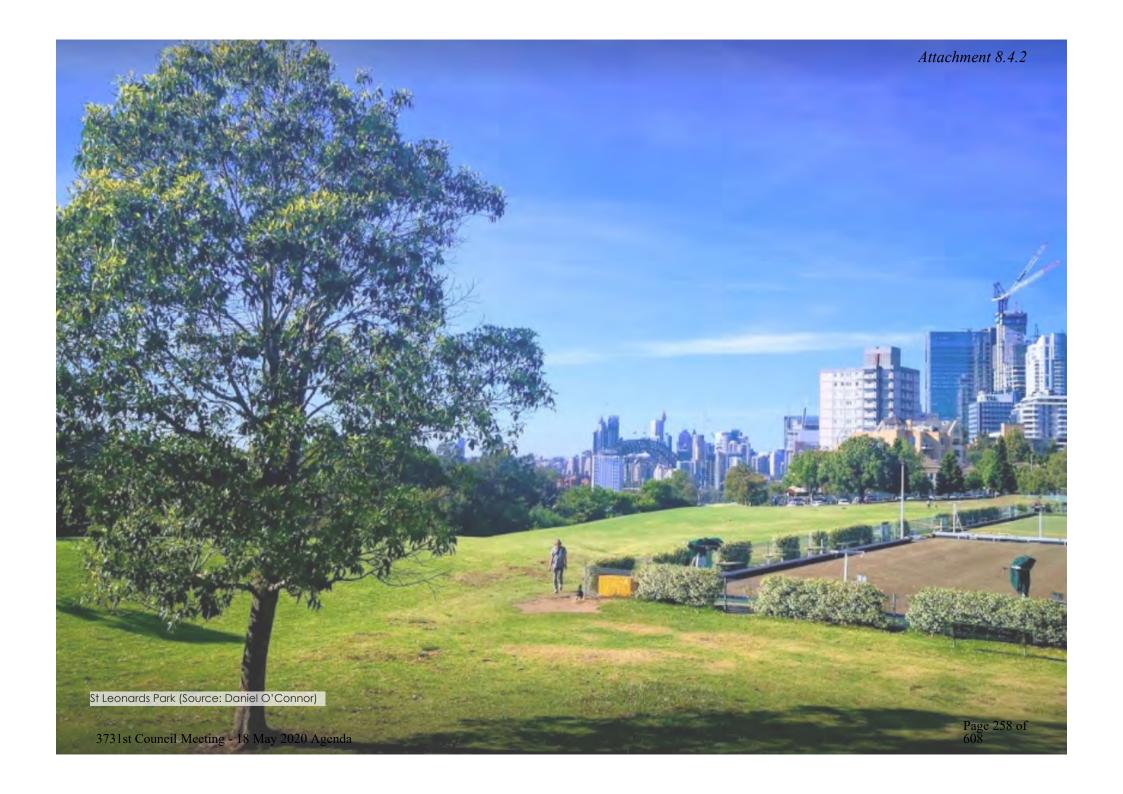


Figure 119: Future Warringah Freeway elevation

CONYBEARE MORRISON



Mouleabara	Toble	The three west desired her efter for	Additional Benefits
Workshop	Table	The three most desired benefits for the table groups	Additional Benefits
One	1	Childcare Community facilities and in particular a Theatre like Angel Place and/ or Chatswood would be transformative	Aged care should be added to childcare
	2	Public domain: More improvements. Also add "major" public domain and include initiatives like putting power lines underground, solar power etc. Community facilities: Include cinemas and activation for the night and weekend.	Affordable housing. Remove - Public Art and Sculpture. Remove "incubator" for Item 9 – Creative/incubator business hub
	3	Community facilities with Cinemas being key Public Domain upgrades Cycleway upgrades and facilities	
Two	1	Public Domain (footpath upgrades) Affordable housing	Active streetscape to keep people in North Sydney after business hours rather than CBD
	2	Community facilities – theatres, libraries Public domain upgrades More green space parks, e.g., Cammeray Golf Course. Greenery parks under freeway and developer funds to pay for that.	
	3	Public DomainAffordable housingCar share bays.	

9.1 DESIRABLE PUBLIC BENEFITS

A two step approach was taken to identify the public benefits the community would like to see in the Study Area.

Nine public benefits were identified by consulting with North Sydney Council and the project team. These public benefits were then presented to the community at two workshops (refer to Chapter 6) to gather feedback. The attendees were asked to identify the three most desired public benefits and to also identify any other benefits that are not on the list. The initial draft public benefits were:

- 1. Childcare centre
- Community facilities includes community meeting rooms, community centres, libraries, theatre / multi-use spaces, etc
- Public domain upgrades including footpath improvements, tree planting, open space, urban plazas, etc
- 4. Affordable housing provision
- 5. Cycleway upgrades and facilities
- 6. Public art and sculpture
- 7. Playgrounds
- 8. Car share spots / EV charging points
- 9. Creative / incubator business hub

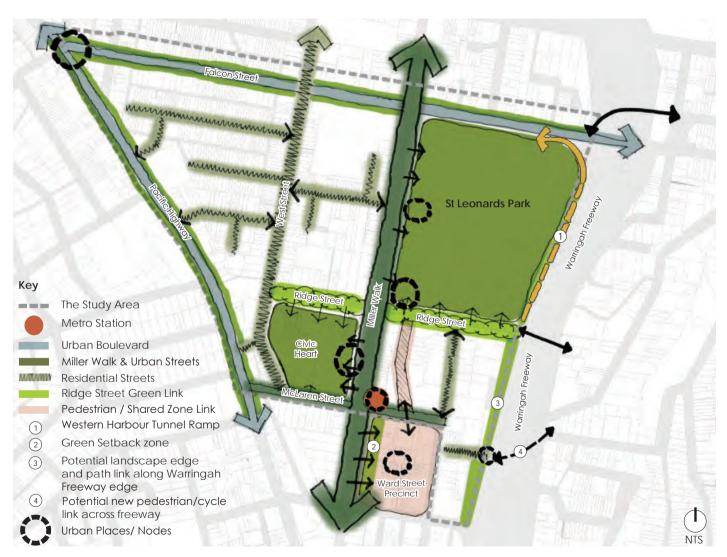
The feedback received during the workshop sessions is summarised in the adjacent table.

Based on the community feedback, the preferred list of public benefits are:

- Childcare centres and aged care facilities
- Community facilities includes theatre / multi-use spaces, community meeting rooms, community centres, libraries, etc
- Public domain upgrades includes footpath improvements, tree planting, underground power lines, open space, urban plazas, etc
- 4. Affordable housing provision
- 5. Cycleway upgrades and facilities
- 6. Public art and sculpture
- 7. Playgrounds
- 8. Car share spots / EV charging points
- 9. Creative business hub

The three public benefits most desired by the community were:

- 1. Community facilities
- 2. Public domain upgrades
- 3. Affordable housing provision



9.2 PUBLIC DOMAIN STRATEGY

The public domain strategy is developed in accordance with the preferred planning strategy for the Study Area and is guided by input received from the community workshops.

The public domain strategy envisions the Falcon Street and the Pacific Highway spines as tree-lined urban boulevards, which provide improved pedestrian amenity.

Miller Street will be the main north-south link which connects the North Sydney CBD, including the Ward Street Precinct, the future Victoria Cross Station / Metro Plaza, the Civic Heart to St Leonards Park. An improved pedestrian environment will be provided along Miller Street, including footpath widening and street tree planting.

Ridge Street will continue to act as a tree-lined east-west green link providing high pedestrian and cycle amenity.

The residential streets in the precinct will be upgraded in terms of landscape treatments and pedestrian and cycle amenity. It will form an attractive treed streetscape.

The precinct's permeability and public domain amenity will be improved with additional open spaces / plazas and through site links and the upgrade of the Warringah Freeway interface.



9.3 PUBLIC DOMAIN PLAN

- Miller Street Footpath and Streetscape Improvement
- Proposed Pedestrian Link
- ③ Proposed Shared Zone
- Proposed Level Crossing
- (5) Upgraded Civic Park
 - Pocket Park with Child's Play Area
- (7) Green Spaces Proposed by Western Harbour Tunnel Upgrades
- (8) St Leonards Park Landscape Masterplan
- West Street Avenue Planting
- (10) Ward Street Precinct Plaza
- Five Ways, Crows Nest Retail and Commercial Centre. Potential for future closure of Willoughby Road.
- (12) Metro Plaza
- North Sydney Oval Southern Entrance Plaza
- (14) St Mary's Plaza/ Garden
- Upgrade pedestrian link (along laneway)
- Pedestrian and cycle link along raised Warringah Freeway edge
- (7) Upgrade pocket park (street closure) and cycle link at west end of Hayberry Street.
- (8) School Campuses Explore potential for through-site links and visually open street frontages.
- (9) Underground car parking. Upgrade of facilities subject to current Council DA design concepts.
- Explore potential for future pedestrian links in the long term.
- **Existing Plaza / Park (private)**
- 22 Miller Street Forecourt

Miller Walk



- Miller Walk North is a continuation of the proposed Miller Place between Berry Street and the Pacific Highway, which is a development of a linear plaza in the North Sydney CBD.
- Allow areas for seating, planting, and encourage activity on the street footpaths.



Passeig De St Joan Boulevard, Barcelona by Lola Domenech Architects



North Terrace, Adelaide by TCL Landscape Architects

Residential Streets



- Add to existing avenue planting in residential streets.
- Additional planting in proposed road blisters will improve pedestrian crossings and amenity.
- Create a continuous green canopy with closed canopy planting along residential streets (including West Street).



Bourke Street Cycleway, Sydney by Group GSA



Bourke Street, Sydney by City of Sydney

Ridge Street



- Ridge Street links St Leonards Park to the residential areas to the west.
- Provide additional planting and widen the footpaths where possible.
- Encourage commercial / retail uses to interface with and spill out to activate the street.

9.4 PUBLIC DOMAIN PRINCIPLES

The desired public domain character is identified for each of the key public domain components. The improvement of the public domain within the precinct should follow the principles and guidelines identified.



David H. Koch Plaza, New York by OLIN Landscape Architects



Bryant Park, New York by OLIN Landscape Architects

Pacific Highway and Falcon Street



- Widen footpaths through a proposed building setback to improve the amenity for pedestrians.
- Provide space for planting and activation of the street frontages.



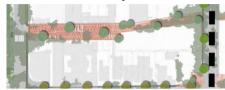
Passeig De St Joan Boulevard, Barcelona by Lola Domenech Architects



Slovenska Boulevard, Slovenia by Dekleva Gregoric Architects

CONYBEARE MORRISON

Metro Block / Laneway



- Create an inviting entry plaza for the Metro Portal.
- Create a pedestrian street / shared zone that connects Elliot Street to McLaren Street.
- Use active building frontages along the street to create a lively, urban pedestrian link with seating and tree planting.



Singapore Waterfront by Context



Mariahilferstrasse, Vienna by Bureau B + B

Civic Heart / Park



The current Civic Heart includes Stanton Library, Council Offices and Ted Mack Civic Park. Upgrade Ted Mac Park in conjunction with Miller Walk. The place is to provide for library users, council staff and fortnightly markets. To achieve this, the following should be implemented:

- Use the Miller Walk design guidelines to ensure consistency for the entirety of Miller Street.
- Continue the Miller Walk treatment into the plaza area which should include outdoor study areas and open spaces and paved areas large enough to house fortnightly markets.
- Provide ample seating and formal lawn for office workers to enjoy in their lunch break.
- Respond to the existing heritage items and public buildings in the precinct including the Council building, heritage facades, the existing fountain and Stanton Library.
- Provide additional trees for shade.
- Provide lighting to allow night time events.



Neue Meile Boblingen, Germany by Bauchplan



Empress Place, Singapore by Context



Campbells Cove, Sydney by Context

CIVIC PRECINCT AND SURROUNDS PLANNING STUDY | 115

Metro Plaza



Metro Plaza is proposed for the corner of McLaren and Miller Streets, and is an extension of Miller Walk. It will provide for people who live and work in the north part of North Sydney. This plaza should be an active space with retail at the ground floor. To achieve this, the following should be implemented:

- Provide several trees in the plaza, to provide shade and green.
- Provide active frontages to the north and west of the plaza to encourage activity and vibrancy in the place.
- Follow the Miller Walk design guidelines to ensure a consistent design language, furniture and materials and finishes palette with the rest of Millers Walk to the south.
- Incorporate engaging and relevant public artwork in the plaza to mark the location, and assist with way finding.



Singapore Waterfront, Singapore by Context

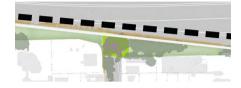


Blooming City, Nieuwegein by Bureau B+B



Campbells Cove, Sydney by Context

Freeway Edge



Provide a pedestrian and cycle connection along the freeway edge, to create a commuting path into the North Sydney CBD. This will allow commuters to avoid busy roads and have a more attractive commute. The following should be implemented:

- Provide a cycle and pedestrian path along the freeway edge connected into the pedestrian paths in the St Leonard Park Masterplan.
- Utilise the same design language as the proposed pedestrian / cycle paths in the St Leonards Park Masterplan.
- Landscape the pedestrian and cycle link; highlighting the natural rock escarpment. Increase vegetation and shade canopy where possible along the freeway edge. This will provide an attractive and safe route and opportunities for views to the bridge and regionally.



Port of Melbourne Bike Path, Melbourne



River Torrens Bridge, Adelaide by Oxigen



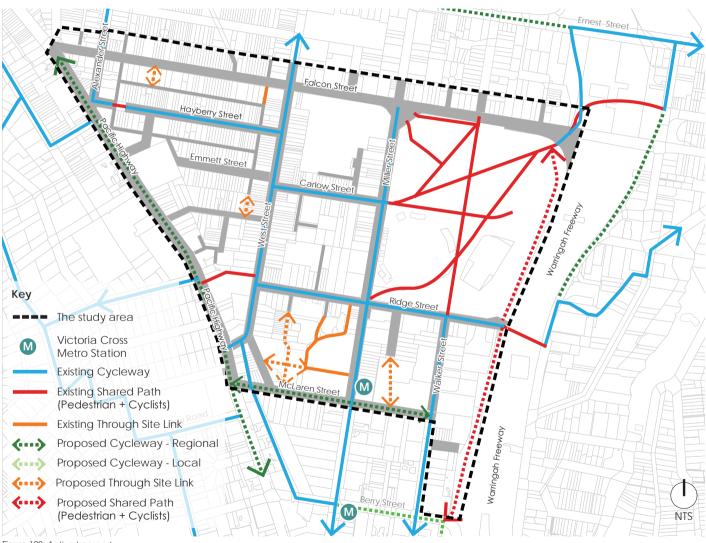
Bicentennial Park Cycle Path, Sydney



9.5 PUBLIC DOMAIN IMPROVEMENTS

The following public domain improvement opportunities are identified in the Public Domain Plan:

- 1. St Leonards Park (refer to St Leonards Park Masterplan)
- 2. Ted Mack Civic Park
- 3. Hayberry Street Reserve
- 4. Hampden Street pocket park
- 5. Victoria Cross North Portal Plaza
- 6. Five Ways plaza
- 7. Falcon Street east
- 8. Falcon Street west
- 9. Alexander Street south
- 10. Pacific Highway north
- 11. Pacific Highway middle
- 12. Pacific Highway south
- 13. McLaren Street west
- 14. McLaren Street east
- 15. Hampden Street
- 16. Walker Street north
- 17. Ridge Street east
- 18. Hamett Street
- 19. Ridge Street west
- 20. Miller Street (Miller Walk)
- 21. Miller Street north (Miller Walk)
- 22. Carlow Street
- 23. West Street south
- 24. West Street north
- 25. Myrtle Street
- 26. Emmett Street
- 27. Hayberry Street
- 28. Ridge Street footbridge upgrade
- 29. Upgrade to pedestrian Link
- 30. Warringah Freeway edge south link
- 31. Hamett and McLaren Street link
- 32. Civic Park link
- 33. Falcon Street link (proposed by the Planning Proposal for 57 Falcon Street)



9.6 ACTIVE TRANSPORT

Encourage active transport, which prioritise pedestrian and cyclist access within an area. This will bring improved health outcomes, assist in reducing traffic congestion, car parking demand and realise a more sustainable North Sydney LGA.

The active transport strategy makes reference to the North Sydney Integrated Cycling Strategy and North Sydney Public Domain Strategy. The key active transport principles are to:

- 1. Encourage walking and cycling by improving the amenity, connectively and safety for pedestrian and cyclists.
- 2. Improve existing cycleway conditions and introduce new cycleways to address demand and to complete gaps in the access network.
- 3. Improve the study area's permeability by providing new pedestrian through site links when new development or upgrades occur. Refer to Figure 120.
- 4. Provide bike parking and storage facilities at key locations in the public domain and within new development.

Figure 120: Active transport

APPENDIX 1

COMMUNITY ENGAGEMENT STRATEGY

COMMUNITY ENGAGEMENT STRATEGY

Northern CBD Planning Study



Revised October 2019

Councils are required under the *Local Government Act 1993* to inform the community of particular issues that potentially affect their way of life. North Sydney Council is committed both in principle and in practice, to engaging on matters affecting the North Sydney community.

The purpose of this project-specific Community Engagement Strategy is to outline the ways stakeholders can be involved in the decision-making process. Community engagement opportunities will be provided across a range of 'engagement' levels.

1. Introduction

This Community Engagement Strategy outlines the steps Council will take to engage the community in the *Northern CBD Planning Study*. Council is committed to engaging the community to ensure that the community are informed of the opportunity to have their say to help shape the final study.

1.1 Council's Community Engagement Protocol

This strategy has been prepared in accordance with Council's *Community Engagement Protocol*. The Protocol is used to determine the level of 'level(s) of impact' applicable to this project/decision (proposal). This proposal has been determined as:

LEVEL OF IMPACT	LEVEL OF ENGAGEMENT
High-Local	Inform and Consult

Council used the framework shown below in Table 1.1 to select the most appropriate 'level(s) of engagement' for this proposal to ensure an appropriate range of engagement 'levels' and methods were offered:

LEVEL	DESCRIPTION
Inform	Providing balanced and objective information to help the community understand
	problems, alternatives, opportunities and/or solutions
Consult	Obtain public feedback on alternatives and/or decisions
Involve	Work directly with the community throughout the process to ensure that public
	concerns and aspirations are consistently understood and considered
Collaborate	Partner with the public in each aspect of the decision including the development of
	alternatives and identification of the preferred solution

Table 1.1 Derived from the IAP2 Public Participation Spectrum

2. Background

The Greater Sydney Commission is responsible for coordinating metropolitan planning for the Greater Sydney Region. It released the Regional Plan for the Sydney Metropolitan area titled *A Metropolis of Three Cities*, which is supported by the more detailed *North District*

1

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Plan (2018) which applies to North Sydney Council and all other councils within the North District region.

Local Environmental Plans (LEPs) are defined by the NSW Department of Planning Industry and Environment as:

LEPs guide planning decisions for local government areas. They do this through zoning and development controls, which provide a framework for the way land can be used. LEPs are the main planning tool to shape the future of communities and also ensure local development is done appropriately.¹

All Sydney metropolitan councils are required to update their LEPs and associated planning instruments so they align with the *North District Plan*. Councils have three years to complete this. The NSW Government is offering financial assistance to support strategic planning to enable the LEP reviews to occur within two years instead of three.

On 28 May 2018 Council resolved:

- 1. THAT Council lodge an application for LEP Review Funding to accelerate strategic planning projects that align the North Sydney LEP with the North District Plan.
- 2. THAT the application for LEP Review Funding be generally consistent with the projects identified in this report.

Amongst other projects, Council's grant funding application included the preparation and delivery of the *Northern CBD Planning Study*.

On 15 October 2018, Council was formally advised that it had been offered this grant funding. On 29 October 2018 Council resolved:

1. THAT Council accept the Accelerated LEP Review Grant Funding in principle as outlined in Attachment 2 subject to agreement with the Department of Planning and Environment on timeframes and delivery dates.

The arrival of the new northern Metro station portal has stimulated development interest and the *Northern CBD Planning Study* seeks to:

- explore the capacity of the North CBD Precinct to accommodate managed development growth through the management of ad hoc Planning Proposals;
- identify and where possible retain the urban values of the precinct including its heritage and character and balance these with any identified growth within the precinct:
- achieve the objectives and actions identified in the North District Plan;
- identify possible improvements in the public domain, including pedestrian linkages, wayfinding, public open space and improved public domain amenity consistent with the broad objectives and directions identified in the draft *North Sydney CBD Public Domain Strategy* (2018); and

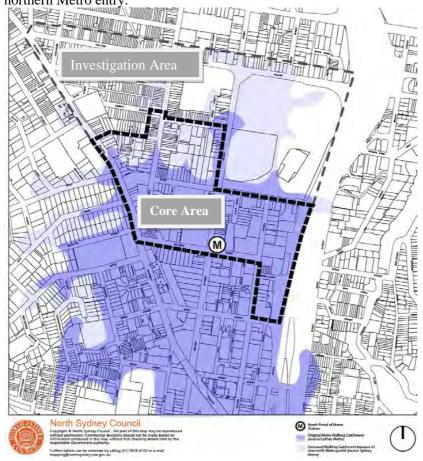
2

¹ https://www.planning.nsw.gov.au/Plans-for-your-area/Local-Planning-and-Zoning accessed 4 March 2019

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- explore possibilities to achieve public benefit within or on the periphery of the precinct as a result of and linked to redevelopment
- get input to the final plan though submissions as part of the exhibition
- encompass early input to:
 - confirm the themes derived from the initial consultation,
 - the planning principles for the study
 - identify density issues and locations of potential density
 - determine the type of public benefit that might result
 - delineate 'local places' in the precinct and define the characteristics for these.

The core study area is the unmasked area below, the study will also consider a broader investigation area which is the lightly masked area approximately bounded by Falcon St, the Pacific Highway and Warringah Freeway. The reason for including the investigation area is because a pedestrian analysis generated by Sydney Metro identified parts of the investigation area as having increased pedestrian access to the Metro station as a result of the proposed northern Metro entry.



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Northern CBD Planning Study - Community Engagement Strategy

3. Community Engagement Strategy

3.1 Who are our community stakeholders?

The Community Engagement Strategy identifies the following groups to engage with in the local community:

- Educational institutes/schools Australian Catholic University, Cammeraygal School, North Sydney Demonstration School, Marist College, Monte Sant' Angelo Mercy College, North Sydney Boys High School, North Sydney Girls High School, St Marys Primary School, Wenona School
- Faith Community Groups St Thomas Anglican Church, St Marys Catholic Church, Jesuit Mission Office, The Hare Krishna Temple of Sydney
- Hotels/Accommodation Dalziel Lodge (Emerald City Hotels), Falcon Lodge,
 McLaren Hotel, North Shore Hotel, North Sydney Hotel, Rydges North Sydney
- Community Groups Precinct Committees including: Stanton, Registry, Hayberry, Holtermann, Edward, Wollstonecraft
- Transport agencies Transport for NSW, Sydney Buses, Sydney Metro
- Major institutional land owners
- Property owners
- Workers
- Residents
- Retailers, restaurants including The Greens

3.2 Key Communication Messages

The arrival of Sydney Metro in 2024 and the Northern CBD precinct's proximity to the North Sydney CBD will place significant development pressure on this area, which this study seeks to manage. The area will benefit from increased accessibility as a result of the arrival of Sydney Metro in 2024; there will be an entry to Victoria Cross Station located at the corner of McLaren and Miller Streets, North Sydney.

The Planning Study will:

- align Council's LEP with the Greater Sydney Commission North District Plan;
- identify and retain the urban values of the precinct, e.g. balance heritage and character with any identified growth
- explore whether the northern CBD area can accommodate managed development growth and manage ad-hoc planning proposals spurred by the new northern Sydney Metro entry portal.
- identify possible public domain improvements e.g. pedestrian links, wayfinding, public open space; consistent with the broad objectives and directions identified in the draft North Sydney CBD Public Domain Strategy (2018).
- explore possibilities to achieve public benefit within or on the periphery of the precinct related to any redevelopment that may be identified

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3.3 Timetable

Community and stakeholder engagement will occur at various times during this Eighteenmonth period. The key project development phases are outlined in the following table:

Stage	Timing
1. Initial Consultation	July to August 2019
2. Research and Analysis	October to November 2019
3. Research, Analysis & Consultation	November – December 2019
4. Documentation of Planning Study	October 2019 to April 2020
5. Public Exhibition	May 2020 to June 2020

3.3.1 Stage 1 - Preliminary Review of Existing Controls

This stage was primarily concerned with understanding and prioritising community and stakeholder's aspirations for the study area. A Project Control Group (PCG) was established internally to collaborate through all stages of the project.

An awareness and promotion campaign informed stakeholders of the consultation opportunities, in an effort to encourage the greatest level of participation possible in Stage 3. Preliminary session/s were held with key stakeholders, including major land owners, Precinct Committees and other interested group. Online engagement and hard copy avenues will identify issues and opportunities and inform, vision and character statements.

Preliminary Principles arising from the consultation conducted to date are:

- Heritage is conserved and celebrated
- Acknowledge and where possible retain the essential characteristics of the precinct parks, tree canopy, significant public spaces, fine grain streets and solar access
- Explore the notion of the "village feel" and how it can be retained and enhanced in the context of the arrival of Metro
- Respect and acknowledge the endorsed St Leonards Park Masterplan
- Extend the pedestrian network/corridor through the Metro north site
- In the context of Metro, identify potential development sites and manage their scale and impact consistent with the desired future character of the precinct.
- Any increases in development density will be required to provide a public benefit directly to the precinct
- Activity?
- Employment?

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Met hod	Target Stakeholders	Engagement Level	Purpose
Webpage	All	Inform	Provide information
Media Release			about the project and
Advert			direct people to how they
Poster/Flyer ²			can have a say e.g.
Email			consultation
Letters			opportunities
Social Media			
(Facebook/Instagram/			
Twitter/Linked In)			
e-Newsletters	Existing subscribers:		
	North Sydney E-news		
	Precinct E-news		
	Business E-news		
Precinct Committees	All Active Precinct	Inform	Encourage Precinct
	Committees		Committees to promote
			engagement to their
			members.
Interviews	Major Land Owners	Inform/Consult	Face-to-face meetings to
	(including schools)		ascertain land owners
			own strategic planning
			for next 10-15 years.
Feedback Form	All	Consult	Ascertain priorities -
			online and hard copy
			formats
Mapping/Places tool	All	Consult	Ascertain priorities via
			visual map - 'pins'
			categorised with same
			themes as Feedback
			Form

Note: In accordance with Council's Community Engagement Framework described on page 1, the 'level of engagement' per engagement method is indicated.

3.3.2 Stage 2 - Research and Analysis

Between October and November 2019, Council and its appointed consultants will analyse feedback received and formulate information for presentation as part of the workshops in Stage 3. Participants and key stakeholders will be informed of progress via a project specific E-news as well as on Council's website/yoursay project page.

3.3.3 Stage 3 - Research, Analysis & Consultation

Between November and December 2019, Council will offer various methods for stakeholders to participate in Phase 3, including two to three face-to-face workshops and online communication and feedback, allowing the community to participate at times that best suit their needs and commitments. The purpose of this phase is to get early input to:

- confirm the themes derived from the initial consultation,
- the proposed planning principles for the study
- density issues and locations of potential density

² Same artwork to be used for TV in Customer Service Centre

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- the type of public benefit that might result
- delineate the local places in the precinct and define the characteristics for these.

The inform and consult methods already listed will continue with the following additional methods to be used:

Method	Target Stakeholders	Engagement Level	Purpose
Workshops	Open but with specific	Consult / Involve	Provide overview of the study,
	invitations to		constraints and the timeline
	representatives of		and get early input to the
	Waverton, Stanton,		Master Plan. In particular:
	Hayberry, Edward and		 what people said in
	Wollstonecraft Precinct		initial consultation.
	Committees. Specific		 Purpose of the
	invitations also		planning study
	potentially to Schools,		 Risks of "do nothing"
	the community centre and selected submitters		- how the planning
	and selected submitters		system works with the
			two proposals and
			relationship to Metro.
			Outline of preliminary
			planning principles to
			guide the planning
			study
			 Good examples of
			accommodation of
			carefully managed
			increases in density
			 public benefits to be
			sought as part of
			increase in density.
Updated HQ/ Your	All – all submitters are	Inform/Consult	-
Say North Sydney	provided an update	IIIIOIII/Consuit	Promote the opportunity to have input on-line on the same
Say North Sydney	provided an update		matters presented at the
			workshops.
			Have the Council report from
			the initial consultation
			available.
			Advertise and receive
			registrations for the
			workshops
Interviews / Face to	Major Land Owners and	Inform/Consult	Council and the CM+ team to
Face meetings	proposal applicants		check-in with key
			stakeholders as required.

3.3.4 Stage 4 - Documentation of study/finalising

This stage involves documenting and drawing of the results so far. A report of the outcomes of the consultation to date and the expressed community's aspirations will be formulated. All

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documentation; reports and presentation materials will be finalised. The preferred scenario will be considered by Council and endorsed for public exhibition.

3.3.5 Stage 5 - Finalising Design(s)/Public Exhibition

Phase 5 will take place between May to June 2020. This Phase involves the public exhibition for a minimum of 28 days and currently planned to be six weeks, whereby stakeholders will have the opportunity to make a written submission during this final stage of the project. Inform methods as per Phase 1 will be used to inform stakeholders of the opportunity to make a written submission during the final stage of this project.

Method	Target Stakeholders	Engagement Level	Purpose
Briefings	All - with targeted invitations	Inform/ Consult	Give understanding of the proposed Master Plan and how the community input has been used in its finalisation.
Updated HQ/ Your Say North Sydney	All – all early submitters (to Stage one) and those involved in the consultation in Stage 3 are provided an update and invited to participate	Inform/Consult	Give understanding of the proposed Master Plan and how the community input has been used in its finalisation. Promote the opportunity to have a say; seek feedback and make submissions.
Drop in kiosk (North Sydney Produce Markets)	All	Inform/Consult	Promote the opportunity to have a say; seek feedback on the draft Planning Study
Advertisement and promotion through social media etc	All	Inform/ Consult	Promote the opportunity to have a say; seek feedback and make submissions.
Written Submissions	All	Consult	Free form feedback accepted by email, posted letter or online form

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4. Opportunity Cost/Rationale

Engaging the community in this proposal may entail financial costs to Council to achieve a high-quality engagement process. If the process is robust, community ownership of the decisions made will ensure efficient outcomes. Insufficient or poor-quality engagement can result in poor long-term decisions requiring further resources to rectify. The aim of a high-quality community engagement process is to make sustainable decisions. The engagement process will help Council staff and/or Councillors to understand the related recommendations rationale.

5. Further Information

For further information, contact Pedro Garcia, Senior Strategic Planner - Urban Design, City Strategy Division:

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APPENDIX 2

NORTH SYDNEY PLANNING STUDY PRELIMINARY HERITAGE ASSESSMENT



North Sydney Planning Study Preliminary Heritage Assessment

Prepared for Conybeare Morrison International Pty Ltd

January 2020—Final

Sydney Melbourne Brisbane Perth

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Introduction

1.1 Project Brief

Conybeare Morrison International Pty Ltd (Conybeare Morrison) engaged Extent Heritage Pty Ltd (Extent Heritage) to undertake a Preliminary Heritage Assessment for the North Sydney Civic Precinct and Surrounds. This report presents a high-level overview of the existing heritage context of the site, provides a broad outline of key heritage constraints and opportunities, and preliminary heritage-specific recommendations for future development.

1.2 Site Identification

The area of assessment ('Study Area') is located to the north of the North Sydney CBD and is referred to as the 'North Sydney Civic Precinct and Surrounds'. The Study Area is bounded by Berry Street to the south and Earnest Lane to the north, including several additional residential and commercial areas and St Leonards Park. For an overview of the Study Area see the below figure.



Figure 1: Road map with Study Area indicated (Source: LPI, Extent Heritage 2019)

1.3 Methodology

This report undertakes an analysis of the site to identify heritage considerations, outlines the potential constraints and makes recommendations as to what mitigations may be offered to



reduce impacts. The analysis is based on information provided within the *Civic Precinct and Surrounds Planning Study*, prepared by Conybeare Morrison in 2019. Desktop research includes a review of the history and recognised heritage significance of items within the Study Area. An overview of the statutory framework applicable with regards to heritage matters is provided.

1.4 Limitations

The historical overview provides sufficient historical background to develop an understanding of the place in order to provide relevant recommendations, however, it is not intended to be an exhaustive history of the site. This report does not consider Aboriginal cultural heritage or archaeology.

The heritage listings search for this report included a search of statutory registers only. Non-statutory registers and archives were not investigated as a component of this report.

1.5 Authorship

The following staff members at Extent Heritage have prepared this report:

Lucy Irwin, Heritage Advisor.

Corinne Softley (Senior Heritage Advisor) and Eleanor Banaag (Senior Heritage Advisor) reviewed the document.

1.6 Terminology

The terminology in this report follows definitions presented in the Australia ICOMOS Burra Charter, 2013 (Burra Charter). Article 1 provides the following definitions:

Place means a geographically defined area. It may include elements, objects, spaces and views. Place may have tangible and intangible dimensions.

Cultural significance means aesthetic, historic, scientific, social or spiritual value for past, present or future generations. Cultural significance is embodied in the *place* itself, its *fabric*, setting, use, associations, meanings, records, related places and related objects. Places may have a range of values for different individuals or groups.

Fabric means all the physical material of the place including elements, fixtures, contents and objects.

Conservation means all the processes of looking after a *place* so as to retain its *cultural* significance.

Maintenance means the continuous protective care of a *place*, and its *setting*. Maintenance is to be distinguished from repair which involves *restoration* or *reconstruction*.

Preservation means maintaining a *place* in its existing state and retarding deterioration.



Restoration means returning a *place* to a known earlier state by removing accretions or by reassembling existing elements without the introduction of new material.

Reconstruction means returning the *place* to a known earlier state and is distinguished from *restoration* by the introduction of new material.

Adaptation means changing a place to suit the existing use or a proposed use.

Use means the functions of a *place*, including the activities and traditional and customary practices that may occur at the place or are dependent on the place.

Compatible use means a *use* that respects the *cultural significance* of a *place*. Such a use involves no, or minimal, impact on cultural significance.

Setting means the immediate and extended environment of a *place* that is part of or contributes to its *cultural significance* and distinctive character.

Related place means a place that contributes to the cultural significance of another place.

Related object means an object that contributes to the *cultural significance* of a *place* but is not at the place.

Associations mean the connections that exist between people and a *place*.

Meanings denote what a *place* signifies, indicates, evokes or expresses to people.

The following additional definitions have been provided below:

Within this report, a Heritage Listed Item refers to an item that is listed on either Schedule 5 of the North Sydney Local Environmental Plan 2013 (LEP), on the State Heritage Register or on a State Government Agency's Section 170 Heritage and Conservation Register (S170). These listings provide legal protection for heritage items under the NSW Heritage Act 1977 and the Environmental Planning and Assessment Act 1979. Heritage items can include buildings, places, structures, vegetation or any combination of these elements.

A **Heritage Conservation Area** (HCA) is a precinct recognised for having a particular cohesive significance, often associated with a historic and/or aesthetic character. HCAs can contain individual listed heritage listed items, but also comprise areas in which the relationship between all elements within a HCA is important such the built form, streetscape pattern or landscape features. HCAs are listed on Schedule 5 of North Sydney LEP.

Buildings within a HCA that are individually heritage listed are identified as Contributory, Neutral or Uncharacteristic places within the HCA. **Contributory places** form part of the collective significance of a HCA, being places that contribute towards the overall significance of the precinct. **Neutral items** do not detract from the overall significance of the HCA, nor do they contribute to the significance of the HCA. **Uncharacteristic items** are places that are considered to be intrusive to the character of that place and therefore detract from the significance of the HCA.



2. Historical Context

2.1 Historical Context

The following brief history of North Sydney has been adapted from the Dictionary of Sydney entry for the suburb (authored by Leonie Masson in 2010) and 'We will see a town rising: a history of North Sydney' a published talk by Dr Ian Hoskins in June 2015.

- 1828 A township plan for North Sydney was authored by Thomas Mitchell.
- 1838 The basic road structure of the town centre was established in a 10-chain grid.
 Initially, half-acre building allotments were offered for purchase by application.
- 1843 The St Leonards township was enlarged by the sale of Crown lots. The area bordering St Leonards park developed as an upper middle-class neighbourhood of Victorian and Federation houses on large blocks.
- 1850-1860s A housing boom encouraged the building of cottages, terraces, villas and mansions.
- 1869 The borough of St Leonards was formed to administer the township.



Figure 2: 'Map of that part of the North Shore of Port Jackson which is opposite to Sydney 1828' (Source: Surveyor General; NRS 13859, Maps and Plans 1792-1880, S.801 [SZ467]).



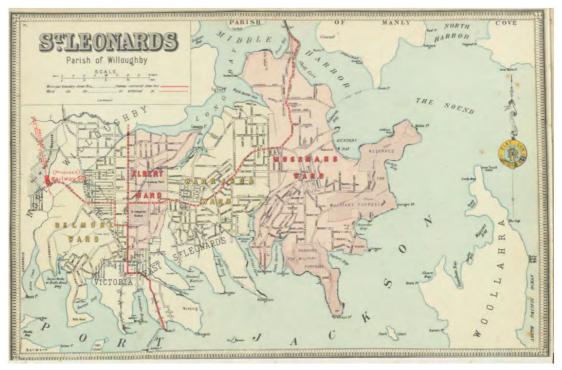


Figure 3: 'St Leonards Parish of Willoughby', This 1880s map shows the Boroughs of the lower North Shore before amalgamation in 1890 (Source: National Library of Australia).

- **1880s** The intersection of Miller and Mount Streets and Lane Cove Road (present day Victoria Cross) was the commercial and civic centre of the town.
- 1926 The town hall was relocated to the heart of middle-class, suburban North Sydney. The Lane Cove Road was extended to the Bradfield Highway and the newly constructed Sydney Harbour Bridge, resulting in the resumption and demolition of an entire street (Junction Street) and the North Sydney Methodist church on the Blue Street intersection.
- 1932 In the wake of the Depression, building activity stalled, land values dropped and the
 population in North Sydney remained static. The Pacific Highway was widened and
 renamed, and the Victoria Cross intersection was formed.
- Late 1930s Development Post-Depression mainly consisted of rebuilding. Large Federation and Victorian houses were converted into boarding houses. Despite this, the population declined across the suburb after World War II.
- 1960s Many of the large Federation and Victorian houses bordering St Leonards park were demolished for high rise buildings. During the building boom of the 1960s, North Sydney was aggressively promoted as the 'Twin City' to Sydney. Cheap rents encouraged smaller firms to lease office space in the commercial centre.
- 1968 and 1973 The Warringah Expressway was built; construction involved the demolition of nearly 500 houses and shops. The building boom resulted in the wholesale demolition of



the nineteenth-century township – Victorian and Federation shops, terraces, houses and public buildings disappeared from the streetscape.

- 1980s Companies were encouraged to build office towers in the formerly low-rise Victorian and Federation shopping centre, extending as far north as McLaren Street.
- Post 1980s A period of reassessment of planning controls in North Sydney occurs, to take into account the needs of local residents. The North Sydney Heritage Study was released in 1982, with a push to balance commercial development with conserving the environmental quality of the central business district and surrounding residential areas.

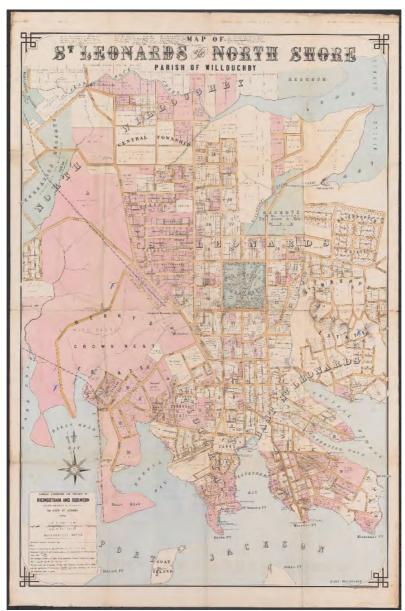


Figure 4. 1887 Map of St Leonards (Source: Provided by Conybeare Morrison).



3. Heritage Status

3.1 Heritage Listings and Significance

A preliminary investigation of listed heritage items and Heritage Conservation Areas (HCAs) was undertaken for the *Civic Precinct and Surrounds Planning Study*, prepared by Conybeare Morrison in 2019. A figure identifying these listed items and HCAs has been provided below, as has a figure identifying the four separate HCAs within the Study Area.

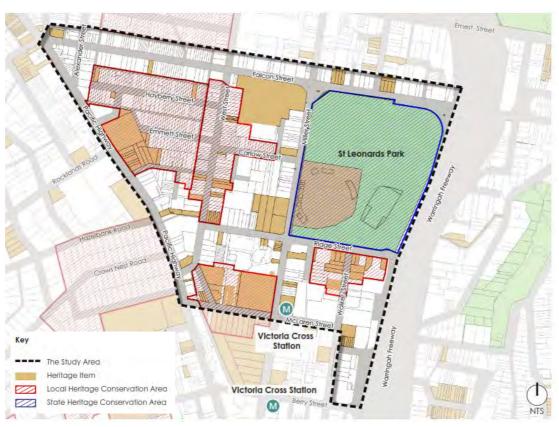


Figure 5. Heritage Items and HCAs (Source: Provided by Conybeare Morrison, 2019).

3.1.1 Heritage Conservation Areas (HCAs)

The Study Area contains four HCAs. A detailed overview of individual HCAs can be found within Appendices A and B.

Table 1: Heritage Conservation Areas

HCA ID	HCA Name				
CA09	Holtermann Estate C Conservation Area				
CA18	Holtermann Estate D Conservation Area				



HCA ID	HCA Name				
CA19	McLaren Street Conservation Area				
CA20	Walker and Ridge Streets Conservation Area				



Figure 6. Aerial showing Heritage Conservation Areas (HCAs) (Source: LPI SIX Maps, Extent Heritage 2019).

3.1.2 Heritage Items

Ninety-three heritage items are located within the Study Area listed on the North Sydney Local Environmental Plan 2013 (LEP), SHR and Sydney Water Section 170 Heritage and Conservation Register. Of these items, ninety-one are locally significant, and two are State significant.

A detailed overview of all heritage listings, including all individually listed items, can be found in Appendix A. A detailed overview of heritage significance can be found in Appendix B.



4. Constraints and Opportunities

The below section provides a broad outline of the key heritage constraints and opportunities associated with the North Sydney Civic Precinct and Surrounds. This includes both an overview of the statutory requirements of the area and an outline of best practice guidelines for new development. Areas have been separated into Locality Areas defined in the North Sydney DCP.

4.1 Heritage Act 1977 (NSW)

4.1.1 State Heritage Register

The *Heritage Act 1977* (NSW) provides protection for items of State heritage significance that are listed on the State Heritage Register (SHR). Pursuant to Section 57(1) of the Heritage Act, the approval of the Heritage Council of NSW is generally required for major works within a site included on the SHR, including works to the grounds or structures.

The following items are listed on the SHR:

- St Leonards Park, and
- Sewer Vent.

Comment

Should future development include items listed on the State Heritage Register (SHR), Heritage Council approval would be required. This would likely include a Section 60 application for a Section 63 approval.

Should future development be adjacent to items listed on the SHR, and it is considered that there may be an adverse impact on adjacent items, approval may be required. This would be at the discretion of North Sydney Council.

4.1.2 Section 170 Heritage Register

Section 170 of the Heritage Act requires that all Government departments or agencies must maintain a Heritage and Conservation Register, which includes all property and assets owned or in the care and control of the relevant department or agency that are of State or Local heritage significance. The Sewer Vent is listed on the Sydney Water Section 170 Heritage and Conservation Register.

Under Section 170A of Heritage Act 1977, Sydney Water is required to provide 14 days prior notice to the Heritage Council of NSW in the event that it:

- a) Removes any item from its register under section 170, or
- b) Transfers ownership of any item entered in its register, or
- c) Ceases to occupy or demolishes any place, building or work entered in its register.



Comment

Should any future development trigger the above changes to the known S170 item, the Heritage Council of NSW should be notified.

4.1.3 Archaeological Relics

Historical archaeological relics, deposits and features are afforded protection under the Heritage Act, whether they occur on land included or not included on the SHR. A relic is defined as:

- [...] any deposit, artefact, object or material evidence that:
- (a) relates to the settlement of the area that comprises New South Wales, not being Aboriginal settlement, and
- (b) is of State or local heritage significance.

Section 57 and 60 of the Heritage Act state that permit or exemptions will be required when any land will be excavated or disturbed that is likely to contain a relic. Section 139 and 140 of the Heritage Act state that an excavation permit will be required under certain conditions, including where there is a reasonable cause to suspect, that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, moved, damaged or destroyed.

Comment

This constraints and opportunities assessment does not include an assessment of archaeological potential. Some heritage items listed on the LEP or SHR within the Study Area may have associated archaeological potential. Where land will be excavated that is <u>likely</u> to contain a relic, Section 57 and 60 of the Heritage Act apply. Where any disturbance will occur where there is <u>reasonable cause</u> that works will result in the discovery or alteration of a relic, Sections 139 and 140 of the Heritage Act apply.

4.2 North Sydney Local Environmental Plan 2013

Clause 5.10 of the *North Sydney Local Environmental Plan 2013* (LEP) contains provisions to protect heritage items, or items within an HCA, identified in Schedule 5 of the LEP. Clause 5.10(2) states:

Development consent is required for any of the following:

- (a) demolishing or moving any of the following or altering the exterior of any of the following (including, in the case of a building, making changes to its detail, fabric, finish or appearance):
 - (i) a heritage item,
 - (ii) an Aboriginal object,
 - (iii) a building, work, relic or tree within a heritage conservation area,



- (b) altering a heritage item that is a building by making structural changes to its interior or by making changes to anything inside the item that is specified in Schedule 5 in relation to the item,
- (c) disturbing or excavating an archaeological site while knowing, or having reasonable cause to suspect, that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, moved, damaged or destroyed,
- (d) disturbing or excavating an Aboriginal place of heritage significance,
- (e) erecting a building on land:
 - (i) on which a heritage item is located or that is within a heritage conservation area, or
 - (ii) on which an Aboriginal object is located or that is within an Aboriginal place of heritage significance,
- (f) subdividing land:
 - (i) on which a heritage item is located or that is within a heritage conservation area. or
 - (ii) on which an Aboriginal object is located or that is within an Aboriginal place of heritage significance,

Clause 5.10(3) qualifies the requirement for consent:

However, development consent under this clause is not required if—

- (a) the applicant has notified the consent authority of the proposed development and the consent authority has advised the applicant in writing before any work is carried out that it is satisfied that the proposed development—
 - (i) is of a minor nature or is for the maintenance of the heritage item, Aboriginal object, Aboriginal place of heritage significance or archaeological site or a building, work, relic, tree or place within the heritage conservation area, and
 - (ii) would not adversely affect the heritage significance of the heritage item, Aboriginal object, Aboriginal place, archaeological site or heritage conservation area, or
- (b) the development is in a cemetery or burial ground and the proposed development—

 (i) is the creation of a new grave or monument, or excavation or disturbance of land for the purpose of conserving or repairing monuments or grave markers, and
 - (ii) would not cause disturbance to human remains, relics, Aboriginal objects in the form of grave goods, or to an Aboriginal place of heritage significance, or
- (c) the development is limited to the removal of a tree or other vegetation that the Council is satisfied is a risk to human life or property, or
- (d) the development is exempt development.

Comment

Development consent would be required from North Sydney Council for any proposed works, as they will likely be within the boundaries of several locally listed heritage items and Heritage Conservation Areas (HCAs).



4.3 North Sydney Development Control Plan 2013

4.3.1 General Development Controls

The North Sydney Development Control Plan 2013 (DCP) outlines a series of controls in order to ensure that significant elements of the past are appropriately managed and respected by new development. Broadly, the objectives of the DCP, as they relate to heritage, are to:

- establish a framework for detailed heritage and conservation planning in North Sydney;
- ensure that Aboriginal heritage and archaeology are taken into consideration;
- ensure that the assessment of applications for works on or in heritage items, heritage conservation areas and in the vicinity of heritage, are based on the identified heritage significance of the heritage item, conservation area, property, location or place;
- ensure that supporting documentation is appropriate to the scale of the proposed works and heritage significance of the heritage item, conservation area, property, location or place;
- facilitate opportunities to improve the understanding and/or appreciation of the heritage significance of any heritage item, conservation area, property, location or place; and
- encourage sustainable development practices through the reuse and recycling of the existing building stock as appropriate.

The heritage provisions are based on the underlying principles that change should be based on an understanding of heritage significance, and that the level of change should respect the heritage significance of the item or area. The provisions have differing objectives depending upon whether the development affects a listed heritage item, a HCA or if it is located adjacent to a heritage item.

Comment

Upon receipt of a detailed design or additional design options, further assessment would be required to assess any future proposed development against the specific provisions and controls listed within the North Sydney DCP.

The below section outlines, at a high level, some additional provisions and controls relating to heritage within the North Sydney DCP.

4.3.2 Area Character Statements

The North Sydney DCP contains a series of area character statements, developed 'to use the desired future character of the areas as the basis for urban design planning and implementation.' The North Sydney LGA is divided into 9 Planning Areas, further subdivided into 61 Locality Areas.





Figure 7. North Sydney DCP Planning Areas (Source: North Sydney DCP 2013 Figure C1-1).

The Study Area is broadly located within the North Sydney and St Leonards/Crow's Nest Planning Areas.

A series of provisions for the **North Sydney Planning Area** have been outlined in Section C2-2 of the DCP. Where these directly relate to heritage, they have been included below:

- The setback on the eastern edge of Miller Street between McLaren Street and Mount Street is maintained and incorporates landscaped areas and actively utilised open space.
- The significance of heritage items is retained, and promotes the rich development history and provides interest in the physical fabric of the area.
- Heritage items are protected, and significant streetscape elements are conserved in the Walker and McLaren Street Conservation Areas.
- Predominant early 20th Century character of the McLaren Street Conservation Area is maintained and protected.



 Victorian and Federation character of streets in the Walker Street Conservation Area is maintained and protected.

A series of provisions for the **St Leonards/Crows Nest Planning Area** have been outlined in Section C3-2 of the DCP. Where these directly relate to heritage, they have been included below:

- The visual characteristics of the Crows Nest neighbourhood's heritage conservation status are reflected in new development, with low-rise smallscale dwellings predominating.
- The heritage items retain their heritage significance, illustrate a rich development history and provide interest in the physical fabric of the area.

As outlined above, within each Planning Area are several smaller Locality Areas. Within the North Sydney Planning Area, these are as follows:

- Civic Neighbourhood,
- Eden Neighbourhood
- Hampden Neighbourhood
- McLaren Street Conservation Area and
- Walker/Ridge Street Conservation Area

Within the St Leonards/Crows Nest Planning Area, these are as follows:

- Crows Nest Town Centre (Falcon Street only)
- Holtermann Estate Conservation Area C
- Holtermann Estate Conservation Area D

Specific heritage provisions relating to each of these localities have been outlined in the below section.



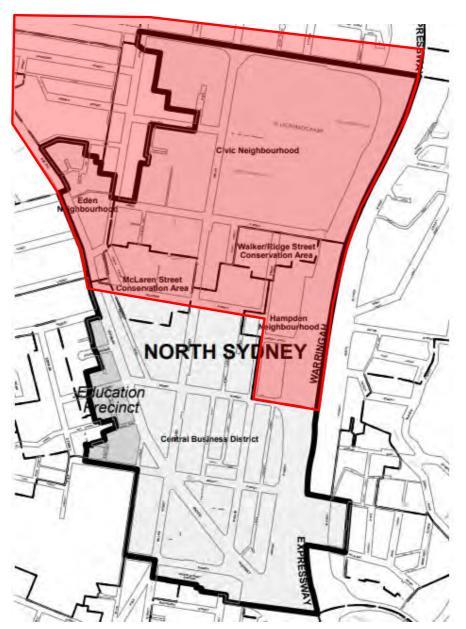


Figure 8. North Sydney Planning Area showing Locality Areas with Study Area outlined in red (Source: North Sydney DCP 2013 Figure C2-1).



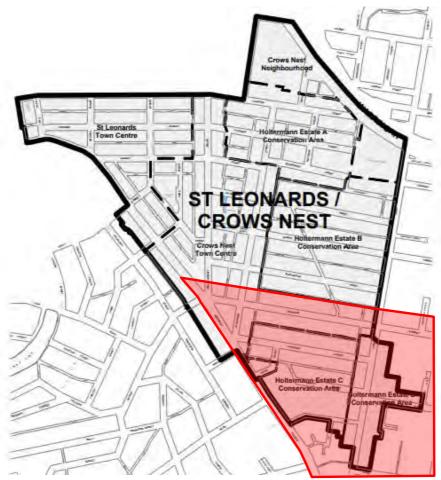


Figure 9. St Leonards/Crows Nest Planning Area showing Locality Areas with Study Area outlined in red. (Source: North Sydney DCP 2013 Figure C3-1).

4.3.3 Characteristic Buildings, Identity and Icons

The North Sydney DCP identifies a series of important items within each Locality Area, broadly described as 'Characteristic Buildings' for developments sharing similar forms and features or, as 'Identity/Icons' for rare or specific built items. The following table presents an outline of items and characteristic buildings, located within each Locality Area. Listed heritage items are identified in **bold**.

Table 2. North Sydney characteristic buildings, identities and icons.

Locality Area	Characteristic Building, Identity or Icon			
	Stanton Library			
Civic Precinct	St Leonards Park & North Sydney Oval			
Civic Fredirict	North Sydney Council Chambers			
	Independent Theatre			



Locality Area	Characteristic Building, Identity or Icon		
	Civic Centre and Park		
	Warringah Freeway a major arterial thoroughfare.		
	Miller and Falcon Streets a major sub-arterial thoroughfare.		
	Union Hotel		
Eden Neighbourhood	Freemasons Hall (Wellbeing Centre)		
	Pacific Highway, a major sub regional thoroughfare.		
	North Sydney Club		
Hampden Neighbourhood	Warringah Expressway, a major arterial thoroughfare.		
	Sandstone wall in the middle of Walker Street		
Walker/Ridge Heritage	Detached and attached Victorian Italianate dwelling houses.		
Conservation Area	Detached Federation Queen Anne and Arts and Crafts style dwelling houses		
McLaren Street Heritage Conservation Area	Victorian and Federation. Buildings		
Crows Nest Town Centre (Falcon Street only)	 Pacific Highway and Falcon Streets, major sub-arterial thoroughfares. 		
Holtermann Estate Conservation Area C	Detached, late Victorian, Federation and Edwardian semi-detached dwelling houses.		
Holtermann Estate Conservation Area D	A mixture of single storey detached and semi-detached dwelling houses and two storey attached dwellings.		

The above elements will be important to consider in any future design development, as these characteristic items contribute directly to the streetscape of each locality area and are considered to be significant and important features from a heritage perspective.

4.3.4 Views

The North Sydney DCP outlines a series of important views and vistas within and from each Locality Area that are to be preserved and, where possible, enhanced. These are outlined in the following table:

Table 3. Important views and vistas.

Area DCP Reference		View/s		
Civic Precinct	C2-11	To Kirribilli and Sydney Harbour from St Leonards Park.		
Eden Neighbourhood	C2-14	 Views to the North Sydney CBD along the Pacific Highway. 		



Area	DCP Reference	View/s
Hampdon Naighbourhood	C2 17	 Maintain views of Kirribilli and the Harbour from Walker Street.
Hampden Neighbourhood	C2-17	 Strong vista along Walker Street to southern part of CBD.
Walker/Ridge Streets	C2-21	 Warringah Expressway Lookout at the end of Ridge Street. Towards St Leonards Park
		War Memorial along Walker Street
McLaren Street	C2-26	 Views within area along Miller and McLaren Streets to St Thomas' Church and the Council buildings.
Crows Nest Town Centre	C3-12	Vista north along Willoughby Road and Pacific Highway.
(Falcon Street only)	G3-12	District views from the upper levels of taller buildings.
Holtermann Estate Conservation Area C C3-28		None. DCP notes that the Holtermann Estate Conservation Area C has limited street views.
Holtermann Estate Conservation Area D	C3-32	Along West and Carlow Streets.

These views can be protected through thoughtful and considered design planning. This includes considering the bulk and scale of any new development so that it will not infringe on any important corridors, and, where this infringement is unavoidable to mitigate any impacts to corridors through materiality, colour, plantings and setback. In particular, views towards Sydney Harbour and Kirribilli are significant elements that should be maintained and enhanced where possible.

4.3.5 Form, Scale and Streetscape

The general form, scale and streetscape, including the future desired built form of development is included in the provisions for each Locality Area within the North Sydney DCP. These have been broadly outlined below:

- Open and active street frontages with low or no fences are encouraged within the Civic Precinct, with a retention of the avenue of trees along the streetscape preferred. This attracts pedestrian traffic along the street. Height provisions in this area are generally limited to one to three storeys in height with a strong relationship to adjacent buildings (i.e. buildings of a similar height grouped together, or with appropriate height transitions).
- The Eden Neighbourhood has a variety of building heights, with an average height of four storeys. Buildings adjacent to HCAs within this Locality Area are encouraged to transition in height to match building height within the HCAs. Building form differs throughout the area, with an emphasis on quality mixed-use design to complement heritage items.



- The Hampden Neighbourhood features sandstone walls and substantial gardens along Walker and Hampden Streets, and generally consists of early and original residential development that complements the topography of the area.
- The Walker and Ridge Streets HCA is characterised by intact groups of single and twostorey dwellings in a mix of Victorian Italianate, Federation, Arts and Crafts and Queen Anne styles. Some modern residential infill exists within this HCA, including educational buildings associated with Wenona. Mature landscaping is a significant feature of the HCA.
- The McLaren Street HCA contains several public buildings including St Thomas's Church and Hall and the North Sydney Council Chambers. Characteristic buildings in the area include Federation, Queen Anne and Victorian residences usually single storey or two storey in height.
- The Crows Nest Locality Area, of which only Falcon Street is included within this study, is characterised by medium density residential development along Falcon Street.
- The Holtermann C HCA is characterised by predominantly single-storey dwellings with rear extensions in groups of identical design and with continuous front verandahs. Styles include Victorian Georgian and Filigree, Victorian Italianate, Federation Queen Anne and Federation Bungalow. Later infill development includes the large Sydney Girls High School Campus.
- The Holtermann D HCA is characterised by modest, speculative cottages that include a mix of late 19th and early 20th century building styles including Victorian Georgian and Filigree, Federation Queen Anne and Federation Bungalow. Most development is single or double storey with reduced height and scale to the rear.

New development should recognise and support the heritage significance and character of the above places, while enabling the place to respond to contemporary needs within the community. While new design in these areas may accommodate a variety of architectural expressions—a respect for the heritage significance of the place must underpin any design considerations.

The planning provisions for each Locality Area will identify appropriate heights and setbacks for any new development.

It is understood that, with regard to built form and scale, Conybeare Morrison have already undertaken an assessment to understand potential zones that could accommodate an increase in building scale as part of the *Civic Precinct and Surrounds Planning Study*.

4.3.6 Uncharacteristic Elements

An opportunity exists within existing HCAs to remove or alter modern additions that are considered intrusive or 'non-contributory.' The North Sydney DCP outlines these uncharacteristic elements.



Table 4. Uncharacteristic elements identified in the Study Area.

	HCA	Uncharacteristic Elements
	McLaren Street	 Modern additions and buildings; painting and rendering of face brick; high fences to street; excessive paved areas for parking; buildings built to the front boundary. Modern shopfront to 5-7 McLaren St
•	Holtermann Estate C	 Modern additions; loss of original detail, painting and rendering of face brickwork; modern infill developments removal of original detailing, front and side dormers and roof lights.
	Holtermann Estate D	Over-scaled, two storey additions; contemporary buildings with laneway frontages; front and side dormers and roof lights; modified roof forms, removal of original detailing; verandah infill; rendered and painted face brickwork; modernised facades; high walls and fences to the street, car parking in front setback, lot amalgamation and loss of original subdivision pattern.

While the above listed elements do not represent all uncharacteristic elements within all Locality Areas, they provide a brief outline of the types of development that are intrusive to each Locality Area. Broadly, these are representative of development that has an overly dominant scale, removes original detailing, has inappropriate setbacks and does not consider the streetscape character of the surrounding development.

4.3.7 Adaptive Re-use of Buildings

If the function of a building becomes redundant or undesirable, it becomes vulnerable to damage through neglect and decay, or in some cases, demolition. Adaptive reuse can help to conserve and celebrate the heritage significance of a place while allowing it to have a new use and an extended lifespan. Renewing and using existing buildings also helps to prevent the unnecessary waste of building materials. While there are benefits to adaptive reuse, adaptation of a building to allow for a new use should be underpinned by an understanding of a buildings significance and should result in a design which creates a relationship between existing and new work (Heritage Council 2008).

Within North Sydney, there would be an opportunity to adaptively reuse public heritage listed buildings that have a redundant function and are currently unused or minimally used. The North Sydney DCP outlines a series of provisions for adaptive reuse to heritage items that are representative examples of their type (i.e. industrial, maritime and/or workshop buildings) Broadly, these provisions seek to:

- Ensure that changes to buildings are sympathetic to the existing fabric and building elements and do not compromise the heritage significance of the buildings;
- Allow for interpretation of significant form and use of these buildings; and to
- Conserve important built elements and any relevant equipment associated with the use of these buildings.



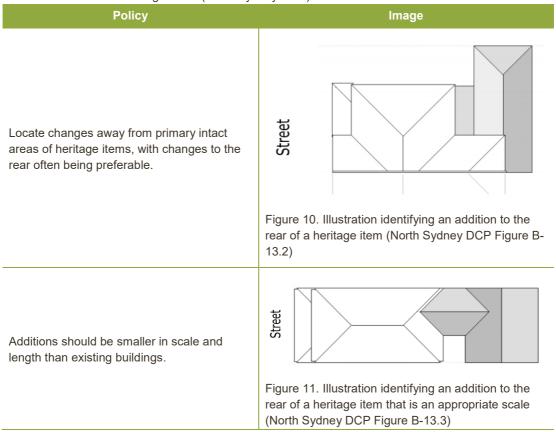
An assessment of buildings within the Study Area that are currently unoccupied or unused would be required to further explore more specific opportunities for adaptive reuse.

4.3.8 Alterations and Additions to Heritage Items

The North Sydney DCP outlines several provisions for altering and adding to listed heritage items. Underpinning these provisions is the objective that any changes to fabric should be based on an understanding of the heritage significance of the specific item. Generally, there is a preference to locate changes away from primary intact elements of a place or highly visible areas, and to minimise the visual dominance of any new work on the place both as an individual item and on the streetscape.

The below table outlines some broad provisions for making changes to heritage items.

Table 5. Alterations to heritage items (North Sydney DCP).





Policy Image

New work should maintain the integrity of the building form, which would allow the original form of the building to be discerned.

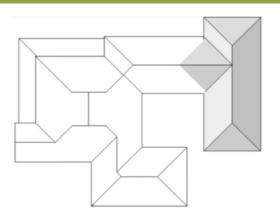


Figure 12. Illustration identifying an addition to the rear of a building that still allows the original form to be discerned (North Sydney DCP Figure B-13.4)

Consideration should be given to using the existing roof spaces of buildings.

The DCP does not generally support wholestorey additions to heritage items, with a preference for utilising the attic space with installation of dormer windows.

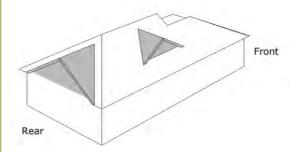


Figure 13. Illustration identifying changes within the roof space of a heritage building (North Sydney DCP Figure B-13.5)

4.3.9 Development within Heritage Conservation Areas

Heritage Conservation Areas have a cohesive sense of place, often containing heritage places that share a common period of development and historical association, with street patterns often a function of historical subdivision. On the most part, HCAs generally have historic, aesthetic and representativeness significance. Development within HCAs is managed to ensure new development will complement and retain the significance and character of the place, specifically to ensure that contributory items are retained, neutral items are improved and intrusive items are removed or modified to be more sympathetic towards the character of the place.

Generally, development in these areas should recognise and complement the predominant architectural scale, form, bulk, curtilage, setbacks and materiality of the HCA to create a unified character. Works should not obstruct or visually dominate existing key views in the public domain. Additional storeys to buildings within HCAs are generally not supported within the North Sydney DCP, but may be permissible if an applicant can demonstrate that the resultant building



will exhibit a similar scale to those in its vicinity, that the design respects the heritage character of the area through materiality, finishes and forms, and that the additional storey will not alter adversely impact contributory items within the HCA with respect to views and settings.

Development of new buildings within a HCA (known as 'infill development') will need to positively respond to the setting and character of the area, as outlined in the 'Area Character Statements' within the DCP. This does not mean that design in a heritage context such as a HCA should imitate existing heritage items or follow inflexible design rules, but that a careful analysis of surrounding buildings is first conducted to ensure that the distinctive identity and sense of place of an area is maintained and enhanced through the new build (NSW Heritage Office 2005:6).

4.3.10 Development adjacent to Heritage Conservation Areas

Development adjacent to HCAs still need to consider heritage matters, particularly in relation to height. This will include ensuring that there is an appropriate transition in height between development outside of the HCA and within the HCA and ensuring that increases in overshadowing of items within the HCA is limited where possible.

The North Sydney DCP does not provide specific provisions for development adjacent to HCAs, however general best practice guidelines suggest several methods to mitigate the impact that taller buildings may have on an adjacent conservation area. This includes the use of a podium with a tower setback for any larger-scale buildings, which allows for better interfacing between smaller adjacent buildings within HCAs. Street walls can also be used to mitigate the visual impact of new development adjacent to HCAs. If designed in response to the character of the streetscape, these walls can create a sense of consistency and cohesiveness within the landscape.

As the character of HCAs within the Study Area differs, further understanding of specific development would be required to explore more specific opportunities for appropriate interfacing.



Recommendations

Based upon the above broad analysis of constraints and opportunities, the following additional recommendations are made:

- Future design development should be developed in consultation with heritage specialists.
- The following guiding principles should be undertaken to reduce impact of future development:
 - All works should be guided by the philosophy of "as much as necessary, as little as
 possible." The project design, where possible, should avoid physical impacts to heritage
 items and significant elements of HCAs, as identified in this study.
 - Should major physical or visual impacts to heritage elements on site be unavoidable, consultation with a qualified heritage specialist should be undertaken to determine the extent of impact and, through a design review, develop an appropriate design option.
 - Interfaces between future development located adjacent to HCAs and heritage items should be carefully considered in terms of height, setback, overshadowing, materiality, colour etc.
 - Proposed plantings must be appropriate to each HCA, as relevant.
 - The significant heritage aspects of the place should be prioritised, through further research and incorporation of these aspects in the design development. Demonstrating these considerations as being integral and of critical importance in the design would contribute some mitigation to offset the potential for impact by the proposal of additional storeys above the general scale of existing buildings.
- Consideration should be given to the implementation of heritage interpretation as part of the redevelopment of the precinct, which draws on key heritage themes of the area for the purpose of public education.
- Appropriate construction methodologies would need to be produced for any works which have the potential to impact significant heritage fabric. This would include recommendations for on-site staff to address any potential impacts, including provisions for notification and recording of any accidental damage to heritage items.



6. References

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NSW Heritage Council (2008) New Uses for Heritage Places: Guidelines for the Adaptation of Historic Buildings and Sites. Available at

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Hoskins, Ian (205) 'We will see a town rising: a history of North Sydney' Published Talk.



Appendix A: Heritage Listings

For the purposes of this assessment, statutory heritage registers <u>only</u> were searched and a tabulated summary of listings has been provided below.

Within the Study Area there are several listed heritage items and Heritage Conservation Areas (HCAs) on the NSW State Heritage Register, Sydney Water Heritage and Conservation Register and Schedule 5 of the *North Sydney Local Environmental Plan 2013* (LEP).

HCAs

Four HCAs are located within the Study Area. These are as follows:

Table 6: Heritage Conservation Areas

HCA ID	HCA Name			
CA09	Holtermann Estate C Conservation Area			
CA18	Holtermann Estate D Conservation Area			
CA19	McLaren Street Conservation Area			
CA20	Walker and Ridge Streets Conservation Area			



Figure 14. Aerial showing Heritage Conservation Areas (HCAs) (Source: LPI, Extent Heritage 2019).



Heritage Items within the Study Area.

There are ninety-three heritage items located within the Study Area listed on the *North Sydney Local Environmental Plan 2013* (LEP), SHR and Sydney Water Section 170 Heritage and Conservation Register. Of these items, ninety-one are locally significant and two are State significant.

Table 7: Heritage Items within the Study Area.

Item Name	ID	Significance	Listed On	Address	Within HCA?
House	10988	Local	North Sydney LEP 2013	185 Walker Street	No
House	10840	Local	North Sydney LEP 2013	2 Hampden Street	No
House	10841	Local	North Sydney LEP 2013	4 Hampden Street	No
House	10842	Local	North Sydney LEP 2013	6 Hampden Street	No
House	10843	Local	North Sydney LEP 2013	8 Hampden Street	No
House	10844	Local	North Sydney LEP 2013	10 Hampden Street	No
House	10845	Local	North Sydney LEP 2013	12 Hampden Street	No
House	10846	Local	North Sydney LEP 2013	14 Hampden Street	No
Shop	10908	Local	North Sydney LEP 2013	243 Miller Street	No
House	10912	Local	North Sydney LEP 2013	255–257 Miller Street	No
The Independent Theatre	10914	Local	North Sydney LEP 2013	269 Miller Street	No
Shop	10905	Local	North Sydney LEP 2013	232–232A Miller Street	No
House	10976	Local	North Sydney LEP 2013	51 Ridge Street	No
Trewyn Terraces	10909	Local	North Sydney LEP 2013	244 Miller Street	No



Item Name	ID	Significance	Listed On	Address	Within HCA?
Trewyn Terraces	10910	Local	North Sydney LEP 2013	246 Miller Street	No
Trewyn Terraces	10911	Local	North Sydney LEP 2013	248 Miller Street	No
Trewyn Terraces	10907	Local	North Sydney LEP 2013	242 Miller Street	No
Trewyn Terraces	10906	Local	North Sydney LEP 2013	240 Miller Street	No
House	10973	Local	North Sydney LEP 2013	45 Ridge Street	No
House	10974	Local	North Sydney LEP 2013	47 Ridge Street	No
House	10975	Local	North Sydney LEP 2013	49 Ridge Street	No
St Mary's Primary School	10970	Local	North Sydney LEP 2013	40 Ridge Street	No
St Mary's Catholic Church	10971	Local	North Sydney LEP 2013	264 Miller Street	No
St Mary's Catholic Presbytery	10972	Local	North Sydney LEP 2013	264 Miller Street	No
North Sydney bus shelters	10407	Local	North Sydney LEP 2013	Various	Dependent upon bus shelter location
House	10967	Local	North Sydney LEP 2013	11 Ridge Street	No
Union Hotel	10960	Local	North Sydney LEP 2013	271 Pacific Highway	No
Shop	10959	Local	North Sydney LEP 2013	265 Pacific Highway	No
Former Masonic Temple	10961	Local	North Sydney LEP 2013	317 Pacific Highway	No
"Courtney Creche"	10999	Local	North Sydney LEP 2013	36 West Street	No



Item Name	ID	Significance	Listed On	Address	Within HCA?
House	10977	Local	North Sydney LEP 2013	63 Ridge Street	Yes – CA20
House	10978	Local	North Sydney LEP 2013	81 Ridge Street	Yes – CA20
Wenona Girls' School Group—79 Ridge Street	10991	Local	North Sydney LEP 2013	186 Walker Street (79 Ridge Street)	Yes – CA20
Wenona Girls' School Group—83 Ridge Street	10992	Local	North Sydney LEP 2013	186 Walker Street (83 Ridge Street)	Yes – CA20
Wenona Girls' School Group— Wenona	10989	Local	North Sydney LEP 2013	186 Walker Street (182 Walker Street)	Yes – CA20
Wenona Girls' School Group— Ralston House	10990	Local	North Sydney LEP 2013	186 Walker Street (184 Walker Street)	Yes – CA20
Wenona Girls' School Group— Karakatta	10993	Local	North Sydney LEP 2013	186 Walker Street	Yes – CA20
House	10979	Local	North Sydney LEP 2013	85 Ridge Street	Yes – CA20
House	10980	Local	North Sydney LEP 2013	87 Ridge Street	Yes – CA20
House	10994	Local	North Sydney LEP 2013	207 Walker Street	Yes – CA20
House	10995	Local	North Sydney LEP 2013	209 Walker Street	Yes – CA20
"St Helen's"	10981	Local	North Sydney LEP 2013	91 Ridge Street	Yes – CA20
"St Malo"	10982	Local	North Sydney LEP 2013	95 Ridge Street	Yes – CA20
North Sydney Council Chambers	10902	Local	North Sydney LEP 2013	200 Miller Street	Building is within CA 19



Item Name	ID	Significance	Listed On	Address	Within HCA?
(including fountain in park adjacent to Council Chambers)					Fountain is not within HCA
North Sydney Council Chambers— Wyllie Wing	10903	Local	North Sydney LEP 2013	200 Miller Street	Yes – CA19
St Thomas' Church	10885	Local	North Sydney LEP 2013	34 McLaren Street	Yes – CA19
St Thomas' Church Rectory	10886	Local	North Sydney LEP 2013	34 McLaren Street	Yes – CA19
St Thomas' Kindergarten Hall	10887	Local	North Sydney LEP 2013	34 McLaren Street	Yes – CA19
Memorial Hall of St Thomas	10888	Local	North Sydney LEP 2013	34 McLaren Street	Yes – CA19
House	10879	Local	North Sydney LEP 2013	12 McLaren Street	Yes – CA19
Playfair House	10962	Local	North Sydney LEP 2013	2 Ridge Street	Yes – CA18
House	10963	Local	North Sydney LEP 2013	4 Ridge Street	Yes – CA18
House	10964	Local	North Sydney LEP 2013	6 Ridge Street	Yes – CA18
House	10965	Local	North Sydney LEP 2013	8 Ridge Street	Yes – CA18
House	10966	Local	North Sydney LEP 2013	10 Ridge Street	Yes – CA18
House	10968	Local	North Sydney LEP 2013	12 Ridge Street	Yes – CA18
House	10969	Local	North Sydney LEP 2013	14 Ridge Street	Yes – CA18
Shop	11006	Local	North Sydney LEP 2013	67 West Street	Yes – CA18
House	11004	Local	North Sydney LEP 2013	58 West Street	Yes – CA18



Item Name	ID	Significance	Listed On	Address	Within HCA?
House	I1005	Local	North Sydney LEP 2013	58A West Street	Yes – CA18
Vera Loblay House	I1002	Local	North Sydney LEP 2013	44 West Street	Yes – CA18
House	11003	Local	North Sydney LEP 2013	45 West Street	Yes – CA18
House	I1001	Local	North Sydney LEP 2013	43 West Street	Yes – CA18
House	I1000	Local	North Sydney LEP 2013	41 West Street	Yes – CA18
House	10998	Local	North Sydney LEP 2013	35 West Street	Yes – CA18
House	10997	Local	North Sydney LEP 2013	33 West Street	Yes – CA18
Crows Nest Hotel	10181	Local	North Sydney LEP 2013	1–3 Willoughby Road	No
Former Church of Christ	10143	Local	North Sydney LEP 2013	69 Falcon Street	No
North Sydney Boys' High School	10830	Local	North Sydney LEP 2013	127 Falcon Street	No
Flat building	10832	Local	North Sydney LEP 2013	184 Falcon Street	No
"Winstone"	10831	Local	North Sydney LEP 2013	182 Falcon Street	No
Flat building	10834	Local	North Sydney LEP 2013	188 Falcon Street	No
Flat building	10833	Local	North Sydney LEP 2013	186 Falcon Street	No
House	10008	Local	North Sydney LEP 2013	3 Lytton Street	No
House	10007	Local	North Sydney LEP 2013	1 Lytton Street	No
House	10835	Local	North Sydney LEP 2013	210 Falcon Street	No



Item Name	ID	Significance	Listed On	Address	Within HCA?
House	10836	Local	North Sydney LEP 2013	212 Falcon Street	No
House	10837	Local	North Sydney LEP 2013	214 Falcon Street	No
House	10838	Local	North Sydney LEP 2013	216 Falcon Street	No
Sewer vent	10839 01641 285047	State	North Sydney LEP 2013 NSW State Heritage Register Sydney Water State Agency Heritage and Conservation Register	Falcon Street (southwest of Warringah Freeway)	No
St Leonards Park	I0916 01941	State	North Sydney LEP 2013 NSW State Heritage Register	Miller Street	No
War memorial	l1123	Local	North Sydney LEP 2013	Ridge Street	No
W. Tunks Memorial Fountain	l1124	Local	North Sydney LEP 2013	Miller Street	No
North Sydney Oval	l1125	Local	North Sydney LEP 2013	Miller Street	No
North Sydney Hotel	10915	Local	North Sydney LEP 2013	292 Miller Street	No
Shop	10176	Local	North Sydney LEP 2013	89A West Street	Yes – CA18
Shop	10175	Local	North Sydney LEP 2013	89 West Street	Yes – CA18
Shop	10174	Local	North Sydney LEP 2013	87 West Street	Yes – CA18
Former hall	10144	Local	North Sydney LEP 2013	14 Hayberry Street	Yes – CA09
House	10145	Local	North Sydney LEP 2013	104 Hayberry Street	Yes – CA09



Item Name	ID	Significance	Listed On	Address	Within HCA?
House	10142	Local	North Sydney LEP 2013	18 David Street	Yes – CA09
North Sydney Girls' High School	10165	Local	North Sydney LEP 2013	365 Pacific Highway (between David and Myrtle Streets)	Yes – CA09



Figure 15. Aerial showing listed heritage items. (Source: LPI, Extent Heritage 2019)



Appendix B: Heritage Significance

The statements of significance for known heritage items within the Study Area have been included below. Statements of significance are quoted from the State Heritage Inventory (SHI) listing sheet for the items, where available. 'None provided' indicates that the SHI does not have a statement of significance for the respective item. Please note that as these statements of significance have been directly quoted, there may be some typographical and grammatical errors within.

Statements of significance for the below HCAs are quoted from the *North Sydney Development Control Plan 2013* (DCP).

HCAs

Table 8: Statements of significance: HCAs.

HCA Name	HCA ID	Statement of Significance
		The Holtermann Estate C Conservation Area is significant:
Holtermann Estate C Conservation Area	CA09	(a) For its late 19th and early 20th century residential character that is characterised by single storey, detached and semi-detached dwelling houses of modest scale in a mixture of late Victorian and early Federation styles.
		(b) As an area that represents the working-class residential development of North Sydney at the turn of the century.
		The Holtermann Estate D Conservation Area is significant:
Holtermann Estate D Conservation Area	CA18	(a) for its consistent late 19th and early 20th century residential character that is characterised by single storey dwelling houses of modest scale and two storey attached dwellings in a mixture of late Victorian and early Federation styles.
		(b) for its regular grid subdivision pattern, the level landform and development over a single main development period.
		The McLaren Street Conservation Area is significant:
McLaren Street Conservation Area	CA19	(a) As an area that is close to the centre of North Sydney that retains representative details from its development from the late 19th and early 20th centuries, including street formation, buildings, gardens and fencing.
		(b) For its landmark qualities and associations with St Thomas' Church, North Sydney Council buildings, park and public court
Walker and Ridge		The Walker/Ridge Streets Conservation Area is significant:
Streets Conservation Area	CA20	(a) For its late 19th and early 20th century character defined by the number of intact heritage items in the area.



Heritage Items within the Study Area.

Table 9: Statements of significance: Items within the Study Area.

Heritage Item	ID	Statement of Significance
House	10988	A good example of a late Nineteenth Century stone cottage in the Victorian Regency style, a rare survivor in the area.
House	10840	An example of a two-storey brick terrace house in the Federation Filigree style. Part of Hampden Street Terraces Group, see listing 1413.
House	10841	An example of a two-storey brick terrace house in the Federation Filigree style. Part of Hampden Street Terraces Group, see listing 1413.
House	10842	An example of a two-storey terrace house in the Victorian Filigree style, one of a row of five. Part of Hampden Street Terraces Group (see listing 1413).
House	10843	An example of a two-storey terrace house in the Victorian Filigree style, one of a row of five. Part of Hampden Street Terraces Group (see listing 1413).
House	10844	An example of a two-storey terrace house in the Victorian Filigree style, one of a row of five. Part of Hampden Street Terraces Group (see listing 1413).
House	10845	An example of a two-storey terrace house in the Victorian Filigree style, one of a row of five. Part of Hampden Street Terraces Group (see listing 1413).
House	10846	An example of a two-storey terrace house in the Victorian Filigree style, one of a row of five. Part of Hampden Street Terraces Group (see listing 1413).
Shop	10908	A very good example of a of [sic] storey house in the Federation Arts and Crafts style in a prominent location in [an] area dominated by buildings of the same period. Example of the work of local architect George M. Pitt's work. Important stylistic and physical relationship to Council Chambers.
		Fine, restrained example of Arts and Crafts style house in prominent location in area dominated by buildings of the same period. Example of local architects work. Important stylistic and physical relationship to Council Chambers and McLaren Street Group.
House	10912	Good example of Federation Arts and Crafts house with high quality brickwork and decoration. It is an important location on Miller Street. It relates well to other similar houses in the vicinity, such as Council Chambers and McLaren Street Group.
The Independent Theatre	10914	An impressive building with a long history of varied uses, the most important being as the original tram depot and the later use as the



Heritage Item	ID	Statement of Significance
		Independent Theatre. Important early theatre as the Coliseum. Important streetscape item and rare example of this style locally.
Shop	10905	The former 'Miller's Treat' is the oldest of the group of buildings, 232-248 Miller Street. The original stone sections of the building, which appear to have been built in the 1850s, have historic significance as the remnants of one of North Sydney's early buildings, and may possibly have been built in 1857 by the owner, the stonemason John Jago. The alterations carried out in 1931 which extended the building, and converted the house to a shopfront illustrate the changing uses of older buildings in the area.
House	10976	One of a fine, characteristic row of four substantial late-nineteenth century terrace houses (nos. 45, 47, 49, 51 Ridge Street) whose recent renovation has enhanced their strong streetscape presence. Important group for establishing the street character and an important relic of the early development of this street.
Trewyn Terraces	10909	One of a fine set of terraced shopfronts with upper floor residences from the late nineteenth century in an important location on Miller Street opposite the former tram terminus. The 'Trewyn terraces', Nos. 240-248, which are the surviving five of the original seven terraces, demonstrate the scale and character of the former streetscape of Miller Street in the 1880s, and have continuously provided community retailing services since the 1880s. The terrace is also associated with James Taylor, Mayor of North Sydney 1877-1878, who was the original owner and builder.
Trewyn Terraces	10910	One of a fine set of terraced shopfronts with upper floor residences from the late nineteenth century in an important location on Miller Street opposite the former tram terminus. The 'Trewyn terraces', Nos. 240-248, which are the surviving five of the original seven terraces, demonstrate the scale and character of the former streetscape of Miller Street in the 1880s, and have continuously provided community retailing services since the 1880s. The terrace is also associated with James Taylor, Mayor of North Sydney 1877-1878, who was the original owner and builder.
Trewyn Terraces	10911	One of a fine example of a late-nineteenth century corner shop and residence in an important location on Miller Street. Indicative of local development trends of the late nineteenth century, being opposite former tram terminus. The 'Trewyn terraces', Nos. 240-248, which are the surviving five of the original seven terraces, demonstrate the scale and character of the former streetscape of Miller Street in the 1880s, and have continuously provided community retailing services since the 1880s. The terrace is also associated with James Taylor, Mayor of North Sydney 1877-1878, who was the original owner and builder.
Trewyn Terraces	10907	One of a fine set of terraced shopfronts with upper floor residences from the late nineteenth century in an important location on Miller



Heritage Item	ID	Statement of Significance
		Street opposite the former tram terminus. The 'Trewyn terraces', Nos. 240-248, which are the surviving five of the original seven terraces, demonstrate the scale and character of the former streetscape of Miller Street in the 1880s, and have continuously provided community retailing services since the 1880s. The terrace is also associated with James Taylor, Mayor of North Sydney 1877-1878, who was the original owner and builder.
Trewyn Terraces	10906	One of a fine set of terraced shopfronts with upper floor residences from the late nineteenth century in an important location on Miller Street opposite the former tram terminus. The 'Trewyn terraces', Nos. 240-248, which are the surviving five of the original seven terraces, demonstrate the scale and character of the former streetscape of Miller Street in the 1880s, and have continuously provided community retailing services since the 1880s. The terrace is also associated with James Taylor, Mayor of North Sydney 1877-1878, who was the original owner and builder.
House	10973	One of a fine, characteristic row of four substantial late-nineteenth century terrace houses (nos. 45, 47, 49, 51 Ridge Street) whose recent renovation has enhanced their strong streetscape presence. Important group for establishing the street character and an important relic of the early development of this street.
House	10974	One of a fine, characteristic row of four substantial late-nineteenth century terrace houses (nos. 45, 47, 49, 51 Ridge Street) whose recent renovation has enhanced their strong streetscape presence. Important group for establishing the street character and an important relic of the early development of this street.
House	10975	One of a fine, characteristic row of four substantial late-nineteenth century terrace houses (nos. 45, 47, 49, 51 Ridge Street) whose recent renovation has enhanced their strong streetscape presence. Important group for establishing the street character and an important relic of the early development of this street.
St Mary's Primary School	10970	Important local school and associated with St. Mary's Church, the main parish church for North Sydney. Important relic of divergence of Protestant/Catholic educational systems in Australia. Direct successor to first Catholic school on the North Shore.
St Mary's Catholic Church	10971	The prime Catholic church in North Sydney and the direct successor to the original Catholic church on this site. An impressive and remarkable building of monumental scale in a prominent location.
St Mary's Catholic Presbytery	10972	None Provided.
North Sydney bus shelters	10407	Small and effective functional buildings of handsome design and good workmanship. They are traditional and conservative in form and detail and designed to their varied locations. Historic interest as



Heritage Item	ID	Statement of Significance
		elements of a particular and controversial attempt at Municipal civic design.
		Good example of a two-storey terrace house in the Victorian Italianate style. Only Original building in a row of modified and reproduction terrace style houses.
House	10967	Fair example of a late nineteenth century terrace house with Italianate detailing. Only original building in a row of modified and reproduction terrace style houses. Key element in street-scape and relates well to No's 2-14 Ridge Street.
Union Hotel	10960	The Union Hotel extensively rebuilt in 1938 is an early example of the International style, streamlined with stylised decoration and extensive use of vertical and horizontal lines. Designed by Prevost and Ancher it is aesthetically and historically significant as a relatively intact Interwar period hotel in North Sydney retaining many features of the period. The building has local social significance as a physical reminder of a site which has continuously housed a hotel since the 1870s, which continued to contribute to and serve the public.
Shop	10050	A very unusual example of a three-storey brick commercial [building] in the Victorian Free Gothic style with decorative coloured brickwork and decoration. Unique in the Municipality and prominent in the local streetscape.
Ο ΠΟ ρ	10959	Interesting design and rare commercial example of Victorian Free Gothic style in the area. Prominent on highway and relic of commercial history of this roadway. Influential design on present streetscape.
Former Masonic Temple	10961	Fine substantial building illustrating the Inter-war Free Classical style with an imposing face to the Pacific Highway. Indicative of importance and prominence of Free Masonry in the period of construction. Significant item in streetscape. A picturesque and imposing building.
		The interior is also of significance.
"Courtney Creche"	10999	Fine example of a large two storey late-Federation residence in the Federation Arts and Crafts style which is of substantial size and modest detailing. One of a group of large houses on the boundary between Crows Nest and North Sydney and has an important relationship to Ridge Street and Playfair House.
House	10977	Visually prominent nineteenth century house. Important for it's contrasts to nearby single-storey houses and indicative of development pattern along Ridge Street. Provides important visual and historical links to either end of Ridge Street, where similar period and type buildings are located.
House	10978	See under 'Ridge Street Houses Group' NSHS0882.



Heritage Item	ID	Statement of Significance
Wenona Girls' School Group—79 Ridge Street	10991	See under 'Ridge Street Houses Group' NSHS0882
Wenona Girls' School Group—83 Ridge Street	10992	Large Federation house built as a mansion but acquired and used by a prominent local private school since the early twentieth century. Interesting mixture of architectural details and front garden and relationship to street remain intact. Relic of early development of the vicinity and one of a number of similar survivors in the vicinity. Reminder of the once 'select' character of the vicinity.
Wenona Girls' School Group— Wenona	10989	None provided.
Wenona Girls' School Group— Ralston House	10990	None provided.
Wenona Girls' School Group— Karakatta	10993	Well-formed and characteristic Federation townhouse which is located in a group of similar buildings encompassing a range of styles. Relic of the early development of this vicinity and reminder of the once 'select' character of this vicinity, now on the fringe of the commercial centre. Now associated with prominent local private school.
House	10979	Part of a fine, attractive building with prominent roofscape and timber detailing, in important corner location. Restrained example of Federation Queen Anne building. Important relationship with neighbouring No. 89, which was originally very similar but now modified, as well as houses in Walker Street.
House	10980	Part of a fine, attractive building with prominent roofscape and timber detailing, in important corner location. Restrained example of Federation Queen Anne building. Important relationship with neighbouring No. 89, which was originally very similar but now modified, as well as houses in Walker Street.
House	10994	Good quality, well proportioned late nineteenth century large house. Located in an area once typified by such buildings and of which there are several survivors in the vicinity. Indicative of the once 'select' character of this area near St. Leonards Park.
House	10995	Good example of late-nineteenth century mansion with Italianate detailing, located in important position on high ground near St. Leonards Park. One of a number of survivors in the vicinity of this type of house. Indicative of the once 'select' character of this area, now on the fringe of the commercial centre.
"St Helen's"	10981	Fine example of a Victorian Italianate villa. Original owner and resident was prominent local identity and Mayor. Associated with



Heritage Item	ID	Statement of Significance
		neighbouring hospital. Indicative of early relationship of Ridge Street to the township of St. Leonards/North Sydney.
"St Malo"	10982	Fine Victorian house in prominent position which was built by prominent Sydney identity and has long history of association with medical services, particularly with the St. Ives Hospital. Good relationship to site and neighbouring St. Helens. Significant interiors.
North Sydney Council Chambers (including fountain in park adjacent to Council Chambers)	10902	Important example of it's style in a prominent corner location. Associated with early medical practice and was significant local hospital at one stage. Later associations as Council Chambers and generally an important local public building. Work of significant local architect.
North Sydney Council Chambers— Wyllie Wing	10903	The Wyllie Wing is a physical record of the growth and stature of local government in North Sydney. For architectural historians, it also demonstrates an uncompromising modernist approach by one of Australia's most prominent modernist architects - Harry Seidler. It is a representative, rather than exceptional, example of a 1970s modernist style building.
St Thomas' Church	10885	Excellent example of a Victorian Gothic church, designed by Edmund Blacket. Important regional church and probably the most important historically on the North Shore. Imposing building on prominent hill with associations to surrounding buildings. Associated with a number of important historical figures, such as Rev. Clarke and Conrad Martens and with many prominent attendees. The interior is also of significance.
St Thomas' Church Rectory	10886	Excellent example of early Arts and Crafts style house and example of work of prominent local architect. Associated with adjacent important church and with complementary design features. Part of a group of important ecclesiastic buildings.
St Thomas' Kindergarten Hall	10887	The oldest standing school house on the North Shore and the second established, predating National Schools and public education. It forms a historic link with the original St. Thomas Church and remains part of the St. Thomas Church precinct. Has a long history of school usage and has associations with other significant local schools.
Memorial Hall of St Thomas	10888	Good example of substantial church hall of the early twentieth century. Associated with and located within grounds of historic local church. Surrounded by important group of Federation houses and compatible with these, though stylistically different. Part of a civic precinct.
House	10879	See under McLaren Street Group NSHS0856
Playfair House	10962	A good example of Victorian Filigree style of residence. Related items: Nos 2-10 & 14 Ridge Street.



Heritage Item	ID	Statement of Significance
		Grouping: Nos 2-14 Ridge Street are a single terrace development of the Victorian Filigree style with current consistently high levels of intactness of original detail.
		Part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
		A good example of Victorian Filigree with some Italianate features style of residence.
		Related items: No 6-14 Ridge Street.
House	10963	Grouping: No's 4-14 Ridge Street are a single terrace development of the Victorian Filigree style with current consistently high levels of intactness of original detail.
		Part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
	10964	A good example of Victorian Filigree style of residence.
		Related items: No's4, 8-14 Ridge Street.
House		Grouping: No's 4-14 Ridge Street are a single terrace development of the Victorian Filigree style with current consistently high levels of intactness of original detail.
		Part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
		A good example of Victorian Filigree style of residence.
		Related items: No's4-6 & 10-14 Ridge Street.
House	10965	Grouping: No's 4-14 Ridge Street are a single terrace development of the Victorian Filigree style with current consistently high levels of intactness of original detail.
		Part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
	10000	A good example of Victorian Filigree style of residence.
House	10966	Related items: Nos 4-6 & 12-14 Ridge Street.
		Transcention for the first form



Heritage Item	ID	Statement of Significance
		Grouping: Nos 2-14 Ridge Street are a single terrace development of the Victorian Filigree style with curent consistently high levels of intactness of original detail.
		Part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
		A good example of Victorian Filigree style of residence.
		Related items: Nos 2-10 & 14 Ridge Street.
House	10968	Grouping: Nos 2-14 Ridge Street are a single terrace development of the Victorian Filigree style with curent consistently high levels of intactness of original detail.
	10968	Part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
		A good example of Victorian Filigree style of residence.
		Related items: Nos 2-12Ridge Street.
House	10969	Grouping: Nos 2-14 Ridge Street are a single terrace development of the Victorian Filigree style with curent consistently high levels of intactness of original detail on the front facade.
	10909	Primary significance is as part of a row of exceptional late nineteenth century terrace houses which are an important streetscape element and a major character element in an important historic street. Associated with Thomas Playfair, a prominent Sydney meat merchant and Lord Mayor of Sydney, and his family.
	11006	Grouping: No 61- 67 form a 2-storey terrace row comprising ground floor shop fronts and first floor dwellings. No 61-65 are part of a single development, adjacent to the corner terrace at No 67
Shop		Interesting Inter-war Free Classical corner shopfront with residence which is a prominent streetscape item and opposite a minor group of operating retail buildings. Indicative of the early importance of West Street and relic of urban development in the pre-motor car era.
House	11004	A good example of Federation Queen Anne style semi-detached residence, with elaborate timber detailing. Prominent example amongst a streetscape of smaller and plainer examples.
		Related items: No 58A West Street.
		No 54 and 56 West Street are similar in form and detail.



Heritage Item	ID	Statement of Significance
		Grouping: No 54-58 West Street, are a group of 2 pairs of semi- detached dwellings, with similarities in form and detail, built by the same builder.
		A good example of Federation Queen Anne style of residence.
		Related items: No 58A West Street. No 54 and 56 West Street are similar in form and detail.
House	11005	Grouping: No 54-58 West Street, are a group of 2 pairs of semi- detached dwellings, with similarities in form and detail, built by the same builder.
		Fine and characteristic example of a Queen Anne style semi- detached house, with elaborate timber detailing. Prominent example amongst a streetscape of smaller and plainer examples.
		A good example of Federation Filigree/ Queen Anne style of residence.
Vera Loblay House	I1002	Interesting house which graphically illustrates the stylistic transition underway around the turn of the century. Fine example of late, ornate cast-iron lace. One of a group of substantial houses on the border of North Sydney and Crows Nest, and indicative of the local character of the period. The front garden landscape setting and the interior are also of significance.
		A good example of Late Victorian Italianate / Filigree style of residence.
		Part of a row of three identical terraces forming fine group of late nineteenth century semi-detached houses. Part of a fine group of late nineteenth century semi-detached houses, exhibiting many characteristics of the type. Interesting for their large size and an important indicator of the local character of the vicinity at that time. For this reason, it has an important relationship with other large houses of the period nearby.
House	I1003	This is a Contributory item. A good example of Late Victorian Italianate / Filigree style of residence.
		Related items: No 41 & 43 West Street.
		Grouping: No 41-45 are a row development of 3 identical terraces, separated by a narrow alley way from a further group of two terrace style dwellings No 47 & 49 West Street.
		A fine group of late nineteenth century semi-detached houses, exhibiting many characteristics of the type. Interesting for their large size and an important indicator of the local character of the vicinity at that time. For this reason, it has an important relationship with other large houses of the period nearby.
House	I1001	This is a Contributory item. A good example of Late Victorian Italianate / Filigree style of residence.



Heritage Item	ID	Statement of Significance
		Related items: No 43 & 45 West Street.
		Grouping: No 41-45 are a row development of 3 identical terraces, separated by a narrow alley way from a further group of two terrace style dwellings No 47 & 49 West Street.
		A fine group of late nineteenth century semi-detached houses, exhibiting many characteristics of the type. Interesting for their large size and an important indicator of the local character of the vicinity at that time. For this reason, it has an important relationship with other large houses of the period nearby.
		This is a Contributory item. A good example of Late Victorian Italianate / Filigree style of residence.
		Related items: No 43 & 45 West Street.
House	11000	Grouping: No 41-45 are a row development of 3 identical terraces, separated by a narrow alley way from a further group of two terrace style dwellings No 47 & 49 West Street.
		A fine group of late nineteenth century semi-detached houses, exhibiting many characteristics of the type. Interesting for their large size and an important indicator of the local character of the vicinity at that time. For this reason, it has an important relationship with other large houses of the period nearby.
		This is a Contributory item. A good example of Late Victorian Italianate / Filigree style of residence.
		Related items: No 35 West Street.
Harra		Grouping: refer to streetscape notes.
House	10998	A good pair of late-nineteenth century houses which exhibit many of the finer characteristics of the type. Interesting for their relatively large size and an important indicator of the local character of the vicinity at that time. Important relationship to other large houses of the period nearby.
House	10997	A good pair of late-nineteenth century houses which exhibit many of the finer characteristics of the type. Interesting for their relatively large size and an important indicator of the local character of the vicinity at that time. Important relationship to other large houses of the period nearby.
Crows Nest Hotel	10181	Interesting large urban Edwardian hotel on an important intersection which has buildings similar in materials, form, style and period on each corner. A good example of the Free Classical Style from the early twentieth century, it has powerfully detailed elevations with Egyptian motifs set on a prominent corner site. It is an important local hotel.
Former Church of Christ	10143	Interesting example of an inter-war small church for a minority Christian group. Contrasts the buildings and churches of the



Heritage Item	ID	Statement of Significance
		mainstream Christian groups and important reminder of the diversity of beliefs present throughout the area's development. Provides evidence of the size, philosophy and activity of the church and its adherents. An interesting example of a two-storey brick church in the Inter War Gothic style that has been built on a residential subdivision for a minority church. The interior is also of significance as the main volume of the church is still evident although having been adapted for residential use.
North Sydney Boys' High School	10830	Important regional High School with associations stretching back to the origins of public education on the North Shore. Fine example of early twentieth century school architecture.
Flat building	10832	A very good example of a two-storey residential building in the Federation Queen Anne style with a well modelled façade and ornate joinery detailing. The decorative interior features are also of significance. The front landscape setting is significant to both the building and the group of items. Part of a row of four that form an impressive streetscape. See under 'Falcon Street Flats Group NSHS0977.'
"Winstone"	10831	A very good example of a two-storey residential building in the Federation Queen Anne style with a well modelled façade and ornate joinery detailing. The interior room configuration and interior decorative features contribute to the significance of the building. The streetscape garden setting contributes to the significance of the building and that of the group. Part of a row of four that form an impressive streetscape. See under Falcon Street Flats Group NSHS0977 Properties 182-188 Falcon Street.
Flat building	10834	A very good example of a two-storey residential building in the Federation Queen Anne style with a well modelled façade and ornate joinery detailing. Part of a row of four that form an impressive streetscape. The front landscape setting and interior are also of significance. See under 'Falcon Street Flats Group' NSHS0977.
Flat building	10833	A very good example of a two-storey residential building in the Federation Queen Anne style with a well modelled façade and ornate joinery detailing. Part of a row of four that form an impressive streetscape. The interior and front garden setting are also of significance. See under 'Falcon Street Flats Group' NSHS0977
House	10008	A very good example of a two-storey house in the Federation Queen Anne style with high quality detail and decoration. See under Falcon and Lytton Streets Group NSHS0890. Title: "Burrundullah."
House	10007	An elaborate and richly detailed two storey semi-detached house in the Federation Queen Anne style one of the most elaborate houses



Heritage Item	ID	Statement of Significance
		in the area.
		See under Falcon and Lytton Streets Group NSHS0890.
House	10835	An elaborate and richly detailed two storey semi detached house in the Federation Queen Anne style one of the most elaborate houses in the area. See under Falcon and Lytton Streets Group NSHS0890
House	10836	An elaborate and richly detailed two storey semi detached house in the Federation Queen Anne style one of the most elaborate houses in the area. See under Falcon and Lytton Streets Group NSHS0890.
House	10837	An elaborate and richly detailed two storey semi detached house in the Federation Queen Anne style one of the most elaborate houses in the area. See under Falcon and Lytton Streets Group NSHS0890.
House	10838	An elaborate and richly detailed two storey semi detached house in the Federation Queen Anne style one of the most elaborate houses in the area. See under Falcon and Lytton Streets Group NSHS0890.
The North Sydney Sewer Vent is an excellent example of the tall brick sewer ventilation shall constructed around the turn of the century to it functioning of the major outfall sewers. Its functioning of the major outfall sewer ventilation shall constructed around the turn of the century to it functioning of the major outfall sewer ventilation shall constructed around the turn of the century to it functioning of the major outfall sewer ventilation shall constructed around the turn of the century to it functioning of the major outfall sewer ventilation shall constructed around the turn of the century to it functioning of the major outfall sewers. Its successful application of around the turn of the century to it functioning of the major outfall sewers. Its function shall sewers. Its functioning of the major outfall sewers ventilation shall constructed around the turn of the century to it functioning of the major outfall sewers. Its functioning outfall sewers. Its functioning outfall sewers. Its functioning outfall sewers. Its functioning outfall sewers outfall sewers. Its functioning outfall sewers outfall sewers. Its functioning outfall sewers. Its functioning outfall sewers outfall sewers. Its functioning outfall sewers outfall sewers outfall sewers outfall sew		The North Sydney Sewer Vent is an excellent representative example of the tall brick sewer ventilation shafts which were constructed around the turn of the century to facilitate the efficient functioning of the major outfall sewers. Its functional design is embellished by a successful application of architectural motifs, such as line and texture which lend the structure an element of formalism and classical detail including entasis. In addition, it displays high quality workmanship in the brickwork. By virtue of its scale and form, it has landmark value within the Cremorne/ Crows Nest area and acts as a navigational beacon for motorists using the Bradfield Highway. It has the potential to invoke a sense of the past as its style and design contrast dramatically with the modern styles of architecture endemic to the North Sydney streetscape. It is currently serves the Northern Suburbs Ocean Outfall Sewer (NSOOS) in the manner in which it was originally designed.
St Leonards Park	I0916 01941	St Leonards Park is of state heritage significance for its historical values as one of the earliest established public parks in NSW being set aside as a recreation reserve in 1838 and gazetted as a public park in 1867. It also contains one of Australia's oldest, continuously used cricket grounds and bowling clubs established in 1867 and 1887 respectively.
		St Leonards Park is of state heritage significance as a surviving



Heritage Item	ID	Statement of Significance
		and relatively intact example of a Victorian public park in the gardenesque style. It retains many of its original aesthetic characteristics including an axial layout, formal pathways culminating in memorials, including an ornate Victorian style memorial fountain to Mayor William Tunks, and picturesque vistas. The park also includes distinctive examples of early 20th century architecture and Modern Movement architecture which are aesthetically distinctive and demonstrate a high level of creative and technical achievement.
		St Leonard's Park has state significant historic associations with Mr Edgar Herbert a noted pioneering specialist in physical education who was involved in the establishment of the playground and educational programs at the park in the 1930s and early 1940s.
		The site is of state heritage significance as a rare and representative example of a largely intact Victorian era park designed in the gardenesque style with its original layout still appreciable. It is also rare as a continuously used cricket ground and bowling club dating from 1867 and 1887 respectively. The post WWII music shell and Modern Movement style Bowling Club are rare surviving examples of these types of structures.
War Memorial	I1123	See also under St. Leonards Park Group 2180911. Prominent and imposing monument which occupies significant position on Ridge Street, opposite Walker Street. Important local representative of such monuments, in major central parkland.
W. Tunks Memorial Fountain	11124	Reconstructed fountain first erected in 1885 in this location. Built as a memorial to William Tunks, a figure in local politics, being first Mayor of East St. Leonards and Member of State Parliament for the area for ten years. Important to the park as Tunks was largely responsible for maintaining the integrity of the park in the 1870s. See also under St. Leonards Park Group.
North Sydney Oval	l1125	The most used and familiar facility in St. Leonards Park and one of the oldest cricket grounds still in use in Australia. Fine example of a traditional Cricket Oval, with Edwardian features and buildings creating an ambience associated with the English cricketing tradition. Contains some interesting buildings, the Grandstand, Scoreboard and three Turnstile buildings all dating from 1928 and the Bob Stand (1895) relocated from the Sydney Cricket Ground. (See also under St. Leonards Park Group).
North Sydney Hotel	10915	Important local hotel in the Inter War Free Classical style, on the main street and opposite St. Leonards Park and Oval. Interesting example of Free Classical architecture, which in it's context, produces a robust yet natural element in this streetscape.
Shop	10176	A good example of Inter-war Californian Bungalow/ Free Classical style of residence.



Heritage Item	ID	Statement of Significance
		Related: No 87 and 89A West Street. Grouping: No 87 and 89A West Street are part of a single shop front terrace row development. One of a set of shops (87, 89, 89A West Street) representative of the terrace shop building type, distinguished by its special glasswork and the intactness of the period detailing. This isolated example is indicative of the mixed development typical of this street. The ground floor entrances and glasswork have aesthetic, historic and social values; the building is of representative value only
Shop	10175	A good example of Inter-war Californian Bungalow/ Free Classical style of residence. Related: No 87 and 89A West Street. Grouping: No 87 and 89A West Street are part of a single shop front terrace row development. One of a set of shops (87, 89, 89A West Street) representative of the terrace shop building type, distinguished by its special glasswork and the intactness of the period detailing. This isolated example is indicative of the mixed development typical of this street. The ground floor entrances and glasswork have aesthetic, historic and social values.
Shop	10174	A good example of Inter-war Californian Bungalow/ Free Classical style of residence. Related: No 89 and 89A West Street. Grouping: No 87 and 89A West Street are part of a single shop front terrace row development. One of a set of shops (87, 89, 89A West Street) representative of the terrace shop building type, distinguished by its special glasswork and the intactness of the period detailing. This isolated example is indicative of the mixed development typical of this street. The ground floor entrances and glasswork have aesthetic, historic and social values.
Former hall	10144	Good example of a Federation Free Classical styled hall, well-proportioned and neatly detailed, on an important fringe of the Crows Nest commercial centre. The interior is also of significance.
House	10145	Excellent Queen Anne style cottage which, though modest in size, is well-detailed and attractive. Interesting in the context of a small-allotment, high-density working-class subdivision and illustrative of the variety of stylistic solutions to this environment.
House	10142	A fine example of Late Italianate villa style. Interpreted on a narrow-fronted, high-density subdivision. Despite the context, the building is well proportioned and attractive, though presently rundown, and it usefully illustrates the range of stylistic solutions applied to the residences of the working class of the period. Grouping: part of a former semi-detached pair.
North Sydney Girls' High School	10165	Important local school with significant place in the historical development of public education on the North Shore. Substantial and relatively intact original building from establishment of the school. Local and regional significance for former and current



Heritage Item	ID	Statement of Significance
		pupils and staff. Buildings are representative of period school architecture and philosophy.

APPENDIX 3

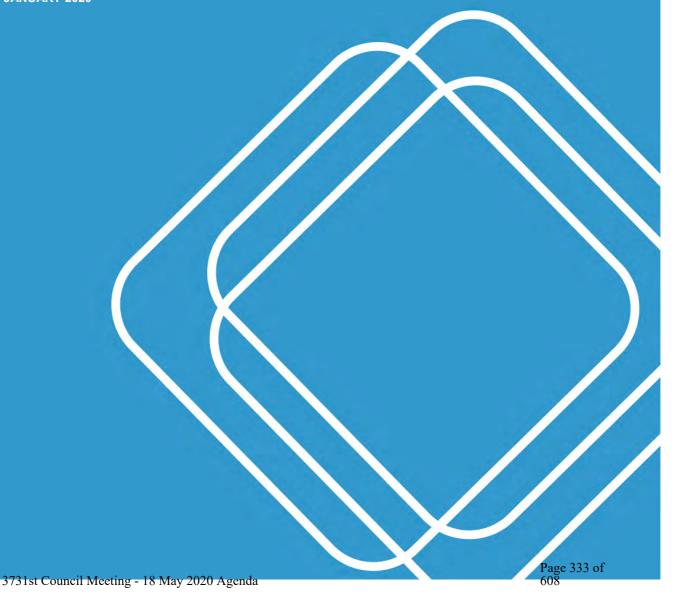
NORTH SYDNEY CIVIC PRECINCT PLANNING STUDY BASELINE TRAFFIC AND TRANSPORT CONDITIONS REPORT



CIVIC PRECINCT AND SURROUNDS PLANNING STUDY

Baseline Traffic and Transport Conditions Report

7 JANUARY 2020





Quality Assurance

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Client:	North Sydney Council	ABN:	32 353 260 317
Prepared by:	SCT Consulting PTY. LTD. (SCT Consulting)	ABN:	53 612 624 058

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1.0 Introduction

1.1 Background

SCT Consulting was engaged by CM+ on behalf of North Sydney Council to provide traffic and transport services to support the preparation of the Civic Precinct and Surrounds Planning Study¹. The aim of the planning study is to prepare an integrated Precinct Planning Study for the area to the north of the Central Business District, located in the vicinity of the new northern portal of Victoria Cross Metro Station at the northeast corner of the Miller Street / McLaren Street intersection.

The planning study is intended to:

- Ascertain the existing planning control framework and constraints;
- Ascertain the implications of the new northern Metro portal on the study area;
- Ascertain the requirements of the community and key stakeholders;
- Identify the desired future character and produce a 'community brief' for public benefit and infrastructure improvements;
- Produce some scenarios for the future of the study area;
- Identify the preferred future scenario for the study area;
- Produce documentation for the Precinct Planning Study;
- Produce and coordinate a Public Exhibition; and
- Incorporate feedback and produce final data and documentation for handover.

North Sydney CBD is Sydney's third largest commercial centre, with some 830,000 m² of commercial floor space and a growing mixed-use and residential periphery. Council's policies seek to grow and strengthen the commercial core of the CBD. Given the introduction of the new northern portal of the Metro, there will be greater accessibility to the Civic Precinct. Sydney Metro expects that providing the northern portal in addition to the southern portal will increase the patronage of Victoria Cross Station by five percent² because of the increased size of the station's walking catchment to the north.

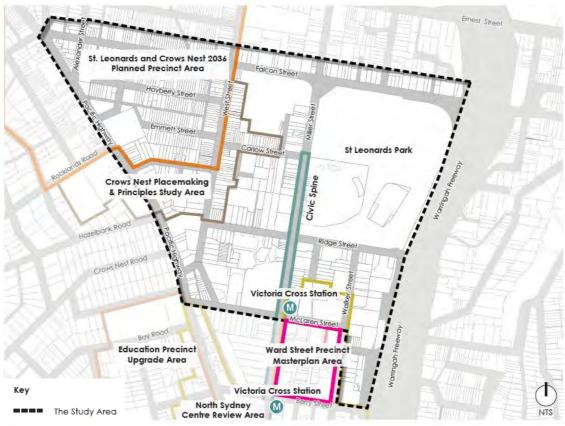
Hence, the study area for the supporting transport study follows the boundary defined in the Civic Precinct and Surrounds Planning Study map, shown in **Figure 1-1**.

¹ Civic Precinct and surrounds planning study, 2019

Victoria Cross Station and Artarmon Substation Modification Submission Report, 2017



Figure 1-1 Location of the study area



Source: CM+ (2019), Civic Precinct and Surrounds Planning Study

The study area is bounded by the Pacific Highway to the west, Warringah Freeway to the east, Falcon Street to the north (offset to rear of the property boundary for urban design purpose) and McLaren Street to the south, with a subdivision extended to Berry Street to the southeast.

1.2 Purpose of report

The purpose of this report is to undertake a baseline review of traffic and transport conditions of the study area, which aims to provide a holistic vision and guide the future development of the area following the introduction of the northern portal of Victoria Cross Metro Station. The Baseline Traffic and Transport Conditions Report has considered:

- Strategic transport planning context for the study area, including a review of relevant State and Council transport strategies and committed developments;
- Base Case transport conditions including an understanding of key movement patterns of all transport modes within the study area. This includes a review of publicly available data and information provided by Council, including an assessment of interchange capacity and catchment provided by the northern Metro portal; and
- Transport opportunities and constraints around the northern Metro portal based on high-level transport planning principles such as modal hierarchy and pedestrian desire lines, to support development of potential planning scenarios by CM+.



1.3 Report structure

This report has been structured into the following sections:

- Section 2 describes the existing transport conditions for all modes of transport;
- Section 3 considers the future transport planning context; and
- Section 4 summarises the opportunities and constraints around the study area.



2.0 **Existing Conditions**

2.1 Study area

The purpose of this assessment is to arrive at a better understanding of the current condition of the Civic Precinct and Surrounds. The existing zoning of the study area is shown in Figure 2-1.

Figure 2-1 The existing study area zoning Land zoning Legend Study area Land use zones B1 Neighbourhood Centre B2 Local Centre B3 Commercial Core B4 Mixed Use E2 Environmental Conservation E4 Environmental Living R2 Low Density Residential R3 Medium Density Residential R4 High Density Residential RE1 Public Recreation RE2 Private Recreation SP1 Special Activities SP2 Infrastructure UL Unzoned Land

Source: NSW Department of Planning, Industry and Environment; SCT Consulting, 2019

The study area comprises multiple land use types, including R2 - R4 Low to High Density Residential, B4 Mixed Use, RE1 Public Recreation and SP1 Special Activities. Land uses surrounding the study area include B3 Commercial Core to the south and northwest and residential land uses to the west and north.

The study area includes many major employment, activity destinations, health / education and open spaces such as St Leonards Park and North Sydney Oval in the northeast;

- North Sydney Boys High School in the north;
- North Sydney Girls High School in the west;
- Marist College North Shore, St Mary's Primary School and St Mary's Catholic Church in the centre; and
- North Sydney Community Centre, Stanton Library and North Sydney Council Chambers in the south.

Other nearby destinations include Mater Hospital and North Sydney Demonstration School to the west. A footbridge over the Warringah Freeway at Ridge Street provides a connection to Neutral Bay to the east.



2.2 Travel behaviour

2016 Journey to work data from the Bureau of Transport Statistics was analysed to determine current travel behaviour of the existing residents within the study area during peak periods. The Statistics Area Ones (SA1s) that represent the study area are shown in **Figure 2–2**.

Study area

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Figure 2–2 SA1s analysed for the travel behaviour of the study area

Source: SCT Consulting, 2019

At the time of the journey-to-work (JTW) data being collected in 2016, approximately 1,420 trip samples were included in the survey for the selected SA1s. According to the Australian Bureau of Statistics, a person in employment are those of working age who, during a short reference period, were engaged in any activity to produce goods or provide services for pay or profit.

The study area is serviced by North Sydney Station and frequent buses along Miller Street and the Pacific Highway, contributing to a high public transport mode share at 42 percent in 2016, two times of Sydney average level. Despite this, it still had a higher level of car commuters (31%) compared to the City of Sydney CBD, where single car commuters make up only 15 percent of trips to Pitt Street and Martin Place³.

22 percent of the residents walk only for their journey due to its location close to employment and good accessibility, showing that pedestrian facilities are a crucial factor in the success of the transport network for the study area. Cycling to work is low at only one percent, indicating potential needs to improve the cycle infrastructure.

2.3 Walking

The North Sydney CBD Public Domain Strategy notes that the pedestrian experience in North Sydney CBD is currently less than ideal. Pedestrians experience a CBD that is dominated by vehicle movements, vehicle noise and pollution, freeway signage, long wait times at major intersections and long expanses of passive and inactive frontages. Despite the high mode share involving walking, infrastructure currently prioritises vehicular passage over

³ Sydney Metro (2018), Sydney Metro City & South West Victoria Cross Over Station Development: Traffic and Transport Report, Table 1.



pedestrian movement⁴. Currently the dominant pedestrian movements are on Walker Street and Miller Street, which lead to and from North Sydney station.

2.3.1 Pacific Highway

The Pacific Highway to the west of the study area remains a major physical divide within the North Sydney CBD. It has large lengths of inactive street frontages, large land holdings, vehicle entrances and lack of variety in uses and outdoor dining due to building design, footpath arrangement, noise and air pollution and passing traffic.

2.3.2 Miller Street

Miller Street is the key north-south street that contains many of the important civic functions, heritage items and cultural institutions of North Sydney:

- The section between the Pacific Highway and Ridge Street established a 12-metre landscape setback zone to allow for a potential road widening on the east side. This setback area is a leafy paved space with good access to sunlight, views of the street and sky. The form of this space is currently fragmented and without structure with the public domain design relating to the individual entrances and buildings facing the street;
- The setback of the Council chambers and the library on the western side of Miller Street between Ridge Street and McLaren Street provides a similar setback that can be paved and planted in a consistent manner to maintain continuity and connectivity to the CBD; and
- On the east side of Miller Street between Falcon Street and Ridge Street, there is an opportunity to upgrade the curtilage of the park, grandstand colonnade and footpaths.

2.3.3 Walker Street

Walker Street in the east of the study area is steep between McLaren Street and Ridge Street. There are footpaths provided on both sides. It terminates at the southern gate of St Leonards Park at Ridge Street, where the North Sydney War Memorial is located.

2.3.4 West Street

West Street in the west of the study area has a pleasant walking environment with multiple refuges and pedestrian crossings. It has an active street frontage consisting of cafés, restaurants, specialty shops and hotels and on-street parking is provided on both sides of the street.

2.3.5 Ridge Street

Ridge Street is located between West Street and the eastern Ridge Street Lookout at the Warringah Freeway, and has multiple refuges and mid-block pedestrian crossings. A footbridge further extends over the Warringah Freeway to Neutral Bay.

There are two other major east-west corridors in the study area including McLaren Street and Falcon Street from the south to the north. They are generally designed with wide footpaths on both sides of the road to distribute the pedestrians from the major spine of Miller Street and extended to residences, education and parks in the study area.

The Civic Precinct also includes a range of minor roads to facilitate site access. These minor roads generally have low traffic volumes, low speeds and are walkable.

Within the study area, there are extensive footpath networks provided, offering safe pedestrian access and connectivity to a wide variety of local and regional facilities.

Walking catchments from the northern portal of the Victoria Cross Metro Station and southeast portal of Crows Nest Metro Station is shown in **Figure 2–3**.

Civic Precinct and Surrounds Planning Study

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⁴ North Sydney Council (2018), North Sydney CBD Public Domain Strategy Place Book Stage 1: Public Spaces Vision



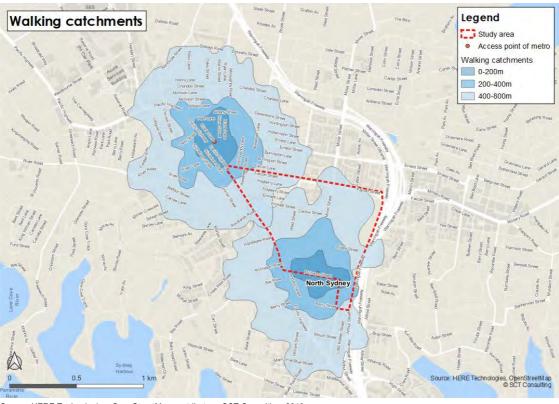


Figure 2-3 Walking catchment map of Victoria Cross Metro Station northern portal and Crows Nest south eastern portal

Source: HERE Technologies, OpenStreetMap contributors, SCT Consulting, 2019

From the north portal of the Victoria Cross Metro Station, the 200-metre walking catchment covers most of McLaren Street and activities on Miller Street south of Ridge Street. The 400-metre catchment further includes North Sydney Oval, Wenona School and residential areas in the west of the study area. North Sydney Girls High School is within 800-metre walking distance from the northern portal whereas North Sydney Boys High School is just beyond the 800-metre catchment area together with some residences and retail development in the northwest corner of the study area. However, those residences and retail components are more accessible from the Crows Nest Metro Station, which are within an 800-metre walking catchment area.

The area located in proximity to the intersection of West Street / Falcon Street is not within 800-metre walking catchment area of both future metro station portals.

2.4 Cycling

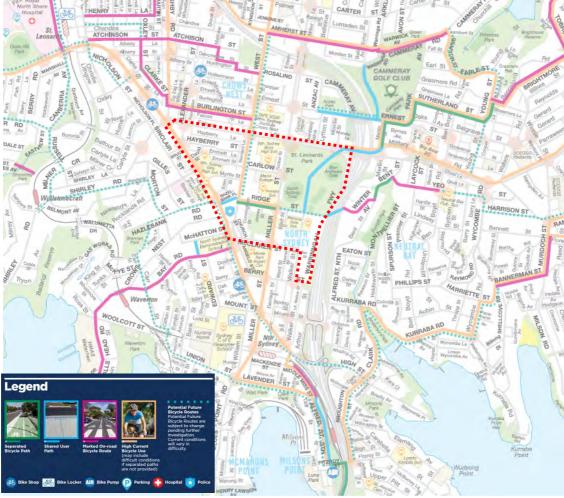
The North Sydney Transport Strategy notes that the proportion of people cycling in North Sydney is low compared to international standards⁵. However, it is a mode that Council is committed to increasing. Gaps in the network and difficult sections have a negative impact on cycling participation as poor amenity or potentially dangerous sections of road are a disincentive for those who might otherwise take up cycling. Cyclists must manoeuvre around frequently stopping buses and high traffic flow, particularly on Miller Street. At times, cyclists ride on the footpath to avoid difficult or dangerous sections, increasing the potential for conflict with pedestrians.

Within the study area, West Street, Ridge Street, Miller Street and the Pacific Highway are high-frequency bike routes as shown in **Figure 2–4**.

⁵ North Sydney Council (2017), Transport Strategy.



Figure 2-4 Cycle routes serving the study area (highlighted in red)



Source: North Sydney Council, Northern Sydney Cycle Guide + Map

The overall cycle infrastructure in the study area is limited to a separated bicycle path on Ridge Street (between West Street and Miller Street), a shared path in St Leonards Park and a short section on Hazelbank Place in the west.

Marked on-road cycle routes are present only on Bay Road to the west of the study area and Burlington Street / Clarke Street to the northwest of the study area.

The North Sydney Integrated Cycling Strategy⁶ identified the following gaps and difficult aspects of the cycling network:

- Cyclists are forced to ride along busy roads and negotiate busy intersections without formal bicycle facilities;
- Cyclists encounter pinch points where road space is suddenly lessened, and they are forced to merge with fast
- Cyclists encounter situations which compromise the mobility advantages gained from riding (such as wheel ramps up steps); and
- Cyclists ride on the footpath to avoid difficult or dangerous sections, increasing the potential conflict with pedestrians.

⁶ North Sydney Integrated Cycling Strategy, 2014



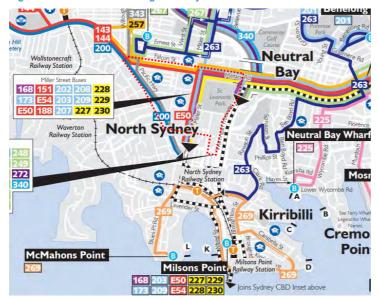
2.5 Public transport

The study area accommodates major bus services and it is an approximately 10-minute walk from North Sydney Station to the southern boundary of the study area. The close proximity of regular public transport is reflected in the relatively high public transport mode share for North Sydney. Council continues to prioritise public transport over private vehicles, and it is expected that this mode share will continue to increase. Victoria Cross Metro Station is current under construction and the northern portal is located at the northeast corner of the McLaren Street / Miller Street intersection within the study area.

2.5.1 Bus

The study area is serviced by up to 38 bus routes on Falcon Street, Miller Street and the Pacific Highway, as illustrated in **Figure 2–5**. Bus stops are located in pairs on Miller Street to the south of McLaren Street, which are both close to the northern portal of the Victoria Cross Metro Station.

Figure 2–5 Bus routes servicing the study area



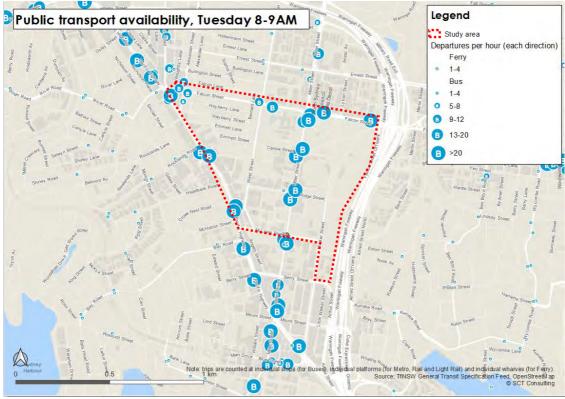
Source: TfNSW (2019), State Transit northern beaches and lower north shore network map Note: Not all bus services operate in this area are shown on this map

Bus frequencies on Miller Street and the Pacific Highway are mostly greater than one service every three minutes during a typical weekday AM peak hour, as shown in **Figure 2–6**. Less frequent services are provided at some bus stops on Falcon Street in the north of the study area.



Consulting

Figure 2-6 Service frequency during a typical weekday AM peak



Source: TfNSW, © OpenStreetMap contributors, SCT Consulting; 2019

2.5.2

North Sydney Station is part of the T1 and T9 lines and provides frequent services to destinations across Greater Sydney. To the south of the study area, there is an escalator portal on Miller Street to connect to the concourse through Greenwood Plaza. The portal bypasses all street crossings and is less than 300m to the station. Alternatively, the station can be accessed via Miller Street and Blue Street at ground level.



2.6 Road network

The major roads in the study area include the Pacific Highway, Miller Street and Falcon Street as shown in **Figure 2–7**.

Road classification

Character Street

Character

Figure 2–7 Road network of the study area

Source: NSW SIX maps, OpenStreetMap contributors, SCT Consulting, 2019

- Pacific Highway to the west of the site is a Primary Road which has a standard footpath. It is dominated by its
 movement function for local and through vehicle traffic at a speed limit of 60 km/hr. Transit lanes and bus lanes
 are designated within the study area for northbound and southbound travel respectively. There are 40 km/hr
 School Zones near Emmett Lane and Rocklands Road. Pedestrian crossings are provided at signalised
 intersections with an average spacing of about 200 metres;
- Miller Street is currently a north-south Arterial Road travelling through the study area with peak hour clearways implemented on both sides of the street. On-street parking is provided on most of the road segments within the study area. The frequent provision of school zones, pedestrian crossings at intersections and reasonably generous footpaths, make it a pedestrian friendly environment; and
- Falcon Street is an east-west Arterial Road in the north of the study area. It provides access to local
 residences, retail, restaurants and businesses. Clearways are present in both directions and parking is
 prohibited along most of the road segments with a signposted speed limit of 60 km/hr. A 40 km/hr School Zone
 is designated near North Sydney Boys High School and footpaths are provided on both sides of the street.

The rest of the road network within the study area are classified as Distributor Roads or Local Roads, that usually allow on-street parking and provide standard footpaths.



3.0 Strategic Context

3.1 The NSW Government Future Transport 2056 Strategy

NSW Government's Future Transport Strategy 2056⁷ sets the long-term vision for transport in NSW. The transport strategy builds on the Greater Sydney Region Plan⁸, which identifies that Sydney will grow as a global metropolis with benefits distributed more evenly across the city. It sets out a vision of three cities: the Eastern Harbour City, the Central River City and the Western Parkland City. This vision will guide many of the planning decisions that will deliver faster, convenient and reliable travel times to major centres, as shown in **Figure 3–1**.

The study area is located in the heart of the Eastern Harbour City, which covers the North Sydney and City of Sydney CBDs. In the Eastern Harbour City, new mass transit connections, including Sydney Metro, will vastly increase the capacity of the public transport network from 2024 onwards – crucial to supporting growth and tackling congestion.

Specific outcomes listed as part of the Future Transport Strategy which will benefit the Eastern Harbour City include:

- 30-minute public transport access for customers to their nearest Centre by public transport 7 days a week;
- Encouraging walking, cycling and public transport; and
- Attractive spaces where people can meet and enjoy their leisure time.

Western Sydney
AirportBadgerys Creek
Aerotropolis

Campbelltown
-Macarthur

Central River City

Eastern Harbour City

Eastern Harbour City

Farramatta

Harbour
CBD

Figure 3-1 A future metropolis of three cities

Source: Greater Sydney Commission (2018), Greater Sydney Region Plan: A Metropolis of Three Cities

Implications for Civic Precinct and Surrounds Planning Study: North Sydney's Civic Precinct will be directly serviced by Victoria Cross and Crows Nest Metro Stations (currently under construction; planned to open in 2024). This supports the aspiration of 30-minute access to employment centres by public transport for everyone. The new Metro stations present opportunities to encourage public transport use in the study area, including through interchange with other modes.

⁷ NSW Government (2018), Future Transport Strategy 2056.

⁸ Greater Sydney Commission (2018), Greater Sydney Region Plan: A Metropolis of Three Cities.



3.2 North District Plan

The Greater Sydney Commission's North District Plan⁹ is a 20-year plan to manage growth in Sydney's North District, supporting the long-term vision for Sydney as a metropolis of three cities. The District Plan assists Councils to plan for and support growth and change and align their local planning strategies to place-based outcomes. It guides the decisions of State agencies and informs the private sector and the wider community of approaches to manage growth and change.

The vision for the North District is to enhance the Eastern Economic Corridor (which North Sydney CBD is part of), supporting jobs growth in strategic centres and creating and renewing great places (**Figure 3–2**).

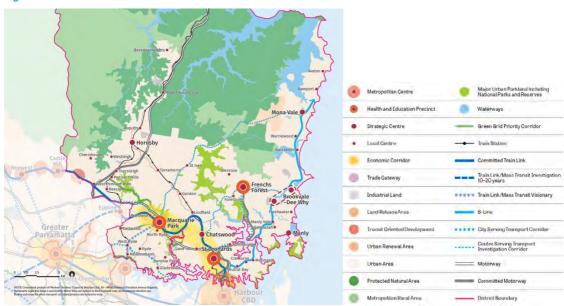


Figure 3-2 Future of the North District

Source: Greater Sydney Commission (2018), Our Greater Sydney 2056: North District Plan

North Sydney is identified as being part of the Harbour CBD. The Harbour CBD contains 22 percent of Greater Sydney's jobs and is Australia's financial business capital, containing a large proportion of the regional headquarters of multinational and national companies. The North District Plan notes that maintaining a long-term supply of office space will maintain Greater Sydney's global economic role and should not be compromised by residential development. Future planning for the Harbour CBD will balance high levels of development with high levels of amenity. North Sydney is directed to grow its job base from 60,000 in 2016 to at least 76,000 by 2036 (**Figure 3–3**).

Figure 3-3 North Sydney job range targets



Source: Greater Sydney Commission (2018), Our Greater Sydney 2056: North District Plan

In particular, the North District Plan directs North Sydney to:

 Grow jobs in the North Sydney CBD to maximise the land use opportunities provided by the new Victoria Cross Metro Station:

Civic Precinct and Surrounds Planning Study

13

⁹ Greater Sydney Commission (2018), Our Greater Sydney 2056: North District Plan.



- Grow jobs in the centre and maintain a commercial core;
- Strengthen North Sydney's reputation as an education centre, to grow jobs and add diversity;
- Expand after-hours activities;
- Encourage growth in business tourism as a conference location that takes advantage of North Sydney's identity
 as a business hub, its location, access and views;
- Improve amenity by reducing the impact of vehicle movements on pedestrians; and
- Prioritise infrastructure investments, particularly those focused on access to the transport network, and those which enhance walkability within 2 kilometres of metropolitan or strategic centres.

Implications for Civic Precinct and Surrounds Planning Study: The growth of jobs and diversity in North Sydney's CBD is supported by the North District Plan. In particular, Victoria Cross Metro Station will provide excellent access to employment, education and activities in the study area by further use of sustainable transport modes. The investment priority of enhancing walkability is particularly relevant to the Civic Precinct and Surrounds study area, as pedestrian volumes will increase significantly with the delivery of the Victoria Cross northern portal.

3.3 North Sydney Transport Strategy

The North Sydney Transport Strategy sets out North Sydney Council's vision for transport and modal hierarchy in North Sydney¹⁰. According to the Strategy, transport will be planned and managed through safe travel, transport security, social well-being, active health, fair access to parking, environmental sustainability, local environments, transport affordability, minimising congestion and business activity which are prioritised in the modal hierarchy below. Based on these priorities the Strategy established the following modal hierarchy for the future of North Sydney transport planning:

Figure 3-4 Modal hierarchy for North Sydney

Priority 1	Walking	
Priority 2	Cycling	
Priority 3	Public Transport	
Priority 4	Local Deliveries & Freight	
Priority 5	Private Vehicles	

Source: North Sydney Council (2017), Transport Strategy

Planning for infrastructure and land use should be informed by this modal hierarchy, prioritising in the order of walking, cycling, public transport, local deliveries & freight and then private vehicles.

Implications for Civic Precinct and Surrounds Planning Study: The study area has great opportunity to reinforce the attractiveness of North Sydney's top three modal priorities by improving amenities for pedestrians and cyclists. The proximity to the Metro Station will encourage people to use public transport. Walking and cycling will need to be prioritised to ensure that access to the station is comfortable and convenient.

3.4 Draft North Sydney CBD Transport Masterplan

The Draft North Sydney CBD Transport Masterplan¹¹ was prepared by North Sydney Council to explore placemaking opportunities and identify travel safety and amenity issues arising from the delivery of Victoria Cross Metro Station. It envisions that by 2036, public domain, accessibility, travel safety and amenity will be improved to create a more attractive, vibrant, active and flourishing North Sydney CBD.

The Masterplan identifies four guiding principles to support this vision:

¹⁰ North Sydney Council (2017), Transport Strategy.

¹¹ North Sydney Council (2018), Draft North Sydney CBD Transport Masterplan.



- 1. Prioritising the delivery of walking infrastructure that best accommodates significant local trip growth;
- 2. Minimising local traffic growth;
- 3. Creating slow speed traffic environments within the CBD; and
- 4. Encouraging regional through traffic to use alternative routes to bypass the CBD.

A VISSIM microsimulation model has been developed for the North Sydney CBD Transport Masterplan, where one of the main objectives was to analyse the feasibility / impacts of reducing through traffic volumes in North Sydney to improve walking. Prioritising Falcon Street as key route between Pacific Highway and Warringah Freeway via Falcon Street saw transfer of regional trips away from the CBD and onto Falcon Street. It is proposed to prioritise the left turn (increase green time) from Pacific Highway to Falcon Street and prioritise the right turn from Falcon Street to the Pacific Highway in the AM and PM respectively. The removal of regional traffic from the CBD was identified as a strategic principal for the project based on internal and external consultation.

Lower speed, local cycling access will be encouraged through the delivery of slow speed traffic environments in the North Sydney CBD whilst higher speed, regional cycling activity will be encouraged for access into, out of and around North Sydney CBD by providing high quality separated cycling infrastructure via a CBD bypass route on classified roads. This will assist in minimising conflict between local walking movements and regional cycling movements within the CBD core.

The Masterplan suggests multiple options to enhance the CBD's public domain for pedestrians, with the preferred option to pedestrianise Miller Street from Pacific Highway to Berry Street (as shown by the green area in **Figure 3–5**).

Bus Stop

Central Laneways

Central Laneways

Australian Start

Bus Stop

Treet-house #656

Bus Stop

Bus Stop

Bus Stop

Mount St

Bus Stop

Bus Stop

Mount St

Bus Stop

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Bus Stop

Figure 3-5 Key Draft North Sydney CBD Masterplan recommendations

Source: North Sydney Council (2018), Draft North Sydney CBD Transport Masterplan

The CBD Transport Masterplan identifies that the McLaren Street / Miller Street intersection will see a 213 percent increase in pedestrians from 2014 to 2036, with 2,500 pedestrians per peak hour using the intersection following the completion of the Victoria Cross Station. The intersection is forecast to have sufficient pedestrian capacity, with a Level of Service C for all holding areas as shown in **Figure 3–6**.



Figure 3-6 Holding areas for McLaren St / Miller St



Source: PSA consulting (2018), North Sydney CBD Transport Masterplan Final Report

Since the northern portal for the Victoria Cross Station will be located at the northeast corner of this intersection, pedestrians who use this portal and travel north may cross Miller Street mid-block north of the intersection. Further mitigation is needed to consider the pedestrian desire line and cater for the growing pedestrian demand from / to the northern portal.

Implications for Civic Precinct and Surrounds Planning Study: The study area should consider Council's intention to increase walking and cycling infrastructure and create vibrant places on key CBD streets through reassigning regional traffic to Falcon Street as the bypass. Although the McLaren Street / Miller Street intersection is forecast to have sufficient capacity to accommodate future pedestrian volumes, any issues with pedestrians crossing Miller Street mid-block from the northern portal should be monitored following the opening of the Metro station and mitigated if necessary.

3.5 North Sydney CBD Public Domain Strategy

North Sydney Council's CBD Public Domain Strategy¹² establishes a vision for the CBD's public domain and identifies a suite of short to long term projects to deliver this vision. The objective is to ensure that CBD growth and the Victoria Cross Metro Station are complimented by a public domain that improves the safety, amenity, vibrancy and overall appeal of the centre for workers, residents, students, visitors and investors.

¹² North Sydney Council (2018), North Sydney CBD Public Domain Strategy Place Book Stage 1: Public Spaces Vision.



The first stage of the Public Domain Strategy was published for exhibition and community consultation in October 2018. In May 2019, Council resolved to move forward with Stage 2, which will develop a plan to deliver the ideas and proposals included in the Public Domain Strategy¹³.

3.5.1 Miller Walk

The Public Domain Strategy recommends a 5-phased pedestrianisation of Miller Street between the Pacific Highway and Berry Street, named 'Miller Place'. The 5 phases consisting of trials, traffic changes, partial closure, implementation of a transit bus mall and finally the full opening of Miller Place in 2024 when the Metro becomes operational.

North of Berry Street, the Public Domain Strategy proposes to change the existing Miller Street to 'Miller Walk' as shown in **Figure 3–7**. Miller Walk will unite the key public institutions and public spaces along Miller Street to visually connect Miller Place to North Sydney Oval and St Leonards Park.

COMMUNITY CENTRE

COUNCIL CIVIC PARK LIBRARY

TRE

VICTORIA
CROSS
METRO

MILLER WALK
CENTRAL

VICTORIA
CROSS
METRO

OVAL
PLAZA
PLAZA
PLAZA
PLAZA

Figure 3-7 Indicative map of the Miller Walk

Source: North Sydney Council (2019), Public Domain Strategy

The Strategy suggests many opportunities for the northern Metro Station portal to better engage with local school, events and communities as part of Miller Walk. This includes:

- Exploring a scramble crossing at the Miller Street / McLaren Street intersection;
- Upgrading the main entrance to Stanton Library to better connect the building and activities to Miller Street;
- Upgrading the south entry plaza area to the oval, a sunny square with new seating, gardens, and paving, connecting to the War Memorial at Walker Street and Ridge Street intersection;
- Upgrading the north entry plaza area to the oval and park with new seating, gardens, palm trees, and paving;
 and
- Possible extension of Miller Walk to Falcon Street with widened footpaths, gardens, seating and new avenue planting. Connect Miller Street Civic Spine to Music Shell outdoor concert area, park green spaces and upgraded playground.

3.5.2 Future cycling improvements

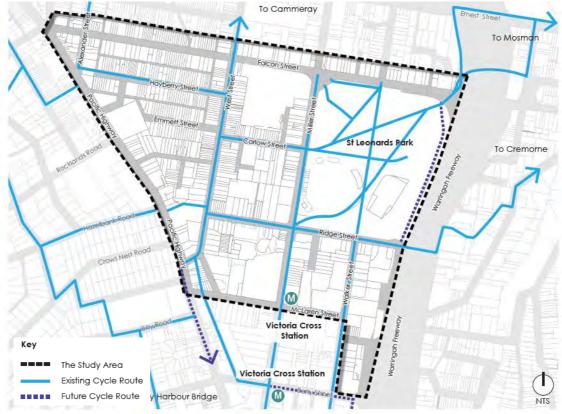
Cycling has the ability to influence and change the face of transport in North Sydney. A north-south pedestrian / bike connection from the Metro North Portal to St. Leonards Park through the metro site and Elliot Street is also proposed, which extends the Central Laneway Project from Blue Street, via Greenwood Plaza, Denison Street and through the Ward Street masterplan.

¹³ North Sydney Council (2019), Public Domain Strategy – Stage 1 Update June 2019. Available from: https://www.northsydney.nsw.gov.au/Building Development/North Sydney CBD/Public Domain Strategy - Stage 1



The Civic Precinct and Surrounds Planning Study¹⁴ illustrated the existing bicycle routes and future bicycle routes as shown in **Figure 3–8**.

Figure 3-8 Bicycle routes in and around the study area



Source: CM+ (2020), Civic Precinct and Surrounds Planning Study

A new separated cycleway is proposed for the eastern edge of St Leonards Park as part of the 2018 CBD Transport Masterplan connecting the Falcon Street footbridge to Ridge Street in additional to on-street routes on Miller Street and Walker Street. Other cycle routes are planned on Pacific Highway and Berry Street.

Implications for Civic Precinct and Surrounds Planning Study: Pedestrians walking to / from the CBD experience a generally level environment via Miller Street and West Street. Miller Walk will accommodate a greater range of public spaces and urban experiences. There is also potential to significantly improve the cycle environment in the Civic Precinct and cycle connections to surrounding areas.

¹⁴ CM+ (2019), Civic Precinct and Surrounds Planning Study



3.6 Victoria Cross Station northern portal

The Victoria Cross Metro Station is currently under construction to the south of the study area. The underground station is due to open in 2024, together with the completion of the Sydney Metro City & Southwest Line from Chatswood to Bankstown via the City of Sydney CBD (**Figure 3–9**).

Tallawong

Rouse Hill

Schofields

Kellyville

Hills
Showground

Cherrybrook

Castle Hill

Norwest

Castle Hill

Westmad

Westmad

Westmad

Rouse Hill

Showground

Cherrybrook

Castle Hill

Construction
Sydney Marco Rouse

Westmad

Westmad

Rouse Hill

Norwest

Construction
Sydney Rouse Outer Sydney International (Norwest Sydney International Rouse International R

Figure 3-9 Sydney Metro Northwest, City & Southwest and West lines; Victoria Cross Station is highlighted in red

Source: Adapted from Sydney Metro (2019), Victoria Cross Integrated Station Development Planning and information booklet

Victoria Cross Station will be 31 metres below ground level. Station access and entry will be via the pedestrian plaza opening to Miller Street and Berry Street via Denison Street, and via a northern portal on McLaren Street one block further to the north (**Figure 3–10**).



Figure 3–10 Vertical indicative planning of the Victoria Cross Metro Station

Source: Victoria Cross Integrated Station Development, 2019



An underground pedestrian walkway about 38 metres long will be located within the underground services building connecting the northern station portal to the northern end of the station mezzanine level and platforms. This would shorten the overall walking distance particularly for northbound users since the main concourse is located between the northern and southern portal.

The Sydney Metro City & Southwest line is currently under construction and due to open by 2024. At Victoria Cross Station, the approved works include:

- Victoria Cross Station, located beneath Miller Street between McLaren Street and south of Berry Street; and
- A northern station portal and services building at 50 McLaren Street. The northern station portal will include a lift-only pedestrian entry to Victoria Cross Station as shown in Figure 3–11.

Entry plaza

McLoren Street

Concourse

O 25 50m

Indicative only, subject to design development

KEY

Bus stop

Proposed kiss-and-ride

Existing taxi rank

Figure 3-11 Northern portal of the Victoria Cross Metro Station

Source: Sydney Metro (2017), Victoria Cross Station and Artarmon Substation modification report, Part-1

The northern portal will support better pedestrian access to Victoria Cross Station by extending the northern catchment of the station. It includes pedestrian access from the corner of Miller Street and McLaren Street and a ticket gate line. Four lifts will provide access to the platforms. The lifts will have a capacity to accommodate approximately 27 persons per lift and would be 'through' lifts whereby the entry doors open a few seconds after the exit door so the entry and exit sequence overlaps. Kiss-and-ride and taxi facilities are also proposed on the northern side of McLaren Street.

Demand forecasts in various existing documents indicate person trips during a typical weekday AM peak in 2036 as shown in **Table 3-1**. It is estimated that the northern portal will account for nearly half of the total station entry trips at AM peak, indicating the important function for residents in the study area. On the other hand, the northern portal exit trips in the AM peak only account for about 10 percent of the total person trips.

Table 3-1 2036 AM peak demand forecast summary for Victoria Cross Metro Station

Direction	Entry	Exit
Total person trips for Victoria Cross Metro Station	1,829 to 2,300 trips	10,900 to 12,863 trips
Total person trips for Northern Portal	800 to 1,050 trips	1,000 to 1,250 trips

Source: Sydney Metro Victoria Cross Station and Artarmon Substation Modification Report, June 2017 and North Sydney CBD Transport Masterplan, August 2018



Based on Sydney Metro's forecast patronage, there would be a maximum queue at the lifts of 20 people and a maximum wait time of 21 seconds. With one of the four lifts out of service the maximum queue would be 33 people with a wait time of 48 seconds, indicating generally a good level of service for customers.

Implications for Civic Precinct and Surrounds Planning Study:

Current condition: The precinct surrounding the future station includes high density commercial centre (to the south), residential areas, a number of educational facilities, civic services and local shops. It is within 10 minutes' walk to the North Sydney Train Station and has frequent bus service up to two services per minute in the nearby bus stops during peak hour. The precinct also allows a high portion of trips made on foot. Traffic count in 2015 indicated that Miller Street carried around 1,100 vehicles in both directions and the intersection of Miller Street / McLaren Street was operated at Level of Service C¹⁵ during AM peak.

Condition with the opening of the Victoria Cross Station northern portal: North Sydney demonstrated historically high public transport mode share with the provision of train stations and extensive bus services. The open of Sydney Metro Northwest has indicated significant travel behaviour change where patronage surveys by Transport for NSW showed up to 66,000 passengers per day on the metro line, equivalent to traffic on a six lane arterial road worth of mode shift from car. Travel time savings from the Sydney CBD and northwest are both around 15 minutes when travelling with the new metro to Victoria Cross. The northern portal will support better pedestrian access to Victoria Cross Station by extending the northern catchment of the station. It is currently unknown how public transport services and operations will change as a result of the opening of Victoria Cross Station.

¹⁵ Victoria Cross OSD - Transport, traffic and parking assessment report



3.7 Ward Street Precinct Master Plan

The Ward Street Precinct lies to the south of the study area and consists of the block bounded by Miller Street, McLaren Street, Walker Street and Berry Street. A master plan has recently been prepared for the precinct, with key design principles including integrating with the Metro, completion of the pedestrian core, enhancement of public space, establishment of a hub for public life and strengthening the commercial centre. The final Ward Street Precinct Master Plan is shown in **Figure 3–12**.

Figure 3-12 Final Ward Street Master Plan



Source: North Sydney Council (2019), Ward Street Precinct Master Plan Final

The Final Precinct Master Plan contains over 5,400 m² of total public open space, an upgrade of the podium including a knowledge hub and cultural hub, floor area uplift of existing buildings and provision of multiple pedestrian links from the west, north and south to the central square and green square.

The loading and parking strategy supports the development of a car free pedestrian precinct, with the existing Harnett Street car park providing access to the basement areas. It will enable loading and parking to occur below the pedestrian precinct of Ward Street and the Central Square and reduce the traffic load on Berry Street. The final masterplan delivers less parking within the precinct than the current level of parking.

Implications for Civic Precinct and Surrounds Planning Study: The proposed Ward Street Precinct intends to enhance the pedestrian connection from surrounding roads to the central plaza, and also provides the opportunity for through site links for people to access the Metro. The precinct will minimise the vehicular traffic through reducing the parking provision and implementing car free precinct. The Ward Street Precinct will extend the much improved pedestrian environment to the Civic Precinct.



3.8 Western Harbour Tunnel and Beaches Link (WHTBL)

The NSW Government continues to progress planning for the proposed Western Harbour Tunnel and Beaches Link. The Western Harbour Tunnel would deliver a new crossing of Sydney Harbour to take pressure off the Sydney Harbour Bridge and Tunnel and creates a western bypass of the Sydney CBD. The project also includes upgrading four kilometres of the Warringah Freeway.

In the vicinity of the study area, an on-ramp is proposed from Berry Street to connect to the southbound carriageways. An off-ramp is proposed to connect to Falcon Street westbound, as shown in **Figure 3–13**.

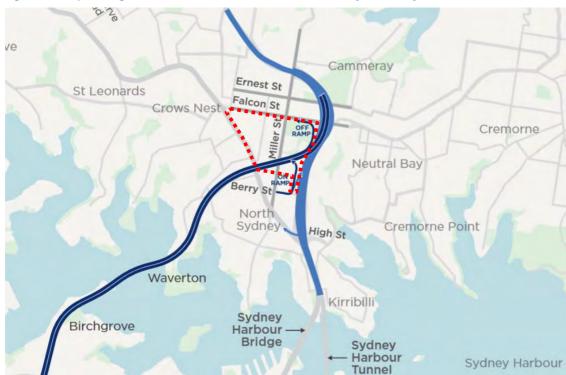


Figure 3-13 Proposed alignment of the Western Harbour Tunnel in the vicinity of the study area

Source: Roads and Maritime Services (2018), Western Harbour Tunnel project update

Work on the Warringah Freeway will include a new, uninterrupted bus lane from Cammeray to the Sydney Harbour Bridge, which will feature an access link to North Sydney and allow express buses from the Northern Beaches and North Shore to have direct access to Sydney Trains at North Sydney Station and the new Sydney Metro at Victoria Cross. The project will also create the potential to introduce direct express bus access between the Inner West and Lower North Shore.

With the project, new cycleway infrastructure and improvements will contribute to the North Shore Link Cycleway. Ridge Street Bridge will be rebuilt to provide more room for cyclists and pedestrians. A dedicated cycleway between Miller Street, Cammeray Park and Falcon Street will be implemented, and the Ernest Street Bridge across the Warringah Freeway will be upgraded to link Cammeray Golf Course with Anzac Park, better connecting local communities and schools.



Implications for Civic Precinct and Surrounds Planning Study:

Current condition: Berry Street and Falcon Street provides on ramp and off ramp for Warringah Freeway, respectively, which play very important role for the vehicular access of the Civic Precinct. Berry Road to the south is the entry point for both northbound and southbound of the Warringah Freeway that forms Sydney's eastern part of the orbital motorway system whilst Falcon Street exit is the second exit after entering North Shore following the exit at North Sydney / Pacific Highway and mainly services Crow Nest. Based on desktop review and typical traffic conditions provided by Google Maps, road sections in the vicinity of the existing ramps show slow speed performances during peak hours probably due to heavy traffic volume associated with traffic to / from North Sydney CBD, and Crows Nest and long waiting time at signalised intersections for pedestrians and opposing vehicles.

Condition with the opening of WHTBL: Travel time to North Sydney can be saved by around 10 minutes especially from Sydney Inner West¹⁶. The proposed on / off-ramps of the Western Harbour Tunnel at Berry Street and Falcon Street could attract more motorised traffic in and through the study area. This may increase conflicts between walking, cycling and car traffic and result in a less walkable environment for the precinct, in particular along Miller Street. The impacts on Miller Street could be minimised if Pacific Highway becomes the main feeder route to the WHTBL on ramp via Berry Road.

Benefits of the new tunnel and the associated freeway upgrade also include improved cycleways and potential new bus connections, which further encourage sustainable transport use to / from the precinct. Further quantitative assessment is required to estimate the additional car trips on the road network due to WHTBL and assess intersection performance and its impact on pedestrians / cyclists.

3.9 St Leonards / Crows Nest Planned Precinct

The boundary of St Leonards / Crows Nest Planned Precinct is shown in **Figure 3–14**, which overlays with a small proportion of the study area in the southeast including residential and retail land use bounded by Falcon Street and Pacific Highway.

New mixed use, high density between two prepare an integrated stantegy for the Health and Educational Precinit.

Retain inclustral and urban services land to protect and encourage jobs.

Retain inclustral and urban services land to protect and encourage jobs.

Investigate site for a new primary and high school.

New St.teonards Plaza over an illine proposed by Lane Cove Council protect Heritage.

Improved pedestrian crows page across Pacific Highway

Retain commercial zone and protect Heritage.

Retain valuage feel of Willoughty Read including fine grain and human scale regional open space cross pagin.

Retain commercial zone and screen and protect the proposal for independent reviews and proposal for independent reviews and proposal for independent reviews and falus Reserve.

Rev. north-south open space white the proposal for independent reviews and falus Reserve.

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Figure 3–14 Draft 2036 Plan for St Leonards and Crows Nest Planned Precinct

Source: Department of Planning and Environment, October 2018

¹⁶ Western Harbour Tunnel Project update, Page 5



The draft 2036 Plan for St Leonards and Crows Nest Planned Precinct is based on a vision for growth and improvement in the area to 2036.

North Sydney CBD Capacity and Land Use Strategy implied that Falcon Street is expected to be one of the main gateways to North Sydney CBD. There will be increased pedestrian and cyclist crossing movements particularly at the intersection of Falcon Street / Miller Street to access North Sydney CBD and future Victoria Cross Metro Station.

Implications for Civic Precinct and Surrounds Planning Study: It is acknowledged there could be increase of traffic volume on Falcon Street to connect between Pacific Highway and Warringah Freeway and to support local access to North Sydney CBD. Falcon Street is expected to form the main feeder traffic routes to / from Warringah Freeway such that through traffic within the precinct including Miller Street can be minimised.

3.10 North Sydney Development Control Plan (DCP)

North Sydney DCP regulates <u>maximum</u> parking rates for all development to minimise the reliance on private car and encourage sustainable transport use. For example, residential flat buildings adopt one space for studio, 1-2 bedroom apartments, and 1.5 space for 3 bed apartments. Visitor parking adopts 0.25 spaces per dwelling.

North Sydney Council has recently worked with State Agencies including Transport for NSW and Department of Planning, Industry and Environment to adopt a minimalist approach to determine parking rates including consideration of the feasibility of zero parking allocations to some land uses at St Leonards and Crows Nest Planned Precinct, given its proximity to St Leonards train station and Crows Nest Metro station as well as high frequency public transport corridors with the intention to minimise the reliance on private car uses.

Feedback from Council notes that there are very few developments in North Sydney that have been approved with zero parking unless there are physical constraints of building car parks. Zero / minimal parking could still contentious with local residents as they are concerned about flow-on impacts to on-street parking.

Other recent examples of restrained parking rates for developments with excellent accessibility to public transport include State Significant Development Applications (SSDAs) for Kellyville and Bella Vista Station Precinct. Transport for NSW recommended lower parking rates for high density residential development that is highly accessible to public transport based on:

- 0.4 spaces per 1 bedroom unit;
- 0.7 spaces per 2 bedroom unit;
- 1.20 spaces per 3 bedroom unit; and
- 1 space per 10 units (visitor parking).

Car sharing schemes are also designed to enhance sustainable transport modes, such as walking, cycling and public transport by filling a "mobility gap" – that is providing access to a vehicle on an "as needs" basis, without the high cost of ownership or private parking space provision to minimise the impact on the safety and efficiency of existing roads.



3.11 Summary of the potential projects

Table 3-2 summarises the status of the projects that may have impact on the study area.

Table 3-2 Summary of the potential projects

Project	Status	Implication to the study area
North Portal of Victoria Cross Metro Station	Under construction	Extend the northern catchment of the station and increase the metro patronage by 5%. It is currently unknown how public transport services and operations will change as a result of the opening of Victoria Cross Station.
Western Harbour Tunnel and Beaches Link (On / off ramp at Berry St and Falcon St)	Committed ¹⁷	More car trips associated with the new entry / exit points and impact on active transport. Public transport and cycle infrastructure upgrade promote the use of sustainable transport.
Ward Street Precinct Master Plan	Committed (Endorsed by Council)	Enhance the pedestrian connection from surrounding roads to the central plaza and provides the opportunity for through site links for people to access the Metro. The precinct will minimise the vehicular traffic through reducing the parking provision and implementing car free precinct. The Ward Street Precinct will extend the much-improved pedestrian environment to the Civic Precinct.
Miller Place - pedestrianisation of Miller Street (between Pacific Highway and Berry Street)	Potential	Creating a high quality urban heart and a ceremonial centre for North Sydney associated with the open of the Metro station, simplification of the Pacific Highway/ Miller Street intersection.
Miller Walk (between Berry Street and Falcon Street)	Potential	Uniting the key public institutions and public spaces along Miller Street to visually connect Miller Place to North Sydney Oval and St Leonards Park.
New separate cycle way on the western edge of St Leonards Park	Potential	Improve safe cycle trips and facilitate the connection from Neutral Bay across the Warringah Freeway.
North-south pedestrian / bike connection from the Metro North Portal to St. Leonards Park via Elliot Street	Potential	Extend the central Laneway from the North Sydney Train Station to St Leonards Park.

Source: SCT Consulting, 2019

¹⁷ https://infrastructurepipeline.org/project/western-harbour-tunnel/



4.0 Summary of Opportunities and Constraints

The opportunities and constraints for the Civic Precinct and Surrounds from the transport perspective are summarised as:

4.1 Opportunities

- Mode share data for the study area indicates a high public transport mode share for residents, given access to North Sydney Station and the high numbers of bus services, and easy access to nearby employment;
- Walking in the study area is generally safe, with dedicated pedestrian crossings at signalised intersections and School Zones present along the Pacific Highway, Miller Street and Falcon Street;
- Pedestrians walking to / from the CBD experience a relatively level environment via West Street and Miller Street in the study area;
- After completion of the Victoria Cross Metro Station northern portal, the McLaren Street / Miller Street intersection is forecast to have sufficient pedestrian capacity at all holding areas;
- The Victoria Cross Metro Station northern portal will shorten the walking distance between the station concourse and the study area. It is also expected to contribute to increase the patronage of Victoria Cross Station by five percent;
- The proposed Western Harbour Tunnel and Warringah Freeway upgrade could contribute to the construction of the North Shore Link Cycleway and provide direct bus routes between North Sydney and the Inner West;
- Both State and Council transport plans and strategies support job growth and promote better walking, cycling
 and public transport in the study area, including investment in footpath and public space provision;
- North Sydney Council has proposed plans for Miller Walk, which would unite the streetscape with the surrounding public institutions and public spaces along Miller Street;
- There is potential to significantly improve cycleways in the area and deliver connectivity for local cyclists to the wider cycle network;
- The Ward Street Precinct Master Plan and existing planning proposals for multiple sites within the study area focus on better pedestrian environments and new through-site links, as well as minimising the impacts of car traffic. The project is committed and endorsed by Council;
- Falcon Street, Pacific Highway and Berry Street should be designed to be main feeder traffic routes for through traffic to / from Warringah Freeway (subject to regional traffic guidance measures) and the future WHTBL such that the through traffic within the precinct especially on Miller Street can be minimised; and
- North Sydney Council DCP regulates maximum parking rates for all types of land use and further reduction of the parking provision is encouraged for sites that are close to high frequency public transport corridors.

4.2 Constraints

- Cycle infrastructure in the study area is limited, with gaps and difficult sections forcing cyclists to ride along high-volume roads, negotiate busy intersections, merge with fast moving traffic and conflict with pedestrians;
- The Victoria Cross northern portal could lead pedestrians to cross mid-block on Miller Street, north of the intersection with McLaren Street, which could become a future safety issue. The project is under construction;
- Although Sydney Metro's patronage forecasts indicate that the northern portal's lift capacity will be sufficient, the
 actual usage of the lifts once the Metro opens in 2024 is as yet uncertain. Consequently, the portal's lift capacity
 remains a risk for any major future job growth in the Civic Precinct;
- The proposed on / off-ramps of the Western Harbour Tunnel could attract more vehicular traffic to the study area. This may increase conflicts between walking, cycling and local traffic and result in a less walkable environment, in particular Miller Street if not managed properly. The project is committed;
- Miller Street is expected to become an important spine for pedestrians in the study area, but it will also continue
 to carry major bus routes and could see more car traffic due to the WHTBL project, leading to more conflicts and
 longer waiting times at intersections if not managed properly.

Civic Precinct and Surrounds Planning Study



APPENDIX A

Existing planning proposals review



Appendix A – Existing planning proposals review

Three existing planning proposals received by North Sydney Council have been reviewed in order to understand the transport context / implications of potential developments within the Civic Precinct and Surrounds.

173-179 Walker Street and 11-17 Hampden Street

This planning proposal contains a three level podium along Hampden and Walker Street with two towers for residences totalling to 24,000 m² Gloss Floor Area (GFA), as shown in **Figure 1**.



Figure 1 Concept plan for 173-179 Walker Street and 11-17 Hampden Street

Source: SJB (2019), 173-179 Walker Street and 11-17 Hampden Street Planning Proposal Urban Design Scheme

Access to the existing lots on Walker Street and Hampden Street is already constrained due to the topography, i.e. the three metre level difference between Walker Street and the service road. The pedestrian staircase in the north will be retained and the traffic volume needs to be reduced on the Walker Street service road and on Hampden Street through limiting access points and upgrades of pavement due to the proposed shared street environment. On-street parking on both Walker Street and Hampden Street will be maintained. A pedestrian connection is also proposed in the north to connect to the Hamden Street Pocket Park in the northeast of the site.

The Western Harbour Tunnel access currently via Berry Street will increase traffic volume in the local area and create a pedestrian barrier in the vicinity. Consideration should be given to a pedestrian through-site link from the Ward Street Precinct to the west to create better east-west connections for pedestrians across Walker Street. This will also facilitate the pedestrian movements to / from the two portals of the proposed Victoria Cross Metro Station to the northwest and southwest.

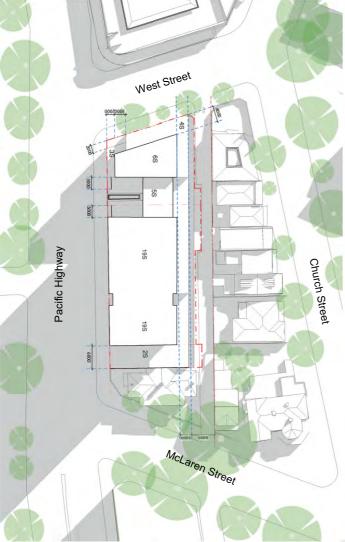
The proposed residential parking provision of 227 spaces, is below the maximum allowable number of spaces permitted under Council's DCP of 369 spaces and results in very low vehicular trip generation and minimal traffic impacts.



253-267 Pacific Highway

This proposal seeks to intensify land use within walking proximity of the two proposed Metro Stations and proposes a total of 10,500 m² GFA including residential, commercial and retail development as shown in **Figure 2**.

Figure 2 Site plan for 253-267 Pacific Highway



Source: GMU (2019), 257-267 Pacific Highway Urban Design Scheme

The site, located on the Pacific Highway between McLaren Street and West Street, is well connected to local public transport and road infrastructure. There are regular bus services which connect to Chatswood and the Sydney CBD. Around the site the street pattern is an interconnected grid system which offers a range of pedestrian and vehicular routes to North Sydney and St Leonards as well as the highway itself to supplement public transport.

The new Metro line (currently under construction) will further strengthen the site's accessibility and connectivity from / to surrounding key destinations such as Sydney CBD, Barangaroo, Macquarie Park and Macquarie University. The northern portal of Victoria Cross Station is approximately 260 metres (a two-minute walk) east of the site.

The site is located within a short walking distance of a number of other facilities, including a community centre, Stanton Library and St Leonards Park. The location of the site enables it to play an integral part in the strategic planning and future growth of North Sydney and the existing development spine along the Pacific Highway.



The size of the consolidated site enables a holistic approach to be taken to provide intensification within the vicinity of two new Metro Stations, with a sympathetic response to the existing heritage context and an improved pedestrian environment

The proposed parking provision of 40 spaces is well below the maximum allowable number of spaces permitted under Council's DCP of 80 spaces. This number of spaces is considered appropriate to meet the parking needs of the development while also minimising the impact on the adjacent road network by reducing traffic generation.

However, it is noted that the vehicular access of the proposal could be made via Church Lane to minimise conflicts with pedestrians along the Pacific Highway.

27-57 Falcon Street

This planning proposal is for buildings of varying heights and typologies including eight townhouses, 79 apartments and 340 m² of retail as shown in **Figure 3**.

BULINGS
BULING

Figure 3 Site plan for 27-57 Falcon Street

Source: AJ+C et al (2019), 27-57 Falcon Street Urban Design Report

It is within close proximity to many public transport options including multiple bus stops serving routes to a number of destinations around Sydney and is an approximately five-minute walk from the future Metro Station at Crows Nest to the northwest. Through-site links are proposed to connect Falcon Street and Hayberry Lane in the middle of the site.

No specific traffic impact study is available for this site.



APPENDIX 4

CIVIC PRECINCT AND SURROUNDS PLANNING STUDY WORKSHOP REPORT



Workshop Report

Civic Precincts and Surrounds Planning Study

North Sydney Council

Prepared by PlanCom Consulting Pty Ltd

December 2019

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1. Background

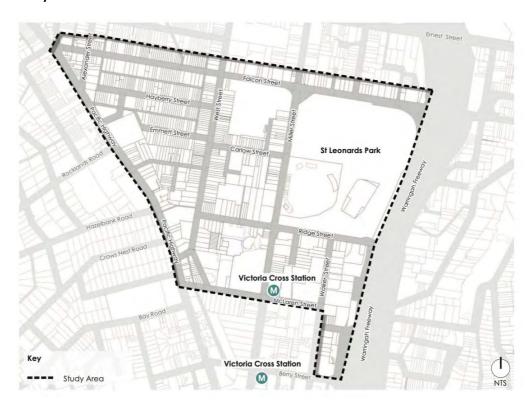
The Civic Precinct and Surrounds is located north of the North Sydney Central Business District (CBD), see map below. The commencement of the operation of Sydney Metro in 2024 and the precinct's proximity to the North Sydney CBD will place significant development pressure on this area in coming years. The Study is important for Council to manage this pressure and meet its commitments to the NSW Government's *North District Plan*.

Initial consultation occurred from 11 July to 8 August 2019. In response to the feedback received, the Northern CBD Planning Study was renamed to 'Civic Precinct and Surrounds Planning Study'.

Preliminary community priorities identified were as follows:

- Heritage is conserved
- The Civic feel and "village character" of the precinct is respected
- Parks and public spaces are protected
- Access, safety and amenity of streets is improved
- The scale, design and impact of new development is carefully managed
- More opportunities for social interaction and activity are created

Study Area





For further detail on Stage 1 consultation outcomes, please read the Council report that can be found in the document library which is accessible through the Your Say North Sydney website.

This is a report of the outcomes of two workshops that sought community perspectives on the study. Council invited to these workshops:

- Identified key stakeholders (total of 27)
- those who submitted comments to the initial consultation phase (total of 89),
- those providing email contact details via Your Say for the Stage 1 consultation, and
- representatives of each of the Precinct Committees.

The workshops were also advertised through the Your Say North Sydney website.

The workshops built on the earlier consultation and generated input to the Principles for the study and the potential outcomes, including locations of increases in density and potential community benefits to result.

2. Overview of the workshops

Participants could register for either of two workshops and both were held at the Ros Crichton Pavilion. They were at the following dates and times:

- Thursday 21 November 2019 from 4pm-6.30pm 15 participants attended.
- Thursday 28 November 2019 from 6pm- 8.30pm 13 participants attended.

A total of 15 people came to the first workshop and there were 13 participants for the second workshop.

The workshops were facilitated by Margaret Harvie and documented by Julian Ardas, both of PlanCom Consulting. Conybeare Morrison International (CM+) are leading the project and PlanCom Consulting are providing community engagement services as part of the CM+ team.

2.1. Objectives

The objectives of the workshop and this project were to:

- Confirm the purpose of the study
- Build on the consultation conducted during July and August 2019 by confirming the themes heard through that consultation
- Get input to the planning principles for the Urban Design of the Study Area
- Look at potential locations that might be acceptable for density
- Determine the type of public benefit that the community hopes might result from future development
- Confirmation of the valued 'local places'
- Provide information on the future steps for the project including timing for the exhibition and future discussions with the community



2.2. The Agenda

The Agenda for each workshop was as follows:

Agenda Overview

- Welcome and Introductions
- Setting the Scene and Purpose of the Study Marcelo Occhiuzzi
- Questions
- What Council heard in the consultations to date Marcelo Occhiuzzi
- Feedback on the Identified Community Priority Areas considerations and initiatives
- Current Planning Proposals Marcelo Occhiuzzi
- Urban Planning Principles and discussion on these- All
- Urban Design Analysis and density scenarios David Appleby CM+
- Discussion based on images presented and potential locations
- Desirable resulting public benefits discussion All
- Next Steps and timing for the Study

2.3. Workshop process

The room was arranged in tables to allow for participants to work in groups for the discussions. Some of the input in this report is divided by group contributions.

Each workshop commenced with Council confirming to participants the purpose of the study, its planning context, including context in relation to the Local Environmental Plan (LEP) and the study boundaries. The presentation slides are provided at the end of this this report.





2.3.1. Building on what we heard in the earlier consultation

Council gave an overview of the consultation conducted in July and August 2019 and what Council learned from the community as part of these consultations.

Participants were invited to identify 'Considerations' and 'Initiatives' under the following identified 'Community Priority' areas. These were presented on flip chart paper around the room and people were invited to freely make contributions to the relevant topics.

- Community Priority #1 The precinct's heritage value is conserved
- Community Priority #2 The village feel and civic character of the precinct is respected
- Community Priority #3 Parks and public spaces are highly valued for both active and passive recreation and should be protected
- Community Priority #4 The access, safety and amenity of local streets is improved
- Community Priority #5 The scale and design of new development is carefully managed
- Community Priority #6 More opportunities for social interaction and activity are created.

There was an additional paper for people to add the Community Priorities that they felt had been missed under the six listed priority areas above.

2.3.2. Overview of the Current Planning Proposals

Council provided an overview of the current planning proposals and the risk of the 'do nothing' approach. Questions and comments were provided by participants.

2.3.3. The Urban Planning Principles

The preliminary Urban Planning Principles to guide the planning study were presented and comment on these Principles was invited. The Principles presented were:

- Preserve and respect the heritage and fine grain of the study area
- Preserve the village feel and civic character of the precinct
- Managing growth and development which is of appropriate scale and character
- Prioritise walking and cycling amenity and connectivity
- Capitalise on placemaking and land use opportunities resulting from the Victoria Cross Metro Station
- Promote housing diversity and affordability
- Attract smaller scale businesses and employment to complement the North Sydney
- Maximise the potential of existing open space and explore the potential for new places
- Advocate design excellence and sustainability in new building and public domain
- Provide safe, attractive and high-quality public and community spaces
- Ensure transparency where the leveraging of public benefits is pursued in exchange for additional development potential





2.3.4. Urban Design Analysis and Density Scenarios

CM+ presented the current constraints in the area including maps showing these constraints. Character areas and features that should be retained in these areas were identified.

Some images of density were shown, and people were asked, as part of their table group to discuss and identify the types of development that might be acceptable based on these images. They were also asked to identify the locations in which this type of development might be acceptable.

2.3.5. Desirable Resulting Public Benefits

Participants were asked to work in groups to identify the three most desired public benefits for that group and also identify other benefits that may not be on the list. The list provided was:

- Childcare centres
- Community facilities includes community meeting rooms, community centres, libraries, theatre / multi-use spaces, etc
- Public domain upgrades includes footpath improvements, tree planting, open space, urban plazas, etc
- Affordable housing provision
- Cycleway upgrades and facilities
- Public art and sculpture
- Playgrounds
- Car share spots / EV charging points
- Creative / incubator business hub



3. Workshop outcomes

3.1 Workshop One – 21 November 2019

3.1.1. Building on what we heard in the earlier consultation

Some overall comments from the fifteen participants in response to presentations included:

- Importance of the area as an education precinct
- Crows Nest (on Pacific Highway) is ordinary
- · Difficulty in dictating where retail should go
- Active transport (cycling, walking) needs promotion for example bike parking at the Portal
- Western Harbour Tunnel and Beaches Link, on and off ramps, will add traffic to the area
- Nervousness regarding height and density around the Pacific Highway, Falcon Street and Ridge Street
- Reference to the 50-storey proposal on the highway that was withdrawn
- West Street A gem of North Sydney
- Need to stay low scale, consistent and open



- "Do Nothing" does not satisfy the Greater Sydney Commission's "Metropolis of Three Cities" plan.
- Seeking assurance from Council that they do push back on targets
- Feeling that the conservation areas are done well
- The density should be near the CBD
- Council indicated that St Leonards/Crows Nest have been identified in the Metropolis of Three Cities plan as needing to have a share of density

The following table captures the contributions of the participants to Considerations and Initiatives as part of the Community Priorities.

Community Priority #1 - The precinct's heritage value is conserved

Considerations

Active Protection

Keeping all of the existing buildings

Heritage standings should be reviewed by area

Heritage areas provide an offset to greater density elsewhere



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Initiatives

Review heritage listings for areas surrounded by proposed development and close to new metro

If a non-conforming site is redeveloped, it should conform to the character of the precinct and
reinforce local streetscape

Community Priority #2 - The village feel and civic character of the precinct is respected.

Considerations

Village = Low rise

Heritage (#1) = Village feel

Avoid creating wind tunnels

Avoid offices and large commercial buildings - these should stay in the CBD

Ensure the ground plane of new projects are best practice and high quality

Slow traffic on minor roads e.g. West Street Ridge Street are good examples

Initiatives

Power links underground

- Stop disfigurement of trees and their replacement by ugly dead tree power poles

Centralise development around new Metro

Retain Council and related offices in this area

Community Priority #3 – Parks and public spaces are highly valued for both active and passive recreation and should be protected.

Considerations

Resist creeping encroachment

Parks are already widely used by community for widely different activities

Increasing population and development creates need for more open space. Is it possible to find additional open space areas?

Issue of schools using all available land needs addressing – pressure on public parks

Initiatives

Keep bicycles out of main part of St Leonards Park – stick to perimeter

Stop corporatisation of St Leonards Park

Separate bike path through St Leonards Park

Improve children's amenities/playgrounds

Link up a new North-South Corridor (for cyclists + pedestrians) from Park and North Sydney railway station.

Community Priority #4 - The access, safety and amenity of local streets is improved.

Considerations

Public through links connecting West Street to Miller Street to improve surveillance safety and public realm (via Ted Mack Civic Park), this needs careful consideration as the school uses between West and Miller Streets are inactive at night so provide no passive surveillance.

Reduce traffic speed throughout study area by narrowing roads/calming measures.



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Initiatives

Transform area between Ted Mack Civic Park and West Street (including church parking area etc) into public realm with high value public amenity and place making (cafes, art, more parkland)

Increase dedicated cycle paths

Investigate opportunities for cross block pedestrian connections.

Community Priority #5 - The scale and design of new development is carefully managed.

Considerations

Fairness of now putting high rise to the east of all the new residential development on Miller Street, south of McLaren Street

Clear limits to development

Flexibility on height and use in strategic locations that can enhance the area

Protection of sun access all year to parks and streets and schools - with outdoor cafes

Protect street character of West Street, McLaren Street, Ridge Street and Carlow Street

Protect heritage buildings from local development

Initiatives

Development controls for buildings

"Ban" high rise buildings over 6 storeys

Develop special building controls in DCP for street frontages/setbacks/minimising driveways in the special character streets mentioned above and for heritage precincts

Centralise development around Metro

Community Priority #6 - More opportunities for social interaction and activity are created.

Considerations

Ensure study does not solely focus on buildings and built form

Consider activities associated with future developments and current buildings and places

Retain/develop small cafes, etc at street level such as NSC tennis courts

Activate area around new Metro station

Initiatives

Promote use of public transport for events at North Sydney Oval

Expand Stanton Library

Community Priority/s - Those that may have been missed

Considerations

Low rise

Role of community facilities and activity and importance to whole community (e.g., library, community centre)

Greater focus on active transport/safety

Consider connectivity between North Sydney and Crows Nest along Pacific Highway so as to encourage renewal and upgrade streetscapes

Lack of street activation on Pacific Highway



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Need to consider effects of Western Harbour Tunnel and potential changes to traffic flows and increased traffic numbers

North Sydney population growth of 16% is too high by 2036

Initiatives

Limit vehicle numbers

Improve cycleways (particularly north to south)

Private/public and corporate buildings should install rainwater tanks

Greater Council encouragement for buildings solar panels and neighbourhood sharing and electrical generation

More garbage bin options (kitchen food scrap bins, battery/E-waste)

The balance of education so it is a valuable part of the community and not an intrusion

High traffic speeds on Pacific Highway makes footpath areas inhospitable to pedestrians

3.1.2. Feedback on the Urban Planning Principles

General comment that some felt the Principles for the study are too vague. Specific comments on the Principles from each table, were as follows:

Number	Principle	Comments – sorted by the three table groups	Changes/ additions
1	Preserve and respect the heritage and fine grain of the study area	Table 1 - Hampden Street terraces are heritage and surrounded by new developments. Good principle but if there is all development around it the principle should be changed Table 2- Heritage items alone are limited to what is scheduled	Table 1 - Height: Should be consistent with surrounding buildings Table 2 - Add "Conservation Value" between "and" and "fine".
2	Preserve the village feel and civic character of the precinct	Table 3 - Village feel. Lots of tree canopy has been lost. Reference to power line trimming. Power lines should be underground.	
3	Managing growth and development which is of appropriate scale and character		Table 2 - New associated principle: Gradient of development should be another principle. Scale of development between lowest scale and high-rise development.
4	Prioritise walking and cycling amenity and connectivity	Table 3- Support more bikes and walking. Suggest reduced car use and it should be a goal.	



Number	Principle	Comments – sorted by the three table groups	Changes/ additions
5	Capitalise on placemaking and land use opportunities resulting from the Victoria Cross Metro Station	Table 2 - Unclear about the term "Capitalise on placemaking" – needs definition or alternative term used.	
7	Attract smaller scale businesses and	Table 1 - New developments seek higher rents	
	employment to complement the North Sydney CBD	Table 2 - Unsure of how "attracting small scale business" can be achieved.	
11	Ensure transparency where the leveraging of public benefits is pursued in exchange for additional development potential	Table 2 - Need to describe what a public benefit is - a list or details.	
There wer	There were no comments from the tables on the following Principles		
6	Promote housing diversity and affordability		
8	Maximise the potential of existing open space and explore the potential for new places		
9	Advocate design excellence and sustainability in new building and public domain		
10	Provide safe, attractive and high-quality public and community spaces		

Additional new Principles that groups 1 and 2 wanted to add were the following:

Table 1	Respecting the area as a Key Education Precinct	
Control overshadowing of parks and schools		
Table 2	Locate and protect tree cover	
Overshadowing of parks from overdevelopment.		

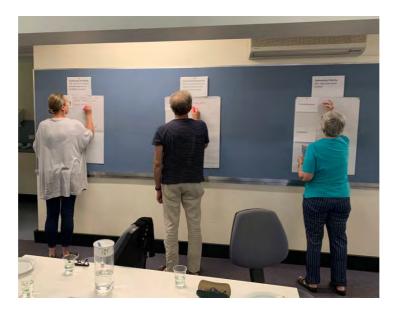
3.2 Workshop Two – 28 November 2019

3.2.1 Building on what we heard in the earlier consultation

Some overall comments from the thirteen participants in response to presentations included the following:

- What is the timing of the study?
 Response: 12 months with the purpose of pre-empting ad hoc development that might be initiated by the Metro.
- There was an expressed preference for California bungalows and terrace houses.
- Heritage listing and that this might be at State and Local Government levels.
 Response: The Local Government Area has a high density of heritage with approximately 1,000 heritage items and six Heritage Conservation Areas.





The following table captures the contributions of the participants to Considerations and Initiatives as part of the Community Priorities.

Community Priority #1 - The precinct's heritage value is conserved

Considerations

Architecture of developments to be sympathetic to older buildings

Add conservation areas to include heritage streets

Preserve all buildings over 90 years old

Preserve older precincts

Don't delist heritage buildings

Preserve low rise MLC building and setbacks

Preserve MacAfee building and setbacks

Initiatives

Stricter constraints on architecture (scale and appearance)

Low rise

Dense but low rise

Low rise, but be aware of North Sydney's infamous Wind Tunnel

Community Priority #2 - The village feel and civic character of the precinct is respected.

Considerations

Reduce rat runs

School drop-off cars queuing a long time

Enhance and develop urban streetscape



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Parking

Heights determined by urban design, i.e., enclosure and scale, not just size

Completely different feel on Monday to Friday, from weekends

Need to generate small business that will generate village interaction after work hours

Keep multi-storey buildings in CBD. There is scope to increase building heights and older style office buildings.

Initiatives

Get cyclists off the footpath

Keep development to three storeys maximum

Identify the village precincts. Not all areas in zone have village feel.

Incentives for weekend business and small business, not just Monday to Friday 9am to 5pm

More dense but low rise keeps village feel and fosters connections.

Community Priority #3 – Parks and public spaces are highly valued for both active and passive recreation and should be protected.

Considerations

Multi-sport facilities, not just indoor and outdoor

St Leonards Park for locals as well as national and international sport

More calm green space in the parks

Control exercise groups and abuse of parkland and facilities

Yes, restrict exercise groups

No exclusive non-resident school use of sports facilities

Dogs on leads except for off-lead areas.

More child friendly

Don't turn Cammeray Golf Course into playing fields.

Pocket parks preferred - social interaction

Cycle track around outside of St Leonards Park

Coal Loader is underused.

Initiatives

Create parks over Warringah Freeway (no high rise)

More community gardens such as Coal Loader

Require developers to create green spaces in their developments

Return greens to active healthy use

Community Priority #4 - The access, safety and amenity of local streets is improved.

Considerations

Tripping hazards due to excessive development

Pavers replaced and patched constantly. Consider aged population

Limit speeds of traffic – by urban design and speed limits, e.g., cobblestones = self-regulation



More roundabouts/chicanes rather than streetlights/too many accidents on Pacific Highway and Bay Road

Shared zones

Raised pavements with steep bump. Similar to Mount Street. Truly slows traffic and stops rat runs.

Initiatives

More frequent repairs or temporary consideration during development

Less artificial traffic calming – more traffic control by urban design – widths, finishes etc – physical scale of development

Slow down traffic – especially the corner of Walker and McLaren Streets - getting a lot of speeding

Cycling infrastructure

Eliminate pedestrian blind spots, i.e. where you can't see down the road after dark leads to feeling unsafe

Community Priority #5 - The scale and design of new development is carefully managed.

Considerations

Limit excessive high rise/intense density/traffic

Traffic flow to be properly tested-considered

Balance inevitable development, increased density versus preserve amenity

Education and health facilities to keep pace with increased population

How much more?

Existing buildings?

Identify areas or zones that can take greater height/density

Preserve rights of existing residents, i.e. do not block access and views with new oversized developments.

Initiatives

Planning approval process/criteria

No more than three storeys

Consider more car sharing facilities to decrease parking concerns

Areas in parts of this study zone already have heights over three storeys

Depends on amenity that future buildings can bring to the area

Existing 12 storey in West Street (too high)

Street podiums of three to four storeys only with six to seven storey setbacks behind

Community Priority #6 - More opportunities for social interaction and activity are created.

Considerations

Develop urban streetscape – activate the street at ground level

People needed for street activation

Safe footpaths

Cafes/parks - places for people to meet

Traffic calming on West Street so people feel safe to cross the road as they walk around



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Be mindful of opportunities of others to live in this area – don't lock it off to youth, disadvantaged, social housing

Council 'build to rent' projects within the precinct

Multi-purpose facility – Theatre/Indoor sport for commercial events. Reason for people to stay after 5pm

Replace Walker Street Cinema

Initiatives

Make developers include community facilities and hold them to it. Not just a 7/11 store

Community gardens

Consider successful cities as prototypes – Barcelona, Munich, Rome

All six to eight storeys with Nil high-rise towers

Community Priority/s - Those that may have been missed

Considerations

Try to lower noise pollution, i.e. below WHO recommendations of no more than 53 decibels during the day

Sound barriers to expressway

More bus stops down Pacific Highway

Traffic congestion as Metro becomes more connected with bus lines

As more initiatives (social and community) increase, parking will be more difficult

Grocery stores in North Sydney CBD is limited, fresh producers, i.e., butcher, fruit/vegetable not close (not counting supermarkets)

Walkway over Berry Street to link Ward Street precinct with Metro

Initiatives

Limit or review car access

Encourage public transport use and petition for better timetables on weekends

Bicycle routes and safe zones included in parks for children to learn

Wind amelioration strategies

3.2.2 Feedback on the Urban Planning Principles

Two of the three table groups indicated that they generally agreed with the Principles. The other table of participants suggested the need to prioritise the Principles and they identified contradictions for example between Priority 1 and Priority 6.

One table questioned the ability to execute the Principles.

Specific comments on the Principles were as follows:



Number	Principle	Comments – sorted by the three table groups	Changes/ additions
1	Preserve and respect the heritage and fine grain of the study area	Table 1 and 2 – What is meant by the term "fine grain"- needs definition? Should it be replaced with "small scale", "single storey" or "small scale environment"? Table 2- Heritage items alone are limited to what is scheduled.	
2	Preserve the village feel and civic character of the precinct	Table 1 and 2 - Village feel. No, too much traffic	
4	Managing growth and development which is of appropriate scale and character Prioritise walking and cycling amenity and connectivity	Table 1 and 2 – How do you manage growth? Table 3 – Issue of traffic planning and increased density Beaches Tunnel off ramp The area is special and there are reasons to keep it that way. West Street is not a through street – it is a cycling street. Where there are no through streets there should be cycle and pedestrian path linkages. Table 1 - Concerns about cycling – Use of footpaths, cyclists cursing/swearing at pedestrians. Table 2 – Fail to see cyclists using the tracks.	Table 2 – Improve walking connectivity. Berry Street crossing is busy and conflicts with pedestrians. Improve
6	Promote housing diversity and affordability	Table 1 – How do you promote housing diversity? Table 3 – 30 Minute City – Example of Westmead How do you achieve housing diversity and affordability? There are generational needs. Fewer younger people are found in the area.	connectivity through the use of small buses. Table 1 – More buildings developed for renting rather than sale.



Number	Principle	Comments – sorted by the three table groups	Changes/ additions
9	Advocate design excellence and sustainability in new building and public domain	Table 1 – Council does not advocate. Example given of Council's apricot coloured Seidler building.	Table 2 — Replace "Advocate" with "Execute". Should include affordable housing and diversity.
10	Provide safe, attractive and high-quality public and community spaces	Table 2 – Need places or placemaking such as Pitt Street Mall. It needs attractions such as a Cinema to keep people. Table 1 – Reference to Rome,	
11	Ensure transparency where the leveraging of public benefits is pursued in exchange for additional development potential	Barcelona, Paris, Amsterdam. Table 2 – Reference to St Leonards high rise development and the need to leverage public benefits such as an art gallery.	
There was	There was a question from one table about "how you do this?" for the following Principles		
7	Attract smaller scale businesses and employment to complement the North Sydney CBD		
8	Maximise the potential of existing open space and explore the potential for new places		
There wer	e no comments from	the tables on the following Principles	
5	Capitalise on placemaking and land use opportunities resulting from the Victoria Cross Metro Station		

There were no new Principles that these groups wanted to add.

3.3 Combined Workshop results

3.3.1 Urban Design Analysis and Density Scenarios

Responses to CM+ Presentation included:

- That the Western Harbour Tunnel and Beaches Link access and egress points could change access and egress for the entire area. Berry Street for example.
- 25 Council Precinct Committees use the term "precinct". Suggested using the term "area" instead.
- What opportunities exist to progress and test these ideas at other forums?



- Improve landscaping on freeway. Loss of views due to random plantings and lack of tree maintenance. Lack of agreement on who is responsible for road reserve boundary and maintenance.
- Loss of million-dollar harbour views and poor landscape maintenance of freeway reserve trees.
- Bay Road/Pacific Highway whole block is North Sydney Demonstration School.
 Reference to previous education precinct study. Area has the highest number of schools in Australia.
- Walker Street School precinct versus high rise development.
- SAP Building (168 Walker St) Approved 3-4 years ago for a 20 storey, 415 apartment development.

The consolidated results from the placement of dots on maps from each of the 6 groups (3 groups per workshop) is as follows. These dots indicate the locations that types of development might be acceptable in the view of the various table groups. The key below indicates the meaning of the various dots.





3.1.3. Desirable Resulting Public Benefits

Workshop	Table	The three most desired benefits for the table groups	Additional Benefits
One	1	 Childcare Community facilities and in particular a Theatre like Angel Place and/ or Chatswood would be transformative 	Aged care should be added to childcare
	2	 Public domain: More improvements. Also add "major" public domain and include initiatives like putting power lines underground, solar power etc. Community facilities: Include cinemas and activation for the night and weekend. 	 Affordable housing. Remove - Public Art and Sculpture. Remove "incubator" for Item 9 – Creative/incubator business hub
	3	 Community facilities with Cinemas being key Public Domain upgrades Cycleway upgrades and facilities 	
Two	1	Public Domain (footpath upgrades)Affordable housing	Active streetscape to keep people in North Sydney after business hours rather than CBD
	2	 Community facilities – theatres, libraries Public domain upgrades More green space parks, e.g., Cammeray Golf Course. Greenery parks under freeway and developer funds to pay for that. 	
	3	Public DomainAffordable housingCar share bays.	



4 Summary

The following common issues across the workshops have been derived from Word Clouds that are used to show the frequency of words raised.

Word Clouds have been developed for the three key activities in which participants contributed input as part of both workshops. It is understood that frequency of the mention of items is just one way to analyse and act on the outcomes of the workshops.

4.1 Building on what we heard in the earlier consultation

When people were asked to look at the considerations and initiatives as part of the previously identified six community priority areas key words from the feedback included - development, buildings, storeys, traffic and park.

Comments raised by participants related to:

- controlling development type including the height, location and character,
- traffic congestion (now and in the future), and
- protection and management of existing parkland in terms of uses and sunlight and more smaller parks.



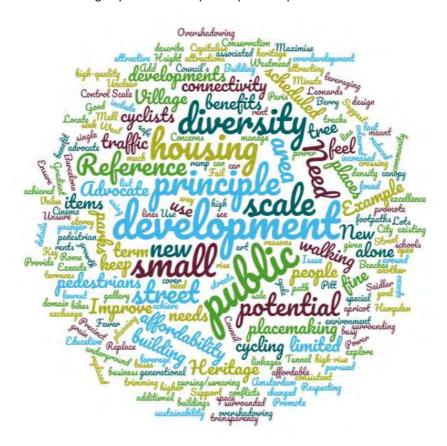


4.2 Establishing the Urban Planning Principles

When people were asked to comment or add to the identified urban planning principles the key words from this feedback presented in this report included - development, public, housing, diversity and small.

Comments raised by participants related to:

- controlling development type including height, location and character. *Note that this was the same theme as for the community priorities above.*
- · desire for the promotion of housing diversity, and
- ensuring any future development provides public benefits.



4.3 Desirable Resulting Public Benefits

When people were asked to look at the identified public benefits key words from this feedback presented in this report included - public, facilities, community, domain, housing, affordable and upgrades.

Comments raised by participants related to:



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- increased and improved community facilities (childcare, theatre) noting that there was also the suggestion that aged care should be added to childcare.
- upgrades to the public domain (footpaths, trees, streetscape, parks), and
- The need for more affordable housing





Participants (Confidential)



Workshop participants

WORKSHOP ONE	
Name	Representing (if relevant)
Barry Johnston	Balmoral Partners
Naomi Fiegel	Hayberry Precinct Committee
Andrew Leake	Wenona School
Bernhard Hengst	
Coral Hengst	
Marcus Hinzack	Ford Land Company Pty Ltd
Margaret Petrykowski	Waverton Precinct Committee
lan Grey	Waverton Precinct Committee
George Ford	
Bill Hartnell	
Phil Jedlin	
John Mariano	
Godfrey Santer	Hayberry Precinct Committee
David Watt	
Jon Cook	Edward Precinct Committee



WORKSHOP 2	
Kitty Chan	
Kathy Wilson	
Ania Bokina	
Sue Fryda- Blackwell	Stanton Precinct Committee
Dipanka Chojohury	
John Prentice	Stanton Precinct Committee
Rob Carveth	
Jack Taylor	Jack Taylor Pty Ltd
John Fitzgerald	Hayberry Precinct Committee
Karen Foster	
Amanda Judd	
Denise Hunter	
Angus Finney	Edward Precinct Committee



Copy of the Presentation





CIVIC PRECINCT AND SURROUNDS PLANNING STUDY OVERVIEW

NOVEMBER 2019

Agenda

- Welcome and Introductions
 Purpose of the Study Marcelo Occhiuzzi
 Questions
- \bullet What Council heard in the consultations to date Marcelo Occhiuzzi
- Feedback on Community Priority Areas considerations and initiatives All
 Current Planning Proposals Marcelo Occhiuzzi
- Urban Planning Principles and discussion All
- Urban Design Analysis and density scenarios David Appleby CM+
 Discussion based on images presented and potential locations
- Desirable resulting public benefits discussion All
 Next Steps and timing for the Study

Introductions

Margaret Harvie - Facilitator

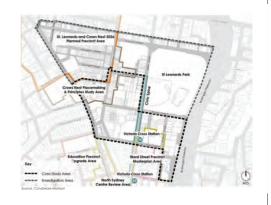
North Sydney Council

- Marcelo Occhiuzzi Manager Strategic Planning
- Emma Booth Team Leader Design
- Karen Buckingham Executive Strategic Planner

- David Appleby Associate Director
- Will Wang Associate Urban Design

STUDY AREA

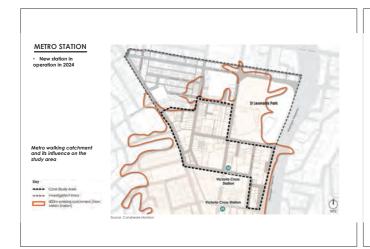
Purpose of the study Study area



Workshop Objectives

- Confirm the purpose of the study
- Build on the consultation conducted in July and August 2019 by confirming the themes heard through that consultation
- Get input to the planning principles for the Urban Design of the Study Area
- Look at potential locations that might be acceptable for density
- Determine the type of public benefit that the community hopes might result from future development
- · Confirmation of the valued 'local places'
- Provide information on the future steps for the project including timing for the exhibition and future discussions with the community

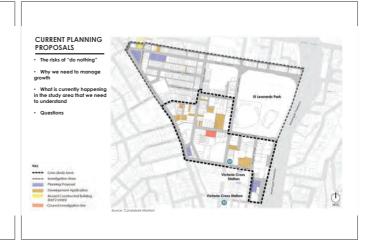




Exercise - Constraints and Initiatives

- . Community Priority #1 The precinct's heritage value is conserved
- Community Priority #2 The village feel and civic character of the precinct is respected
- Community Priority #3 Parks and public spaces are highly valued for both active and passive recreation and should be protected
- Community Priority #4 The access, safety and amenity of local streets is improved
- Community Priority #5 The scale and design of new development is carefully managed
- Community Priority #6 More opportunities for social interaction and activity are created
- Community Priority/ies that might be missing?

Questions



WHAT WE HEARD

- value is conserved
- Community Priority #2 The village feel and civic character of the precinct is respected.
- Community Priority #3 Parks and public space are highly valued for both active and passive
- amenity of local streets is improved
- Community Priority #5 The scale and design new development is carefully managed
- Community Priority #6 More opportunities for social interaction and activity are created.

Workshop exercise



Questions

Planning system and planning proposals

Preliminary Urban Planning Principles to guide the planning study:

- Preserve and respect the heritage and fine grain of the study area
- Preserve the village feel and civic character of the precinct
- Managing growth and development which is of appropriate scale and character
 Prioritise walking and cycling amenity and connectivity
- Capitalise on placemaking and land use opportunities resulting from the Victoria Cross Metro Station
- · Promote housing diversity and affordability
- Attract smaller scale businesses and employment to complement the North Sydney CBD
- Maximise the potential of existing open space and explore the potential for new place.
 Advocate design excellence and sustainability in new building and public domain.
- Provide safe, attractive and high quality public and community spaces
- Ensure transparency where the leveraging of public benefits is pursued in exchange for additional development potential.

List of potential example Public Benefits

- Child care centres
- Community facilities includes community meeting rooms, community centres, libraries, theatre / multi-use spaces, etc.
- Public domain upgrades includes footpath improvements, tree planting, open space, urban plazas, etc.
- Affordable housing provision
- Cycleway upgrades and facilities
- Public art and sculpture
- Playgrounds
- Car share spots / EV charging points
- · Creative / incubator business hub

In Groups decide the top three benefits that you would like to see and why?

Questions at your tables

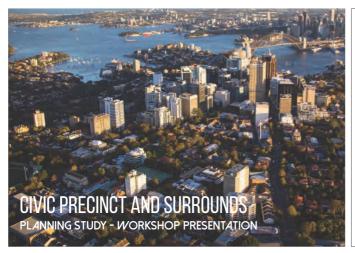
- Do these Principles cover what you think might be the key considerations for the Study?
- Are there Principles or considerations of concern that are missing?
- REPORT BACK FROM YOUR TABLE

Next Steps

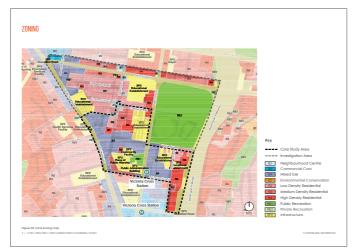
- Workshop outcomes report will be placed on 'Your say'
- Planning Study will to go to Council in early 2020
- Public Exhibition of the Draft Study open to submissions -May-June 2020
- In conjunction with the Public Exhibition will be:
- o briefings,
- o updated Your Say North Sydney,
- o notification about the exhibition
- Please sign up to Your Say for further updates

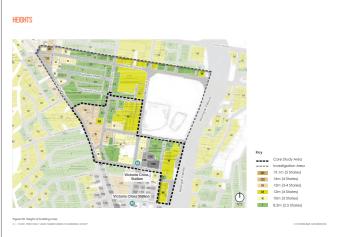
Increased density discussion

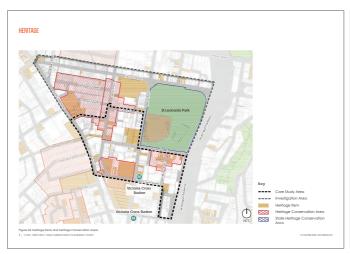
- How do you feel about the building types in the pictures?
- Indicate by placing dots (colour coded to the image) on potential locations for the type of development shown. Place the dots on the map.



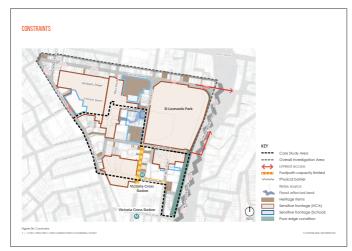


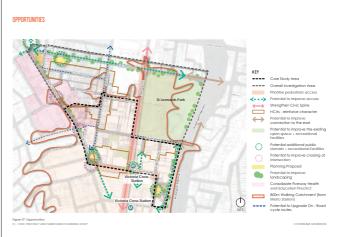


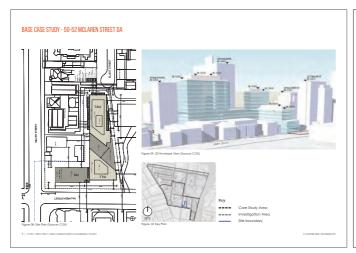


















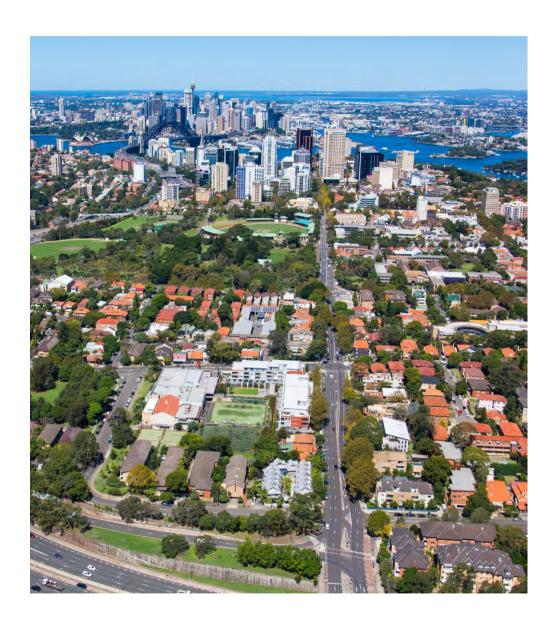






APPENDIX 5

NORTH SYDNEY CIVIC PRECINCT AND SURROUNDS PLANNING STUDY, SUSTAINABILITY REVIEW



North Sydney Civic Precinct and Surrounds Planning Study, Sustainability Review

For North Sydney Council

December 2019
Sense Strategy Consulting

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1. Introduction

1.1 Background

Conybeare Morrison (CM+) have been appointed by North Sydney Council to undertake the North Sydney Civic Precinct and Surrounds Planning Study. The North Sydney Civic Precinct and Surrounds (the Study Area, see Figure 1) is an approximately 65ha area bounded by Falcon Street to the north, by the Pacific Highway to the west, the Warringah Freeway to the east and McLaren and Berry Streets to the south (North Sydney Council 2019).

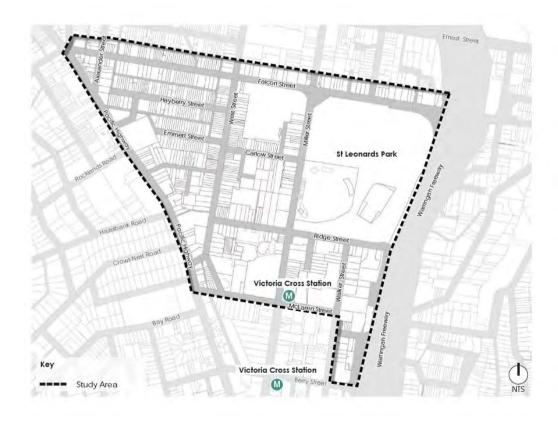


Figure 1.1 The North Sydney Civic Precinct and Surrounds Planning Study Area



There are three main catalysts of growth in the Study Area, which have led to a need to review its planning:

- Release of the North District Plan by the Greater Sydney Commission, which sets the 20 year strategic planning framework for the region from North Sydney in the south, to Hornsby in the north (Greater Sydney Commission 2018).
- Construction of Sydney Metro City & Southwest, and the introduction of a new Victoria Cross Station North Portal at 50 McLaren Street, to supplement the previously proposed Portal below Miller Street (Transport for NSW 2019).
- The lodgement of several Planning Proposals for sites in or adjacent to the study area.

The purpose of the North Sydney Civic Precinct and Surrounds Planning Study is to review the current development controls and urban design of the Study Area, in light of the new access to public transport that will be provided by the Victoria Cross Metro Station Northern Portal, and the release of the Northern District Plan. The Study seeks to strengthen the community values of the precinct and identify any public benefit that may result from new development.

1.2 Purpose of this Study

Ecologically Sustainable Development (ESD) principles provide a framework for undertaking development which improves the total quality of life, both now and in the future, in a way that maintains the ecological processes on which life depends. As such, it is appropriate that a Sustainability Review be conducted for the North Sydney Civic Precinct and Surrounds Planning Study to ensure that opportunities to enhance the benefits of the proposed activities for current and future communities is enhanced.

Sense Strategy Consulting was engaged to conduct such a Sustainability Review.

The purpose of this Sustainability Review is to:

- a. Describe the strategic context for the North Sydney Civic Precinct and Surrounds Planning Study, particularly the sustainability related aspects.
- b. Review the sustainability related work done to date, particularly the Stage 1 Planning Study Report produced by North Sydney Council and the outcomes of the Preliminary Consultation Plan
- c. Identify any potential issues, areas of concerns and potential solutions from the sustainability view point, and provide a brief summary. This will involve re-visiting and updating the Sustainability Assessment Questionnaire produced in the Stage 1 Report.
- d. Provide sustainability advice and comments on the preferred Planning Study scenario, including formulating sustainability plans and controls for the Study Area.



1.3 Structure of this Report

This report has been structured primarily to address items a., b. and c., described in Section 1.2 above, and although some work on item d. has been included, this will be expanded upon in future iterations of the report. This report should therefore be seen as preliminary in nature and will be revisited as the project develops.

2 Strategic Context for the Study

2.1 The Greater Sydney Regional Plan and the North District Plan



Figure 2.1. The Metropolis of Three Cities, the Region Plan for Greater Sydney

In 2018, the Greater Sydney Commission (GSC) prepared the Greater Sydney Region Plan, A Metropolis of Three Cities, which sets a 40 year vision (to 2056) to accommodate Sydney's anticipated population growth, and the associated increase in demand for jobs and dwellings (Greater Sydney Commission, 2018 a). It envisages the creation of three distinct cities that will each make a contribution to Sydney's economic, social and cultural development through to 2056, including:

- The Eastern Harbour City an established city with a strong transport network, with the Sydney and North Sydney CBDs as its hub and economic heart.
- The Central River City a developing city with Parramatta as its economic and cultural centre.
- The Western Parkland City a planned city focused on Western Sydney Airport and its associate developments.

As indicated above, the North Sydney CBD is located within the North District of the Eastern Harbour City, and a North District Plan has been developed by the GSC (Greater Sydney Commission 2018 b) to guide the implementation of the Greater Sydney Region Plan at a district level.

The North District Plan sets some ambitious targets for the District, including a 20 year housing target of 92,000 additional dwellings. It also specifies a five-year dwelling target of 3,000 additional dwellings for the North Sydney LGA.



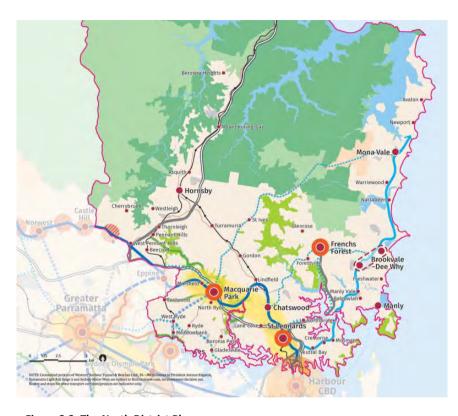


Figure 2.2. The North District Plan

The Plan emphasises the important links between the economy of the North District and that of the Harbour CBD, and emphasises its importance both as supplementing that economy but also supporting a unique one of its own. More specifically, North Sydney provides half a million jobs and is the largest office market in the North District, and the North District Plan identifies that 15,600-21,000 additional jobs are to be created within the North Sydney CBD, and another 6,900-16,400 jobs are expected within the St Leonards precinct by 2036.

There is significant transport and infrastructure investment underway and planned for the North District, including the Sydney Metro City & Southwest and the St Leonards Health and Education Precinct. The focus of growth in the North District will be around well-connected places that build local strengths and focus on the quality of the public domain.

However, the Plan also provides strategies to protect the environmental sustainability of the landscape with strategies that enhance waterways, bushland, biodiversity and green open spaces. This is given life through the establishment of the Greater Sydney Green Grid which will build on existing natural features and parklands of the North District to create green links which support and encourage walking, cycling and community access to open space. Landscape and scenic features, including views from the Harbour and views to the Harbour and foreshores are to be preserved and enhanced. This confirms the strong commitment to sustainability in this Plan.

2.2 The Sydney Metro City and Southwest and the Victoria Cross Station

The Sydney Metro City & Southwest project, the second stage of Sydney Metro, will see a new 30 kilometre metro line extending from Chatswood, under Sydney Harbour, through new CBD stations and southwest to Bankstown. The project provides a fully automated rail system across Sydney, supporting high demand with a high capacity, turn-up-and-go service.





Figure 2.3. Artists Impression of the Victoria Cross Station

An Environmental Impact Statement (EIS) for the Chatswood to Sydenham component of Sydney Metro City & Southwest was prepared and exhibited from 11 May to 27 June 2016, and planning approval was granted by the Minister for Planning on 9 January 2017.

A clear commitment to sustainable design and operation was made in the EIS, and focused on an assurance to build public transport for current and future generations that optimises environmental and sustainability outcomes, transit service quality and cost effectiveness. An extensive sustainability assessment of the proposed development was undertaken and specific sustainability principles were incorporated into the design of both the construction activities and operation of the Metro.

Victoria Cross is one of the stations on Sydney Metro City & Southwest, currently under construction (due to open in 2024) and located just to the south of the North Sydney Civic Precinct. The Station will be 31 metres below ground level with station access and entry via the pedestrian plaza opening to Miller Street and Berry Street via Denison Street. Although not in the original proposal, a new northern portal will be established on McLaren Street, one block further to the north of the main station. The entry will be lift-only and connect passengers to an underground pedestrian walkway about 38 metres long, which will connect the northern station portal to the northern end of the station mezzanine level and platforms. This would shorten the overall walking distance particularly for northbound users since the main concourse is located between the northern and southern portal.

The EIS conducted on the original Station design (ie with only a single entry on Miller Street) identified noise impacts associated with the Victoria Cross Station northern services building. This prompted Sydney Metro to investigate alternative locations for the building, and following a

detailed options assessment, it was determined to relocate the northern services building to a new location at 50 McLaren Street. This preferred option would also incorporate a new northern station entry at this location. This proposed change would have improved environmental outcomes, in particular reduced noise impacts, but it would also improve amenity and access for customers, and would increase the reach of the station catchment to include additional regional attractors such as the Mater Hospital, North Sydney Oval, North Sydney Boys High School, the residential and mixed use area to Falcon Street and Neutral Bay



via the footbridge over the Warringah Freeway. A northern entry would also benefit customers using local bus services on Miller Street by providing an alternative and improved transfer opportunity. These impacts are discussed in more detail in Section 4 of this report.

2.3 North Sydney Community Strategic Plan 2018-2028



Sustainability, and sustainable development, is a core principle guiding the growth of the North Sydney Local Government Area. This focus is reflected in North Sydney Council's North Sydney Community Strategic Plan 2018-2028 (North Sydney Council 2018), an important strategic document providing the road map for the future development of the Area. The Plan is focused on five Strategic Directions, necessary to improve North Sydney, namely, Our Living Environment, Our Built Environment, Our Future Planning, Our Social Vitality, and Our Civic Leadership.

Within these Directions, the highest priorities for the North Sydney community are increased access to open space and recreation facilities, environmental sustainability, better use of existing infrastructure, improved urban design, new assets to meet current and future community needs, managing traffic congestion, conserving and celebrating our heritage, retaining the village atmosphere, increasing economic development, making North Sydney a smart city, livability and supporting creative enterprises.

It is stated clearly in this document, that sustainability is seen by Council as an underlying theme running across each of the Directions.

North Sydney Council's approach to sustainability is a broad-based one, founded on the principles of equity, ecologically sustainable development and a quadruple bottom line - ensuring that all decisions address environmental, social, economic and civic leadership considerations. Council, however, takes these principles and reflects those in a very practical way, publishing on their website, a multitude of easy to follow sustainability information sheets which help residents implement sustainability solutions (see https://www.northsydney.nsw.gov.au/Waste Environment/ Sustainability/Sustainability Information Sheets).



2.4 Draft Local Strategic Planning Statement

A Local Strategic Planning Statement (LSPS) (North Sydney 2019 a) sets a 20-year vision for land use in an LGA and is a bridge between strategic and statutory planning legislation and local government legislation.

The North Sydney LSPS focuses on four planning priorities, namely, Infrastructure & Collaboration, Liveability, Productivity and Sustainability. The vision for North Sydney is summarised in its aspiration for a "progressive, vibrant and diverse" place. More specifically it seeks to grow a stronger and more competitive North Sydney CBD, develop innovative and diverse business clusters in the St Leonards/Crows Nest precinct and develop centres which support walking cycling and public transport usage. It also seeks to provide new housing in the areas of high demand, whilst retaining space dedicated to social infrastructure (such as open space, sport and recreational facilities, libraries etc) and preserving North Sydney's distinct local character and heritage.

The North Sydney LSPS also identifies a series of Planning Priorities that are relevant to the sustainability of the study area, dealing with the challenges surrounding the impact human activity on its natural environment. The LSPS recognises that District wide priorities were identified in the Greater Sydney Commission's North District Plan (in which North Sydney is located) - such as improving water quality, protecting bushland, increasing tree canopy and reducing emissions and waste – and that these can be achieved by actions and behaviours at a local level within North Sydney. As such North Sydney has committed to the following priorities:

- S1 'Protect and enhance North Sydney's natural environment and biodiversity';
- S2 'Provide a high quality, well-connected and integrated green space system';
- S3 'Reduce greenhouse gas emissions, energy, water and waste' and
- S4 'Increase North Sydney's resilience against natural and urban hazards'.



3 Sustainability Work Done to Date

3.1 North Sydney Civic Precinct and Surrounds Planning Study Stage 1 Report and Sustainability Assessment

Recognising the likely changes to the Civic Precinct of North Sydney, due to the establishment of the northern entry to the Victoria Cross Station, as well as the planning controls defined in the GSC's North District Plan, North Sydney Council undertook some preliminary planning work in that Precinct, and released a North Sydney Civic Precinct and Surrounds Planning Study, Stage 1 Report.

This report provides useful background information and context for the area but, most significantly, identifies what the community most values in the study area and their desires for the ongoing development of the North Sydney Civic Precinct. These are described further in Section 3.2, below.

As an initial step, however, a preliminary sustainability assessment was conducted as part of this Stage 1 Report. That Report drew on a Sustainability Assessment Questionnaire which was completed by Council's sustainability professionals, and is included as a supporting document to the Stage 1 Report.

The assessment indicated significant positive impacts of the proposed redevelopment on the area, and particularly of the establishment of the North Sydney Metro Station. These included reductions in traffic volumes and therefore on congestion, improved community access to public spaces and the potential role of the Metro as a catalyst for increased economic activity.

3.2 Preliminary Consultation Outcomes

Between 11 July and 8 August 2019 a "pre-consultation" community survey was opened to stakeholders to gain an understanding of community priorities for North Sydney's Northern Civic Precinct and Surrounds. The outputs of this survey were incorporated into the Stage 1 Report, and provides a very useful insight into "what is important to the community" and, particularly relevant to this report, which of those priorities have a sustainability component.

The key insights from the survey are:

- The community particularly values the 'village feel' of the study area, the leafy tree-lined streets, larger parks, sports facilities and small scale retail.
- This is reflected in the places the community values. The area's parks, such as St Leonards Park and Civic Park, the village-style retail along Ridge Street and the leafy areas around the junction of Ridge Street, West Street & Carlow Street (see Figure 3.1).
- The community would certainly like to see that village feel retained in the future, which includes having low scale buildings, accessible parks, enhanced public domains and village-like streetscapes.



The community also supported the activation of the area, including supporting tourism, enhancing the after-hours economy and developing North Sydney as a knowledge centre.

These priorities are relatively consistent with community desires in similar inner city areas around the world, and in Australia. There are clear sustainability aspects to these needs too, namely:

- The preference for leafy tree-lined streets has an environmental and social sustainability aspect;
- Good access to parks, addresses strong social sustainability; and
- The appreciation of the need to create a vibrant and persistent economy has a strong economic sustainability focus to itcommunity also supported the activation of the area, including supporting tourism, enhancing the after-hours economy and developing North Sydney

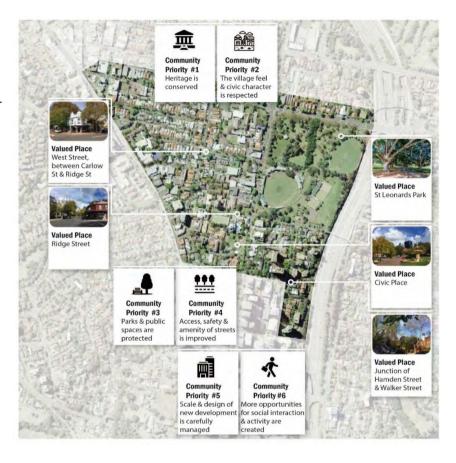


Figure 3.1. Map illustration of the Community's 'most valued' places in the study area

4 Preliminary Sustainability Review

4.1 Sustainability Assessment

In identifying and assessing the sustainability issues relevant to the proposed redevelopment of the North Sydney Civic Precinct and Surrounds the following factors were considered:

- The factors which Council has identified as of relevance to sustainable development in their LGA. This is contained in the Sustainability Assessment Questionnaire.
- The factors the community considers as important to them, and particularly those that are relevant from a sustainability perspective. This is contained in the Community Strategic Plan, the Draft Local Strategic Planning Statement and the recently conducted community consultation on the North Sydney Civic Precinct Development Plan.
- Professional expertise from the author and North Sydney Council staff in allocating and testing a significance rating to each of the sustainability issues.
- Only sustainability impacts during operation have been considered, and only the additional impacts of the north entry to the station have been assessed. It is assumed that factors related to the establishment of the station are outside the scope of this study.

Based on this, the following issues have been identified as of particular significance:

- Improved access to public transport. The introduction of the additional portal will increase the reach of the station catchment to the north dramatically (see Figure 4.1). It will provide greater access to key locations such as the Mater Hospital, North Sydney Oval, North Sydney Boys High School, North Sydney Girls High School, Marist College North Shore, North Sydney Demonstration School, St Mary's Primary School, the residential and mixed-use area to Falcon Street, and Neutral Bay via the footbridge over the Warringah Freeway. The resulting increase in patronage could be as high as 5%, with most of this as pedestrians.
- Improved safety of pedestrians and train passengers. Increases in traffic volumes will occur in the northern area of the CBD, around the new northern Victoria Cross Station entry portal. However, as this provides an alternative alighting stop for passengers, away from the more congested streets around the southern portal, this will be to the advantage of patrons, particularly those with disabilities, who may struggle in more congested and confined areas.
- Reduction in traffic congestion in North Sydney. As indicated above, it is anticipated that the development of the northern entry will generate a patronage increase of 5% at Victoria Cross Station. It is unlikely that these additional patrons will be using vehicles to drive to the station, rather it is anticipated that those patrons who were previously outside "walking distance" (nominally 800m) from the southern entry, may now walk to the northern entry. On balance, therefore, there will likely be little or no impact on the overall traffic volumes associated with that 5% increase. What is



likely, however, is that as there are now two potential access points to the station, the traffic load that is associated with the Station may be distributed between those two access points, spreading the congestion load and potentially reducing the previously anticipated congestion around the southern entry, in particular.

- Improved wellbeing of the North Sydney community, through access to open space and cultural activities. The development of the north entry to Victoria Station will improve access to St Leonards Park and Civic Park, in particular. Various sporting and cultural activities, each of which contributes to wellbeing, occur in these Parks and the improved access to these will therefore be a positive factor.
- **Positive impact on local economy.** The presence of a new station at Victoria Cross, and the associated passenger footfall, will present substantial benefits to local retailers and other businesses. It is forecast that the development of the north entry to the station, specifically, will drive a further 5% increase in patronage to the station, which will increase this opportunity. The presence of a station entry to the north may cause some businesses to establish premises in that part of North Sydney, who may not have with only a single entry.
- Positive impact on noise pollution. Although the relocation of the station entry will have minimal impact on noise levels, the relocation of the services building (to 50 will substantially positively impact on local sensitive receivers by reducing noise at Monet Sant' Angelo Mercy College, in particular.

McLaren Street) which is associated with the station entry,

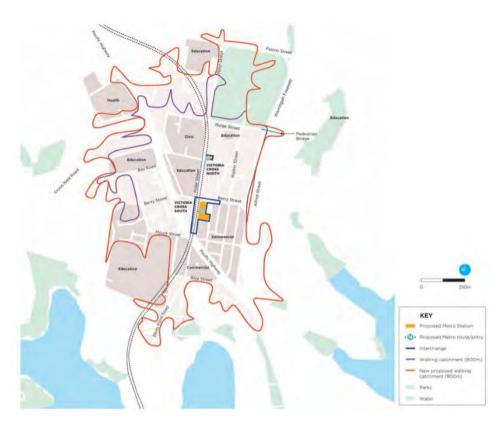


Figure 4.1. Victoria Cross Station walking catchment - with single entry and with proposed new northern entry

There are also no material negative impact on sustainability.

4.2 Preliminary Recommendations

A number of recommendations have been made and should be considered in the development of the North Sydney Civic Precinct and Surrounds Planning Study.

Transport and Access Infrastructure

The most important recommendations focus on ensuring that pedestrian and bicycle access from the surrounding areas, to the northern entry, is efficient, safe and provides a positive experience to patrons, and therefore encourages them to use that entry. This should include ensuring:

- Adequate sidewalk space for pedestrian movement,
- Adequate bike and other mobility paths are available,
- Adequate pedestrian infrastructure for crossing roads is provided, particularly at key intersections
- Adequate parking and storage facilities for bicycles and potentially other micro-mobility vehicles is available at the Station entry
- Landscaping is undertaken to retain and enhance the "leafy" nature of the area (this should include retaining - wherever possible - or introducing mature and fast growing trees, and considering the introduction of features such a green and breathing walls) (see Figure 4.2 of an example of such a wall in St Leonards).



Figure 4.2. The St Leonards green and breathing wall

In addition, the pedestrian and cycling infrastructure should also make it safe, efficient and pleasurable for Station patrons to exit the Station and easily access the various parks, retail and entertainment areas and cultural facilities.

Other Recommendations

Other recommendations include:

- Ensuring that adequate options for retail and other services are provided in the vicinity of the station entry,
- That only locally indigenous plants are used for landscaping, and that the plantings are at a scale and layout to retain the "leafy" nature of the area, and



• That no new sensitive noise receivers are proposed in proximity of the relocated services building.

A full assessment of these sustainability factors and the associated recommendations is summarised in the completed Sustainability Assessment Questionnaire which follows the format used by North Sydney Council. This is included in Appendix A.



5 Conclusions

Based on this preliminary sustainability assessment the establishment of the northern entry to the Victoria Cross Station will generally have a positive impact on the quality of life of current and future North Sydney residents, and will have no material negative impacts on ecological processes, .

The positive impacts revolve around improved access to public transport, improved access to parks, reduced traffic volumes and congestion, reduced noise impacts and positive impacts on the local economy.



References

Greater Sydney Commission (2018 a). Greater Sydney Region Plan: A Metropolis of Three Cities. State of New South Wales

Greater Sydney Commission (2018 b). North District Plan. State of New South Wales.

North Sydney Council (2018). North Sydney Community Strategic Plan 2018-2028.

North Sydney Council (2019 a). North Sydney Local Strategic Planning Statement.

North Sydney Council (2019 b). Draft Stage 1 Report on The Civic Precinct Planning Study.

Transport for NSW (2019). Sydney Metro Chatswood to Sydenham, Victoria Cross Station and Artarmon Substation Modification Report. New South Wales Government.

Appendix A

Sustainability Assessment Questionnaire



eth Sydney Council ustainability Assessment C	uestionnali	re .							
	Northern CBD		ady				T .		
Date	29/11/2019					Officer filling in questionnaire			
Acceptament Question	Type of Impact Positive No Impact Linknown Negative N/A	Magnitude of Impact High Moderate Low	Likelihood of Oogurrence High Moderate Low	Expected Sustainability Impact	Colour indicator regative reutra	Description of Impacts (only operational Impact have been considered as those are relevant for the future Northern CED Planning) If Colour indicator shows Red or Green, provide details on the expected impacts in builet form. Please quantly impacts where possible.	Description of Impacts Prompts Use the questions below to assist with describing and quantifying impacts.	Recommendations (Issues to be taken into consideration in the Northern CBD Planning study)	Addressing Unknown : Negative Impacts if Colour indicator show Red, provide details on in negative impacts will be addressed.
What impact will the proposed project have on energy consumption associated with buildings owned by Council?	Positive	Low	High	Moderate positive	**	Plermanent increase in annual energy consumption in waste treatment plants (approx. 0.6 MW annual total increase for all plants)			New equipment is up to highest energy efficiency standards increase in electricity use cannot be avoided Emvironmental benefits outneigh increase in electricity consumption (separate assessment attached)
What impact will the proposed project have on the quantity of energy consumed?	Unknown	Low	Low	Minor negative	÷	Slight increase in energy demands sue to the additional iff access to the station, with will lead to minimal additional greenhouse gas emissions absuming coal fired generation is the source.	Will this proposal reduce for increase j Council's energy consumption associated with: "audings owned by Council." "Recreations facilities owned by Council." "Recreations facilities owned by Council or Council's contractors." "literatings owned by Council or Council's contractors." "literatings owned by Council." "When materiate protections for Council or Council's contractors." Council y and/or describe improvements (or decline) in the energy efficiency. Has the bulk time here ensigned to manifere energy efficiency. Will the project contribute to the substitution of non-renewable energy? What Impact with the project have on community energy communition."	None	
What impact will the proposed project have on water consumption?	Unknown	LOW	Low	Minor	-	No additional impact on curface or groundwater, beyond which has been seemiled for the southern entry, and being managed there.	Will this proposal reduce (or increase) Council's maints water consumption associated with: "Building owned by Council" "Reversations faulties water by Council or Council's contractions? "New materials purchased by Council or Council's contractions? "New much aim or adiomnated will be recovered and reused per annum (percentages/litres)? Will wastewater will be generated by the proposa? Will be project with the project have on community water consumption? Will the project promote water recycling?	None	
What impact will the proposed project have on the quantity of waste generaled?	Unknown	LOW	Low	Minor negative	-	No significant additional waste stream will be generated during operations.	Will this proposal reduce (or increase) Council's total quantity of waste generated? What impact will the project have on rates of waste evolutionce, reuse analyze recycling by Council? What impact will the project have on use of recyclinate and recycled materials by Council? What impact will the project have on waste amount of waste tad goes to lamiful by Godard? What impact will the project have on waste generated by the community? What impact will the project have on waste generated by the community?	None	
What impact will the proposed project have on the traffic volume on North Sydney's roads?	Fastive	Hgn	Moderate	Very positive	***	The Sydney Metro City and Southwest WII increase the City's rail network capacity by 50% overall, spinificantly resoluting traffic volumes and releving the pressure on the acting road network. This will be reflected in 1 shaft volumes in North Sydney, in terms of the development of Victoria Circa Station, and the Control of Victoria Circa Station is suitable to entiry will present an association in the operation of the Circa		Enurs that pedestrain and bicycle access to the northern entry is efficient, safe and provides a positive experience to patrons, and therefore encourages, patrons to use that entry.	

Impact	of Impact	Likelihood of Occurrence	Sustainability Impact	Indicator	Description of Impacts (only operational Impact have been considered as these are relevant for the future Northern CBD Planning)	Description of Impacts Prompts	Recommendations (lissues to be taken into consideration in the Northern CBD Planning Study)	Addressing Unknown a Negative Impacts
Positive	Moderate	Moderate	Moderate positive	++	Leves of air, dust, soil and water pollution will be minimar. Although the relocation of the station entry will have minimar impact on noise levels, the relocation of the services building is 55 ML-services. Street which is associated with the station entry, all substantially pollution procedure impact on occi sensitive receivers by reducing noise at Monet Sant' Angelo Nerry College.	How will the proposal result in an improvement (or decline) in local air quality/sust? How will the proposal facilitate an improvement (or decline) in the levels of noise pollution effecting commercia and private residents? Will the proposal sed to a compression of soll (or decline) in oil self or How will the proposal facilitate an improvement (or decline) in the quality or quantity of stormwater nur- off! How will the proposal encourage (or discourage) the substitution of non-nervasite resources and/or hazardous materials with resources that are less environmentally thendy?	sinuse that no new sensitive noise receivers are proposed in the proximity of the relocated denrices pullding.	
Unknown	Low	Low	Minor negative		The procosed mostfication would involve some minor cleaning of exotic regulators and potential weeks as part of the procosed modification. It is assert to be a second or the process of Miler Street and McLaren Street.	All the project involve pruning/removal of any native or exotic vegetation (dead or alive/standing or futerion) or public land? Does the project involve disturbance to the soil profile (i.e. excavation/filing/stumping) in or within 50m of a bushand-vegetated area? Does the project involve oil disturbance or markerials storage within 10m of a notisting tree? Will there be a potential increase in nutrient runoff into a bushand-vegetated area (e.g., prosphorus; inflored exist). Will the project result in changes to drainage in a bushand-vegetated area (e.g., increase or decrease with the project result in changes to drainage in a bushand-vegetated area (e.g., increase or decrease existing trainage in the project incomposition of this project? Does the project incorporate the use of non-incolly native plants in any indiscaring/revegetation.	Locally indigenous plants should be used in landscaping and no mature times should removed in the vicinity of the stations.	
						lands?		
Positive	High	Hgh	Very positive	***	The introduction of the additional portal will increase the reach of the dation catchment to the north damadically. It will provide greater access to key locations such as the Mater Hootigs, from Stydney (Six Morth Stydney Boys High School, North Sydney (Six Morth Stydney Boys High School, North Sydney Christ High School, Minst College North Shore, North Sydney Commarkston School, St Marcy Firmings School, the residential and missch-use area to Fation Street, and Neutral Bay via the boothodge over the Warmsgah Heavey, The resulting northead in patronage could be at high at SN, with most of the last pedicatrans.	What additional capacity (ie number of people/services, kms) will there be for sustainable transport options such as washing, cycling and public transport? How warry additional pedestrian and cyclist facilities will be provided? How will be provided? How will be provided encourage increases (or decreases) in pedestrian access, obcycle access and you will be proposed encourage increases (or decreases) in pedestrian access, obcycle access and you will be proposed improve (or reached the motistip of What Bydrings residents, workers and visitors especially people with a disability, older people, and people with children in prains?	Examine the study-area wide peciestrian and cycling connectivity and infrastructure, to optimize access from across to be atte and beyond to the new northern dation error. An examine is the access from the earl of the error portal for Wisches students (amongst others) who currently use a posterial access from the other students (amongst others) and access from the control of the	
Positive	Low	Low	Minor positive	+	stop for passengers, away from the more congested streets around the southern portal, this will be to the advantage of patrons, particularly those with disabilities who may struggle in more congested and confined areas. In addition, the new entry will likely draw patrons from the many schools in that part or North System - Wennas, Mariats College, North System Boys and North	How will the proposal improve physical safety?	Safe and adequately sized pedestrian infrastructure is required, sized to allow high values pedestrian frattic during orbitol opening and cooling times, in particular. Serection are designed to discourage, any minimize	
Positive	Low	Low	Minor positive		The development of the north entry to Victors Station will include additional landscaping and improvements to pedestrian infrastructure in direct proximity to the station entry.	To what extent will the quality of Council infrastructure improve (or decrease)? To what stent will the quality of maintenance of Council infrastructure improve (or decrease)? To what settent will the speed a wider. Council indirective maintenance on infrastructure improve (or decrease)? To what extent will the quality of Council buildings, parks and spaces improve (or decrease)?	Ensure planning of the Northern CBO pedestrian peakons links (no the pedestrian infrastructure around the station erior).	
Unknown	Low	Low	Minor negative	æ	It may be positive but unknown at present	What will be the increase (or decrease) in the range and number of community and cultural services and assets available to the community? How will the proposal improve (or decrease) the quality of community and cultural services and facilities available to the community?		
Positive	Moderate	Moderate	Moderate positive	++	The development of the north entry to Victoria Station will improve access to St. Learnable Fast, and Chic Fast, in particular, Various sporting and cultural additional country to the provided access will improve cult	How will the proposal encourage more (or fewer) people to participate (actively and/or passively) in heading activities of many and the leng activities of decrease) the level of social and cultural activities within North Sydney? Does the proposal reflect the social and cultural deventy of North Sydneys community? How will the proposal encourage more (or fewer) people to participate (actively and/or passively) in social and cultural activities?	Ensure pediestrain access from the entry to St Leonards Plant, is facilitated.	
Unknown	Lbw	Lów	Minor negative		The proceed montration would not streetly impact any additional tentage term, review there is ordered for interest models at the site to totaled immediately additional to be locally significant should at 243 filler street. There are no previously recorded Abordinal heritage terms on the site.	Will the proposal facilitate the protection for lossy of hems, places or areas of significant cultural or herdings value in Nonth Systey (inclining Abongsian herdings)? If the site is located within a Conservation Area, has a Herdings impact Assessment been undertaken las required by NULLEP 2001; NULLEP 2001, NULLEP		
Positive	Low	High	Moderate positive	++	The north entity to the Vildona Station will make St Leonards Park and Clvic Park more accessible, and the inclusion of improved pedestrain links what support that. These parks are used for sporting events, family food and entertailment events, provide access to the titmay and so on.	other one-init value for consent or fitter one-entition? How will the proposal increase (or fore-tense,) the range and number of open space and recreational facilities provided within North Sydney? How will the proposal increase (or decrease) the quality of open space and recreational facilities provided within North Sydney? Will the proposal increase (or decrease) the quality of open space and recreational facilities provided within North Sydney? Will the proposal encourage more (or fewer) people to participate (actively and/or passively) in	Ensure peotestrian access from the entry to St Leohards Park is facilitated.	
	Positive Fositive Fositive Linknown Positive Linknown	Fostive Low Fostive Low Linknown Low	Footive High High Footive Low Low Footive Low Low Unknown Low Low Unknown Low Low Linknown Low Low Linknown Low Low	Positive Low Low Minor positive Linknown Low Low Minor positive Linknown Low Low Minor positive Linknown Low Low Minor positive	Fositive Low Low Minor positive Fositive Moderate Moderate Fositive Moderate Moderate Fositive Moderate Moderate Linknown Low Low Minor positive	Positive Moderate Moderate Moderate Positive Moderate Moderate Moderate Moderate Moderate Moderate Moderate Mode	House Moderate Moderate Moderate Moderate Moderate Positive Moderate Moderate Moderate Moderate Positive Moderate Moderate Positive Moderate Moderate Positive Moderate Moderate Positive Moderate Positive Moderate Positive Moderate Moderate Positive Moderate Moderate Positive Moderate Moderate Moderate Moderate Positive Moderate Modera	Francis Months House Parks of the control of the co

Accessment Question	Type of Impact	Magnitude of Impact	Likelihood of Occurrence	Expected Sustainability Impact	Colour	Description of impacts (only operational impact have been considered as these are relevant for the future Northern CBD Planning)	Description of Impacts Prompts	Recommendations (issues to be taken into consideration in the Northern CBO Planning Study)	Addressing Unknown a Negative Impacts
What long-term financial impact for Council can be expected from the proposed project, including maintenance costs and foreseeable secondary costs?	Unknown	Low	Low	Minor		All maintenance of the proposed station and entity will be undertaken by the NSW Government with no impact on Council	What are the long term operational and maintenance costs likely to be? What are the long term capital renewal, improvement or restacement costs likely to be? How will the proposal contribute to Council's income?		
How will the proposed project impact on Council's financial capability to undertake other projects?	No impact	Low	Low	Neutral	-/+		How will the proposal increase (or decrease) Council's ability to undertake other projects? What is the opportunity cost of undertaking this project?	41-2-1-2-2-2-2-2-2-2-2-2-2-2-2-2-2-2-2-2	1
What impact will the proposed project have on the economy, from local/regional to global?	Positive	Moderate	Low	Moderate positive	++	The presence of a new station at Victoria Cross, and the associated passenger fooths!, will present substitute to energiate local relative and other businesses. It is forecast that the development of the north entry to the station will office a further SN increase in pastronage to the station, which will provide additurner sometimes of relative to resilient.	What effect will the propose have on the North Sydney's commercial arch retail areas (including key possibless sectors - Necommunications, banking and finance, information technology and media and advertising) as a result of the proposal? The Increase (or decrease) levels of Investment in the North Sydney CBD and other retail areas?	Provide options, for retail and other services in the vicinity of the station entity.	
Additional Information on economic Impacts									
What impact will the project/proposal have on the Community Strategic Plan?	Positive	Low	Moderate	Moderate positive	++	The community directions confirmed in the Nieth Sydney Community Strategic Pain Includes a monographer. Increased six case to one missic and resident scaleties, environmental sistematistis, peter use of existing infrastructure, improved ultimo resident, new assets to meet current and future community needs, managing traffic congestion, conserving and celebrating our lentibus, retaining the village almosphere. Increasing economic orecidenter. The air reflected in the North Sydney Clivic Precinct and Surrounds Planning Study, and confirmed in this sustainability assessment.	Identity Direction, Goal, Objective and/or Strategy	Implement all recommendations of this subtainability plan.	
What impact with the project/proposal have on supporting Councits Management Plan?	Unknown	Low	Low	Minor negative			What policies, standards and guidelines relate to the project/proposal? How will the proposal improve (or reduce) openness and transparency in decision making?		
What impact will the project/proposal have on policies, procedures and standards?	Positive	Moderate	Moderate	Moderate positive	++	-	What policies, standards and guidelines relate to the project/proposal? How will the proposal improve (or reduce) openness and transparency in decision making?		
What impact will the proposed project have on the opportunity for stakeholders to participate in Council's decision-making processes?	Positive	Moderate	Moderate	Moderate positive	++	Extensive community consultation has been undertaken on the poject to date and this will be maintained through the subsequent phases.	Will the proposal provide more (or fever) stateholders with the opportunity to participate in Council's decision-making processes? what level of consultation/engagement is required as per Community Engagement Policy? Low = meets minimum legislation; likedium = on top of legislative requirements; High = extensive community consultation/engagement.	Maintain the commitment to consultation throughout the project	
What impact will the proposed project have on the well-being of Council staff involved, including occupational health & safety, staff satisfaction and staff personal development?	Unknown	Low	Moderate	Minor negative		The location of the north entry will reduce the walking time manginally of Council staff from the Station to Council offices.	To what stem will the proposal improve (or decrease) staff satisfaction? To what stem! will be proposal improve (or decrease) at lart eleminor? To what stem! will be proposal servese (or increase) staff stem to ever? To what stem! will be proposal improve (or decrease) staff stem of 10-186? To what stem! will the proposal improve (or decrease) staff development or training?	None	
Additional Information on governance impacts							Consider risk management and OHS implications		4
Additional information									_

8.5. NSLEP Amendment No. 26 (6 Hayes Street, Neutral Bay) - Post Gazettal Report

AUTHOR: Ben Boyd, Executive Strategic Planner and Liam Rogers, Student Town Planner

ENDORSED BY: Marcelo Occhiuzzi, Acting Director City Strategy

ATTACHMENTS:

- 1. LEP Amendment No. 26 Written Instrument [8.5.1 2 pages]
- 2. LEP Amendment No. 26 Map [8.5.2 1 page]

PURPOSE:

This report is to inform the Councillors of the gazettal of North Sydney Local Environmental Plan 2013 (Amendment 26 - 6 Hayes Street, Neutral Bay).

EXECUTIVE SUMMARY:

On 29 October 2018 Council considered a Notice of Motion which arose in response to community concerns around DA 299/18 which sought to demolish dwellings at 6 and 8 Hayes Street, Neutral Bay and construct a residential flat building. In response to these concerns, on 27 February 2019, Council placed an interim heritage order over 6 Hayes Street and commissioned GML Heritage to undertake a detailed heritage assessment of the property.

Following this assessment, on 22 July 2019, Council resolved to adopt the Planning Proposal 5/19, which sought to identify 6 Hayes Street as a local heritage item under North Sydney Local Environmental Plan 2013 (NSLEP 2013) and forward the Planning Proposal to the Department of Planning, Industry and Environment seeking a Gateway Determination.

The Minister for Planning and Public Spaces issued a Gateway Determination on 26 September 2019, allowing the proposal to be placed on public exhibition. In accordance with the Gateway Determination, the Planning Proposal was placed on public exhibition for 28 days from 10 October 2019 to 6 November 2019.

At its meeting on 25 November 2020, Council considered the post exhibition report in relation to the Planning Proposal. At this meeting, Council resolved to forward the unamended Planning Proposal to the Department of Planning, Industry and Environment with a request that an LEP be made.

NSLEP 2013 (Amendment No. 26) gives effect to the Planning Proposal which formally renders 6 Hayes Street, Neutral Bay, an item of local heritage significance. It is now in force following its publication on the NSW legislation website on 1 May 2020.

FINANCIAL IMPLICATIONS:

Nil.

RECOMMENDATION: 1. THAT the report be received.		

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 2. Our Built Infrastructure
- 2.1 Infrastructure and assets meet community needs
- 2.2 Vibrant centres, public domain, villages and streetscapes
- 4. Our Social Vitality
- 4.4 North Sydney's history is preserved and recognised

BACKGROUND

The following outlines the chronology and milestones of the Planning Proposal process to date:

- <u>29 October 2018</u> Council considered a Notice of Motion that arose in response to community concerns made in relation to DA 299/18. This DA sought to demolish the dwellings at 6 and 8 Hayes Street, Neutral Bay and construct a 4-storey residential flat building containing 9 apartments. Council resolved to urgently apply for the imposition of Interim Heritage Orders (IHOs) over 6 and 8 Hayes Street, Neutral Bay.
- <u>12 November 2018</u> The applicant of DA 299/18 lodged a Class 1 Appeal to the Land and Environment Court for the "deemed refusal" of the DA.
- 29 January 2019 Council considered the outcomes of a preliminary heritage assessment of both properties prepared on Council's behalf by GML Heritage. The preliminary assessment concluded that both properties provided a strong contribution to the character of Hayes Street and the Kurraba Point Conservation Area and that 6 Hayes Street had the potential to meet one or more of the NSW Heritage Significance criteria. The preliminary assessment recommended that Council seek the imposition of an IHO over 6 Hayes Street, Neutral Bay to provide temporary protection to allow additional research to be undertaken. GML Heritage also recommended that 8 Hayes Street, Neutral Bay be identified as a "contributory item" under NSDCP 2013 to reflect its contribution to the Kurraba Point Conservation Area.
- 27 February 2019 An IHO placed over 6 Hayes Street, Neutral Bay.
- <u>28 February 2019</u> The IHO relating to 6 Hayes Street, Neutral Bay is published in the NSW Government Gazette.
- <u>27 May 2019</u> Having considered the detailed Heritage Assessment prepared by GML Heritage for 6 Hayes Street, Council resolved to proceed with the subject Planning Proposal and to refer the Planning Proposal to the North Sydney Local Planning Panel (NSLPP) for its advice, prior to determining if the Planning Proposal should be supported to proceed to Gateway Determination.
- <u>5 June 2019</u> The NSLPP recommends Council support the progression of the Planning Proposal for the purpose of obtaining a Gateway Determination.

<u>22 July 2019</u> – Council resolves to forward the Planning Proposal to Department of Planning, Industry and Environment (DPIE) in order to receive a Gateway Determination.

<u>26 September 2019</u> – A Gateway Determination is issued enabling Council to publicly exhibit the Planning Proposal subject to conditions. Council was not authorised to exercise the functions of the Minister for Planning under section 3.36 of the EP&A Act to formally make the Plan after public exhibition.

26 September 2019 – The applicant of DA 299/18 filed a notice of discontinuance for the Class 1 Appeal to the Land and Environment Court over the "deemed refusal" of the DA.

<u>10 October 2019</u> – The Planning Proposal and draft amendment to NSDCP 2013 are placed on public exhibition for 28 days.

<u>25 November 2019</u> – Council considered a post exhibition report in relation to the Planning Proposal and draft DCP amendment and resolved to forward the Planning Proposal in relation to 6 Hayes Street to DPIE with a request that a Local Environmental Plan be made.

CONSULTATION REQUIREMENTS

Community engagement is not required.

DETAIL

1. Amendment 26 to NSLEP 2013

NSLEP 2013 (Amendment No. 26) gives effect to the Planning Proposal which formally renders 6 Hayes Street, Neutral Bay, an item of local heritage significance. It is now in force following its publication on the NSW legislation website on 1 May 2020. A copy of the instrument is provided at Attachment 1 and a copy of the amended Maps at Attachment 2.

The Amendment identifies 6 Hayes Street, Neutral Bay as a local heritage item.



North Sydney Local Environmental Plan 2013 (Amendment No 26)

under the

Environmental Planning and Assessment Act 1979

The following local environmental plan is made by the local plan-making authority under the *Environmental Planning and Assessment Act 1979*.

LUKE DOWNEND
As delegate for the Minister for Planning and Public Spaces

Published LW 1 May 2020 (2020 No 185)

North Sydney Local Environmental Plan 2013 (Amendment No 26)

under the

Environmental Planning and Assessment Act 1979

1 Name of Plan

This Plan is North Sydney Local Environmental Plan 2013 (Amendment No 26).

2 Commencement

This Plan commences on the day on which it is published on the NSW legislation website.

3 Land to which Plan applies

This Plan applies to Lot 5, DP 192932, 6 Hayes Street, Neutral Bay.

4 Maps

The maps adopted by *North Sydney Local Environmental Plan 2013* are amended or replaced, as the case requires, by the maps approved by the local plan-making authority on the making of this Plan.

5 Amendment of North Sydney Local Environmental Plan 2013

Schedule 5 Environmental heritage

Insert in appropriate order in Part 1—

Neutral Bay House 6 Hayes Street Lot 5, DP 192932 Local I1137



8.6. North Sydney Council Draft Trailer Parking Policy Report

AUTHOR: Michaela Kemp, Manager Traffic & Transport Operations

ENDORSED BY: Duncan Mitchell, Director Engineering Services

ATTACHMENTS:

- 1. Draft Trailer Parking Policy [8.6.1 6 pages]
- 2. Community Engagement Strategy Draft Trailer Parking Policy [8.6.2 6 pages]
- 3. Trailer Parking Restrictions 2020 Community Engagement Strategy [8.6.3 6 pages]

PURPOSE:

This report seeks the approval of the public exhibition of the draft Trailer Parking Policy attached.

EXECUTIVE SUMMARY:

At the Council meeting on 23 September 2019 (Item 2, Min. No. 259) it was resolved (in part):

6. THAT a draft policy for consideration of future requests for trailer parking restrictions including cost considerations and establishment of minimum criteria to manage such requests be prepared and referred to Council for consideration.

The attached draft Trailer Parking Policy has been developed to provide a consistent set of guidelines for actioning requests for trailer parking restrictions and to ensure a consistent approach to the management of on-street parking.

Due to COVID-19 restrictions it is suggested that the public exhibition be extended to 42 days to allow additional time for the community to access the draft Policy and prepare their submission.

FINANCIAL IMPLICATIONS:

The draft Trailer Parking Policy has been developed to provide guidance on how Council responds to and actions requests for trailer parking restrictions, to ensure that Council's resources are appropriately managed.

The related financial implications include cost of investigations, consultation, signage installation and maintenance, and enforcement. The associated costs will be covered by the annual Traffic & Transport Operations and Ranger & Parking Services operating budgets, and Lines and Signs capital budgets.

RECOMMENDATION:

- **1. THAT** the draft Trailer Parking Policy be placed on public exhibition for 42 days.
- **2. THAT** should Council receive submissions, a further report be prepared for Council's consideration. Should Council receive no submissions, Council consider the Trailer Parking Policy as adopted at the end of the closing period for submissions.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 2. Our Built Infrastructure
- 2.1 Infrastructure and assets meet community needs
- 2.4 Improved traffic and parking management
- 5. Our Civic Leadership
- 5.2 Council is well governed and customer focused
- 5.3 Community is informed and consulted

BACKGROUND

Each year Council receives a significant number of complaints from residents claiming that boat trailers, trailers and advertising trailers are causing parking, road safety, residential and visual amenity issues.

In recent years, and after extensive lobbying from NSW councils, including North Sydney Council, the State Government introduced a number of measures to partially address these issues:

- Impounding Act 1993 (Unattended Boat Trailers) which commenced 1 October 2016. This discourages long term parking of boat trailers. Council has power to impound boat trailers which are parked longer than permitted. Applies only to trailers which are principally constructed to transport boats. Trailers can still effectively park for 43 days at a time, including minimum required notice period; and
- SEPP No. 64 Advertising and Signage (Amendment No. 3) which commenced 1 March 2018. This prohibits display of advertisements on a trailer parked on a road or road related area, prohibits display of advertisements on a trailer parked on other land visible from a road or road related area without consent from consent authority; and Council has power to issue penalties between \$1,500-\$3,000. Note; this does not apply to advertisements that are ancillary to the dominant purpose of the trailer.

However, Council continues to receive regular complaints about long term parking of trailers including box trailers, tradesman trailers, caravans and boat trailers in residential streets and adjacent to parks, making it difficult for residents and visitors to find parking.

One method to restrict parking of trailers and caravans, while allowing unrestricted parking for motor vehicles is to install "No Parking - Motor Vehicles Excepted" signage. These relatively new restrictions were initially trialled in three locations in 2019. The trial found that the restrictions, where installed, are very effective at preventing trailers parking, but that the restrictions tend to result in trailers being shifted to other nearby streets. Council receives additional complaints and requests from residents in those areas, as well as concerns from trailer owners who may or may not reside in the North Sydney Council area.

Consideration also needs to be given to costs of community engagement and consultation, sign installation, maintenance and enforcement whenever new parking restrictions are proposed for any location.

At the Council meeting on 23 September 2019 it was resolved, in part (Item 2, Min. No. 259):

6. THAT a draft policy for consideration of future requests for trailer parking restrictions including cost considerations and establishment of minimum criteria to manage such requests be prepared and referred to Council for consideration.

The attached draft Trailer Parking Policy has been developed to provide a consistent set of guidelines for actioning requests for trailer parking restrictions and to ensure a consistent approach to the management of on-street parking.

CONSULTATION REQUIREMENTS

Community engagement will be undertaken in accordance with Council's Community Engagement Protocol.

DETAIL

The attached draft Trailer Parking Policy (draft Policy) has been developed to provide a consistent set of guidelines for actioning requests for trailer parking restrictions and to ensure a consistent approach to the management of on-street parking.

It should be noted that specific legislation applies to different types of trailers. The draft Policy therefore outlines how Council should manage requests relating to different types of trailers – including trailers displaying advertisements, boat trailers and all other types of trailers.

In developing the draft Policy, a comparison was undertaken of the requests for trailer parking restrictions received by Council since October 2019. The main complaint that residents claim about these trailers is that they are often parked for long periods and take up the limited parking resources and make it difficult for residents and visitors to find parking.

The North Sydney Integrated Traffic & Parking Strategy (2015) outlines the hierarchy for parking priorities and maximum occupancy targets for the demand management of the finite on-street parking resource. Council generally aims for a maximum occupancy rate of 85% to best make use of the parking resource whilst still ensuring motorists are able to find a parking space. Therefore, the draft Policy has been designed with these targets in mind to ensure that Council's resources are focused on the areas experiencing the greatest demand for parking.

The draft Policy outlines the proposed circumstances under which Council will pursue trailer parking restrictions and proceed with community consultation on such a proposal.

Advertising Trailers

Advertising trailers are defined in the draft Policy as "a trailer displaying an advertisement, but does not include an advertisement that is ancillary to the dominant purpose of the trailer, or

an advertisement on a trailer parked by or on behalf of a public authority in the exercise of its functions."

These trailers are prohibited to be parked on the street without consent under SEPP No. 64 Advertising and Signage (Amendment No. 3), and therefore enforcement action can be taken to discourage advertising trailers being parked on-street without the need for sign-posted parking restrictions.





Figure 1 Examples of advertising trailers (Source: https://live.staticflickr.com/1159/4605303294_0c99286959_b.jpg & https://rlsigns.com.au/mobile-billboards)

Boat Trailers

Long-term parking of boat trailers (greater than 28 days) is currently enforced under the Impounding Act 1993 (Unattended Boat Trailers). It is recognised that many boat trailer owners are also residents and, in some areas, there is more tolerance for boat trailers from surrounding residents than others. The Impounding Act 1993 (Unattended Boat Trailers) is useful for discouraging trailers to be parked in any one location for a long period of time (several months or more) but can be labour-intensive to administer.

The draft Policy includes provisions for pursuing sign-posted trailer parking restrictions in locations where regular patrols and enforcement is expected due to complaints about boat trailers. Criteria for trailer parking restrictions to be considered include a minimum number of requests (at least 30%) from different household or properties in the same street within a 60-day period and confirmed observations of at least two trailers parked in the street on two occasions over a 28 to 60-day period.

The purpose of the minimum number of requests from different households is to discourage neighbourhood disputes being disguised through these types of requests.

The purpose of the time limitations is to allow adequate time for residents to coordinate a request with their neighbours if they are so inclined to do so, to allow adequate time for staff to undertake appropriate investigations, and to provide a timeframe for Council staff to close out requests that don't meet the minimum criteria to pursue further. The minimum 28-day

period also aligns with the enforcement of the Impounding Act 1993 (Unattended Boat Trailers).





Figure 2 Examples of boat trailers

Other Types of Trailers

"Trailer" is defined in the Road Transport Act 2013 and means a vehicle that is built to be towed, or is towed, by a motor vehicle, and is not capable of being propelled for normal use on a road or road related area without being towed by a motor vehicle. Common examples of trailers include boat trailers, box trailer and caravans.

For all other requests related to trailers other than boat trailers and advertising trailers, the draft Policy includes provisions for a minimum number of requests (at least 30%) from different households or properties in the same street within a 60-day period and confirmed observations of at least two trailers parked in the street on two occasions over a 28 to 60-day period. The provisions also require a minimum average occupancy of unrestricted parking spaces in the street to be 85% or greater, and average occupancy for the whole street including restricted parking spaces to be greater than 65%, in accordance with the maximum occupancy targets outlined in the *North Sydney Integrated Traffic & Parking Strategy (2015)*.

The purpose of the minimum number of requests from different households is to discourage neighbourhood disputes being disguised through these types of requests.

The purpose of the time limitations is to allow adequate time for residents to coordinate a request with their neighbours if they are so inclined to do so, to allow adequate time for staff to undertake appropriate investigations, to ensure that the Policy addresses the issue of long-term trailer parking and not unnecessarily prevent the short-term parking of trailers where needed, and to provide a timeframe for Council staff to close out requests that don't meet the minimum criteria to pursue further.

The purpose of the minimum average occupancy rates is to ensure that trailer parking restrictions are only pursued where parking is in high demand. In addition, the draft Policy outlines that parking restrictions will not be pursued where a resident parking survey has

already been undertaken in the street in the past 24 months with regard to either trailer parking restrictions or timed parking restrictions. This is to prevent consultation fatigue of any sector of the community.



Figure 3 Examples of other trailer types

Sight Obstructions and Hazards at Particular Locations

From time to time Council receives requests to prohibit the parking of trailers claiming that the trailer is causing reduced sight lines at a particular location. To ensure any safety issues are addressed in an appropriate manner, the draft Policy includes provisions for such requests to be first considered by the Traffic Committee to recommend whether to proceed to community consultation, if the concerns are acceded by Council's Traffic Engineer.



[REF]

Page 1 of 5

Policy Owner: Director Engineering & Property Services

Category: 2. Our Built Environment

1. STATEMENT OF INTENT

- 1.1 To outline Council's objectives in managing and restricting trailer parking on local streets and to provide a consistent set of guidelines for staff when actioning requests for trailer parking restrictions.
- 1.2 To ensure that access to on-street parking is effectively managed and maximum parking occupancy targets are achieved.
- 1.3 To enhance residential amenity by reducing opportunities for long term parking of trailers in high demand unrestricted parking areas.
- 1.4 To ensure that appropriate community engagement is undertaken where trailer parking restrictions are proposed, and prevent consultation fatigue.
- 1.5 To ensure the effective management of Council's resources.

2. ELIGIBILITY

- 2.1 This Policy applies to all Councillors and employees of Council, involved in the management of trailer parking restriction requests and enforcement.
- 2.2 This Policy applies to all persons who park trailers within the North Sydney local government area
- 2.3 Any person who resides, works or owns a property within the North Sydney local government area may request trailer parking restrictions for their street

3. **DEFINITIONS**

3.1 **Advertising trailer** – a trailer displaying an advertisement, but does not include an advertisement that is ancillary to the dominant purpose of the trailer, <u>or</u> an advertisement on a trailer parked by or on behalf of a public authority in the exercise of its functions.

- 3.2 **Boat Trailer** means a trailer constructed for, or used for, the conveyance of a boat
- 3.3 **Motor Vehicle** as defined in the NSW Road Rules 2014 means a vehicle that is built to be propelled by a motor that forms part of the vehicle
- 3.4 **On-street parking** all public streets, roads and road related areas within the North Sydney local government area
- 3.5 Trailer as defined in the Road Transport Act 2013, means a vehicle that—
 (a) is built to be towed, or is towed, by a motor vehicle, and
 (b) is not capable of being propelled in the course of normal use on roads or road related areas without being towed by a motor vehicle.

Common examples of trailers include boat trailers, box trailers, and caravans.

3.6 **Trailer parking restrictions** – regulatory on-street parking restrictions that apply to a section of road sign-posted with "No Parking Motor Vehicles Excepted" signs.

4. PROVISIONS

- 4.1 Requests for trailer parking restrictions will be managed in accordance with the provisions in Section 4 of this Policy, including provisions for specific types of trailers in Clauses 4.8, 4.9, and 4.10.
- 4.2 Where resident parking consultation (timed or trailer parking restrictions) has been undertaken in the street in the past 24 months prior to the receipt of a trailer parking restriction request, the request will not be supported. This is to prevent consultation fatigue of any sector of the community.
- 4.3 Community consultation will be undertaken in accordance with the Trailer Parking Community Engagement Strategy.
- 4.4 The outcomes of the community consultation will be reported to the next available Traffic Committee meeting at the conclusion of the community consultation period.
- 4.5 In assessing a request for trailer parking restrictions consideration may be given to parking conditions for the whole street or part of the street. For short streets (shorter than 150 metres), the whole street should be taken into consideration. For long streets (longer than 150 metres) it may be appropriate to only consider part of the street, depending on the nature of the request and existing parking controls in the street, in which case the part of the street should consist of no less than 20 parking spaces and consist of a block or series of blocks between two (2) adjoining roads.

- 4.6 Notwithstanding all other provisions of this Policy, if a trailer of any type is considered by Council's Traffic Engineer to cause a hazardous sight obstruction to pedestrians and/or vehicles, such as on a bend, or near an intersection, the matter shall be referred to the next available North Sydney Traffic Committee to determine whether to pursue the community engagement with regard to the installation of "No Parking Motor Vehicles Excepted" in the location of the sight obstruction. Consideration should also be given to motorbike parking if parking of any vehicle larger than a motorbike at the location would cause similar hazardous sight obstructions.
- 4.7 Where requests for trailer parking restrictions do not meet the criteria outlined in this policy, Council will respond to the customer(s) that the request is not supported and outline the reasons in accordance with this policy.

4.8 **Advertising Trailers**

- 4.8.1 The placement of advertising trailers is regulated under State Environmental Planning Policy No 64—Advertising and Signage
- 4.8.2 A person must not display an advertisement on a trailer parked on a road or road related area.
- 4.8.3 A person must not display an advertisement on a trailer parked on land other than a road or road related area, but visible from a road or road related area, except with the consent of the consent authority
- 4.8.4 Council will enforce placement of advertising trailers in accordance with State Environmental Planning Policy No 64—Advertising and Signage

4.9 **Boat Trailers**

- 4.9.5 The whole of the North Sydney local government area is a declared area for the purposes of the Impounding Act 1993 (Unattended boat trailers)
- 4.9.6 Council will undertake community consultation with regard to community preference for the introduction of "No Parking Motor Vehicles Excepted" for streets (or parts thereof) which meet the following criteria:
 - 4.9.6.1 In any 60-day period Council receives requests (or a petition) from people from at least 30% of the households or properties within the same street for trailer parking restrictions to be installed in their street due to boat trailers and;
 - 4.9.6.2 A minimum of two site observations by Council staff, at between 28 and-60 day intervals, confirm on each occasion there are two (2) or more trailers (of any type) parked; or trailers occupy 10% or more of the unrestricted parking in the street and;

4.9.6.3 Council's Manager Ranger and Parking Services recommends trailer parking restrictions be pursued due to the need for regular enforcement under the Impounding Act 1993 (Unattended boat trailers) for that street.

4.10 Other types of trailers

- 4.10.7 Council will undertake community consultation with regard to community preference for the introduction of "No Parking Motor Vehicles Excepted" for streets (or parts thereof) which meet the following criteria
 - 4.10.7.1 In any 60-day period Council receives requests (or a petition) from people from at least 30% of the households or properties within the same street for trailer parking restrictions to be installed in their street due to trailers of any type and;
 - 4.10.7.2 A minimum of two site observations by Council staff, at between 28 and 60-day intervals, confirm on each occasion there are two (2) or more trailers (of any type) parked; or trailers occupy 10% or more of the unrestricted parking in the street and;
 - 4.10.7.3 The average occupancy of the unrestricted parking spaces in the street (or part thereof) is **85% or greater** and;
 - 4.10.7.4 Where there is a combination of restricted and unrestricted parking in the street (or part thereof), the average parking occupancy is **between 65% to 85%**.
- 4.10.8 Where criteria 4.10.7.1 to 4.10.7.3 are met, but the average parking occupancy for the street (or part thereof) is 85% or greater and no resident parking consultation has been undertaken in that street in the 24 months prior to criteria 4.10.7.1 being met, Council will undertake community consultation in the street, or Resident Parking Area that includes the street, with regard to the community preference for options including (a) timed parking restrictions with exemptions for resident parking permit holders, and (b) "No Parking Motor Vehicles Excepted"

5. RESPONSIBILITY/ACCOUNTABILITY

5.1 Council's Traffic and Transport Operations Department is responsible for the provision and management of trailer parking restrictions.

- 5.2 Council's Ranger and Parking Services Department is responsible for the enforcement of parking controls and relevant legislation.
- 5.3 The North Sydney Traffic Committee is responsible for making recommendations to Council on the regulation of traffic on Council roads, including implementation of parking controls.
- 5.4 Council's Manager Traffic & Transport Operations will review this Policy every four years or as required by Council or senior management.

6. RELATED POLICIES/DOCUMENTS/LEGISLATION

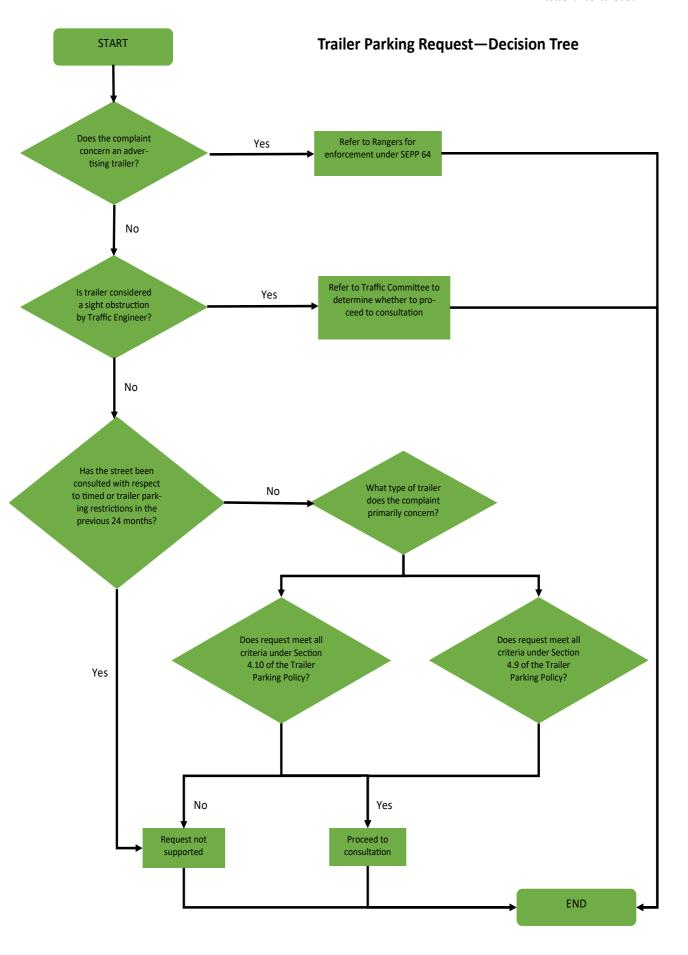
The Policy should be read in conjunction with the following Council policies and documents:

- Resident Parking Permit Policy
- Parking Management and Enforcement Policy
- North Sydney Integrated Traffic & Parking Strategy (2015)
- North Sydney Transport Strategy (2017)
- North Sydney Local Area Traffic Management Action Plans
- Trailer Parking Community Engagement Strategy

The Policy should be read in conjunction with the following documents/legislation:

- Impounding Act 1993 (Unattended boat trailers)
- SEPP No. 64 Advertising and Signage (Amendment No. 3)
- NSW Road Rules 2014

Version	Date Approved	Approved by	Resolution No.	Review Date
1	#	#	#	2024/25



COMMUNITY ENGAGEMENT STRATEGY

Draft Trailer Parking Policy



Prepared May 2020

Councils are required under the *Local Government Act 1993* to inform the community of particular issues that potentially affect their way of life. North Sydney Council is committed both in principle and in practice, to engaging on matters affecting the North Sydney community.

The purpose of this project-specific Community Engagement Strategy is to outline the ways stakeholders can be involved in the decision-making process. Community engagement opportunities will be provided across a range of 'engagement' levels.

1. Introduction

This Community Engagement Strategy outlines the steps Council will take to engage the community in adopting the Draft Trailer Parking Policy. Council is committed to engaging the community to ensure that the parking restrictions are balanced in accordance with parking demands and Council's parking hierarchy as outlined in the *North Sydney Integrated Parking Strategy (2015)*.

1.1 Council's Community Engagement Protocol

This strategy has been prepared in accordance with Council's *Community Engagement Protocol*. The Protocol is used to determine the level of 'level(s) of impact' applicable to this project/decision (proposal). This proposal has been determined as:

LEVEL OF IMPACT	LEVEL OF ENGAGEMENT
Level 3 (LGA Wide/ Low Level Impact)	Inform, Consult, Involve

Council used the framework shown below in Table 1.1 to select the most appropriate 'level(s) of engagement' for this proposal to ensure an appropriate range of engagement 'levels' and methods were offered:

LEVEL	DESCRIPTION	
Inform	Providing balanced and objective information to help the community understand	
	problems, alternatives, opportunities and/or solutions	
Consult	Obtain public feedback on alternatives and/or decisions	
Involve	Work directly with the community throughout the process to ensure that public	
	concerns and aspirations are consistently understood and considered	
Collaborate	Partner with the public in each aspect of the decision including the development of	
	alternatives and identification of the preferred solution	

Table 1.1 Derived from the IAP2 Public Participation Spectrum

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2. Background

Each year Council receives a significant number of complaints from residents claiming that boat trailers, trailers and advertising trailers are causing parking, road safety, residential and visual amenity issues.

In recent years, and after extensive lobbying from NSW councils, including North Sydney Council, the State Government introduced a number of measures to partially address these issues:

- Impounding Act 1993 (Unattended Boat Trailers) which commenced 1 October 2016. This discourages long term parking of boat trailers. Council has power to impound boat trailers which are parked longer than permitted. Applies only to trailers which are principally constructed to transport boats. Trailers can still effectively park for 43 days at a time, including minimum required notice period; and
- SEPP No. 64 Advertising and Signage (Amendment No. 3) which commenced 1 March 2018. This prohibits display of advertisements on a trailer parked on a road or road related area, prohibits display of advertisements on a trailer parked on other land visible from a road or road related area without consent from consent authority; and Council has power to issue penalties between \$1,500-\$3,000. Note; this does not apply to advertisements that are ancillary to the dominant purpose of the trailer.

However, Council continues to receive regular complaints about long term parking of trailers including box trailers, tradesman trailers, caravans and boat trailers in residential streets and adjacent to parks, making it difficult for residents and visitors to find parking.

One method to restrict parking of trailers and caravans, while allowing unrestricted parking for motor vehicles is to install "No Parking - Motor Vehicles Excepted" signage. These relatively new restrictions were initially trialed in three locations in 2019. The trial found that the restrictions, where installed, are very effective at preventing trailers parking, but that the restrictions tend to result in trailers being shifted to other nearby streets. Council receives additional complaints and requests from residents in those areas, as well as concerns from trailer owners who may or may not reside in the North Sydney Council area.

Consideration also needs to be given to costs of community engagement and consultation, sign installation, maintenance and enforcement whenever new parking restrictions are proposed for any location.

At the Council meeting on 23 September 2019 it was resolved (in part):

That a draft policy for consideration of future requests for trailer parking restrictions including cost considerations and establishment of minimum criteria to manage such requests be prepared and referred to Council for consideration.

A Draft Trailer Parking Policy has been developed to provide a consistent set of guidelines for actioning requests for trailer parking restrictions and to ensure a consistent approach to the management of on-street parking.

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3. Community Engagement Strategy

3.1 Who are our community stakeholders?

The Community Engagement Strategy identifies the following groups to engage with in the local community:

- Local residents and their visitors/guests
- Local businesses, including their customers
- Property owners
- Local schools staff, students, P&C, parents and guardians
- Precinct Committees
- Park and recreational facility users
- Trailer owners
- Traffic Committee
- Community members who have requested trailer parking restrictions since the initial trial in 2019.
- Internal Works Department; Ranger and Parking Services Officers

3.2 Key Communication Messages

- The purpose and intent of the Draft Trailer Parking Policy is:
 - To outline Council's objectives in managing and restricting trailer parking on local streets and to provide a consistent set of guidelines for staff when actioning requests for trailer parking restrictions.
 - o To ensure that access to on-street parking is effectively managed and maximum parking occupancy targets are achieved.
 - O To enhance residential amenity by reducing opportunities for long term parking of trailers in high demand unrestricted parking areas.
 - o To ensure that appropriate community engagement is undertaken where trailer parking restrictions are proposed, and prevent consultation fatigue.
 - o To ensure the effective management of Council's resources.
- Council will undertake public exhibition of draft Policy including an online feedback form
- It is anticipated that the result of the public exhibition will be reported to Council August 2020 should submissions be received during exhibition period.
- Once adopted, Trailer Parking Policy will be published on Council's website and in Council's Policy Manual.

3.3 Timetable

Community and stakeholder engagement will occur over the next four months. The key project development phases are outlined in the following table:

Page 4

Phase	Timing
1. Research and Draft Trailer Parking Policy	May 2020
2. Public Exhibition	May to July 2020
3. Finalise Trailer Parking Policy	July 2020

Note: In accordance with Council's Community Engagement Framework described on page 1, the 'level of engagement' per engagement method is indicated.

3.3.1 Phase 1 - Research and Draft Trailer Parking Policy

This phase involves research and development of the Draft Trailer Parking Policy, taking into consideration feedback from the initial trial at three locations in 2019, and requests for similar restrictions in other locations since the trial. The Draft Trailer Parking Policy will be referred to the May 2020 Council meeting.

3.3.2 Phase 2 - Public Exhibition

This phase involves seeking community feedback through public exhibition of the Draft Trailer Parking Policy which will be undertaken in May to July 2020.

To do this we will undertake the following activities between May and July 2020. Not listed in priority order.

Method	Target Stakeholders	Engagement Level	Purpose
Social Media	All	Inform	Inform local community members
E-news,	Subscribers of relevant Council's newsletters such as North Sydney Enews Precinct Enews, Business Enews and relevant Keep Informed Enews (i.e. those who made a submission in 2019 Trailer Parking Trial)	Inform	about the public exhibition of the Draft Trailer Parking Policy, where to access and how to make a submission.
Direct letter/email to customers who have requested trailer parking restrictions since the initial trial	Community members who have indicated they are affected by trailers parked on streets	Inform	
Precinct System ¹	All Active Precinct Committees	Inform	Encourage Precinct Committees to promote consultation opportunity to their members
Webpage	All	Inform, Consult	Online access to the Draft Trailer Parking Policy and online form to collect feedback.
Public Exhibition	All	Inform, Consult	Provide access to the Draft Trailer Parking Policy in Customer Service, including details on how to make a

¹ For more information about the Precinct System visit Council's website https://www.northsydney.nsw.gov.au/Council Meetings/Community Engagement/Precincts

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Method	Target Stakeholders	Engagement Level	Purpose
			submission
Written	All	Inform/Consult	Free form feedback accepted by
Submissions			email or post and online form.

3.3.3 Phase 3 - Finalise Trailer Parking Policy

This phase involves reviewing any submissions received during the public exhibition period, updating the Trailer Parking Policy as required and reporting the Policy to Council for adoption. If no submissions are received during the public exhibition period, the Draft Policy will be adopted.

Once adopted Council will inform stakeholders and the community of the Policy and outcome. To do this we will undertake the following activities between May and July 2020. Not listed in priority order.

Method	Target Stakeholders	Engagement Level	Purpose
Council Report	Councillors	Inform	Report the outcomes of the public exhibition to Council and the proposed Policy for adoption
Webpage	All	Inform	Online access to the adopted Trailer Parking Policy including Council's Policy Manual
Direct Email/ Letter	All that made a submission in the 2019 and 2020 consultations	Inform	Inform submitters about Council's decision on the Policy
E-news	Subscribers of relevant Council's newsletters such as North Sydney Enews Precinct Enews, Business Enews and relevant Keep Informed Enews (i.e. those who made a submission in 2019 Trailer Parking Trial)	Inform	Inform local community members about Council's decision on the Policy
Precinct Committees	All Active Precinct Committees	Inform	
Direct letter/email	Customers who have requested trailer parking restrictions since the initial trial	Inform	Inform local community members about Council's decision on the Policy and provide information on the process to request for restrictions to be reviewed.

4. Opportunity Cost/Rationale

Engaging the community in this proposal may entail financial costs to Council to achieve a high-quality engagement process. If the process is robust community, ownership of the decisions made will ensure efficient outcomes. Insufficient or poor-quality engagement can

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result in poor long-term decisions requiring further resources to rectify. The aim of a high-quality community engagement process is to make sustainable decisions. The engagement process will help Council staff and/or Councillors to understand the related recommendations rationale.

5. Further Information

For further information please contact Council's Manager Traffic & Transport Operations, Engineering & Property Services Division:

Phone: 9936 8100

Email: <u>council@northsydney.nsw.gov.au</u>
Website: <u>www.northsydney.nsw.gov.au</u>

COMMUNITY ENGAGEMENT STRATEGY



Trailer Parking Restrictions, Implementation of Policy

Prepared May 2020

Councils are required under the *Local Government Act 1993* to inform the community of particular issues that potentially affect their way of life. North Sydney Council is committed both in principle and in practice, to engaging on matters affecting the North Sydney community.

The purpose of this project-specific Community Engagement Strategy is to outline the ways stakeholders can be involved in the decision-making process. Community engagement opportunities will be provided across a range of 'engagement' levels.

1. Introduction

This Community Engagement Strategy outlines the steps Council will take to engage the community with regard to the introduction of trailer parking restrictions in a particular street. Council is committed to engaging the community to ensure that the parking restrictions are balanced in accordance with parking demands and Council's parking hierarchy as outlined in the *North Sydney Integrated Parking Strategy (2015)* and the *Trailer Parking Policy*.

1.1 Council's Community Engagement Protocol

This strategy has been prepared in accordance with Council's *Community Engagement Protocol*. The Protocol is used to determine the level of 'level(s) of impact' applicable to this project/decision (proposal). This proposal has been determined as:

LEVEL OF IMPACT	LEVEL OF ENGAGEMENT	
High, Local	Inform, Consult	

Council used the framework shown below in Table 1.1 to select the most appropriate 'level(s) of engagement' for this proposal to ensure an appropriate range of engagement 'levels' and methods were offered:

LEVEL	DESCRIPTION		
Inform	Providing balanced and objective information to help the community understand		
	problems, alternatives, opportunities and/or solutions		
Consult	Obtain public feedback on alternatives and/or decisions		
Involve	Work directly with the community throughout the process to ensure that public		
	concerns and aspirations are consistently understood and considered		
Collaborate	Partner with the public in each aspect of the decision including the development of		
	alternatives and identification of the preferred solution		

Table 1.1 Derived from the IAP2 Public Participation Spectrum

2. Background

Each year Council receives a significant number of complaints from residents claiming that

Page 2

boat trailers, trailers and advertising trailers are causing parking, road safety, residential and visual amenity issues.

In recent years, and after extensive lobbying from NSW councils, including North Sydney Council, the State Government introduced a number of measures to partially address these issues:

- Impounding Act 1993 (Unattended Boat Trailers) which commenced 1 October 2016. This discourages long term parking of boat trailers. Council has power to impound boat trailers which are parked longer than permitted. Applies only to trailers which are principally constructed to transport boats. Trailers can still effectively park for 43 days at a time, including minimum required notice period; and
- SEPP No. 64 Advertising and Signage (Amendment No. 3) which commenced 1 March 2018. This prohibits display of advertisements on a trailer parked on a road or road related area, prohibits display of advertisements on a trailer parked on other land visible from a road or road related area without consent from consent authority; and Council has power to issue penalties between \$1,500-\$3,000. Note; this does not apply to advertisements that are ancillary to the dominant purpose of the trailer.

However, Council continues to receive regular complaints about long term parking of trailers including box trailers, tradesman trailers, caravans and boat trailers in residential streets and adjacent to parks, making it difficult for residents and visitors to find parking.

One method to restrict parking of trailers and caravans, while allowing unrestricted parking for motor vehicles is to install "No Parking - Motor Vehicles Excepted" signage. Council has developed a Trailer Parking Policy to provide a consistent set of guidelines for actioning requests for trailer parking restrictions and to ensure a consistent approach to the management of on-street parking.

3. Community Engagement Strategy

3.1 Who are our community stakeholders?

The Community Engagement Strategy identifies the following groups to engage with in the local community:

- Local residents and their visitors/guests
- Local businesses, including their customers
- Property owners
- Local schools staff, students, P&C, parents and guardians
- Precinct Committees
- Park and recreational facility users
- Trailer owners
- Traffic Committee
- Internal Works Department; Rangers and Parking Service Officers

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Trailer Parking Restrictions, Implementation of Policy, Community Engagement Strategy

3.2 Key Communication Messages

- The purpose of the restrictions is to better manage the finite parking resource in accordance with Council's parking management hierarchy, and Local Area Traffic Management (LATM) Action Plans to meet the needs of the community.
- The proposal for trailer parking restrictions has been considered in accordance with the provisions of the Trailer Parking Policy
- Trailers that are hitched to motor vehicles would be exempt from the prohibitive restrictions.
- Community feedback is invited regarding a proposal to install parking restrictions which will prohibit the parking of trailers in a particular location.
- The outcomes of the community consultation will be referred to the Traffic Committee to determine if Council should proceed with installation of the proposed restrictions.
- Updates and outcomes regarding the outcomes of the review will be advised through Council's Your Say webpage and corporate website, as well as direct email/letter to those on the 'keep informed' list.

3.3 Timetable

Requests for trailer parking restrictions may be submitted to Council at any time. If the request meets the criteria outlined in the Trailer Parking Policy, the following phases will occur.

Phase	Level of Engagement	Timing
1. Initial Investigation	Inform	60 days
2. Community Consultation (if proposal meets	Inform, consult	Min. 28 days
Policy criteria at Phase 1)		
3. Collate and analyse responses	Inform	10 business days
		following conclusion of
		community consultation
4. Referral to Traffic Committee	Inform	Next available meeting
		following consultation
5. Implementation including notification (if	Inform	Following adoption at
proposal is recommended & adopted)		Council meeting

Note: In accordance with Council's Community Engagement Framework described on page 1, the 'level of engagement' per engagement method is indicated.

3.3.1 Phase 1 - Initial Investigation

Phase 1 will involve investigation of the extent of the problem on receipt of the request. If the request meets the criteria outlined in the Trailer Parking Policy, Council will proceed to community consultation in Phase 2. and if adopted, notification will be undertaken in Phase 2. To do this Council will undertake the following activities during the first 60 days of receipt of the initial request (not listed in priority order):

Method	Target Stakeholders	Engagement Level	Purpose
Customer	All	Inform	Customer submits request to Council
request	All	Inform	for consideration

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Method	Target Stakeholders	Engagement Level	Purpose
Internal consultation	Rangers and Parking Service Officers	Inform, consult	Internal consultation and investigation/data gathering
Direct Email/ Letter	Customer(s) who initiated the request	Inform	Keep customer informed of the status of their request and whether the request meets the criteria in the Trailer Parking Policy

3.3.2 Phase 2 - Community Consultation

If the initial investigation at Phase 1 determines that the proposal meets the criteria within the Policy, Council will proceed to Community Consultation (Phase 2) to consult with all residents, businesses and property owners in the street.

To do this we will undertake the following activities for a minimum 28-day period (not listed in priority order):

Method	Target Stakeholders	Engagement Level	Purpose
Direct Letter	Residents/ Businesses/ Property Owners	Inform	To inform residents, businesses and property owners in the affected street about the proposal and how they can have a say.
Online survey (Your Say)	All	Inform.,Consult	Conducted via online Your Say portal. Purpose is to obtain views about the parking restrictions.
Submissions	All	Consult	Seek feedback on the parking restrictions either by letter, email or online feedback form.
Webpage	All	Inform	To provide information about the proposal and how the community can have a say.
Social Media (geotargeted)	All	Inform	Inform residents, businesses and property owners in the affected street about the proposal and how they can have a say.
Notice/signage	Boat trailer/caravan owners/users in the initial areas of implementation	Inform	To inform boat trailer/caravan owners/users in the vicinity about the proposal and how they can have a say.
Precinct Committees	Precinct(s) covering the affected street	Inform	Encourage Precinct Committees to promote engagement to their members.
E-news	Subscribers to: Council E-news, Precinct E- news, Business E-news and relevant Keep Informed E-news	Inform	Inform subscribers of the proposal and the opportunity to provide feedback.

3.3.3 Phase 3 & 4 - Collation, Analysis and Referral to Traffic Committee

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This Phase involves evaluating the community consultation; including reporting to the Traffic Committee and Council on the outcomes and any community feedback received. Following Phase 2, the responses to the community consultation will be collated, analysed and a report will be referred to the next available Traffic Committee meeting, including recommendations based on the outcomes of Phase 2. The following activities will be undertaken in Phase 3 & 4.

Method	Target Stakeholders	Engagement Level	Purpose
Traffic Committee	Roads & Maritime Services, NSW Police, Local State Member, Councillors, General Public	Involve	Report the outcomes of the community consultation to the Traffic Committee and to seek recommendations.
Webpage	All	Inform	Inform the affected community
Social Media	All	Inform	about the outcomes of the review.
Direct Email/ Letter	Contributors to engagement	Inform	Inform submitters about the outcomes of the community engagement and links to the relevant report.
Precinct Committees	All	Inform	Inform Precinct Committees about the outcomes of the community engagement and links to the relevant report.
E-news	Subscribers to: Council E-news, Precinct E- news, Business E-news and relevant Keep Informed E-news	Inform	Inform subscribers about the outcomes of the community engagement and links to the relevant report.

3.3.4 Phase 5 - Implementation

Following consideration of the final report/Traffic Committee recommendation, the Council will decide whether to proceed with installation of the proposed parking restrictions or retain the existing parking. Stakeholders will be informed by the methods detailed below.

Method	Target Stakeholders	Engagement	Purpose
		Level	
Webpage	All	Inform	Inform the affected community
Social Media	All	Inform	about Council's decision on the
			proposal.
Letterbox Drop	All	Inform	If Council decides to proceed with
-			the proposal to change parking
			restrictions, Council will inform all
			residents, businesses and property
			owners in the affected street of
			Council's decision and when the
			changes are likely to occur
Direct Email/	Contributors to	Inform	Inform submitters about Council's
Letter	engagement		decision and when the changes are
			likely to occur
Notice/signage	Boat trailer/caravan	Inform	Inform boat trailer/caravan
	owners/users in the		owners/users in the vicinity of the

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Method	Target Stakeholders	Engagement Level	Purpose
	initial areas of implementation		proposal about Council's decision and when the changes are likely to occur
Precinct Committees	All	Inform	Inform Precinct Committees about Council's decision and when the changes are likely to occur
E-news	Subscribers to: Council E-news, Precinct E- news, Business E-news and relevant Keep Informed E-news	Inform	Inform subscribers about Council's decision and when the changes are likely to occur

4. Opportunity Cost/Rationale

Engaging the community in this proposal may entail financial costs to Council to achieve a high-quality engagement process. If the process is robust, community ownership of the decisions made will ensure efficient outcomes. Insufficient or poor-quality engagement can result in poor long-term decisions requiring further resources to rectify. The aim of a high-quality community engagement process is to make sustainable decisions. The engagement process will help Council staff and/or Councillors to understand the related recommendations rationale.

5. Further Information

For further information, please contact Council's Manager Traffic & Transport Operations, Michaela Kemp, Engineering & Property Services Division:

Phone: 9936 8100

Email: <u>yoursay@northsydney.nsw.gov.au</u>
Website: <u>www.northsydney.nsw.gov.au</u>

8.7. Review of Voluntary Smoking Ban in North Sydney CBD

AUTHOR: Mark Richardson, Manager Ranger & Parking Services

ENDORSED BY: Marcelo Occhiuzzi, Acting Director City Strategy

ATTACHMENTS:

- 1. Proposed Smoking Ban Chambers Precinct [8.7.1 1 page]
- 2. Proposed Smoking Ban North Sydney CBD [8.7.2 1 page]

PURPOSE:

To respond to a Council resolution of 22 June 2019, relating to the ban on cigarette and ecigarette smoking within specifically defined areas within the North Sydney central business district.

EXECUTIVE SUMMARY:

Following Council's resolution on 22 July 2019, a self-enforcing ban on smoking within the public areas of the North Sydney CBD commenced on Monday 28 October 2019.

Prior to the commencement of the ban, Council wrote to businesses located in the North Sydney CBD to advise of the commencement of the ban. The ban was also promoted through social media, newsletters and Council's website. Advertising signage was also provided at North Sydney railway station.

A total of 54 round decal pavement signs were strategically placed on the footpaths within and at the borders of the designated non-smoking area.

The decals and the implementation of the ban were designed to deliver a "soft" launch supported by an education strategy aimed at informing, rather than infringing, to achieve compliance with the smoking ban.

This report discusses the feedback and learnings during this non-smoking period and recommends a way forward. Most people felt that the ban was not working as it was not being enforced. More signage, a level of enforcement, designated areas for smokers including with ashtrays are some of the recommendations of this report aimed at seeking more significant reductions in the impact of smoking in public areas than what is currently apparent.

It should be noted that if Council supports a level of enforcement of the ban, this will be limited by resourcing and likely to be incidental at this stage. To target strict compliance would require a reduction of resourcing of other areas of Rangers' enforcement activities in order to prioritise such work.

This report also recommends that the ban be further reviewed within 12 months to gauge the success of the measures taken and adapt as necessary.

FINANCIAL IMPLICATIONS:

There are various small items of infrastructure that will be required to be installed should Council proceed with the smoking ban. These include rubbish bins, ashtrays and signage. Annually, these are estimated to cost \$42,600. This would comprise \$15,000 (installation of three stainless steel rubbish bins for smokers), \$21,600 (servicing rubbish bins twice daily) and \$6,000 (enforceable signage). Our existing budget provisions do not cover these items and as such, a budget allocation for this initiative will be required. In addition to this, should Council wish to more proactively enforce such ban, it is considered that additional staff may be required to be employed depending on the extent of proactivity desired.

RECOMMENDATION:

1. THAT Council:

- Install signage banning smoking in designated areas that is enforceable and visible within the ban area/s as shown on the maps attached to this report.
- Adopt an enforcement strategy of warning in the first instance and then infringing for all or any subsequent acts by an individual.
- Reduce the size of the current ban area to smaller zones to reflect those areas where smokers congregate to the annoyance of the non-smoking public as identified in this report.
- Include within or close to the ban area small locations where smoking will be permitted that does not affect the non-smoking public and erect signage advising of the approval to smoke.
- Install appropriate bins with ashtrays in each designated smoking area.
- Review the performance of these recommendations in 12 months.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 3. Our Future Planning
- 3.2 North Sydney CBD is one of NSW's pre-eminent commercial centres
- 3.5 North Sydney is regulatory compliant

BACKGROUND

At the 3721st Council Meeting held on 22 July 2019 it was resolved (Min. No. 188):

- 1. THAT Council note the outcomes of the consultation.
- 2. THAT Council resolves to proceed with banning smoking in the North Sydney CBD.
- 3. THAT Council:
- a. defines the geographical boundary where smoking is prohibited as being the North Sydney CBD boundary defined in the North Sydney Local Environmental Plan (LEP) 2013 (Attachment 1).
- b. rescinds the Smoking in Public Places Policy adopted by Council in 2011 and amended in 2017;
- c. adopts the Smoke Free Environment Declared Public Areas Policy (Attachment 3); and
- d. Undertakes a two-stage program to introduce the smoking prohibition as outlined in this report, with a Council report on Stage 1 by March 2020.
- 4. THAT the area where smoking is prohibited be extended to include the Council Chambers and the Civic Centre.

This report is the review of the outcome of Stage 1 of the ban, as required of resolution 3d above.

CONSULTATION REQUIREMENTS

Community engagement is not required in this instance as it has been undertaken prior to the implementation of the CBD ban.

DETAIL

Following Council's resolution on 22 July 2019, a self-enforcing ban on smoking within the public areas of the North Sydney CBD commenced on Monday 28 October 2019.

Prior to the commencement of the ban, Council wrote to businesses located in the North Sydney CBD to advise of the commencement of the ban and requested that they inform their staff. The ban was also promoted through social media, newsletters and Council's website. Advertising signage was also provided at North Sydney railway station.

A total of 54 round decals were strategically placed on the footpaths within and at the borders of the designated area as defined in the submission to the Council meeting of 22 July 2019.

The decals and the implementation of the ban were designed to deliver a "soft" launch supported by an educative strategy aimed at informing, rather than infringing, to achieve compliance with the smoking ban. The total cost of implementing this ban was approximately \$9.000.

Ranger and Parking Services staff were tasked only with advising people found to be smoking in the area of the ban. Enforcement action was not possible during this period as the decals, consistent with a "soft" launch approach, excluded reference to the Local Government Act or any penalties applicable for non-compliance.

During the period since the launch of the ban up to the time of writing this report Council has received approximately 50 communications from members of our community. The vast majority of these supported a ban and 13 objected. The common themes of the communications received are:

- 1. Inadequate signage decals are too small and not numerous enough. Requesting signage on walls.
- 2. Signs have not made any impact everyone is still smoking wherever they like.
- 3. Not being policed and self-regulation is not working. Community wants people fined for breaching the policy.
- 4. Residents finding that people are congregating in large groups smoking in front of buildings and people's gates. Smoke infiltrating residents' homes and having to clean up butts people are dropping.
- 5. Whilst the policy is supported and 'good in theory", it is not working well.
- 6. Smokers are becoming abusive and confrontational when they have been approached to observe the ban.
- 7. Smokers advising people are abusing and harassing them if they are smoking.
- 8. Community is supportive of the policy but asking for council to provide dedicated areas for people to smoke.
- 9. Ashtray bins are still left within the no smoking area, encouraging people to smoke.
- 10. Requests to extend the ban further some streets specifically mentioned include Arthur Street and Little Walker Street. Some have recommended the ban to be implemented across the North Sydney local government area.

In January 2020, Council staff attended various areas in the CBD to monitor the effect of the ban and interview willing persons about the ban.

The result of that monitoring found that between 0930 and 1030 over several weekdays more than 100 people per day were smoking within the area of the ban.

People interviewed included both smokers and non-smokers. They were asked their opinions of the ban and generally responded as follows:

- Nearly all people were in favour of the ban, including many smokers.
- Only 30% of people were aware of the ban.
- Very few people had noticed the decals.
- All people interviewed felt the ban is not working.
- Council provides ashtrays on bins in the ban area and this sends confusing messages.
- The ban has a greater chance of success if the displaced smokers are provided with a location to go and are provided with bins and ashtrays.

Reviewing this ban, it is apparent that the current strategy of self-regulating is not effective. Non-smokers have sought enforcement by Council Officers for non-compliance and have expressed their disappointment when advised of the self-regulating strategy. Smokers are defiant of the ban in the knowledge that no penalties are to be applied for non-compliance. Exacerbating the self-regulating strategy, people are not willing to confront a smoker or smokers who ignore the ban.

It is undeniable that there are benefits to be gained for a community from the implementation of smoking bans in public areas as research has shown. The first and most significant benefit is the reduction of the exposure to second-hand smoke by non-smokers passing through an area where smoking is or has been tolerated and where smokers tend to gather.

A benefit also shown by research, has been a measurable increase in the number of smokers quitting their smoking habit due to the emerging anti-social view of smoking as evidenced by alienated and isolated groups of smokers congregating in allocated areas approved for smoking.

Another benefit of a ban on smoking is the reduction in cigarette butt litter in general and a reduction in the concentrated litter in the areas to be included in the ban where smokers currently congregate.

Several papers researched stated that an area ban has a greater likelihood of success if the displaced smokers are accommodated. This point of view is also supported by the Environment Protection Agency (EPA) citing that successful bans have buy-in from smokers in the form of contribution to the development and implementation of the ban. The EPA stated that the identification of an area or areas where the displaced smokers are accepted is tantamount to the acceptance and ultimately, the success of a ban.

This point of view may very well be of major significance and warranting strong consideration given the self-regulating nature of the ban by North Sydney Council.

The self-regulating strategy needs to be able to escalate to penalise repeated non-compliance. Brisbane City Council has successfully implemented smoking bans in several pedestrian malls. They state on the website that a first act of non-compliance will receive a warning and subsequent act/s will be infringed.

The defined area is large and includes streets where very few smokers are seen, unlike the central parts of the CBD. The ban in these outer areas causes unnecessary antagonism between the odd smoker and residents for no perceivable benefit.

Consideration must be given to the displaced smokers for the ban to have the greatest likelihood of succeeding. This consideration would be in the form of designated areas where smoking is permitted and where there is provision for waste disposal.

WAY FORWARD

The following provides a summary of the actions proposed to make the current ban more successful and address the stakeholder feedback received to date. As with any new initiative, it is proposed to review its operation after 12 months to gauge its success and identify potential improvements.

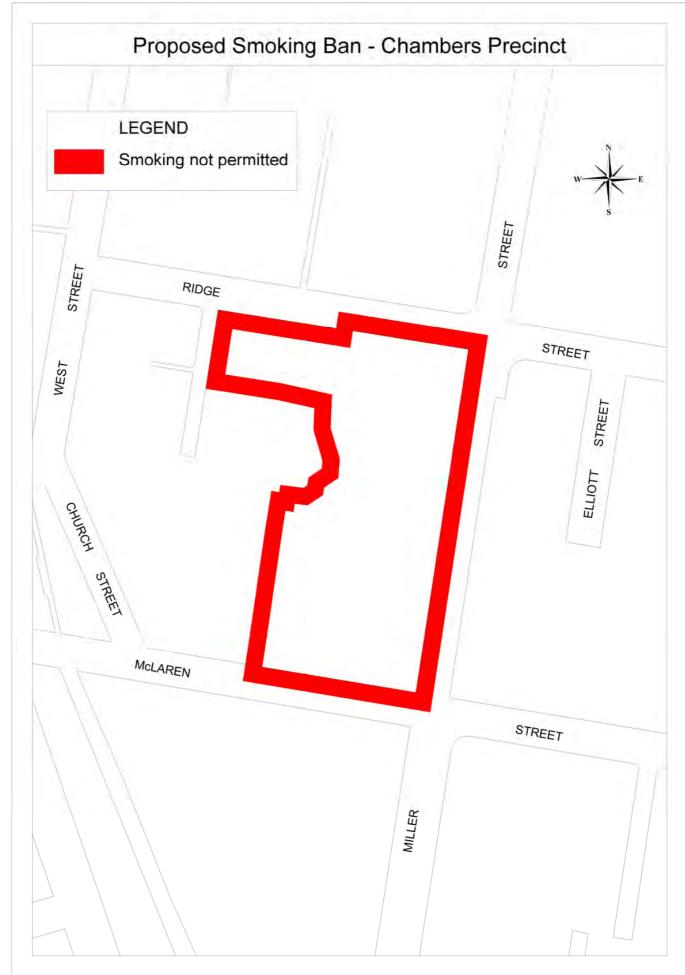
- 1. Install signage that is enforceable and visible within the ban areas.
- 2. Adopt an enforcement strategy of warning in the first instance and then infringing for all or any subsequent acts by an individual.
- 3. Reduce the size of the ban area to make it reflect those areas where smokers congregate to the annoyance of the non-smoking public. An area where a ban could better be managed is the area defined by Arthur Street, Berry Street to Pacific Highway, Pacific Highway to William Street, William Street to Blue Street then to Arthur Street (attachment 1).
- 4. Introduce a new area in the vicinity of the Council precinct as shown in attachment 2.
- 5. Include within the ban area small locations where smoking will be permitted that does not affect the non-smoking public and erect signage advising of the approval to smoke. Areas recommended are the south eastern corner of the intersection of Spring Street and Denison Street, Little Walker Street between Berry Street and Mount Street and Dorothy Fitton Park.
- 6. Install appropriate bins with ashtrays in each designated smoking area.

The "new" proposed areas are:

CBD – The blocks bounded by:

- Berry Street (from Miller to Walker Streets).
- Walker Street (from Berry to Pacific Highway).
- Mount Street (from Walker Street to William Street and including Brett Whitley Plaza and Elizabeth Plaza).
- Miller Street (Between Pacific Highway and Berry Street).

<u>COUNCIL CHAMBERS PRECINCT</u> – The block bounded by McLaren Street (north side) to Kelrose Lane through and including Ted Mack Civic Park to James Place (including Ridge Street Car Park) to Ridge Street then to Miller Street (west side) back down to McLaren Street.



3731st Council Meeting - 18 May 2020 Agenda

8.8. Bondi to Manly Walk - Draft Service Agreement

AUTHOR: James Marshall, Economic Development Coordinator

ENDORSED BY: Marcelo Occhiuzzi, Acting Director City Strategy

ATTACHMENTS:

1. Draft Service Agreement [8.8.1 - 6 pages]

PURPOSE:

To seek approval for the Service Agreement between Council and the Bondi to Manly Supporters and for the annual fee of \$5,000 plus GST.

EXECUTIVE SUMMARY:

The Bondi to Manly Walk is an 80km walk linking all the existing coastal and harbour-side walking tracks around Sydney Harbour, between Bondi Beach and Manly Beach.

The Walk was developed in partnership by the Bondi to Manly Supporters (the Supporters) and the Bondi to Manly Steering Committee (the Steering Committee). Council joined the Steering Committee following the resolution at the Council meeting held 25 June 2018 (Min. No. 224):

1. THAT Council supports the proposal of the Bondi to Manly Harbourside Walk and enters into the Memorandum of Understanding with this community group.

The way finding signage installed throughout the North Sydney local government area (LGA) section of the Walk was funded from Council's annual open space capital signage budget.

The Walk's official opening took place on 8 December 2019 at Milsons Point.

Council staff continue to participate on the Steering Committee which is focused on increasing promotion of the Walk. To support the Walk's ongoing administrative and promotional requirements, the Supporters are currently seeking stakeholder feedback on a Draft Service Agreement (Attachment 1) inclusive of an annual fee of \$5,000 plus GST per Steering Committee stakeholder for the financial years 2019/20, 2020/21 and 2021/22.

It is recommended that Council continue to financially support the Walk, which is a key initiative under the *North Sydney Visitor Economy Strategy and Action Plan* objective: Creating vibrant communities and world-class visitor experiences.

FINANCIAL IMPLICATIONS:

This request has not been budgeted for. Funding is, however, available from the 2019/20 non-recurrent economic development budget which is currently under spent. Future commitments cannot be made at the present time, and it would be prudent to determine the value gained from this contribution prior to making such future commitments.

RECOMMENDATION:

1. THAT Council supports the Service Agreement between Council and the Bondi to Manly Supporters, inclusive of the \$5,000 plus GST annual fee for 2019/20 and that future contributions be informed by data such as trends in visitor numbers and visitation to determine value for investment.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 3. Our Future Planning
- 3.1 Prosperous and vibrant economy

BACKGROUND

The Bondi to Manly Walk (the Walk) is an 80km walk linking all the existing coastal and harbour-side walking tracks around Sydney Harbour, between Bondi Beach and Manly Beach. The Walk is delineated by way finding signage, which display the distinctive black and yellow Aboriginal Whale symbol. Walkers can plan their trip by visiting the Bondi to Manly Walk website: www.bonditomanly.com and downloading the free app.

North Sydney marks the halfway point on the Walk. Local highlights along the North Sydney section include Luna Park, the North Sydney Olympic Pool, Wendy Whiteley's Garden, Nutcote Museum and MacCallum Pool.

The Walk was developed in partnership by the Bondi to Manly Supporters (the Supporters) and the Bondi to Manly Steering Committee (the Steering Committee). The Supporters is a not-for-profit incorporated association, and the Steering Committee comprises the nine (9) major landholders along the Bondi to Manly Walk - six councils; plus one Federal and two State Government agencies.

Council joined the Steering Committee following resolution at the Council meeting held 25 June 2018 (Min. No. 224) as follows.

1. THAT Council supports the proposal of the Bondi to Manly Harbourside Walk and enters into the Memorandum of Understanding with this community group.

CONSULTATION REQUIREMENTS

Community engagement is not required.

DETAIL

Council's responsibilities under the non-legally binding Memorandum of Understanding (MOU) include:

- final determination on track;
- final determination of the location of each individual way mark in each individual Council/landholder area;
- installation of discrete way finding signage;
- ongoing maintenance of the way finding signage; and
- ongoing maintenance of the walking tracks, and associated services.

The way finding signage throughout the North Sydney LGA section of the walk was installed by Council's Parks and Reserves Department, with funding from Council's open space capital signage budget. The Walk's official opening took place on 8 December 2019 at Milsons Point.

Now that the signage has been installed and the Walk is officially operating, Council's Economic Development Coordinator has replaced Council's Director Open Space and Environmental Services as its representative on the Steering Committee. This reflects the linkage between the Walk and the *North Sydney Visitor Economy Strategy and Action Plan*, which identifies the long-term vision and actions required to develop North Sydney's visitor economy. In the Strategy, the Walk is one of the key initiatives under the objective: "Creating vibrant communities and world-class visitor experiences". The Walk adds to the visitor appeal of the North Sydney LGA to target visitor markets including visiting friends and relatives, business visitors and other high spending groups. The economic benefits will be maximised by providing information about local attractions and other places to visit.

In March 2020, the Supporters requested feedback from the Steering Committee on a Draft Service Agreement (Attachment 1), outlining the secretarial and administrative services to be provided by the Supporters to the Steering Committee for the financial years 2019/20, 2020/21 and 2021/22 in exchange for an annual fee of \$5,000 plus GST per stakeholder.

The services provided the Supports to the Steering Committee includes:

- quarterly progress reports on physical/digital track use including raw walking data, and data analysis from the Bondi to Manly App. The data will help to inform Council's marketing and visitor servicing work under the *Visitor Economy Strategy*;
- strategies to drive awareness, use and promotion of the Walk including working with Destination NSW to drive promotion of the Walk through their channels and partnerships, registering and updating the Walk's promotion across state and national tourism bodies, media/social media presence through news, events partners as well the many cultural, historic, heritage and indigenous sites along the Walk and working with local community groups promoting local businesses and experiences.
- walker support including downloadable maps, responding to general enquiries, for example about the status of particular walking tracks, where to stay, bespoke itineraries, public transport and amenities;
- increasing community awareness of the historical, cultural, heritage, health and environmental attributes of the Walk via the app, website, events and continued public advocacy. Including hosting 'trial walks' to encourage community engagement and organisation/promotion of a large scale event along the Walk to drive use, awareness and enjoyment of the Walk;
- protecting the intellectual property of the Walk; and
- evaluation, to be undertaken as a consultative process between the Supporters and the Steering Committee.

Whilst it is recognised that the current Covid19 situation is placing Council under increased financial pressure, the contribution sought is relatively modest and can be managed within existing budgets for 2019/20. This is particularly so given the strategic recognition of the Walk in Council's recently adopted Visitor Economy Strategy. Council should, however, determine

whether it makes future financial contributions based on visitor numbers and other tangible benefits, determined through the 2020/21 financial year.				

SERVICE AGREEMENT

Between

Bondi to Manly Walk Steering Committee member

And

Bondi to Manly Walk Supporters

Next Review Date:

June 2023



Bondi to Manly Walk Supporters ABN: 42 913 243 569

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- 1. Glossary of Terms
- 2. Purpose of agreement
- 3. Description of Services
- 4. Service Fees
- 5. Evaluation
- 6. Liability
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1. Glossary of Terms

Agreement: A document that sets out the rights and obligations of service clients and service providers. The agreement may cover a variety of issues relating to service provision, including services, fees and evaluation of the service.

Bondi to Manly Walk Supporters: Bondi to Manly Walk Supporters is a not-for-profit incorporated association established with the mission of protecting, enhancing and promoting the public land and natural environment on Sydney Harbour Foreshore along the Bondi to Manly Walk, including by;

- managing and enhancing the Bondi to Manly Walk with Partners
- improving community awareness of the following attributes of the Walk
 - Environmental
 - Cultural
 - o Indigenous
 - Heritage
 - o health
- encouraging the use and enjoyment of the Walk by the community.

Bondi to Manly Walk ('The Walk'): The Bondi to Manly Walk is an 80km walk linking all the existing coastal and harbour-side walking tracks between Bondi Beach and Manly Beach around Sydney Harbour.

Bondi to Manly Walk Steering Committee ('The Steering Committee'): Comprises the 9 major landholders along the Bondi to Manly Walk; 6 Councils (Wayerlay, Washara, City of Sydney, North Sydney, Magnan and Northern

(Waverley, Woollahra, City of Sydney, North Sydney, Mosman and Northern Beaches), 2 State agencies (Property Management NSW and NSW National Parks and Wildlife Services) and one Federal Agency (Harbour Federation Trust) as well as The Greater Sydney Commission, Destination NSW and Bondi to Manly Walk Supporters.

Bondi to Manly Walk Steering Committee landholders: Comprises the 9 major landholders along the Bondi to Manly Walk; 6 Councils (Waverley, Woollahra, City of Sydney, North Sydney, Mosman and Northern Beaches), 2 State agencies (Property Management NSW and NSW National Parks and Wildlife Services) and one Federal Agency (Harbour Federation Trust).

Digital Infrastructure: The digital support provided to walkers, through the Bondi to Manly Walk website and apps, which is managed by Bondi to Manly Walk Supporters.

Evaluation: The process used to describe the process of measuring the value or worth of a program or service.

1. Purpose of Agreement

The purpose of this agreement is to document an arrangement between members of the Bondi to Manly Walk Steering Committee and Bondi to Manly Walk Supporters. Bondi to Manly Walk Supporters agrees to provide the secretarial and administrative services outlined in this Service Agreement to the Bondi to Manly Walk Steering Committee. Any changes to the services and/or support listed in this agreement will require prior authorisation from all parties.

2. Description of Services

Bondi to Manly Walk Supporters will continue to provide secretarial and administrative support to the Steering Committee as required, through;

- Chairing the Steering Committee
- Facilitating and recording the consensus based decision making process
- Sourcing and negotiating collective procurement needs of Steering Committee partners, and reporting on these procurement projects
- Providing regular reports and recommendations on signage, track updates, funding, IP and digital infrastructure to all Steering Committee members
- Delivering progress reports on physical and digital track use
- Providing raw walking data, and data analysis services, to Steering Committee members based on usage data collected from the Bondi to Manly App.
- Devising and implementing strategies to drive awareness, use and promotion of the Walk to achieve economic and social benefits for the people of Sydney and more broadly Australia
- Maintaining licenses protecting the Intellectual Property regarding the Bondi to Manly Walk
- Maintaining licenses protecting the Bondi to Manly Walk Aboriginal Whale Symbol and corresponding Bondi to Manly Walk brand
- Maintaining digital licenses for the delivery of information and services through the Bondi to Manly Walk digital infrastructure
- Providing walker support to assist with the planning, preparation, needs and suggestions of walkers
- Advocating and supporting improvements to the Walk, supported by landholders along the Walk.
- Management and continued improvement of the digital assets pertaining to the use of the Walk including any relevant changes or updates to the route, utilities and services
- Audit of digital infrastructure to provide improved historical, cultural and heritage related information as well as enhanced identification of points of interest and utilities along the Bondi to Manly Walk
- Increasing community awareness of the historical, cultural, heritage, health
 and environmental attributes of the Walk through the Bondi to Manly Walk
 app, website, events and continued public advocacy

- Provision of resources including printable maps, GPX data and extensive track notes - for walkers with limited technical support
- Continuous enhancement of digital platforms to improve the user experience.

Bondi to Manly Walk Supporters will work with partners to achieve the following:

- Improved signage and accessibility
- Increased digital support for walkers
- Identification of significant potential track upgrades and route extensions
- Promotion of the Walk locally, nationally and internationally to encourage use and enjoyment of the Walk.

Bondi to Manly Walk Supporters will endeavour to include marketing and safety alerts provided by Steering Committee members on the Bondi to Manly Walk digital infrastructure.

3. Liability

Nothing in this Service Agreement changes the underlying liabilities held by both parties to the agreement. Bondi to Manly Walk Supporters will maintain responsibility for the digital infrastructure supporting the Bondi to Manly Walk and any liabilities associated with this digital infrastructure. Bondi to Manly Walk Supporters does not take any liability for any use of those public lands which may be on, or situated near, the Bondi to Manly Walk.

4. Service Fees

An annual invoice will be delivered to all Steering Committee members for \$5000 (+GST) per annum for three years, 2020, 2021, and 2022. If preferred, Steering Committee members can make one lump sum payment. Payment can be made directly to Bondi to Manly Walk Supporters:

Commonwealth Bank of Australia

BSB: 06 2220 Account: 1026 0264

5. Evaluation

To monitor the quality of the outcomes relevant to the aim of this service agreement, the Steering Committee representatives and Bondi to Manly Walk Supporters will participate in a quality assurance process as requested. If requested, each party will independently complete a quality/evaluation. A consultative meeting will then be held to discuss any issues arising or changes that might be requested.

6. Signatures

with Bondi to Manly	y Walk Supporters:	ber confirms the servic	e agreemer
I, Agreement.	understand, as a , accept and agree to the	representative for information outlined in	this Service
Signed:	·	Date:	
Title:			
Agreement accepte	ed and signed on behalf of E	ondi to Manly Walk Su	pporters
Name:			
Signed:		Date:	
Title:		_	

8.9. NYE2019 and Options for NYE2020

AUTHOR: Sandra Moore, Manager Communications and Events

ENDORSED BY: Margaret Palmer, Director Corporate Services

ATTACHMENTS:

1. Letter from NSW Police - Ticketing New Year's Eve 2020/21 [8.9.1 - 1 page]

PURPOSE:

The purpose of this report is to provide an overview of NYE19 and recommend the continuation of ticketing for NYE20 if the event goes ahead.

EXECUTIVE SUMMARY:

At its meeting of 24 June 2019, Council resolved to ticket Bradfield Park, Mary Booth Reserve, Blues Point Reserve and Quibaree Park at a cost of \$10 (plus booking fee) for NYE. The aim was to manage access to the reserves and reduce the size and impact of overflow crowds in surrounding areas.

Overall, NYE 2019 was very successful. All four ticketed reserves were sold out and there were no major incidents. Crowd numbers in all precincts, including south of the harbour, were reduced as a result of the extreme heat, high winds and concern about the bush fire crisis. There were high levels of ticketholder satisfaction with all reserves except Bradfield Park, where changes are needed to ensure customer safety and the ongoing success of ticketing. This report provides an overview of all vantage points in the North Sydney LGA and summarises feedback received from residents and ticketholders in each area.

While events are currently not going ahead during the COVID-19 pandemic, the planning and contracts for NYE operate on long cycles and Council needs to plan for NYE20, in coordination with external agencies, with the flexibility to alter or hibernate plans if required. This report therefore recommends a continuation of ticketing at all four sites, with staff to review how Bradfield Park is managed.

FINANCIAL IMPLICATIONS:

NYE has been included in the Delivery Program and will be funded in the 2020/21 budget with an allocation of \$740,500. The allocated budget for this year incorporates the ongoing increased costs related to crowd control and an increase for inflation, as well as staff salaries that had previously been budgeted elsewhere. It is anticipated that costs will be offset by income from ticket sales and stallholder fees of approximately \$340,000.

Council originally allocated a budget of \$406,246 for NYE19. The total cost for NYE19 was \$564,714. The additional expenses above the budgeted amount were for the additional toilets, power and lighting, security, traffic, fencing and event staff required for the ticketed sites.

Total income for NYE19 was \$355,863, comprising primarily of \$338,140 from ticketing, as well as revenue from a temporary telecommunications land lease and food stalls. It should be noted that the income for the land lease is unlikely to continue in the future if telecommunications companies move to cellular network technology.

While ticketing increased the cost to Council of providing the event, the additional revenue received reduced the overall final cost to Council from the budgeted amount of \$406,246 to \$208,851.

RECOMMENDATION:

- **1. THAT** Council endorse ticketing for Blues Point Reserve, Quibaree Park, Bradfield Park and Mary Booth Reserve for NYE 2020.
- **2. THAT** a further report be provided outlining any significant changes required to NYE arrangements as a result of the COVID-19 pandemic.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 4. Our Social Vitality
- 4.2 North Sydney is creative and home to popular events

BACKGROUND

The City of Sydney funds a NYE fireworks display which is operationally supported by multiple state and local government agencies. Up to one million people view the fireworks live from the Sydney Harbour foreshore and an estimated one billion watch them on television and online. The City of Sydney research undertaken at NYE 2019 indicates that 25% of people who attend the fireworks on the foreshore are international tourists. The fireworks contribute to Sydney's positioning as a global city.

Council's NYE responsibilities have grown in line with the event's popularity and status. Historically, Council's primary focus has been to manage the event to reduce its impact on residents. However, in recent years, the devolution of responsibility for crowd safety to landowners has placed greater pressure on Council to implement safety strategies.

In 2019, Council actively managed four reserves as ticketed, alcohol prohibited venues: Blues Point Reserve, Bradfield Park, Mary Booth Reserve and Quibaree Park. In addition, Council also provided toilets, bins and lighting towers at Cremorne Point, Kurraba Point, Waverton Park, Balls Head Reserve and St Peters Park. Additional toilets were provided at St Leonards Park and on Burton and Fitzroy Streets.

Food stalls/trucks were arranged for Bradfield Park, Blues Point Reserve, Quibaree Park, Cremorne Point and Waverton Park. Council also provided digital message boards and barriers for crowd and traffic management across the foreshore area. In 2019, Council was asked to provide a hostile vehicle mitigation plan and source heavy vehicles for its implementation. This was not budgeted for or supported for 2019, but NSW Police verbally reiterated the request that this be included in planning for NYE20 at the NYE19 debrief.

Transfer of responsibility and costs associated with New Year's Eve from State Agencies to Council continues to be a point of contention each year. Council has continued to maintain that higher order policing and counter-terrorism measures are a matter for policing agencies and not a matter on which local government has expertise.

At its meeting of 28 May 2018, Council resolved (Min. No. 141):

- 1. THAT Council endorse ticketing at Blues Point Reserve for 2018 at a cost set to recover the actual costs of conducting the event on the site.
- 2. THAT Council support the closure of Balls Head Reserve on NYE for 2018.
- 3. THAT a further report be provided to Council outlining options for cost recovery and revenue generating opportunities, together with a community engagement strategy.

At its meeting of 30 July 2018, Council resolved (Min. No. 255):

1. THAT Council endorse the NYE Community Engagement Strategy.

At its meeting of 24 September 2018, Council resolved (Min. No. 347):

- 1. THAT Council support partial cost recovery for the trial of ticketing at Blues Point Reserve for 2018 at the cost of \$40 for adults, \$20 for children (plus booking fee) and infants 0 to 2 free.
- 2. THAT the review of the ticketing trial at Blues Point Reserve include a review of the cost models and feedback from the community consultation.

At its meeting of 29 October 2018, Council resolved (Min. No. 381):

- 1. THAT Council open Balls Head Reserve to the public this New Year's Eve.
- 2. THAT the post NYE report make specific reference to Balls Head Reserve.

At its meeting of 29 January 2019, Council resolved (Min. No. 7):

- 1. THAT Council acknowledges the difficulties encountered with paid ticketing in Blues Point Reserve on NYE 2018.
- 2. THAT Council's Events staff prepare a report to Council on the 2018 NYE Event and address alternative options of managing crowds at Blues Point Reserve for future NYE events.

At its meeting of 24 June 2019, Council resolved (Min. No. 176):

- 1. THAT Council resolves to ticket Bradfield Park, Mary Booth Reserve, Blues Point Reserve and Quibaree Park at a cost of \$10 (plus booking fee) in order to manage access to New Year's Eve 2019 at these sites.
- 2. THAT Council resolves to ticket the above parks and opportunity be given for residents to acquire up to 20% (6,800 tickets) prior to sale to the general public.
- 3. THAT the ticket system include pass-out wrist bands to enable attendees to access local businesses and tickets will be issued to specific individuals and not transferable to avoid resale/scalping.

This report provides a summary of NYE 2019 in the North Sydney LGA, including feedback from residents and ticketholders and discusses options for NYE 2020, including crowd management options for Bradfield Park.

CONSULTATION REQUIREMENTS

Community engagement will be undertaken in accordance with Council's Community Engagement Protocol.

DETAIL

This report is divided into three sections. They are:

- Overview of NYE19 vantage points, including community and ticketholder feedback for each area;
- Issues to be addressed for NYE20; and
- Options for NYE20.

1. Overview of NYE Vantage Points 2019

Overall, NYE was a successful event across the North Sydney LGA vantage points. Tickets at all four ticketed reserves were sold out and the overflow crowds outside these areas were smaller than usual in most areas. This was due to a combination of factors including the high temperatures during the day and the cool southerly change in the evening. Another significant factor was the bush fire crisis which resulted in calls for the fireworks to be cancelled and less community interest in celebrating NYE than usual.

There were no major incidences in the LGA and with transport running smoothly, the reserves and streets around North Sydney and Milsons Point station were cleared before 1.30am.

Generally, a third of tickets were scanned in by 4pm, two-thirds by 8.30pm and the remainder before midnight. This pattern was consistent across all four parks/reserves.

Community Feedback

Overall, Council received 42 pieces of feedback relating to New Year's Eve, with 16 people writing directly to Council and 26 people providing comments as part of the Have Your Say NYE19 survey. This feedback comprised both positive and negative comments.

While several issues were raised, there were two common themes:

- 1) the impact of ticketing, particularly the allocation of tickets and the changed nature of the event from reduced crowd numbers;
- 2) the impact of managed access on residents.

A collated summary of the key issues is provided in the two tables below.

Table A: POSITIVE FEEDBACK

KEY ISSUE	SUMMARY
Ticketing – reduced crowd numbers	 Spectators, including those with young families, felt safe and had a more enjoyable event experience. Event sites and vantage points were far more comfortable than in
	 previous years. Public transport and accessibility to and from the event was easier. Less anti-social behaviour and litter.
Ticketing – impact on residents	 Ticketing and pass outs made it easier for residents to arrive later at night to event sites. Tickets give everyone fair access to the event.
	• Paid tickets allow for a reduced cost burden for Council and ratepayers.

Table B: NEGATIVE FEEDBACK

KEY ISSUE	SUMMARY
Managed access	Residents inconvenienced by managed access restrictions.
– impact on residents	Residents unable to have vehicle access in or out of their property.
	Managed access infrastructure prohibits resident access to public
	land.

Ticketing – impact on residents	 Ticket requirement and limited accessibility to vantage points increased crowd numbers in public spaces outside of managed access areas, threatening the safety and security of nearby residents. Residents should not have to pay for tickets as they already pay expensive rates and should be compensated for the inconveniences of managed access.
Interaction with Police, security, event staff	 Unsatisfactory interactions with security/staff/Police who were enforcing the managed access areas, road closures and/or event sites. Positioning of security within the managed access area.
Ticketing – impact on attendance and atmosphere	 Ticketing and decreased crowd numbers reduces the atmosphere of the event. Ticketing meant visitors who were not aware of the requirement missed out on being able to attend. Tickets should be available to purchase on the day. Reduced crowd numbers impact local businesses.

Ticketholder Survey

On Council's behalf, our ticket provider Ticketbooth sent an email inviting ticketholders to complete a short survey. The email was sent to everyone who had purchased one or more tickets and had opted in to receive future emails from Ticketbooth. The response rate (rounded) for the survey was: 11% of 3551 survey recipients from Bradfield Park; 27% of 524 survey recipients from Mary Booth Reserve; 17% of 1492 survey recipients from Blues Point Reserve; and 20% of 128 survey recipients from Quibaree Park.

A dropout rate is expected for all ticketed events but is always higher for free or inexpensive tickets. Resident ticketholders had a much higher dropout rate (ranging from 34% to 65%), than general admission ticketholders (17% to 25%).

The primary reason given for ticketholders choosing to purchase a ticket at all reserves was that the ticketholder wanted a guaranteed place to view the fireworks. The second most common reason was that the park/reserve was a great vantage point, except for Quibaree Park where the second most common reason was that the reserve was close to home. A summary of the key findings is in Table 1 below.

Table 1: NYE TICKETHOLDERS

Satisfaction levels & margins of error	Blues Point Reserve	Quibaree Park	Mary Booth Reserve	Bradfield Park
margins of error	Nesei ve	Talk	Reserve	1 al K
% satisfied or very	92	92	93	71
satisfied				
% rate value above	84	96	91	66
average or excellent				
% likely or very likely	83	95	90	60
to attend again				
% prefer to pay more	24	19	16	32
for entertainment				
Margin of error*	+/- 6%	+/- 19%	+/- 8%	+/- 5%
*The margin of error for Quibaree Park reflects the smaller sample size.				

There were high levels of satisfaction with all reserves except Bradfield Park which had a lower

proportion of satisfied ticketholders and a lower proportion who considered it value for money. Importantly, only three in five ticketholders would purchase a ticket to Bradfield Park again.

The likely causes for this are discussed under the separate Bradfield Park section, however, it should be noted that unless changes are made to the arrangements at Bradfield Park, tickets are unlikely to sell out in future years, resulting in increased costs to Council for NYE.

Blues Point Reserve

This was the second year that Blues Point Reserve was ticketed. Tickets sold out on 21 November. In response to community feedback in early 2019, the reserve was not closed until 6pm on 30 December, re-opening to ticketholders at 8am on 31 December.

Table 2: TICKETS SOLD AND USED BLUES POINT RESERVE

Total tickets sold	8,000
resident tickets	10%
general admission tickets	90%
% total tickets scanned	73%
% resident tickets scanned	55%
% general admission tickets scanned	76%

There were about 200 people queueing in an orderly fashion on the footpath on the eastern side of Blues Point Road near French Street prior to 8am. This was fewer than in 2019, when the opening time of the reserve was 12pm.

As shown in Table 2, Blues Point Reserve ticketholders were satisfied with the event, considered it good value for money and are likely to purchase tickets for the site again. Fewer ticketholders took the opportunity to leave and return to the reserve than in other areas; the 1km walk uphill to the shops and the hot weather probably created a disincentive. There were no issues with alcohol and the reserve had cleared by 12.30am.

There was mixed feedback from McMahons Point residents, with residents close to the reserve saying the closure arrangements worked well for them. However, there were two complaints from residents, one about being denied access to the reserve and one about vehicular access to Blues Point Road. Both residents also complained about security and/or staff.

Quibaree Park

Tickets for Quibaree Park sold out on 5 November. The park was not closed and cleared until 6am on 31 December, opening to ticketholders at 8am.

TABLE 3: TICKETS SOLD AND USED QUIBAREE PARK

Total tickets sold	1,000
resident tickets	20%
general admission tickets	80%
% total tickets scanned	71%
% resident tickets scanned	59%
% general admission tickets scanned	75%

Although only 20% of tickets sold were from the resident allocation, some residents may have bought from the general admission pool as a significant proportion of ticketholders passed out

after the 9pm fireworks and returned for the midnight show. The only feedback from residents related to access to Wilona Avenue; this matter has been raised with NSW Police for NYE20.

Mary Booth Reserve

Mary Booth Reserve was the first to sell out with all tickets sold by 14 November. The park was closed and cleared at 8pm on 30 December, reopening to ticketholders at 8am on 31 December.

Ticketholders were scanned, wrist banded, and their bags were checked at the entrance to Bradfield Park on Fitzroy Street. They had to show their wristband at the Mary Booth entrance which was relocated this year from the Jeffrey Street Wharf to Kirribilli Avenue.

TABLE 4: TICKETS SOLD AND USED MARY BOOTH RESERVE

Total tickets sold	3000
resident tickets	19%
general admission tickets	81%
% total tickets scanned	71%
% resident tickets scanned	66%
% general admission tickets scanned	76%

Moving the entry gate to Kirribilli Avenue added additional space to the ticketed site. With only 75% of ticketholders turning up, this meant the reserve was significantly underutilised. It would be appropriate to increase the number of tickets for Mary Booth Reserve for NYE20. However, changes would need to be made to Bradfield Park as some ticketholders who arrived towards midnight were not able to make their way through the crowds on Broughton Street to reach the entry gate to Mary Booth Reserve.

Bradfield Park

Bradfield Park did not sell out until 11pm on 30 December. The park was closed and cleared at 8pm on 30 December, reopening to ticketholders at 8am on 31 December. This was a more difficult task than expected, partly due to the delay in the installation of fencing by Council's contractor and partly due to the large number of people in the park including tourists, fishers and family groups having picnics.

TABLE 5: TICKETS SOLD AND USED BRADFIELD PARK

Total tickets sold	22,000
resident tickets	1%
general admission tickets	99%
% total tickets scanned	82%
% resident tickets scanned	39%
% general admission tickets scanned	83%

There were a number of concerns with Bradfield Park that need to be addressed for NYE20.

Some of these were:

- 1. Entry with restricted items although this information was provided with the tickets, customers did not understand what could and couldn't be brought into the reserve, leading to issues with alcohol, unidentified drinks, picnic knives, large chairs, etc. While this was an issue in all reserves, the larger number of ticketholders at Bradfield Park resulted in significantly more arguments and aggravation at the entry gate than in other areas.
- 2. Alcohol because ticketholders were allowed to arrive at any time and pass in and out of the reserve, there were many more people in the reserve who were affected by alcohol than in previous years. While a few inebriated people were refused admission, the presence of alcohol adds an additional risk in managing crowd behaviour.
- 3. Crowd flow because there were no restrictions on the time people could arrive, up to half of the crowd arrived after it was dark. This meant they were unable to see the whole park or where there were places to sit. This resulted in over-crowding on the upper right-hand (Luna Park) side of the bridge before the 9pm fireworks and overcrowding on the left-hand side before the midnight fireworks. There was enough space within the park to accommodate the crowds comfortably, however, to manage the crowd safely the park will either need to be broken up into sections or ticketholders will need to arrive before dark and not be allowed to pass out.

Cremorne Point

There were no major concerns in Cremorne Point Reserve this year. Council does not undertake crowd counting at Cremorne Point, however, residents advised Council staff that numbers were down on previous years. This is consistent with other areas. Police have informally expressed concern about the widespread consumption of alcohol in the reserve.

Waverton

Staff made the decision to close Balls Head Reserve on Monday 30 December as a severe fire rating was released for NYE. National Parks closed Bradleys Head in the Mosman LGA and cancelled the events scheduled there. Signage was installed at Waverton Station on 30 December and fencing and signage was installed to close the reserve at 6am on 31 December. There were fewer people in Waverton than in previous years.

St Leonards Park

Council staff checked St Leonards Park at 5.30pm. There were only a few groups in the park at that time, well below the usual numbers. There were no reports of crowd behaviour received by Council.

2. Key Issues to be Addressed

Overflow Crowds

While there is an expectation that ticketing messages will ultimately reduce overflow crowds, there are still many streets near the foreshore where people can view the fireworks. There were a number of crowded overflow areas, in particular around the corner of Kirribilli Avenue and Jeffrey Street, which had an adverse impact on residents. Council staff will work with NSW Police to determine how crowds can be better managed in these areas.

Loss of Vibrancy

There were a number of complaints about the loss of vibrancy during the day, however, the benefits of ticketing outweigh this concern. At Milsons Point there were specific concerns about fencing in Bradfield Park north. The complaints included both the early installation of the fencing and the impact of the black scrim used.

The fencing was erected by Sydney Trains to manage the flow of people into the station at egress. It is difficult to manage the timing of the installation as suppliers provide infrastructure to multiple sites on NYE and need to juggle delivery schedules. They also try to avoid deliveries on weekends because it can be difficult to get staff during the holiday season and it increases the price to customers. Council staff will work with Sydney Trains to discuss reducing the extent of the fencing and delaying the installation of the scrim.

Reduced Business

The impact on businesses is mixed. Businesses in the McMahons Point village claim that ticketing has significantly reduced their NYE trade. While more tickets will be sold when the Metro returns part of Blues Point Reserve to Council, this will not necessarily resolve this issue. Ticketing allows people to arrive later and in Blues Point Reserve around 40% of ticketholders arrived after the dinner period. Additionally, there are no longer overflow crowds waiting on Blues Point Road for the opportunity to enter the reserve after the 9pm fireworks. Providing VIP ticketing options for businesses may ameliorate the impact. This is discussed below under the options section.

There were no complaints from businesses in the Kirribilli village which benefited from the late entry time and pass outs at Bradfield Park. Many people bought food in the village or exited and re-entered the park between the 9pm and midnight fireworks to buy additional food and drinks.

Impact of Alcohol

The policy of entry at any time and allowing pass outs and re-entry has increased the number of people in the reserve who have consumed alcohol. Research shows that alcohol can reduce affiliative behaviour, which in turn increases risks for managing crowd safety, especially if an incident occurs. Council staff will work with NSW Police and security to ensure that the alcohol-free message is clear in all messaging and that intoxicated people are not able to enter the reserves.

Bradfield Park

The number of tickets sold for Bradfield Park was appropriate, however, there was over-crowding in some areas because people could not easily see the areas where there was more space. Ticketholders arriving for the midnight fireworks gravitated towards the left-hand side of the bridge, causing unsafe levels of crowding in that area. Ticketholders who had sat waiting for a good vantage point were adversely affected by latecomers and/or Police and security moving them on to create walkways where none had previously existed. If entry up to midnight is maintained, this will need to be resolved by dividing the park into sections. This is discussed further under the Options section below.

Access to and within Mary Booth Reserve

Some Mary Booth Reserve ticketholders were unable to return to the reserve before midnight as they were unable to make their way through the dense crowd gathered on Broughton Street. Ticketholders were also more likely to leave the reserve to use the toilets in Bradfield Park

than to make their way to the upper reserve near Waruda Street where the toilets were located. Council staff will work with NSW Police to review access to the reserve and better promotion of the toilets provided within the reserve.

Drop-off Zones

Many people heading to Bradfield Park or the restaurants in Kirribilli chose to arrive by taxi or Uber. This created a traffic jam at the roundabout on the corner of Broughton and McDougall Streets where the road was closed, resulting in residents being unable to reach their properties and traffic jams along Clark Road. Council staff will explore options with Police for creating and advertising a second drop-off zone in the Kirribilli area.

3. Options for NYE20

Most of the issues listed above will be addressed by negotiation with other agencies and/or small changes in the way things are managed. In addition to the changes proposed below, Council will need to be flexible in the plans that are developed.

As a result of COVID-19, all events are currently cancelled, although the number of virtual events is growing. All agencies, including the City of Sydney which hosts the event, have long planning and contract cycles for NYE and planning is therefore proceeding on a business as usual basis until Council is advised otherwise. Current medical advice indicates that physical distancing will be required for some time to come so it is possible that even if people can view the fireworks from the foreshore, the number of people who can attend will be significantly reduced. Planning for NYE20 will need to be undertaken with flexibility to alter or hibernate the plans if required.

There are two changes that will make a difference to the community perception of how NYE is managed and they are discussed here for Council's consideration.

Business Ticketing Packages

One of the options for encouraging business in the McMahons Point village would be to offer a ticket package to restaurants. The restaurants could include access to the reserve for their guests as part of a NYE offer to customers.

If this is supported, it is proposed that it would be offered on a trial basis to up to two food service venues. The arrangement would not be available to hotels or venues that primarily served alcohol. This arrangement would not alter the number of tickets sold but would result in an area of Blues Point Reserve being cordoned off for restaurant guests.

Sectioning Bradfield Park

Ticketholders were not satisfied with Bradfield Park and there will need to be some change to the way Bradfield Park is managed in the future. To reduce over-crowding in Bradfield Park, Council needs to section off areas of the park. This can be done by creating temporary closures to direct crowds to less populated areas or by selling tickets to a particular section of the park. Mary Booth Reserve is already sectioned off from Bradfield Park so the additional sectioning would be an extension of the existing arrangements. Under this arrangement, ticketholders would buy tickets to a section of Bradfield Park and be issued with wristbands for that area.

RECOMMENDATIONS:

It is therefore recommended:

- **1. THAT** Council endorse ticketing for Blues Point Reserve, Quibaree Park, Bradfield Park and Mary Booth Reserve for NYE 2020.
- **2. THAT** a further report be provided outlining any significant changes required to NYE arrangements as a result of the COVID-19 pandemic.

FOR OFFICIAL USE ONLY

NSW Police Force

Ms Joanna Gibson **Team Leader Events** North Sydney Council 200 Miller Street North Sydney NSW 2060

D/2020/479229

Dear Joanna

RE: Ticketing - New Years Eve 2020/2021.

The North Shore Police Area Command will again strongly support and advocate for North Sydney Council undertaking ticketing of New Years Eve 2020/21celebrations for the following venues:

- Blues Point Reserve
- Ouibaree Park
- Bradfield Park and
- Mary Booth Reserve

The success of the 2019 New Years Eve response around safety and the community is largely due to the ticketing of the nominated areas.

This agreement is issued providing that COVID19 restrictions allow for the proposed events to proceed.

Yours sincerely

Simon Jones A/Superintendent North Shore Police Area Command 14 May 2020

NORTH SHORE POLICE AREA COMMAND

Chatswood Police Station

63 Archer Street, Chatswood NSW 2067

Telephone 02 9414 8499 ENet 68499 Fax: 02 9414 8594 EFax 68594 TTY 9211 3776 (Hearing/Speech impaired)

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8.10. North Sydney Council Seniors Festival 2020

AUTHOR: Camelia Tobia, Access & Inclusion Co-ordinator

ENDORSED BY: Rebecca Aukim, Acting Director Community and Library Services

ATTACHMENTS:

1. North Sydney Seniors Festival 2020 [8.10.1 - 5 pages]

PURPOSE:

This report provides a summary of the North Sydney Seniors Festival 2020.

EXECUTIVE SUMMARY:

The Seniors Festival is an annual event, supported by the State Government, to celebrate the lives of older people. North Sydney Council has delivered the Seniors Festival for many years and in 2020 coordinated thirty events over the ten-day Festival period for seniors to enjoy. The Festival officially launched with an awards ceremony hosted by Felicity Wilson MP and over the Festival period approximately 813 older people attended one or more of the scheduled events and activities.

FINANCIAL IMPLICATIONS:

There is an allocated budget of \$4,000 to deliver the Seniors Festival annually.

RECOMMENDATION:

1. THAT the North Sydney Seniors Festival 2020 report be received.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 4. Our Social Vitality
- 4.1 North Sydney is connected, inclusive, healthy and safe
- 4.2 North Sydney is creative and home to popular events
- 4.3 North Sydney supports lifelong learning

BACKGROUND

North Sydney Council has been celebrating the Seniors Festival for many years, in both coordinating and delivering programs, events and activities. To create a Festival program that is both innovative and appealing and also reflects community needs and interests. Council partners with local services and community centres in the Festival design. Partners receive financial support from Council in the form of small grants to deliver Festival activities.

CONSULTATION REQUIREMENTS

Community engagement is not required, however a pre-Festival survey was created to influence the content of the Festival. Surveys were distributed via Council's community engagement platform. Hard copies were also distributed to all community centres in North Sydney. In total we received 38 responses to the survey.

DETAIL

Council's Community Development team coordinate the Festival with support by other departments within Council; Stanton Library, North Sydney Olympic Pool, Sustainability, Road Safety and the Coal Loader Centre for Sustainability, as well as community partners including the Crows Nest Centre, North Sydney Community Centre, The Kirribilli Centre, Neutral Bay Community Centre, the Seaborn, Broughton and Walford Foundation, CCNB and North Sydney Men's Shed. For the first time this year Community Connect Transport provided transport services for participants who would otherwise not be able to access the Festival.

The development of the Festival program is designed to encourage ongoing participation and engagement beyond the Seniors Festival and is identified as an opportunity to address social isolation amongst our older residents in the community. The Festival was designed to provide a balance of activities and programs that promoted learning and growth, health and wellbeing, as well as activities that were designed to provide entertainment and social connection.

Whilst the Seniors Festival is designed to attract older people living in the North Sydney LGA it does not preclude older people who reside elsewhere. Anecdotal feedback from participants noted the attraction of the planned events organised by North Sydney Council as the reason why they chose to travel to North Sydney to attend one or more of the Festival activities. In total, 30% of the total participants who attended the North Sydney Seniors Festival did not live in the LGA, some travelling from neighbouring suburbs but others travelling from further away, including Ryde, Bayside and Ku-ring-gai LGAs.

North Sydney Seniors Festival 2020



The Seniors Festival is an annual celebration supported by the State Government and coordinated by local Councils. North Sydney Council has been celebrating the Seniors Festival for many years and this year Council held 30 events over the 10-day festival period (12 - 23 February 2020).

Council's Community Development team coordinate the Festival with support by other departments within Council and community partners. The development of the Festival program is designed to encourage ongoing participation and engagement beyond the Seniors Festival and is identified as an opportunity to address social isolation amongst our older residents in the community.

Seniors Festival Design

In developing a Seniors Festival program that reflected the interests of the community a prefestival survey was distributed to residents asking for their input and suggestions. This was one of the recommendations from the evaluation of the 2019 Seniors Festival. In total we received 38 responses to the survey. These responses were incorporated, where possible, in the Festival program.

Additionally, the program was designed to encapsulate a broad range of interests and appeal to the diverse interests and needs of the community. It also encouraged other areas of Council, as well as community groups and services to showcase programs and activities they regularly offer to the community throughout the year as a way of promoting ongoing engagement.

Seniors Festival Highlights

The Seniors Festival opened with an awards ceremony hosted by local member Felicity Wilson MP where North Sydney seniors were recognised for their valuable contribution to the community. Held at the Crows Nest Centre, Deputy Mayor Stephen Barbour was the Master of Ceremony on the morning and Acting Minister Sport, Multiculturalism, Seniors and Veterans, the Hon Dr Geoff Lee presented to the audience, thanking older people for their service and dedication to North Sydney. State Government organisations, including Services NSW, NSW Police and Legal Aid also attended and provided information and resources to participants.





Other highlights included performances by pianist John Martin and actor Grant Dodwell, most famous for his role in A Country Practice, hosted at the Seaborn, Broughton and Walford (SBW) Foundation in Neutral Bay.





For those more interested in North Sydney Council's history Stanton Library offered a Behind the Scenes tour of Council's archives whilst the Coal Loader provided a guided tour of the historical site.







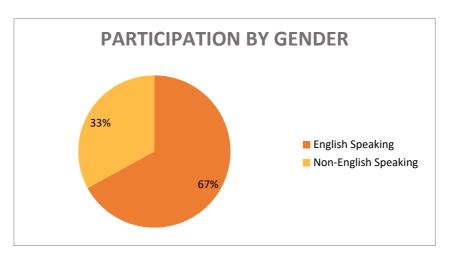


For the more active seniors in our community there were a number of different healthy ageing activities on offer. North Sydney Olympic Pool offered a free Pilates and aqua aerobics class, whilst the Crows Nest Centre provided tasters in yoga, Feldenkrais and Tai Chi. There were also events for older people interested in brushing up their IT skills. The library held a couple of drop in tech classes, offering one-on-one tech support, as well as a tech discovery program.

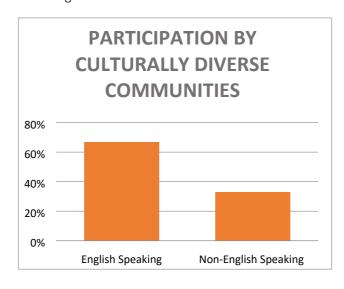
Seniors Festival Participation

It is estimated 813 people attended one or more of the 30 scheduled events over the Festival period. The Seniors Festival, whilst intended for North Sydney LGA residents, did not preclude older people from outside of the area attending. This year our Festival attracted seniors from neighbouring Councils, including Mosman, Lane Cove and Willoughby, as well as others who had travelled from Ryde, Bayside and Kuringai LGA's.

The Seniors Festival attracted both men and women. Whilst there is a significant gender disparity of participants it was pleasing to note there was an increase in participation rates for older males, increasing from 22% in 2019 to 33% this year. This had been highlighted in the 2019 North Sydney Seniors Festival evaluation and is therefore pleasing to note this was achieved.



North Sydney is a culturally diverse community and whilst the population rates of people who speak a language other than English is lower in North Sydney compared to Greater Sydney 1 it was positive to see the Seniors Festival attracted older people from multicultural backgrounds, and in fact increased by 11% from the previous year. This was in part due to the programming of events targeting culturally diverse community members, such as the Bollywood Music morning tea held at the Crows Nest Centre.



¹ https://profile.id.com.au/north-sydney/language





People who agreed to complete the survey were asked to indicate if they would like to be advised of future events offered by North Sydney Council, specifically those targeting older residents. Of the completed surveys 62% of respondents indicated they wished to receive further updates from Council.

Seniors Festival Partners

The success of the Festival would not have been possible without the support of other Departments within Council and community partners.

- Felicity Wilson MP
- Crows Nest Centre
- The Kirribilli Centre
- North Sydney Community Centre
- Neutral Bay Senior Citizens Centre
- Seaborn, Walford and Broughton Foundation
- CCNB
- North Sydney Men's Shed
- Community Connect Transport

Other Council Departments involved in the Festival included;

- North Sydney Olympic Pool
- Coal Loader Centre for Sustainability
- Sustainability
- Arts and Culture
- Road Safety

Summary and Conclusion

The North Sydney Seniors Festival is one of the largest Council-run events for older people. It is a Festival enjoyed by many with the success measured by the participation rates of the events, many of which reached or exceeded capacity. As we plan for next years Festival we will continue to build upon our existing model of delivering the Festival, whilst also

considering new and innovative ways of engaging our older people that reflects their needs and interests.

8.11. Draft Plan of Management for St Leonards Park

AUTHOR: Megan White, Landscape Planner/Architect

ENDORSED BY: Rob Emerson, Director Open Space and Environmental Services

ATTACHMENTS:

- 1. Draft St Leonards Park Plan of Management [8.11.1 66 pages]
- 2. Draft St Leonards Park Plan of Management Native Title Manager Advice [8.11.2 4 pages]

PURPOSE:

To undertake the processes required by the *Crown Land Management Act 2016* to enable the new draft Plan of Management for St Leonards Park to be put on public exhibition for community information and comment.

EXECUTIVE SUMMARY:

Section 3.23(6) & (7) of the *Crown Land Management Act 2016* requires all NSW Councils to adopt Plans of Management for all reserves for which they are the appointed Crown Land Manager by 30 June 2021. The new Plans of Management must meet both the requirements of the *Crown Land Management Act 2016* and the *Local Government Act 1993*.

The new draft PoM for St Leonards Park is the first Crown land Plan of Management to be prepared for North Sydney. When adopted, it will supersede Council's St Leonards Park Plan of Management 2011.

As part of the development process, Council must attest that it has considered Native Title Manager advice in relation to the draft Plan of Management. Council must also refer the draft Plan of Management to the Department of Planning, Industry and Environment – Crown Lands and seek endorsement to place it on public exhibition.

Preparation of a new draft Plan of Management for St Leonards Park is now complete, and the draft Plan of Management is ready to be referred to the Department.

FINANCIAL IMPLICATIONS:

Nil.

RECOMMENDATION:

1. THAT Council attests that it has considered Native Title Manager advice in relation to the draft Plan of Management for St Leonards Park.

- **2. THAT** Council requests approval from the Department of Planning, Industry and Environment Crown Lands to publicly exhibit the attached draft Plan of Management for St Leonards Park.
- **3. THAT** upon receipt of approval from the Department, Council amends the draft Plan of Management, as and if required by the Department, and places it on public exhibition as per Section 38 of the *Local Government Act 1993*.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 1. Our Living Environment
- 1.3 Quality urban greenspaces
- 1.4 Public open space and recreation facilities and services meet community needs

BACKGROUND

In accordance with the *Local Government Act 1993 (LG Act)*, Council has adopted 14 Plans of Management (PoMs) for community lands. These strategic documents also include Crown reserves and they provide a holistic guide to the use, development and management of all public open space in North Sydney.

The new Crown Land Management Act 2016 (CLM Act) was enacted on 1 July 2018. It imposes various requirements on Councils in NSW, including that Councils must categorise and adopt PoMs for all Crown reserves for which they are the appointed Crown land manager. The requirements are such that Council's existing PoMs that include Crown land do not comply with the CLM Act.

We have commenced preparation of a new suite of PoMs that address relevant Crown reserves. Some of the required Plans will be entirely new while others will replace existing Council PoMs. The new draft PoM for St Leonards Park will replace Council's existing PoM for St Leonards Park 2011.

Stage 1

Under Section 3.23 of the *CLM Act*, Council's must initially assign a category or categories of use that they consider to be most closely related to the purpose/s for which each Crown reserve is dedicated or reserved. The Department of Planning, Industry and Environment – Crown Lands must accept Council's category/categories before the required new PoMs can be prepared.

The categories of 'Park' and 'Sportsground' were proposed for R.89592 (St Leonards Park, excluding the North Sydney Oval Complex and the Bowling Club). These categories were deemed to best reflect the purpose of the reservation (Public Recreation) and the use of the Park. They were approved by the Department in December 2019.

Stage 2

Division 3.6 of the *CLM Act* deals with the requirements that Council must meet in relation to PoMs for Council-managed Crown land such as preparation, community engagement, adoption, alteration and publication of PoMs and the need to seek and receive nominated Native Title Manager advice.

The new draft PoM for St Leonards Park has been prepared using Crown land guidelines and taking into consideration the format suggested by the former Department of Industry – Crown Lands and the Office of Local Government. Advice has been received from Council's nominated Native Title Manager that the draft PoM is compliant with the CLM Act 2016 and

with the applicable provisions of the *Native Title Act (Cwlth) 1993 (NT Act)*. The draft PoM has been broadly drafted to enable flexibility of use of the Reserve by Council and the community.

A copy of the draft PoM for St Leonards Park is included as **Attachment 1**.

CONSULTATION REQUIREMENTS

Community engagement will be undertaken in accordance with Council's Community Engagement Protocol.

DETAIL

1. Draft Plan of Management (PoM) for St Leonards Park

With its distinctive Victorian landscape character, its rich history and its many sporting facilities, St Leonards Park is one of North Sydney's largest and best-loved parks.

The draft PoM for St Leonards Park provides an overall framework for management of the Park in coming years. It examines the broad range of issues associated with the Park in a comprehensive and holistic manner; identifying clear objectives for management, maintenance, and future development, and considering the Park's listing on the State Heritage Register as an item of state significance.

The new draft PoM incorporates the findings and recommendations of the St Leonards Park Conservation Management Plan 2013 and the St Leonards Park Landscape Masterplan 2018. It will replace Council's existing PoM for St Leonards Park 2011, which is due for a major review.

The draft PoM for St Leonards Park has been prepared specifically to:

- Meet Council's obligations regarding public land management under the requirements of Section 36 of the *LG Act* and the *CLM Act*.
- Ensure the requirements of the *NT Act* for the management of Crown land are addressed.
- Enable Council to renegotiate or enter into leases, licences and other use agreements.

2. Approval and Community Consultation Process

Council is required to submit the draft PoM for St Leonards Park for approval to the Department of Planning, Industry and Environment – Crown Lands, as the representative of the owner of the land, as per the requirements of Section 39 of the *LG Act*. This approval process must occur prior to any public exhibition and community consultation.

The Department will review the draft PoM and will advise Council of any properly required provisions or revisions that need to be included. Council must then amend the draft PoM prior to placing it on public exhibition under Section 38 of the *LG Act*.

If the community consultation period does not result in significant changes to the draft PoM being required, Council may then proceed to adoption. However, if community consultation does result in significant alterations being required, the PoM must again be referred to the Native Title Manager for an assessment report which must be considered by Council, and the PoM must again be referred to the Department for approval before it can be adopted by Council.

3. Native Title Requirements

PoMs for Crown Reserves must be compliant with the statutory requirements in relation to native title prescribed by both the *CLM Act* and the *LG Act*.

The *CLM Act* requires Councils to engage a qualified 'native title manager' to oversee and approve dealings and actions on Crown land that may affect native title. Council cannot adopt a PoM until it has obtained written advice from its nominated Native Title Manager that the PoM complies with any applicable provisions of Commonwealth Native Title legislation.

The Department of Planning, Industry and Environment – Crown Lands advices that Native Title Manager engagement and native title consideration should start from the beginning of drafting the PoM, even though formal advice is not required until later in the process. The Department will not process a draft PoM received from a Council unless the Council attests that it has considered Native Title Manager advice.

Native title has been considered in the preparation of the draft PoM for St Leonards Park and Council's nominated Native Title Manager, Robert Emerson, has concluded that:

- 1. The proposed act (preparation of the draft PoM for St Leonards Park) will affect native title, however it complies with the applicable provisions of the *NT Act*, being a valid future act under Subsection J of the future Acts regime.
- 2. The draft PoM authorises activities that could be considered as future acts within the meaning of Section 233 of the *NT Act*
- 3. The draft PoM guidelines provide that only activities that are in accordance with the reserve purpose are authorised.
- 4. Activities that are consistent with the reserve purpose may be validated under the future Acts regime of the *NT Act*.

Activities that cannot be validated have not been included in the draft PoM.

A copy of the Native Title Manager Advice is included as **Attachment 2**.

4. Next Steps

- 1. Council attests that it has considered Native Title Manager advice in relation to the draft PoM for St Leonards Park and resolves to refer the draft PoM to the Department of Planning, Industry and Environment Crown Lands
- 2. The Department reviews the draft PoM and advises Council of any changes required
- 3. Required amendments are made, and draft PoM goes on public exhibition for not less than 42 days

- 4. Community feedback is received and reported to Council:
 - a. If only minor changes are required:
 - i. PoM may be adopted by Council
 - b. If significant changes are required:
 - i. PoM is amended and referred to Council's Native Title Manager for an assessment report
 - ii. Assessment report is considered by Council
 - iii. PoM is again referred to the Department for approval prior to adoption by Council

The timeframe for review of draft PoMs by the Department is unknown. Although the *CLM Act* currently requires completion of compliant PoMs dealing with Crown land by 30 June 2021, this deadline is currently being reviewed by the Department as the workload and complexities involved (for both Councils and the Department) are proving to be significantly greater than anticipated.

5. Conclusion

The new draft PoM for St Leonards Park will guide the use, development and management of this significant public open space. The draft PoM identifies land management issues, sets out objectives, identifies and prioritises required works and actions and nominates performance indicators, as required by the *LG Act*, and is consistent with the requirements of the *CLM Act*.

Referral of the draft POM to the Department of Planning, Industry and Environment – Crown Lands is the next step in the process towards adoption of the required new PoM for this important park as Departmental approval will enable Council to publicly exhibit the draft PoM.



DRAFTSt Leonards Park



PLAN OF MANAGEMENT

NORTH SYDNEY COUNCIL

DRAFT ST LEONARDS PARK PLAN OF MANAGEMENT

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Introduction

Plans of Management provide clear guidelines for the effective short and long-term management of all land owned by Council or under Council's control. The St Leonards Park Plan of Management examines the broad range of issues associated with this significant park in a comprehensive and holistic manner. It identifies clear objectives for park management, maintenance, and future development, considering the Park's listing on the State Heritage Register as an item of state significance. The Plan of Management clarifies management policy and direction, both for Council staff and the community.

With its distinctive Victorian landscape character and its rich history, St Leonards Park is one of North Sydney's largest and best-loved parks. It is an attractive, high profile public recreation space that contrasts with the surrounding densely built-up urban areas and provides many and varied opportunities for recreation and relaxation. Its size and mix of organised sporting facilities and unstructured open space means that it appeals to both locals and the wider Sydney community. North Sydney Oval, the Bowling Club, the Music Shell and the War Memorial all attract specialist user groups, while the open lawn areas regularly host large-scale community events.

Land affected by this Plan of Management is Crown land owned by the NSW State Government. It is managed under the *Local Government Act 1993* and the *Crown Land Management Act 2016*.

This Plan of Management draws on information contained in relevant planning documents and studies, including:

- Local Government Act 1993 (LG Act)
- Crown Land Management Act 2016 (CLM Act)
- Native Title Act (Cwlth) 1993 (NT Act)
- The North Sydney Community Strategic Plan 2018 2028
- Local Environmental Plan 2013, North Sydney Council
- Recreation Needs Study 2015
- St Leonards Park Landscape Masterplan 2017
- St Leonards Park Conservation Management Plan 2013
- North Sydney Integrated Cycling Strategy 2013
- St Leonards Park Pedestrian and Cycling Feasibility Study 2014

Council's generic Sportsgrounds and Playgrounds Plans of Management should also be referred to when reading this document, as management strategies for specific features of the Park including the Bon Andrews Oval, the netball courts and the children's playground are discussed in these documents.

A major review of the St Leonards Park Plan of Management will take place every 5 – 10 years to allow policy and planning issues to be revisited and updated. Implementation of the actions listed in the Plan will be reviewed regularly.

Note:

The Council-owned North Sydney Oval Complex, which includes the Oval itself

as well as the surrounding grandstands, function centre and other associated infrastructure, is not included in this document. It is addressed in Council's North Sydney Oval Plan of Management 2015. However, the impact of the Oval Complex on the rest of St Leonards Park is considered in this Plan.

Similarly, the area of the Park occupied by the bowling club, including the greens, the buildings and the car park, is not included in this document, being the subject of a perpetual lease from the Crown. In addition to offering bowling, the site currently operates as a function centre, and has food and bar services. The impact of the Bowling Club site on the rest of St Leonards Park is also considered in this Plan.

1.0 Management Framework

1.1 Structure of the Plan of Management

The St Leonards Park Plan of Management is divided into 5 parts.

- Part 1 examines what a Plan of Management is, outlines the scope of this Plan, the legislative framework that drives and guides it, its purpose and its core objectives. It explains the link between this Plan of Management and Council's land management goals and details the importance of community engagement. It also examines the issue of leases, licences, permits and other estates
- Part 2 provides a general description of the history of St Leonards Park, its physical makeup and existing site features.
- Part 3 identifies and examines the major planning issues essential to an understanding of the overall directions of the document and discusses current management practices.
- Part 4 sets out the core values and management objectives, determined by Council and the community through previous studies such as the Recreation Needs Study and the park Conservation Management Plan. These objectives have been used as the basis for formulation of the implementation plan.
- Part 5 is the implementation and performance component of the Plan of Management. A matrix sets out the objectives, proposed actions and performance indicators and anticipated timing for each issue and action. An indicative works program further details the staging of all discrete works and actions.
- Part 5 is the Appendices, including maps of the Park.

1.2 Purpose of the Plan of Management

This Plan of Management provides an overall framework for management of St Leonards Park in coming years. Specifically, it has been prepared to:

- Meet Council's obligations regarding public land management under the requirements of Section 36 of the *LG Act* and the *CLM Act*
- Ensure the requirements of the *NT Act* for the management of Crown land are addressed
- Enable Council to renegotiate or enter into leases, licences and other use agreements

The production of this Plan of Management is closely linked with Council's overall land management objectives, as set out in the North Sydney Community Strategic Plan 2018 – 2028. The following information, relevant to St Leonards Park, comes from this Plan.

Council's goal regarding public open space (encompassed in the Community Strategic Plan under Direction 1 – 'Our Living Environment') is:

1.4 Public open space and recreational facilities and services meet community needs'

The following objective is derived from this goal:

1.4.1 Maximise use of existing, and protect, enhance and expand public open space

1.3 Legislative Framework

The LG Act requires that Plans of Management must be prepared for Council-owned community land. Plans of Management for Crown land were previously adopted under the Crown Lands Act 1989; however, the new CLM Act requires Council's to prepare Plans of Management for Crown reserves as per the requirements of the LG Act.

St Leonards Park (excluding the North Sydney Oval Complex) is located on Crown land and is zoned RE1 Public Recreation under Council's Local Environmental Plan 2013. (Refer **Appendix 1**).

1.3.1 NSW Local Government Act 1993 (LG Act)

The *LG Act* provides the legislative framework for Council's day to day operations. It identifies a Council's responsibility to actively manage land and to involve the community in developing a strategy for management.

The LG Act requires all community lands to be covered by a Plan of Management that must identify:

- The category of the land
- Objectives and outcomes for the land
- How Council proposes to achieve objectives and outcomes
- The way by which Council proposes to assess its performance
- Expressly authorise any leases, licences or other estates

The nature and use of community land may not change without an adopted Plan of Management.

1.3.2 Crown Land Management Act 2016 (CLM Act)

The *CLM Act* assigns certain functions to Council managers. As a crown land manager, Council is authorised to classify and manage its dedicated or reserved Crown land as if it were public land within the meaning of the *LG Act*. Dedicated or reserved Crown land may only be used for the following purposes:

- The purposes for which it is dedicated or reserved, or
- Any purpose incidental or ancillary to a purpose for which it is dedicated or reserved, or
- Any purpose specified in a plan of management for the land, or
- Any other purposes authorised by an Act

However, Council may grant short term licences over dedicated or reserved Crown land for any prescribed purpose under Section 2.20 of the *CLM Act*.

Generally, when managing dedicated or reserved Crown land, and for the purposes of this Plan of Management, Council:

a) must manage the land as if it were community land under the LG Act, and

b) has for that purpose all the functions that a local council has under that Act in relation to community land (including in relation to the leasing and licensing of community land).

Objectives of the CLM Act

The objectives of the *CLM Act* are to ensure that Crown land is managed for the benefit of the people of New South Wales, and in particular:

- o To provide for the ownership, use and management of the Crown land of NSW, and
- o To provide clarity concerning the law applicable to Crown land, and
- To require environmental, social, cultural heritage and economic considerations to be taken into account in decision-making about Crown land, and
- o To provide for the consistent, efficient, fair and transparent management of Crown land for the benefit of the people of NSW, and
- To facilitate the use of Crown land by the Aboriginal people of NSW because of the spiritual, social, cultural and economic importance of land to Aboriginal people, and, where appropriate, to enable the comanagement of dedicated or reserved Crown land, and
- o To provide for the management of Crown land having regard to the principles of Crown land management.

Principles of Crown land management

- Observe environmental protection principles in relation to the management and administration of Crown land
- To conserve the natural resources of Crown land (including water, soil, flora, fauna and scenic quality) wherever possible
- o Encourage public use and enjoyment of appropriate Crown land
- o Encourage multiple use of Crown land, where appropriate
- Use and manage Crown land in such a way that both the land and its resources are sustained in perpetuity, where appropriate
- Occupy, use, sell, lease, licence or otherwise deal with Crown land in the best interests of the State, consistent with the above principles

The *CLM Act* provides a new regime for the management of Crown land, and Council is now responsible for compliance with native title legislation for the Crown land that it manages.

1.3.3 Native Title Act (Commonwealth) 1993 (NT Act)

Native title is the legal recognition of the individual or communal rights and interests which Aboriginal people have in land and water, where Aboriginal people have continued to exercise their rights and interests in accordance with traditional law and custom since before the British asserted sovereignty over Australia. Native title rights and interests are formally recognised under the *NT Act*.

On Crown land, native title rights and interests must be considered unless:

- Native title has been extinguished, or
- Native title has been surrendered, or
- Determined by a court to no longer exist

DRAFT St Leonards Park Plan of Management

Council must manage Crown land in accordance with Part 8 of the *CLM Act* in relation to native title and ensure the requirements of the *NT Act* for the management of Crown land are addressed.

All activities on the land (R. 89592 - St Leonards Park) must address the issue of native title. Whilst a successful claim for native title will lead to official recognition of native title rights, native title rights are considered to pre-date such recognition. Native title can therefore be relevant to activities carried out on the land even if no native title claim has been made or registered.

The native title process must be considered for each activity on the land and a native title assessment must be undertaken. Almost all activities and public works carried out on the land will affect native title and require validation under the future act procedures in Division 3 of the *NT Act* by Council's Native Title Manager.

The *NT Act* sets out procedures for notification and opportunity to comment which must be followed in certain circumstances (for example if the proposed activity/act is a 'public work' as defined in Section 253 of the Act).

1.4 Land Categorisation and Core Objectives

Section 3.21 of the *CLM Act* states that dedicated or reserved Crown land may be classified and managed as if it were public (community or operational) land within the meaning of the *LG Act*. Section 3.23(2) of the *CLM Act* requires Crown land to be categorised consistent with the *LG Act*.

St Leonards Park is located on Crown land, and is classified as community land, (i.e. land that must be kept for the use of the general community). The categories of 'Park' and 'Sportsground' have been approved for the reserve by the Department of Planning, Industry and Environment – Crown Lands.

Map 2 in **Appendix 3 – Maps** indicates the boundaries between those parts of the Park categorised as 'park' and those categorised as 'sportsground'.

According to LG Act, land should be categorised as a 'park' if:

The land is, or is proposed to be, improved by landscaping, gardens or the provision of non-sporting equipment and facilities, for use mainly for passive or active recreational, social, educational and cultural pursuits that do not unduly intrude on the peaceful enjoyment of the land by others.

Land Category	Core Objectives
Park	• To encourage, promote and facilitate recreational pursuits in
(LGA 1993	the community involving organised and informal sporting
Section 36G)	activities and games, and
	• To provide for passive recreational activities or pastimes and
	for the casual playing of games, and
	To improve the land in such a way as to promote and
	facilitate its use to achieve the other core objectives for its
	management

According to LG Act, land should be categorised as a 'sportsground' if:

The land is used or proposed to be used primarily for active recreation involving organised sports or the playing of outdoor games.

Land Category	Core Objectives
Sportsground	• To encourage, promote and facilitate recreational pursuits in
(LGA 1993	the community involving organised and informal sporting
Section 36G)	activities and games
	To ensure that such activities are managed having regard to
	any impact on nearby residences

1.5 Leases, Licences, Permits and Other Estates

A lease, license or permit is a contract between a land owner and another entity, granting that entity a right to occupy an area for a specific period. Leases, licenses and permits formalise the use of community land by groups such as sporting clubs, community groups and schools, or by commercial organisations and individuals providing facilities or services for public use.

A lease is typically required where exclusive use or control of all or part of a park or reserve is required. The terms and conditions of a lease should ensure that the lessee undertakes proper management of the facility such that it is maintained in a safe and visually pleasing condition, and that the interests of Council and the public are protected.

Licences allow multiple and non-exclusive use of an area. A licence may be required where intermittent or short-term use or control of all or part of a park is proposed. Several licences for different users can apply to the same area at the same time, provided there is no conflict of interest.

Under the *CLM Act*, Councils are authorised to manage Crown land as if it were community land within the meaning of the *LG Act*. This enables Councils to issue licences, leases, permits and other estates for Crown land reserves in the same way they do for any Council-owned public land, regardless of reserve purpose, if it is in the public interest, and would not be likely to materially harm its use for the dedicated purpose. Essentially, the process for issuing tenures will depend on the classification and categorisation of the land, as well as its applicable planning controls associated with the land zoning.

A lease, licence or permit on Crown land may impact native title rights and interests. Any lease, licence or permit issued on Crown land must be issued in accordance with the future act provisions of the *NT Act* and in accordance with Part 8 of the *CLM Act* unless native title is extinguished. For Crown land which is not *excluded land* this will require written advice from Council's Native Title Manager that it complies with any applicable provisions of the native title legislation. (Refer Section 1.3.3). The advice should also note that the land is not subject to a claim under the *Aboriginal Land Rights Act 1983*.

This Plan of Management expressly authorises the issue of leases, licences, permits or other estates over the areas of St Leonards Park that comprise Crown reserve 89592 and that are categorised as either 'Park' or 'Sportsground', provided that:

- The purpose is consistent with the reserve purpose of the land
- The purpose is consistent with the community land classification and the core objectives for the category of the land ('park' or 'sportsground')
- The purpose is consistent with the zoning of the land under North Sydney LEP 2013 (RE1 Public Recreation)
- The lease, licence, permit or other estate is for a permitted purpose listed in the *LG Act* or the Local Government Regulations 2005
- The issue of the lease, licence, permit or other estate and their respective provisions can be validated by the provisions of the *NT Act*
- The land is not subject to a claim under the Aboriginal Land Rights Act 1983
- The lease, licence, permit or other estate is granted and notified in accordance with the provisions of the *LG Act* or the Local Government Regulations 2005
- The issue of the lease, licence, permit or other estate will not materially harm the use of the land for any of the purposes for which it was dedicated or reserved
- The purpose is consistent with the core values and management objectives listed in Section 4.1 of this Plan of Management
- Sub-leases are only allowable for the same purpose as the original lease in accordance with the requirements of Section 47C (1) (a) of the *LG Act*

Community land must not be leased or licenced for more than 21 years, or 30 years with the consent of the Minister for Local Government. Any lease or licence for more than 21 years must have prior public notice according to the requirements of Sections 47 and 47A of the *LG Act*, and if an objection is made to the tenure because of the notice, the Minister for Local Government's consent is required.

Licenses for short term casual use or occupation of land in St Leonards Park for a range of uses may be granted in accordance with Section 46(1)(b)(ii) for the purposes specified in Section 116 of the Local Government (General) Regulation 2005. All short-term casual uses and occupations are subject to Council's standard conditions of hire, approval processes and booking fees, as well as to native title manager advice.

1.5.1 Prohibited use agreements

Notwithstanding anything else in this document, this Plan of Management does not authorise granting leases, licences, permits and other estates which permit uses that are:

- Prohibited by the zoning of the land, unless otherwise enabled through Council's Local Environmental Plan 2013
- Not in accordance with the aims and objectives of this Plan of Management

1.6 Community Consultation

Community consultation plays an important role in the production of Plans of Management, providing Council with a sound understanding of issues from people who are familiar with and use the relevant parks and reserves. To a large degree, the views expressed by the community guide Council's management of the land.

Public engagement also generates an understanding of Council's land management aims, combats misinformation and misunderstanding, and fosters support for Council's programs and policies. The consent and co-operation of the users of open space facilitates management and lends weight to the status of this Plan of Management.

Since the adoption of the previous St Leonards Park Plan of Management in 2011, various relevant strategic documents have been prepared.

Recreation Needs Study 2015

Prepared with a high degree of community and stakeholder involvement, the Recreation Needs Study (RNS) provides information regarding recreation needs in St Leonards Park that was incorporated into the St Leonards Park Landscape Masterplan and has also been included in this document.

St Leonards Park Landscape Master Plan 2018

Also prepared with a very high level of community and stakeholder involvement, the low-intervention Landscape Master Plan (LMP) drew on information contained in the St Leonards Park Plan of Management 2011, the CMP and the RNS. Engagement activities included online and intercept surveys, drop-in sessions, workshops for staff and the community, as well as targeted stakeholder interviews.

Prior to general public exhibition of a new Crown land draft Plan of Management, Council is required to submit the draft Plan of Management to the appropriate representative of the owner of the land under section 39 of the *LG Act* (Currently the NSW Department of Planning, Industry and Environment – Crown Lands). The Department reviews the draft Plan of Management and advises Council of any properly required provisions to including in the Plan of Management. Council then amends the draft Plan of Management (if required), and places it on public exhibition (under section 38 of the *LG Act*).

Exhibition of the draft St Leonards Park Plan of Management for a 28-day period, followed by a 14-day period to receive submissions allows interested parties to comment on and have input into the final document. The following steps will generate widespread awareness of the draft Plan of Management:

- Posting the draft document on Council's website for information and comment, and providing details of how to make a submission
- Providing the opportunity to comment in accordance with sections 24JB(6) and 24KA(7) of the *NT Act*
- Notifying all Precincts, Council's Sport and Recreation Reference Group and other known stakeholders (including sports clubs, schools and commercial hirers) that a new draft Plan is on display, and inviting comments
- Providing hard copies of the draft document to stakeholders, upon request

 Making hard copies of the draft document available for viewing at Council's Customer Service Centre, upon request

Writing a Submission

Submissions give stakeholders an opportunity to express their opinions, provide information and suggest alternatives to Council's proposed management strategies.

To ensure submissions are as effective as possible:

- (1) List all points according to the section and page number in the Plan of Management
- (2) Briefly describe each subject or issue you wish to discuss
- (3) State which strategies you agree or disagree with and give reasons
- (4) Suggest alternatives to deal with any issue with which you disagree

Written submissions should be sent to:

The General Manager North Sydney Council P O Box 12 NORTH SYDNEY NSW 2059

Attention: Landscape Planner

Email: council@northsydney.nsw.gov.au

Comments regarding this or any other adopted Plan of Management may be submitted at any time. All Plans of Management are reviewed regularly, and at the time of the review new issues and actions can be incorporated and existing actions amended. All adopted Plans of Management are available on Council's website.

Implementation of this Plan of Management will require ongoing engagement between Council and relevant stakeholders in ensure the recommended actions continue to reflect the community's changing needs.

Note:

This Plan of Management requires that for any activity/action specified in it, a native title assessment and validation under the *NT Act* be carried out by Council's Native Title Manager prior to the commencement or approval of that activity. The need for notification of relevant organisations (including NTS Corp and the Metropolitan Local Aboriginal Land Council) will be assessed on a case-by-case basis. If required, it will be undertaken in accordance with the Native Title (Notices) Determination 2011 (No. 1).

2.0 Park Characteristics and Resources

2.1 St Leonards Park – Crown Reserve Details

Reserve Number	89592
Reserve Type	Reserve
Reserve Name	St Leonards Park (minus NS Oval & Bowling Club)
Gazetted Date	19/09/1975
Management Type	Council Crown Land Manager
Manager	North Sydney Council
Purpose	Public Recreation
Lots	Lots 1106-1107 DP 46990, Lot 7321 DP 1149783
Parish	Willoughby
LGA	North Sydney
Council	North Sydney
Suburb	North Sydney
Area	108,492

2.2 Site Location and General Description

St Leonards Park is bounded by Miller, Falcon and Ridge Streets and the Warringah Freeway. It contains North Sydney Oval, as well as organised sports facilities including Bon Andrews Oval, netball courts, cricket nets and a bowling club. Items of heritage significance include the Victorian park structure and path network, the Music Shell, the North Sydney War Memorial, Tunks Fountain, the WWI Field Gun and the gardenesque style of planting and historic alignments of trees.

The Park is positioned at a topographic high point of North Sydney, with spectacular views towards the Harbour and CBD skyline from the south-eastern corner. The Park sits to the north of the North Sydney CBD. It is surrounded by educational facilities, and students from a variety of schools use the Park daily for relaxation and informal sport and games.

The Park provides opportunities for both organised sport and for unstructured recreation. It features a children's playground, a shelter shed, seats, picnic tables, public amenities and BBQ facilities, open lawn areas as well as attractive planted avenues, specimen trees and features of special interest such as the fountain and War Memorial.

2.3 Heritage Value and Conservation Status

St Leonards Park is listed on the State Heritage Register as an item of state significance and has been identified 'as a rare and representative example of a largely intact Victorian era park designed in the gardenesque style with its original layout still appreciable'. The listing also notes that 'it is also rare as a continuously used cricket ground and bowling club dating from 1867 and 1887 respectively. The post WWII music shell and Modern movement style Bowling Club are rare surviving examples of these types of structures.'

A considerable number of artefacts and heritage-listed items are located throughout the Park. Items range in scale from the two large ovals and associated structures, through to small plaques commemorating local figures. It is the presence of these items that contributes to the heritage character that is evident today. The state heritage-listed North Sydney Sewer Vent (1899) was separated from the Park during the construction of the Warringah Freeway.

Items in the Park that are of heritage significance and are listed in Council's LEP 2013 include Tunks Fountain (1885), North Sydney War Memorial (1926), and North Sydney Oval (1867) which includes the Duncan Thompson Stand, Ken Irvine Scoreboard and 3 entrance turnstiles (1928). Several bus shelters along Miller St are also listed (1920's & 1970's) The southern shelter is an original former tram shelter (1920's) and the one on which all other bus shelters in the North Sydney Council area have been modelled.

Locally listed heritage buildings located on the Park boundaries include St Mary's Catholic Church, North Sydney Boys High and Wenona Girls School Group. Conservation neighbourhoods including Crows Nest are close to the Park.

There has been a strong history of avenue plantings in St Leonards Park, with many of the first trees located along the pathways and boundaries. Many of these alignments have been maintained through succession planting, allowing much of the original intention and design of the Park to be still legible today. Specimen trees are evident throughout the Park and includes many unusual species. Several memorial trees are also present.

In 2013 a Conservation Management Plan (CMP) was prepared for the Park by Godden Mackay Logan Heritage Consultants. The CMP provides conservation policies and actions to guide the future conservation, management and interpretation of the heritage significance of St Leonards Park. The policies and actions listed in the CMP were incorporated into the preparation of the Landscape Master Plan 2018 (LMP).

2.4 Site History

It is not currently known if there was Aboriginal activity in the vicinity of St Leonards Park, however, we do know that there were two Aboriginal clans in the region when the First Fleet arrived in 1788 – the Cammeraygal and the Wallumedegal. These two clans were part of the larger Kuringgai Tribe.

Table 3, below, briefly describes significant milestones in the development of St Leonards Park. More detail can be found in both the CMP and the LMP.

 Table 3 Development of St Leonards Park - Selected Milestones

1838	The township of St Leonards is laid out, and a 40-acre site 'The Reserve' is "reserved for
	Square and Public Recreation"
1867	The reserve is formally dedicated as a public park and the first cricket pitch is laid
1869	The second portion of the Park is set aside for public recreation, with St Leonards Council
	(later North Sydney Council) appointed as public trustee
1873	Locals raise funds to fence the park and to plant trees. Fig Tree Lane was probably planted
	now
1879	An Act of Parliament names the reserve "St Leonards Park". The first spectator's pavilion is
	built at North Sydney Oval
1885	A memorial fountain is erected in honour of Mayor William Tunks
1888	The bowling green opens
1897	The Jubilee Fountain is erected

1899	North Sydney Sewer Vent is constructed
1909	A low picket fence is built around North Sydney Oval
1920's	2 tram stops are built along the western Park edge, and 2 tennis courts are constructed in
	the north west area of the Park
1921	A WWI Field Gun is placed in the north-west corner of the Park
1926	The new North Sydney War Memorial is officially unveiled
1928	The Duncan Thompson Stand is opened at North Sydney Oval, and the Ken Irvine
	scoreboard and 3 small turnstile buildings are constructed
1930	The brick Cunningham Pavilion is constructed adjacent to Oval no. 2
1936	North Sydney Oval's picket fence is replaced by a brick wall (resulting in the removal of a
	double avenue of large Moreton Bay Figs along Miller St)
1930's	A low stone wall is constructed around the tennis courts, a children's playground is built west
	of the tennis court, and ornamental gardens are planted near the corner of Miller & Ridge
	Streets
1944	A stone shelter shed is constructed in the north-western quadrant
1948	The Music Shell, designed by Peddle Thorpe and Walker in the Art Moderne style, opens
1940's	WWII air raid trenches are dug in various park locations, and several low sandstone walls are built
1960's	Construction of the Warringah Freeway begins. Park improvements include the construction
	of public toilets and grass netball courts. The Bowling Club expands
1970's	Tunks Fountain is demolished by vandals
1980's	Suite of park improvements including new park furniture, light poles, paving and paint scheme. Entrance gates are installed on Fig Tree Lane, along with a picnic shelter at the netball courts. North Sydney Oval receives new grandstands and a renovation
1982	The Jubilee Fountain is relocated to Civic Park
1988	The Tunks Fountain is repaired
1994	St Leonards Park is listed by the National Trust as an item of heritage significance
1995	Oval No.2 is formally renamed 'Bon Andrews Oval
1990's	Further park upgrades include the construction of cricket nets, stone bollards along the
	bowling club access road, and asphalting the netball courts
2001	The Planet X Youth Centre opens in Music Shell and the children's playground is upgraded
2013	A Conservation Management Plan is prepared for the Park
2015	St Leonards Park is listed on the State Heritage Register
2018	St Leonards Park Landscape Masterplan is completed

2.5 Physical Characteristics

2.5.1 Topography

There is a natural ridgeline, running roughly north south in the eastern portion of St Leonards Park. Bon Andrews Oval and the Bowling Club are both located along this local high-point. The land to the west of the ridgeline gently slopes towards the Park's low point, adjacent to Falcon St in the north. This low point is marked by a drainage pit, which also corresponds to a tributary (now-underground) flowing into Willoughby Creek.

There is a considerable slope in the east of the Park (due to the natural landform and construction of the Warringah Freeway in the 1960's), with a sheer cut adjacent to Bon Andrew's Oval, and the land dropping away from the Park's highest points in the south-east at a steep grade. Due to the elevation in the east of the Park, there are expansive views towards Sydney Harbour and the city beyond.

2.5.2 Geology and Soils

St Leonards Park is situated in the Sydney Basin, a geological province characterised

by sedimentary rocks. Triassic sediments laid down between 230 and 180 million years ago form the dominant rock type within the basin and include (in chronological order of deposition) the Narrabeen, Hawkesbury and Wianamatta groupings. Hawkesbury sandstone is the major rock type in St Leonards Park and is commonly found in massive units with the occasional thin layer of shale or siltstone. The sandstone bedrock can be clearly seen in the Warringah Freeway cutting.

Soils in St Leonards Park closely reflect the nature of the sandstone geology. The topsoil is a brown silty sand, low in organic matter and deficient in nutrients due to leaching. The subsoil varies between clay, silty sand, sandy clay with ironstone accretions, and weathered sandstone. In many areas of the Park soil depth over bedrock is minimal, evidenced by retarded tree growth.

Development of St Leonards Park has caused extensive changes to the original soil profiles. Establishing ornamental gardens and lawn areas required soil modification and the importing of more arable soils. Grading of the sporting ovals entailed extensive modification of the existing soils. Topsoil removed from North Sydney Oval during its redevelopment was used to create mounds in the east of the Park. Soils are heavily compacted in some areas such as adjacent to the Music Shell, the netball courts and the freeway.

2.5.3 Vegetation

Very little of the original vegetation exists at St Leonards Park, aside from a small remnant of Scribbly Gum (<u>Eucalyptus haemastoma</u>). Studies indicate that the indigenous vegetation would have been predominantly Blackbutt (<u>Eucalyptus pilularis</u>) - Sydney Blue Gum (<u>Eucalyptus saligna</u>) association on the west of the ridge merging into Smooth-Barked Apple (<u>Angophora costata</u>) and Scribbly Gum (<u>Eucalyptus haemastoma</u>) associations along the ridge and to the east.

Today, the Park is defined by the configuration of tree plantings. Some historic avenues have been lost, however, established avenues are still evident on the Park frontages and on the primary diagonal and axial pedestrian paths, meaning that much of the original Park design remains legible. Open lawn areas are characterised by informal groupings of established gums including Argyle Apples (Eucalyptus cinerea), Scribbly Gums (Eucalyptus haemastoma) and Turpentines (Syncarpia glomulifera).

The St Leonards Park CMP notes that the Park's internal avenues of Pines, Camphor Laurels and Figs were probably the first tree plantings in the Park. Council records indicate that a row of established Pines interspersed with palms were initially planted on the Park's northern boundary. These were subsequently removed in 1935 and replaced by a line of gums which are still present today.

Council records note that a double row of established Figs was removed from Miller Street in 1936. These were replaced with Brush box (<u>Lophostemon confertus</u>) which are still evident. It is likely that the Canary Island Date Palms (<u>Phoenix canariensis</u>) along the north of this boundary were planted around the same time as the Palms near the War Memorial. Several old, large Fig specimens remain on southern Park boundary and distinctive specimen trees such as Bunya Pines (<u>Araucaria bidwillii</u>) and Strangler Fig (<u>Ficus superba var henneana</u>) are local landmarks and mark key locations. There are also several memorial trees in the Park.

The eastern Park boundary has changed dramatically since the construction of the Warringah Freeway. Dense native specimens have been planted since the introduction of the freeway to offer a visual buffer to the busy arterial road.

2.6 Park Features

St Leonards Park contains a variety of built features and facilities. The most significant of these are discussed here.

2.6.1 North Sydney Oval Complex

Unlike the rest of St Leonards Park, the North Sydney Oval Complex is Councilowned. The playing field is 'community' land, while the remainder of the Complex is classified 'operational' land. The Oval is managed in accordance with Council's North Sydney Oval Plan of Management 2015 and the North Sydney Oval Business Plan 2014. While the Oval Complex is not part of this Plan of Management, it is considered in terms of its relationship to the rest of the Park.

North Sydney Oval is one of Sydney's premier sporting venues used for regular fixtures and various one-off sporting and cultural events. The Oval Complex also includes Council's Parks Depot, commercial/retail outlets facing Miller Street, a childcare centre and playground, and a function centre. These facilities generate considerable movement of vehicles in Fig Tree Avenue. While the original concept for the Oval was based on the idea of a village green; today the Complex relates poorly to the surrounding parkland.

2.6.2 Bon Andrews Oval

The Bon Andrews Oval is situated in the north-eastern section of the Park. It is a well-used sportsground which has retained its open character. It is surrounded by a low picket fence and large fig trees enhance the southern perimeter. Floodlights allow for evening training. Adjoining the Bon Andrews Oval is the Cunningham Pavilion, a brick building dating from the 1930s that houses changing rooms and toilets.

2.6.3 Netball Courts

Two unfenced netball courts are in the south-eastern corner of the Park, between the bowling club and Ridge Street. The previously grassed courts were upgraded to competition-standard hard courts in 1993. The courts are served by a small covered shelter of similar design to the modern buildings in the Oval Complex.

2.6.4 North Sydney Leagues Bowling Club

Located in the exposed south-eastern section of St Leonards Park, the Bowling Club Complex consists of three greens, a main clubhouse building containing a function centre, a smaller maintenance building and a car park that accommodates up to 50 cars. The Club was opened in 1888 as the North Sydney Bowling Club and the present clubhouse building opened in 1961.

In 1960, the area of the Park occupied by the bowling club was revoked from the Crown reserve, and special lease No. 1960/227 was granted to the North Sydney Bowling Club Ltd for a term in perpetuity. The lease is for the purpose of recreation,

bowling greens, buildings, clubhouse, garage and car park.

The term and conditions of the lease are determined and administered by the appropriate representative of the owner of the land (currently NSW Department of Planning, Industry and Environment) under section 39 of the *LG Act*. The annual rental is payable directly to the Crown. The lessee is responsible for the running and maintenance of all facilities associated with the club in accordance with the lease conditions.

2.6.5 Music Shell Entertainment Area

The Art Moderne Music Shell is the major feature in the north-western section of St Leonards Park. Operational since 1948, this entertainment area has been used for activities such as music concerts, children's holiday programs, Planet X youth services, carols by candlelight and plays. The Music Shell was restored in 1993, and further renovated and upgraded in 2002. The large grassed amphitheatre in front of the building provides informal seating for events and space for other park activities.

2.6.6 War Memorial and Tunks Fountain

The War Memorial and Tunks Fountain are major park features. Designed by Frank Thorp of Peddle, Thorp and Walker Architects, the War Memorial was officially unveiled in 1926. Although now somewhat dwarfed by the North Sydney skyline, the War Memorial still commands a prominent position at the top of Walker Street and is visible from many areas. It is used for commemorative ceremonies such as Anzac Day.

The ornate tiered Tunks Fountain is dedicated to the memory of William Tunks, Mayor of East St Leonards from 1867 to 1882. The fountain was erected in 1885 and was restored and rededicated in 1988. The previously strong visual connection between the War Memorial and the Tunks Fountain is interrupted by red-poled park lights, installed more recently in the middle of the pathway that links these features.

2.6.7 World War 1 Artillery Gun

The World War 1 Field Gun located in the north west corner of the Park was captured by Australian troops in France in 1918.

2.6.8 Children's Playground

Located near the centre of the Park, the children's playground is enclosed by a low, depression-era, masonry wall, and a stone and cement entry arch that bears the words 'play area'. The playground contains custom-designed play equipment designed with accessibility in mind. Ancillaries features include a bubbler, picnic shelter, seats, tables and a BBQ.

2.6.9 Other Site Features

A stone shelter shed, built to give pensioners a place to play games, is located west of the Cunningham Pavilion. It retains its original concrete tables and timber seats. There are low sandstone walls at several locations in the Park, and several areas of sandstone curbing. The distinctive red light poles and cast iron bollards used throughout the Park were added in the 1980's. The light poles are situated in the centre of several of the Parks key paths, obstructing axial views and hindering circulation.

The public amenities near the Music Shell and children's playground is in relatively poor condition.

2.6.10 Formal Avenues

St Leonards Park has two key axial paths. The east-west path begins with Fig Tree Lane and loses formality as it runs towards Bon Andrews Oval. The north-south path, Tunks Avenue, remains the Park's key path with the formality of the War Memorial and Tunks Fountain to the south and the dense planting of Ficus macrocarpa var. hilii and other species towards Falcon Street. Several other formal avenues run diagonally to these key paths. Many of the older figs in the Park, including those in Fig Tree Lane, have been replanted in recent years to retain the Park's visual character.

2.6.11 Open Parkland

Much of St Leonards Park has a gardenesque character, with mature specimen trees located in open lawn areas. Some of these trees are either remnants of the area's original vegetation community or regrowth thereof. A few are living trees (eg Eucalyptus haemastoma), while others are tall, dead stumps, retained for their habitat value. Ornamental species include paperbarks, brush box, camphor laurel, firewheel trees and palms. There are also rows of palms around the north-western corner and either side of Tunks Avenue, north of the War Memorial. The corner of Miller and Falcon Streets features planted beds of coloured foliage.

2.6.12 Other Park Spaces

In some areas of the Park such as the playground and the triangular palm grove, the configuration of buildings, paths and tree plantings has created a series of more enclosed garden spaces that have a distinct character and an attractive 'garden room' feel.

Other parts of the Park have an open woodland character. Species vary; in elevated north-east areas lighter canopied gums dominate, creating a visually open vista. In the lower lying north-west areas rainforest trees and pines create more intimate park spaces. Park areas to the south are visually defined by the North Sydney Oval wall that creates a boundary to this area of the Park.

The open lawns in the south east corner offer expansive harbour views.

3.0 **Planning Issues**

3.1 Background

The range of recreation opportunities that St Leonards Park offers, and its consequent popularity means that it is subject to many pressures. The following information examines some of the issues that affect the Park, and that the Park LMP and this Plan of Management seek to address.

3.2 Landscape Character and Heritage

The Park's landscape character has evolved in response to various physical, social and economic influences, and these variables continue to affect its development. By identifying and analysing these variables, those contributing to desirable landscape quality can be encouraged, and those which do not can be addressed. Current issues relating to the Park's landscape character include:

- Many items from the 1980's park upgrade are not sympathetic to the heritage character of the Park, the suite of park furniture is dated and seating is sparse
- Light poles located in the center of some pathways are visually intrusive
- Many Park elements are worn out and need to be repaired or replaced. The current setting of the Tunks Fountain does not reflect its historical significance
- Several landmark and avenue trees have been lost and not replaced, resulting in incomplete avenues and a reduction in landscape character
- Lawn maintenance is difficult in some areas due to a combination of intensive use by groups and dense canopy cover
- The Park lacks visual detail; there are few intimate spaces or landscape 'rooms', due to the general lack of low and medium height shrubs

3.3 Access and Circulation

Council encourages the use of public transport to reduce the impact of cars on the environment, and to reduce problems with traffic congestion and parking. Although St Leonards Park is a 15-minute uphill walk from North Sydney railway station, many buses link the Park with the railway station, the city and other parts of the North Shore.

Parking in the streets surrounding St Leonards Park is generally adequate; however, shortages can be experienced during major events. Parking in adjacent streets is restricted to metered spaces outside of clearway hours. Council's Ridge Street car park (diagonally opposite the Park) offers reduced rates for patrons attending major events and fixtures. The North Sydney Leagues Bowling Club provides members-only parking on-site for up to 50 cars.

Current issues relating to access and circulation include:

- Light posts in the centre of already narrow pathways obstruct movement
- Well-worn desire lines in various locations (for example along the Ridge Street boundary) cause erosion and damage tree root systems
- Conflicts between commuter cyclists and pedestrians
- Conflict between vehicles, pedestrians and cyclists on Fig Tree Lane

3.4 Use

St Leonards Park is an important recreation resource. Premier sporting fixtures at North Sydney Oval and other large-scale special events held in the Park attract visitors from all over Sydney, and the Park is perennially popular with the local community including residents, school students and workers. Management strategies in this Plan consider the significance of the Park to all users and are flexible to provide for recreational needs to change over time.

Consultation for the LMP confirmed that St Leonards Park is well used and well loved. There was a high level of community and stakeholder support for maintaining the current balance of organised, active recreation facilities (i.e. the sports fields and courts) and unstructured areas for informal recreation. User surveys indicated that the main reasons people visit the Park are for walking, relaxation, children's play, dog walking and meeting friends. People most valued the Park for its green, open space and for its tranquillity – an oasis in the city.

The Park is regionally significant, and Council manages it for both locals and for visitors from further afield. Residents have a strong sense of ownership, and some events that provide for the wider community (with the attendant social benefits) can alienate locals if not managed carefully. Council employs best practice management techniques to ensure large events in St Leonards Park run smoothly and to minimise impacts on neighbours and other park users.

Park features such as the playground and the Music Shell attract specific user groups, and commuters (cyclists and joggers) regularly pass through the Park. The War Memorial is a focus for Anzac Day commemorations. Members of the local business community have lunch and play informal sports and ball games in the Park. Other areas of St Leonards Park are used for picnicking, reading, relaxing, dog walking and general socialising.

The Park's open lawns are popular with school and social groups for informal sport and games. Use of these areas by groups of under 60 people is on a first-come-first-served basis; no fees are charged, and no bookings are taken. Council staff working in St Leonards Park monitor those areas used regularly by groups, and areas may be temporarily closed for repair and restoration if wear and tear levels become significant.

Parts of the Park can be used for private functions and special events. Permission is granted at Council's discretion, with factors including the size of the proposed function, the area still available for general public use, the likely impact of the event on the Park and the likely impact on neighbours all being considered. Information on how to make a booking and other associated information in available on Council's website: 'Parks & Reserves Hire & Fees'.

Use of parks in North Sydney by fitness trainers continues to increase, reflecting a nationwide trend. An 'Outdoor Fitness Code of Conduct', available on Council's website, outlines common sense guidelines to ensure that fitness trainers and others involved in these activities do not have a significant negative impact on other park users and neighbours. If issues arise with fitness trainers or with any other park users, Council staff communicate directly with the relevant group.

The Park's popularity can cause problems; intensive use can lead to wear and tear and damage to the root systems of landmark and avenue trees. Conflicts between fast-travelling commuter cyclists using the Park as a thoroughfare and pedestrians and recreational cyclists can also be an issue. The Park's playground is popular with young children but has little to offer older visitors. Conversely, some Park edges currently receive little use due to traffic noise and fumes.

3.4.1 Dogs in Parks and Reserves

With numerous parks where dogs can be exercised off-leash, North Sydney is a dog friendly area. However, there are some restrictions. Under the 'Companion Animals Act 1998 (Section 14)', dogs are prohibited in the following public places:

- In or within 10m of any children's play areas;
- Food preparation and/or consumption areas (unless it is a public thoroughfare such as a road, footpath or pathway);
- Recreation areas where dogs are declared prohibited;
- Public bathing areas where dogs are declared prohibited;
- School grounds (unless with the permission of the person controlling the grounds);
- Child care centers (unless with the permission of the person controlling the center);
- Shopping areas where dogs are prohibited (unless secured in a vehicle, with the permission of the person controlling the place or going to or from a vet or pet shop);
- Wildlife Protection Areas

As per North Sydney's 'Local Companion Animal Management Plan', all Council parks and public reserves are off-leash areas except for:

- St Leonard's Park Oval (No. 1 and 2)
- All playing fields whilst organised sporting events are in progress
- All bushland areas
- Cremorne Reserve, Clark Park and Ancrum Street Park
- The Coal Loader Centre for Sustainability

All dogs in public places must be under the control of a competent person. Council is required by the State Government to enforce the laws concerning dogs, and Rangers may issue on-the-spot fines for infringements of the regulations listed above.

More information about dogs in North Sydney is available in Council's free booklet, 'Dog Control in North Sydney' available from Council's Customer Service Centre.

3.4.2 Organised Sport in St Leonards Park

Note: The following information does not apply to North Sydney Oval which has its own Plan of Management, nor to the bowling club which is the subject of a perpetual lease directly from the Crown. These exclusions are clearly shown in Appendix 3.

St Leonards Park's ovals, courts and cricket wicket and nets cater for local and regional

users. Council carefully regulates use of these facilities by hirers, as factors such as rising participation in sport and a growing community awareness of the benefits of a healthy lifestyle place ever-increasing demand on them.

Good management of the organised sports facilities ensures that they cater for an appropriate range of activities, that conflicts over availability are kept to a minimum, that non-sporting users still have some access to them and that they are appropriately maintained. Management strategies aim to maximise use of Council's sportsgrounds within acceptable limits.

Council manages the seasonal use of sportsgrounds with an allocation and booking system that divides the year into summer and winter seasons. Field and courts allocation considers traditional patterns of use, demand from the community and the need to provide opportunities for new sports and for minority sports and groups. The system aims to maintain the turf playing fields in good condition throughout the playing seasons.

A Council-facilitated meeting for potential sportsfield hirers is held before the start of each season if potential problems, such as booking clashes, are identified. The meeting aims to find mutually acceptable solutions.

Fees are charged for the use of sportsgrounds for organised sporting activities, and bookings must be made to secure exclusive use. The income generated assists with maintenance of the sportsgrounds.

Developing and managing high quality sports facilities is expensive. Funding for long term improvements to playing fields and supporting infrastructure such as amenities blocks currently comes from a variety of sources including Section 94 contributions and general capital reserves. Council keenly explores other opportunities, such as grants, to maximise funding for sports development, as the level of funding available is not adequate to meet the needs and expectations of the sporting community.

More information about sportsground management in North Sydney can be found in Council's Sportsgrounds Plan of Management.

3.4.3 Use of Sportsgrounds for Other Activities

Council manages its open space network for the use and enjoyment of all sectors of the community. While the primary use of sportsgrounds is to cater for organised sports, at other times (when the fields are not booked) they may be used by the general community for informal sports and games and for activities such as dog-walking. Sportsgrounds require a high level of maintenance, and it is important that casual users do not damage the playing surfaces. Use of sportsfields which are designated 'closed' is prohibited, and Council Rangers patrol to ensure and enforce compliance.

3.5 **Drainage**

Council's stormwater drainage system enters St Leonards Park at Carlow Street in the west, and heads in a north-easterly direction under the Park, exiting opposite Bardsley Gardens. All the stormwater runoff generated within the Park drains to the north-western corner where it connects with Council's main stormwater drainage system. Constraints in the stormwater pipe capacity in the Park mean that during storm events stormwater can pond just north of the Music Shell, the Park's low point, causing the

basement level of the building to flood.

3.6 Built Structures and Park Furniture

Park buildings including the Music Shell and the Cunningham Pavilion are sympathetic to the heritage character of the Park, and the colour scheme used on other structures and park furniture creates visual cohesion. Any new structures should complement the established character and be consistent with the recommendations of the CMP and the LMP.

The LMP identifies various issues with built structures and park furniture, and provides recommendations to address them:

 The high concrete/brick wall surrounding the Oval Complex forms a physical and visual barrier to the rest of the Park, limiting views and access particularly from Ridge/ Miller Street intersection, and detracting from the Park's visual appeal

The LMP recommends moving the adjacent pathway away from the wall and planting out the area between the pathway and the wall

- Dilapidated public amenities
 - The LMP recommends upgrading the public amenities and potentially moving them to a more useful location
- Park furniture does not reflect the Park's heritage, is in poor condition and is outdated in style

The LMP recommends upgrading park furniture including seating, lighting, bollards and drinking fountains, to better reflect the Park's heritage and to provide a more coordinated appearance

Public art can add interest, enjoyment and new layers of meaning to a place, and Council pursues opportunities where appropriate. A Public Art Guide and Walking Map available on Council's website provides information about various pieces of public art located around North Sydney. Public art projects range from items commissioned for specific projects to existing artworks which may be suitably placed in one of Council's parks.

3.7 Park Planning

3.7.1 Relevant Past Plans and Studies

The previous Plan of Management for St Leonards Park (2011) acknowledged the Park's local, metropolitan and regional significance, and identified the need to prepare a Landscape Master Plan to guide the enhancement of this important public open space.

The St Leonards Park CMP 2013 assessed the Park's significance and provided objective baseline data to inform the future LMP. It ensures that as the Park is developed to become more attractive and to better cater for the community's recreation needs, the heritage significance is simultaneously conserved. The CMP provides a framework for the ongoing care and management of the heritage of St Leonards Park, and guides decisions about its conservation, use and development into the future.

In 2015, following completion of the CMP, St Leonards Park was listed on the State Heritage Register. As a heritage item under the *NSW Heritage Act*, the Park is protected

from future unsympathetic and inappropriate development (Heritage Council of NSW approval is required for major changes).

The North Sydney Recreation Needs Study 2015 (RNS) identified the importance of St Leonards park as a passive open space and heritage site. The RNS also highlighted:

- The importance of adhering to the St Leonards Park CMP
- The potential for the Park to be overused for active recreation. (while noting that Council takes measures to protect areas experiencing overuse)
- Opportunities for perimeter path exercise circuits that people can use for jogging and walking
- Opportunities to illuminate key areas and routes for recreational walking
- Opportunities to provide more diverse social/family recreation space (for example tables, seating and play spaces for children of different ages)
- The possible redesign of the eastern side of the Park to provide a shared trail

3.7.2 Improvement Works

The following projects, listed in the St Leonards Park PoM 2011, have been completed:

- Bike racks installed at 4 key locations 2012
- Conservation Management Plan prepared for the Park 2013
- Replacement of the synthetic cricket wicket 2013
- Mass planting of natives at the north end of the Park (beside the freeway) 2013
- Restoration of the War Memorial 2014
- New flood lighting installed at the Bon Andrews Oval 2014
- Resurfacing of the netball courts 2015
- Major refurbishment of the Cunningham Pavilion 2016
- Restoration of the Word War I Artillery Gun 2017
- Landscape Master Plan prepared for the Park 2018
- Cricket practice nets extended 2020
- Miller Street Forecourt/Fig Tree Lane Upgrading Works 2020
- Ongoing management of trees in Fig Tree Lane as per arborist's report

3.8 Landscape Master Plan 2018 (LMP)

The LMP provides guidance and direction for future upgrading of St Leonards Park, recognising the Park's importance as a heritage asset, sporting destination and open space.

Specifically, the LMP aims to:

- Improve and enhance the park landscape
- Improve and enhance recreation opportunities for a range of park users while retaining the existing balance between active and passive spaces
- Ensure the Park is accessible and appealing to all members of the community
- Resolve existing circulation and access issues for pedestrians, cyclists, and vehicles

3.8.1 Consultation

Community consultation was undertaken throughout the development of the draft LMP, and the views expressed by the community and other stakeholders informed the process and the outcome.

Stage 1 consultation gathered information to understand how St Leonards Park is used and valued, to inform ideas for improvement. Over 450 surveys (intercept and on-line) were completed, staff workshops were conducted and 32 interviews with known stakeholders (including State government agencies, schools, sporting groups and other community groups) were undertaken.

Stage 2 consultation gathered community and stakeholder opinions on a set of draft 'Guiding Principles' and a set of draft 'Park Improvement Ideas' that would drive the LMP. It consisted of 5 park and market 'pop-in' stalls (over 200 participants), a stakeholder workshop and online surveys (139 responses received).

Development of the final LMP was in accordance with the endorsed Guiding Principles:

- 1 A Park for All
- 2 An Historic Park
- 3 An Accessible Park
- 4 A Generous, Comfortable Park

3.8.2 Key Improvements

The LMP nominated park-wide improvements including detailed proposals to enhance the park structure and elements and improve access. Key improvements and the rationale for them are described below.

1 - Tunks Avenue / War Memorial Commemorative Walk

This was developed to:

- Reinforce the historic park layout and restore Tunks Avenue as the primary memorial space
- Frame and reinforce views of the War Memorial
- Reinstate the visual prominence of the Field Gun

2 - Restoration of Tunks Fountain and Surrounds

This was developed to:

- Restore the historic Tunks Fountain and reinforce its visual prominence
- Create more attractive Fountain surrounds and reinforce War Memorial views
- 3 New and Improved Pedestrian Pathways

This was developed to:

- Improve safety for pedestrians
- Improve the park interface to North Sydney Oval boundary wall
- Reduce informal tracks and degraded lawn areas

4 - Improved Park Spaces

This was developed to:

- Improve amenity for park users
- Define active and passive park spaces
- Provide a garden edge to noisy and busy roads

- 5 A separated Cycleway on the Eastern Edge of the Park This was developed to:
 - Redirect the many commuter cyclists who currently use the diagonal path to connect from Falcon Street to the CBD and minimise conflict with pedestrians

6 - An Expanded Playground and New Toilet

This was developed to:

- Provide a wider range of play opportunities, particularly for older children
- Protect and enhance the existing (young children's) playground
- Upgrade associated amenities including toilets, seats, picnic tables and BBQs

The LMP also developed detailed recommendations for park lighting, furniture, planting and paving. Lighting is to be upgraded throughout the Park, to improve safety as well as to highlight and celebrate significant features. Paths are to be widened, furniture is to be upgraded and a greater diversity of seating opportunities with BBQ's, drinking fountains and picnic tables is to be provided.

3.8.3 Staging Plan

5 Stages, based on community priorities, were identified to help guide implementation of the LMP:

Stage 1 - North Sydney Oval Curtilage, Pathway & Music Shell Flooding

- North Sydney Oval curtilage: carry out upgrading works
- Ridge Street footpath: install pathway / paving
- Music Shell: address flooding issues

Stage 1B – Cycleway and Pathway

- Cycleway: install (commuter) cycleway and expand freeway buffer gardens to east (Note that these works are indefinitely delayed due to the construction of the Western Harbour Tunnel and Northern Beaches Link)
- Diagonal Pathway: realign southern portion of pathway, upgrade paving and lighting, and install gardens between southern portion of pathway and NSO wall

Stage 2 – Memorials, Pathways, Lighting & Garden Upgrades

- Central avenue (Tunks Way): widen and upgrade paving, lighting and seating. Crown lifting / removal of some trees to improve sight lines
- Memorial upgrades: relocate Field Gun and upgrade surrounds. Upgrade War Memorial surrounds including seating, palm planting, reflection pool. Upgrade Tunks fountain surrounds including widening path and gardens, installing seating
- Fig Tree Lane: upgrade paving and lighting (stage 1 west of gates)
- Bowling Club Access road: remove bollards, install edging and upgrade paving
- Perimeter gardens: upgrade gardens on Falcon, Miller and Ridge Street frontages

Stage 3 – Playground & Palm Grove

- Palm Grove: install table seating, upgrade gardens
- Existing Playground: upgrade
- New, expanded playground: expand playground, including surface treatments, play equipment catering to older children, seating, tables and BBQs

Stage 4 – Pathways

• Pathways: upgrade remaining pathways and associated lighting

Note:

This Staging Plan will guide implementation; however, variations are likely, due to:

- The impending construction of the Western Harbour Tunnel and Northern Beaches Link will have a direct impact on the eastern side of the Park, and works proposed for this area will be delayed
- Whether any grant funding can be secured (it is appropriate that implementation of any project receiving grant funding 'leapfrog' over other scheduled projects)
- The annual amount of capital funding available for implementing the LMP

The LMP, together with supporting documentation, is available on Council's website.

3.8.4 Implementation

The following works are currently underway in St Leonards Park, based on the Landscape Masterplan.

Miller Street Forecourt/Fig Tree Lane

This work, drawn from Stage 1 of the Staging Plan, is well underway. It includes paving and other infrastructure on the Park's Miller Street frontage, adjoining Fig Tree Lane. This area is one of the major Park entrances. Also included is the upgrading of the footpath area bordering Ridge Street.

Tunks Way

Detailed design plans are currently being prepared to facilitate Stage 2 works (based on work outlined in Priority 2 of the Staging Plan). These will include relocation of the memorial Field Gun and widening and upgrading the central park avenue (Tunks Way) running from Falcon Street to the cenotaph. It is anticipated construction will commence in late 2020.

More details regarding implementation of works can be found in Section 5.1 – Matrix.

4.0 **Basis for Management**

The St Leonards Park Plan of Management guides the future use, development and management of this important urban park. Careful management ensures that the Park is appropriately maintained, that it continues to cater for a range of activities, that conflicts between user groups are kept to a minimum and that future development is appropriate, sympathetic to the established landscape character and accords with the Park's CMP.

The Plan of Management is flexible to respond to the changing needs of the community and to incorporate future requirements as they arise.

4.1 Core Values and Management Objectives

4.1.1 Core Values

The following core values summarise the qualities that make St Leonards Park important to the community. They must be considered when future management objectives, strategies and actions are developed, to ensure their protection.

Heritage, Character & Aesthetics

- St Leonards Park is one of North Sydney's oldest and most significant parks. It is North Sydney's premier public open space and outdoor concert venue, and houses some of the north shore's most important sporting fields
- The Park is listed on the State Heritage Register and contains items of European heritage significance that contribute to North Sydney's cultural identity and history
- The Park is valued for its open space, its visual amenity and green character, which contrasts with the surrounding urban environment and provides a pleasant outlook
- The Park is valued for its contribution to North Sydney's identity and sense of place
- The south eastern area of the Park affords significant views of Sydney Harbour and the city, there are also significant internal views, such as along Tunks Avenue

Environmental

- The Park's size and proximity to high density housing and major transport corridors means it plays an important environmental role; reducing atmospheric contaminants, urban heat island effects and storm water run-off, and storing carbon
- Underground tanks in the Park irrigate North Sydney and Bon Andrews Ovals. They are part of the North Sydney Stormwater Reuse Project
- Selected dead trees have been retained to provide habitat and nesting spaces for birds and other animals

Social/Recreational

- The Park is a regionally significant recreation resource. Major sporting fixtures and other large-scale, cultural events attract people from all over Sydney
- The Park's sportsfields and courts are the home for many long-established clubs and associations. People come for competitions, coaching, school carnivals and

- for other social community activities and events. Belonging to a sporting group engenders a sense of belonging to a community
- The Park is popular with workers and school children who use the ovals, courts and the open lawn areas for informal sports and games and more passive forms of recreation at lunchtime and in the evenings
- The Park caters for the unstructured recreational needs of the community, including picnickers, dog walkers, joggers and people relaxing and socialising
- The Park facilitates social interaction; as the population increases, the value of the Park as a community meeting place also increases

Health

- Sports fields and courts provide opportunities for physical activity, contributing to general health and well-being. Participation develops physical, social and life skills as communication and co-operation are needed to achieve collective outcomes
- Access to public open space supports community mental health and well-being

4.1.2 Management Objectives

The following broad management objectives are based on:

- legislative requirements
- the values and assets of St Leonards Park
- identified community needs and expectations
- To manage St Leonards Park in accordance with the North Sydney Community Strategic Plan 2018-2028, the *LG Act*, the *CLM Act* and the *NT Act*;
- To ensure all uses of the Park are appropriate; that they comply with the purpose of the land reservation (public recreation), and accord with its zoning under LEP 2013 (RE1 Public Recreation), and with its categorisation under the *LG Act*;
- To upgrade the existing recreational environment and the aesthetics of St Leonards Park as per the LMP 2018, and consistent with the CMP 2013, recognising the Park's importance as a heritage asset, sporting destination and open space resource;
- To take a regional approach to managing North Sydney's organised sports facilities;
- To allow use of the Park by groups, and for appropriate special events;
- To manage large-scale events in the Park using best-practice techniques to minimise potential negative impacts on neighbours and other park users;
- To minimise conflict between different user groups;
- To ensure that Park management is environmentally, socially and economically sustainable, and that any new work considers sustainable principles;
- To maintain the Park to a standard commensurate with its high public profile;
- To manage stormwater in accordance with Total Catchment Management principles;
- To encourage community involvement in the Park's management;
- To provide for public safety;

- To manage the Music Shell in accordance with Council's Community, Cultural and Recreational Facilities Policy and the Planet X Plan of Management.
- To ensure this Plan of Management is flexible and able to evolve with changing community needs and attitudes.

These objectives have been used in the subsequent management issues to guide policy development and formulation of the action plan.

5.0 Policy, Implementation and Performance

The policies established in this document provide a management framework that is consistent with the anticipated availability of resources and anticipated community trends. The priority ratings outlined on the following pages may be modified if special circumstances arise.

Codes used to define priorities in the 'Priority' section of the Matrix:

1	(Priority 1) – Action completed within 2 years	О	(Ongoing) – Action will occur throughout the life of the PoM
2	(Priority 2) – Action completed in 2 – 4 years	С	(Commenced) – Action has started
3	(Priority 3) – Action completed after 4 years		

Codes used to define abbreviations in the 'Reference' section of the Matrix:

- LMP St Leonards Park Landscape Master Plan 2018*
- CMP St Leonards Park Conservation Management Plan 2013*
- GP1 LMP Guiding Principle 1: A Park for All
- GP2 LMP Guiding Principle 2: An Historic Park
- GP3 LMP Guiding Principle 3: An Accessible Park
- GP4 LMP Guiding Principle 4: A Generous, Comfortable Park
- PII1 LMP Park Improvement Idea 1: Heighten the significance of Tunks Avenue and the War Memorial
- PII2 LMP Park Improvement Idea 2: Restore Tunks Fountain and Surrounds
- PII3 LMP Park Improvement Idea 3 Provide new and improved pedestrian pathways
- PII4 LMP Park Improvement Idea 4: Refine and Improve park spaces
- PII5 LMP Park Improvement Idea 5: Provide a separated cycleway on the eastern edge
- PII6 LMP Park Improvement Idea 6: Provide expanded playground and facilities

5.1 Matrix

Note: Council's Sportsgrounds, Playgrounds and North Sydney Oval Plans of Management all provide more information about Council's management of these facilities in St Leonards Park. They are available on Council's website.

LANDSCAPE CHARACTER AND HERITAGE

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Landscape Master Plan (LMP)	Refine and upgrade park spaces to improve amenity for all park users in accordance with the LMP	Install new park benches along primary paths	Suggested locations are shown in the LMP	1	Park benches installed along primary paths including Tunks Way	LMP: GP4, PII4 NSC website
		Install new drinking fountains at the playground, along the diagonal path, and adjacent to the netball courts near Ridge Street		1&2	New drinking fountains installed in the Park	LMP: GP4, PII4
		Expand and refurbish gardens: • Along park edges (Falcon, Miller and Ridge Street frontages) • Under existing trees, and • At other key locations shown in the LMP	These gardens will provide a soft edge to noisy roads, improve visual amenity and increase the Park's passive recreation opportunities by providing defined areas for quiet reflection	1&2	Various garden areas expanded and refurbished	LMP: GP4, PII4
		Provide a more diverse range of flowering plants throughout the Park	Consider seasonal colour and texture. New groundcovers and shrubs help create landscape rooms while retaining sight lines	0	Increase in diversity of flowering plants in the Park	LMP: GP4, PII4
		Provide additional picnic tables in the Palm Grove and southern gardens (beside the southern lawn)	Suggested locations are shown in the LMP	2	Additional seating installed in the Park	LMP: GP4, PII4
		Provide additional seating throughout the Park, including overlooking the playground and near the fountain	Suggested locations are shown in the LMP	2	Additional seating installed in the Park	LMP: GP4, PII4
		Provide additional shade tolerant plantings in the Palm Grove		2	Additional shade tolerant plantings undertaken	LMP: GP4, PII4

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
		Expand freeway buffer gardens on the eastern Park edge	Carry out work in conjunction with new, separated cycleway. Note: although originally prioritised as '1B', this project will be delayed due to Western Harbour Tunnel/Northern Beaches link project	3	New buffer planting installed	LMP: GP4
		Maintain views and the open amenity in the south eastern 'Harbour View' lawn area		О	Views maintained	LMP: GP1
		Upgrade paving and other infrastructure on the Park's Miller Street frontage, bordering the North Sydney Oval complex	This area, adjoining Fig Tree Lane, is one of the major Park entrances	1	Paving and other infrastructure upgraded	LMP: GP3
	To maintain a balance between 'active' and 'passive' park spaces	Use planting to provide a clear definition between various spaces throughout the Park	The LMP illustrates how and where this can be achieved	О	Better definition of spaces in the Park	LMP: GP1, GP2
		Carry out minor regrading to create a flatter ground surface adjacent to netball courts	There is an opportunity for this informal space to be used occasionally for sport, whilst maintaining its primary use as an area for unstructured recreation	2	Minor regrading complete	LMP: GP1, PII4
		Carry out minor regrading to create flatter surface adjacent to junior practice wicket	There is an opportunity to make this informal space useable for occasional sport, with minimal impact on the look and function of this part of the Park	2	Minor regrading complete	LMP: GP1, PII4

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Landscape & Vegetation	To use planting to complement and enhance the Park's character	Select, plant and establish suitable species of trees, shrubs and ground covers	Consider objectives of the LMP, including character of existing plantings, aesthetic appeal of the proposed planting and impact on views	O	Objectives of planting achieved. All new plantings are consistent with the LMP	LMP
	To manage existing vegetation using best-practice horticultural techniques	Carry out regular ongoing maintenance work to conserve appropriate existing vegetation in good health	Refer Issue 'Maintenance'	О	Vegetation is maintained in a healthy condition	
		Control any activities that may harm existing trees	Potentially harmful activities may include tree climbing and digging around roots	0	Trees maintained in a healthy condition	
		Improve management and maintenance of open lawn areas to improve their condition	Some open lawn areas regularly become degraded due to high levels of use	О	Improved condition of open lawn areas	LMP: GP1
	To retain and celebrate the strong historical layout of the Park's trees	Inspect all avenue and other significant specimen trees in the Park regularly	Inspections to be carried out by a qualified arborist	0	All significant trees maintained in a healthy condition	
		Manage avenue and other significant trees proactively and maintain all significant trees in the best possible condition	Over-mature, diseased, dying or damaged trees are removed and replaced as required. Staged removal minimises the visual impact on the Park	О	Significant trees managed proactively	LMP: GP1, GP2
	To control biosecurity and environmental weeds in the Park	Undertake weed removal programs when and where necessary	Weed removal is a standard component of Council's maintenance operations	0	Weed levels in the Park remain low	
		Implement the Biosecurity Act consistently and effectively		О	Biosecurity Act implemented by staff	Biosecurity Act 2015

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Tunks Avenue / War Memorial Commemorative Walk	To enhance the significance of Tunks Avenue as an historic, commemorative walk, linking landmarks, as per the LMP	Plant a single line of figs along Tunks Avenue south, from Fig Tree Lane to Tunks Fountain to frame and reinforce views along Tunks Avenue to the War Memorial	Planting these trees will help re- establish Tunks Ave as the primary memorial space. The presence of underground water storage tanks will prevent the planting of canopy trees in some areas; so only partial restoration will be possible	1	New figs planted, and significance of Tunks Avenue enhanced	LMP: GP2, PII1
		Crown lift figs bordering Tunks Avenue and remove selected other trees	This will enhance views and improve sight lines along Tunks Avenue	1	Crown lifting work carried out	LMP: GP2, PII1
		Plant new palms along the southern portion of Tunks Avenue		1	New palms planted	LMP: GP2, PII1
		Install new park benches along the Avenue	See also Issue: 'Landscape Master Plan'	1	New park benches installed	LMP: PII1
		Widen the existing pathway and install appropriate edging	The LMP recommends providing a new, wider asphalt concrete path	1	Pathway widened, and edging installed	LMP: GP2, PII1
		Install accent lighting along the Avenue	This work will help re-establish the significance of the Avenue	1	New accent lighting installed	LMP: GP2, PII1
		Improve the aesthetics and functionality of the North Sydney War Memorial surrounds	Interpretive features integrated into the reflection pool could include text, imagery and lighting	2	Significance of War Memorial is enhanced	LMP: GP2, PII1
		Relocate the Field Gun to the northern end of Tunks Avenue to 'anchor' it and install accent lighting	In its current location, the Field Gun is partially obscured from the street and lacks visual prominence	1	Significance of Field Gun is enhanced	LMP: GP2, PII1
	To restore the historic Tunks Fountain and its surrounds, as per the LMP	Restore the Tunks Fountain and provide a new elevated base	This will emphasise the significance of the fountain	2	Tunks Fountain restored, and significance enhanced	LMP: GP2, PII2

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
		Create a more attractive surrounds for the Fountain by: • Situating it within a new garden bed and installing new seating and accent lighting • Providing seasonal colour in the surrounding gardens	This will amplify the Fountain's visual prominence by day and by night, and provide new opportunities for visitors to sit, overlook and enjoy the Fountain	1	Amenity and aesthetics of Tunks Fountain is increased	LMP: GP2, PII2
		Reorient the diagonal pathway to intersect at the Fountain	This will increase the Fountain's visual prominence. Refer also 'Access and Circulation'	1	Visual prominence of Fountain increased	LMP:GP2, PII2

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Conservation & Recognition of Heritage Items	To protect and conserve all identified heritage items	Manage and maintain St Leonards Park is accordance with the CMP and the LMP	The Park is listed on the NSW State Heritage Register. Consult with appropriate organisations on the management of heritage items and fulfil requirements of the Heritage Act	0	Co-ordinated and effective management of heritage items	CMP, LMP Heritage Act 1977
	To recognise, conserve and interpret the cultural and natural heritage of St Leonards Park	Explore opportunities to highlight the Park's history and install innovative and thoughtful interpretive material in the Park	The LMP explores various methods for presenting information. Any signage should be low-key and visually unobtrusive	O	Installation of interesting interpretive material in the Park	LMP: GP2

ACCESS AND CIRCULATION

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Access to the Park	To control vehicular access to St Leonards Park	Prohibit and prevent general vehicular access		О	General vehicular access to the Park prevented	
		Allow service and emergency vehicles and essential vehicles associated with special events to enter the Park	All access by vehicles associated with special events to be supervised by Council staff to minimise disturbance and ensure safety	О	Ease of entry for service and emergency vehicles	
		Maintain access to the bowling club car park	The car park is for the use of bowling club patrons only	О	Access to bowling club car park maintained	
		Upgrade the access road to the bowling club as per the LMP	Upgrading this roadway will improve circulation systems and safety (by slowing drivers), and improve overall park amenity	1	Access road upgraded as per the LMP	LMP: PII3
		Maintain access for North Sydney Council vehicles to the Parks depot	Vehicular access is via Fig Tree Lane which is also a well-used pedestrian route	0	Access to Depot maintained for Council vehicles	
		Upgrade Fig Tree Lane as per the LMP	Work includes new surfacing, raised thresholds, new kerb and gutters, new lighting and more. Carrying out this work will improve circulation systems as well as safety and park amenity	1	Fig Tree Lane is upgraded as per the LMP	LMP: PII3
		Install controlled vehicle security access in Fig Tree Lane	This work will improve safety	1	Unauthorised access prevented	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
	To encourage residents and visitors to walk, cycle or use public transport to get to the Park	Increase public awareness of alternative ways to get to the Park (other than by car)	Relevant information available on Council's website includes: • 'Walk North Sydney' maps and brochures • 'Northern Sydney Cycling Map' Numerous buses service the Park	O	Decrease in complaints regarding lack of parking spaces	NSC website
	To manage parking associated with the childcare centre in Fig Tree Lane to minimise impacts on residents	Liaise with Council's Traffic Department and the childcare centre as necessary and inform and educate parents and staff on existing parking restrictions and suitable parking sites	The childcare centre is a part of the Oval Complex	O	Liaison occurs when required. Reduction in complaints regarding inappropriate parking	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Circulation in the Park	To upgrade Park paths to improve circulation, safety and amenity, as per the LMP	Upgrade Park paths by: Resurfacing identified paths Widening key paths Relocating lamp posts away from the centres of paths	These works will improve circulation, reduce conflicts between commuter cyclists and pedestrians and improve general amenity	2 & 3	Pathway improvements completed on time and to budget	LMP: GP1, GP3
		All new pathway works to consider needs of people with impaired mobility	Many areas of the Park are relatively flat, and most of the existing pathway system is accessible to all	О	Improved level of access for all	Australian Standard 1428
		Upgrade paving and other infrastructure on the Miller Street frontage, near Fig Tree Lane, adjacent to the Oval complex		1	Work completed on time and to budget	LMP: PII3

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
	To discourage commuter cyclists from using the Park's diagonal path	Install a separated, commuter cycleway on the eastern edge of the Park, connecting Falcon Street to Ridge Street	The LMP locates the cycleway on the lower edge of the slope to protect harbour views and the well-loved open lawns above. This project will be delayed due to Western Harbour Tunnel /Northern Beaches link project	3	New, separated cycleway installed	LMP: GP3, PII5
	To reduce informal tracks in the Park	Install a new asphalt pedestrian path along the Ridge Street boundary	New gardens will be planted in association with the new path. Root systems of the existing trees along Ridge Street must be protected	1	New pathway installed along Ridge Street	LMP: PII3
	To improve the interface between the Park and the exterior wall of North Sydney Oval, as per the LMP	Realign the existing diagonal path south of the Fountain and install path lighting	Moving the path away from the wall will improve circulation and allow gardens to be planted to soften the Oval wall	1	Diagonal path realigned	LMP: GP3, PII3
	•	Install gardens between the southern portion of the diagonal pathway and the Oval wall	These will improve visual amenity	1	New gardens installed	LMP: GP3, PII3
		Install bench seats and other park furniture such as picnic and chess tables in the gardens adjacent to the Oval wall	These will provide gathering points for park visitors	1	New benches and other park furniture installed	LMP: GP3, PII3

PARK USE

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Use of St Leonards Park	To provide a range of recreational opportunities to meet the needs of a variety of users, within the carrying capacity of the Park	Ensure all activities occurring in the Park are appropriate and are consistent with Council's management goals and objectives	St Leonards Park is a large, urban park that caters for the recreational needs of the local and wider community	O	Reduction in complaints received regarding inappropriate use of the Park	
		Ensure all activities occurring in the Park are consistent with the relevant zoning	St Leonards Park is zoned 'Public Recreation'	О	Reduction in complaints received regarding inappropriate use of the Park	Appendix 1 – Zoning of St Leonards Park LEP 2013 NSC
		Ensure all activities occurring in the Park are consistent with the classification and categorisation of the Park	The Park is classified as 'community' land and is categorised 'park' and 'sportsground'	О	Reduction in complaints received regarding inappropriate use of the Park	CLM Act LG Act
		Ensure all use of the Park is consistent with the LMP	The LMP was developed with significant input from residents and other stakeholders and accurately reflects the community's needs	O	Reduction in complaints received regarding inappropriate use of the Park	LMP
		Maintain the current balance between active and passive recreational opportunities and facilities in the Park	LMP consultation revealed a very high level of satisfaction with the current balance of organised sport facilities (ovals and sports courts) and open parkland areas suitable for informal recreation	0	Current balance maintained	LMP

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Use of Open Parkland Areas	To encourage use of the Park's open areas for appropriate activities	Permit groups (including schools) to use the open parkland areas for social gatherings and informal sports and games	There is no charge for social gatherings of less than 60 people. Space is available on a first-come-first-served basis. No bookings are taken, and no structures may be erected	0	Condition of open parkland	Appendix 2 - Definitions
		Temporarily prohibit public use of open parkland areas suffering from excessive wear and tear		О	Condition of open parkland	
		Permit groups to hire sections of the Park for appropriate private functions and special events	Bookings are taken, and fees are charged for the use of Park areas for private functions by groups of 60 or more, and for special events (these are open to the public). Permission for functions is at Council's discretion, considering: • Size of proposed event • Area of Park still available for general public use • Likely impact on neighbours and the Park	O	Condition of open parkland	Annual Parks & Reserves Hire & Fees NSC
		Patrol the Park to ensure compliance with the above actions	Patrols are carried out by Council Rangers	О	Regular Ranger patrols carried out	
		Enforce Council's Fitness Training Code of Conduct as required	The Code provides fitness trainers with common sense guidelines to ensure their activities have minimal impact on the Park, on other users and on neighbours	0	Rangers carry out patrols in the Park and respond to complaints	Outdoor Fitness Training Code of Conduct NSC
		Liaise directly with individuals or groups if complaints are received regarding use of the Park	All parks users should be courteous and considerate toward other users	O	Reduction in the annual number of complaints received	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
	To acknowledge and promote the Park as a focal point for the community, and a venue for outdoor events	Promote the Park as a suitable venue for a variety of large-scale community activities and events	Large urban parks such as St Leonards Park have an important role to play in the social life of the local and wider community	О	Ongoing promotional activities undertaken	
		Encourage wider use of areas of the Park that are suitable for specialist activities	For example, the Music Shell stage and the associated amphitheatre are available for functions and activities	О	Diverse range of activities and events occurring in the Park	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Use of Sports Fields & Courts for Organised Sport	To manage the Park's sports fields and courts for organised sport	Permit use of the Park's designated sports facilities for organised sport	These facilities include the Bon Andrews Oval, the netball courts and the cricket nets. (Management of the North Sydney Oval complex is addressed in Council's North Sydney Oval PoM)	O	Appropriate use of sportsgrounds	Appendix 2 - Definitions. Sportsgrounds Plan of Management 2017
		Facilitate the hire of organised sports facilities to appropriate user groups for organised sport	Sports clubs, schools and others use the facilities for seasonal training and competition. Bookings secure exclusive use of a nominated sports field or court, and terms and conditions issued with each booking govern use	0	All sports facilities are hired to appropriate user groups	Sportsgrounds Hire & Fees NSC
		Charge fees for sporting clubs, schools and commercial hirers using the sportsfields and courts for organised sport	Use by community groups and schools is encouraged and fees are less for these users. Fees are reviewed annually	О	Revenue raised assists with sportsground maintenance	Sportsgrounds Hire & Fees NSC
		Continue to implement an efficient and equitable system of ground allocation with clear procedures	Pre-season hirers' meetings are held when potential bookings clashes are identified to give all parties a chance to discuss and resolve issues	O	Pre-season hirers' meetings occur as needed	
		Restrict the number of hours per week that the Bon Andrews Oval is used for organised sport	Restrictions are required to maintain the quality of the grassed playing surface	0	Condition of sportsground	
		Ensure pre-season use of the Bon Andrews Oval does not unduly damage it	No use of studs for pre-season games	0	Condition of sportsground	
		Close the Bon Andrews Oval during and after rain if play is likely to cause significant damage	Fines may be issued to users of a sportsfield that has been designated 'closed'	О	Condition of sportsground	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Use of Sports Fields & Courts for Other Activities	To manage the Park's sports facilities to accommodate other appropriate activities	Allow other recreational activities that will not cause significant damage to the sports facilities to take place	Sports facilities can cater for informal sport and games and fitness training when not being used for organised sport. No use of closed fields	0	Condition of sports fields and courts	Appendix 2 – Definitions
		Liaise directly with individuals or groups if complaints are received regarding their use of sports fields and courts	The primary purpose of sports fields and courts is to provide specialist organised sports facilities for fee-paying users, and other users must consider this	О	Reduction in the annual number of complaints	
		Allow appropriate cultural activities and events to take place on the sportsfields	Permission is at Council's discretion, and fees may be charged to offset additional maintenance required to return the fields to playing condition	O	Range of appropriate non- sporting activities accommodated	Annual Parks & Reserves Hire & Fees NSC

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Dogs in the Park	To accommodate dogs in the Park	Allow dog-walking in the Park in accordance with the Companion Animals Act	Dogs are not permitted within 10 metres of children's play equipment	О	Reduction in the annual number of complaints	Companion Animals Act 1998 (S14)
		Allow dog walking on the Park's sportsfields and courts in accordance with the North Sydney Local Companion Animals Management Plan	Dogs must be on a leash whilst organised sport is in progress at the netball courts, and always on the Bon Andrews and North Sydney Ovals	O	Reduction in the annual number of complaints	NS Local Companion Animals Management Plan 2004
		Patrol the Park to ensure compliance with above actions	Patrols are carried out by Council Rangers	О	Regular Ranger patrols carried out	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Music Shell	To manage the Music Shell to maximise community benefit	Promote the Music Shell to raise public awareness of the facility and to encourage use	Council's Community and Library Services Division carry out promotion	О	Level of Music Shell use	Community, Cultural and Recreational Facilities Policy NSC 2018
		Allow use of the Music Shell for community events	The Music Shell's stage and the associated grassed amphitheatre are available for hire. Relevant Council staff assess suitability of proposed events	О	Music Shell is used for appropriate community events	Community, Cultural and Recreational Facilities Policy NSC 2018

BUILT STRUCTURES & PARK FURNITURE

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Built Structures, Park Furniture & Other Park Facilities	To upgrade the public amenities in St Leonards Park, as per the LMP	Design and construct new public amenities to replace the existing aged facility near the playground	This will occur when the new play space is constructed. The LMP includes concept design ideas and inspiration for the new amenities block	3	New amenities block constructed	LMP: GP4, PII6
	To upgrade park furniture and other facilities to meet the identified needs of users, as per the LMP	Select park furniture and paving to complement and enhance the heritage character of the Park	Consider accessibility for all, as well as Council's Public Domain Style Manual and Design Codes	О	Character of new park furniture reflects the Park's heritage	LMP: GP2, PII4. Public Domain Style Manual & Design Codes 2019 NSC
		Provide new park furniture to improve the amenity of the Park	Park furniture includes picnic tables, benches, drinking fountains, BBQ facilities etc	О	New park furniture installed in appropriate locations	LMP: GP4, PII4
		Investigate replacing the existing wire fence around the northern and eastern sides of the Bowling Club greens	Liaise with Bowling Club	3	Investigations carried out; recommendations made	LMP
		Provide new Park lighting, as per the LMP	Lighting works will generally be carried out in association with pathway works	1,2 & 3	New lighting reflects the Park's heritage character	LMP: GP4, PII4
	To provide other structures and facilities to meet Park user needs	Install a fitness station with outdoor exercise equipment in the Park	The LMP recommends a location between the netball courts and the Bowling Club access road	2	Fitness station installed	LMP: GP4, PII4
	To encourage appropriate public art projects in St Leonards Park	Pursue appropriate opportunities to place public art in St Leonards Park as they arise		O	Increase in the amount of public art in the Park	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Play Spaces	To provide a diverse range of play opportunities in the Park, as per the LMP	Expand the Park's play space and provide more challenging activities for older children	The need to expand the play space was first identified in the previous PoM for the Park. Access for all will also be considered	3	New play space constructed	LMP: GP4, GP1, PII6 St Leonards Park PoM 2011
		Provide a new, custom-designed play space that encourages a variety of activities (climbing, swinging, hiding, jumping, balancing and sliding) in a landscaped setting	The LMP includes a schematic design for a play space based on interconnected 'bird's nest' cubby houses	3	New play space constructed	LMP: GP1, PII6
		Provide new ancillary facilities in the expanded playground	These include drinking fountains, picnic tables, seats and BBQs	3	New play space constructed	LMP: GP1, PII6
		Install gardens with flowering plants, and provide nature play as part of the expanded play space	The new play space will have a nature/landscape character	3	New gardens installed as part of new play space	LMP: GP1, PII6
	To retain and enhance the existing playground	Upgrade the existing playground to better cater for younger children, ensuring it is safe, functional and attractive	The well-loved sand play and music room themes will be retained, and additional play items will be provided within the existing walled space	3	Existing playground upgraded	LMP: GP1, PII6
		Manage and maintain play equipment and surrounds as per Council's Playgrounds Plan of Management		О	Playground managed as per Playgrounds Plan of Management	Playgrounds Plan of Management 2016 NSC

MANAGEMENT

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Native Title Obligations	To ensure Council meets its obligations under the <i>CLM Act</i> and the <i>NT Act</i> regarding the management of Crown land	Carry out a native title assessment and validation under the <i>NT Act</i> as part of the terms of authorisation for the commencement of any building, development or infrastructure activity (act) on areas of the Park included in R. 89592	Assessment is carried out by Council's Native Title Manager	O	Native title assessment and validation carried out prior to commencement of all relevant activities	Native Title (Cwlth) Act 1993 Crown Land Management Act 2016
		Carry out appropriate notification and 'opportunity to comment' procedures if the proposed act (above) is a 'public work' as defined in Section 253 of the <i>NT Act</i>	Council will notify NTS Corp and the Metropolitan Local Aboriginal Land Council of the proposed act and provide the opportunity to comment (at least 30 days)	O	Appropriate notification and opportunity to comment provided as required	Native Title (Cwlth) Act 1993

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Management & Administration	To provide efficient, open and responsive administration and management	Implement efficient and effective systems designed to make the best possible use of financial, human and natural resources		O	Ongoing cost- effectiveness of Council's management systems	
		Respond to public suggestions, attend to enquiries promptly and provide information to the community as required		0	Level of community satisfaction	
	To manage the Park's sports facilities in an efficient, effective and accountable manner	Manage the organised sports facilities as per Council's Sportsgrounds Plan of Management		O	Sports facilities managed and maintained as per Sportsgrounds PoM	Sportsgrounds Plan of Management 2017 NSC
	Liaise with sporting clubs and other hirers to monitor the sporting requirements of the community	O	Satisfaction of user groups as determined by feedback			
	To consider region- wide issues in the management of the sports facilities in St Leonards Park	Implement the recommendations of the Regional Sportsgrounds Strategy as they relate to the Park's sportsfields and courts	The Strategy was developed by the Northern Sydney Region of Council's (NSROC) in 2017	O	Strategy recommendations considered and implemented, where feasible	Regional Sportsground Strategy Review - Final Report 2017

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Leases, Licences, Permits & Other Estates	To facilitate the use of St Leonards Park for a range of appropriate recreational, sporting and cultural activities	Grant leases, licences, permits and other estates to recreational, sporting and cultural organisations and other groups to formalise use of parts of the Park for appropriate activities	A Native title assessment and validation under the NT Act must be undertaken by Council's Native Title Manager prior to the preparation, exhibition and adoption of any leases, licences, permits or other estates in the areas of the Park included in R. 89592 and categorised as Park or Sportsground. Other applicable legislative and other planning controls governing leases, licences, permits and other estates are described in Section 1.5 of this Plan. All lessees/licensees should provide facilities or services for public use and should ensure the leased area is safe and attractive and that the interests of Council and the public are protected	O	Any leases or licences are prepared, exhibited and adopted in accordance with the provisions of the <i>LG Act</i> and the <i>CLM Act</i> . Native Title Manger advice is received for all proposed leases and licences	Local Government Act 1993 Crown Land Management Act 2016 Native Title (Cwlth) Act 1993 North Sydney LEP 2013

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Maintenance	To maintain the Park to meet the needs and expectations of users and the community, commensurate with Council resources	Carry out regular maintenance to a high standard as befits the Park's status as a regional recreation resource and ensuring provision of a safe and attractive park	Park maintenance regimes consider that the periodic special events require extra maintenance	О	Maintenance program implemented on time and within budget. Condition of the Park	
		Undertake emergency repairs as required	Council staff are pro-active; identifying problem areas and responding to public requests	О	Expenditure/ revenue ratio achieved	
		Monitor the cost of maintenance work so that adequate resources can be allocated, and future problems prevented		О	Cost data recorded and monitored	
	To ensure the playground is functional, safe and attractive	Manage and maintain the playground as per Council's Playgrounds PoM		О	Playground maintained as per Playgrounds PoM	Playgrounds Plan of Management 2016 NSC
		Inspect the playground daily to ensure all equipment and ancillary facilities are in good condition	The playground is occasionally subject to vandalism due to its 'hidden' location in the centre of the Park	О	Playground inspected daily, and damage attended to in a timely manner	
	To maintain all sports fields and courts in a safe, usable and attractive condition	Maintain the Bon Andrews Oval and the cricket nets and netball courts as per the Sportsgrounds PoM		О	Maintenance as per Sportsgrounds PoM	Sportsgrounds Plan of Management 2017 NSC

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
	To maintain Park amenities and facilities as per Council's Amenities Condition Report & Maintenance Plan (ACRMP)	Carry out minor repair works to the amenities block near the playground prior to construction of the new facility	Minor repair works only will be undertaken as a new amenities block will be constructed in future, as per the LMP. The ACRMP has a 10-year lifespan	O/2	Work carried out as per the ACRMP	LMP: GP4, PII6 ACRMP 2018 NSC
		Maintain the Bon Andrews Pavilion as per the ACRMP	Work includes maintaining epoxy coating to floors, external painting, minor repairs, replacing quad gutters to the brick section of the building, internal painting and replacing change room floor	0	Work carried out as per the ACRMP	ACRMP 2018 NSC
		Maintain the netball courts as per the ACRMP	Courts require resurfacing every 8 years, commencing in 2025	О	Work carried out as per the ACRMP	ACRMP 2018 NSC
		Maintain the stone shelter as per the ACRMP	External and internal painting and minor repairs are required every 10 years, commencing in 2022	0	Work carried out as per the ACRMP	ACRMP 2018 NSC

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Safety and Risk Management	To minimise Council exposure to complaints, compensation claims and litigation	Continue to carry out an inspection program to identify and eliminate potential hazards in a timely manner	The regular maintenance inspections also function as risk assessment inspections	O	Inspection program regularly carried out	
	To minimise vandalism in the Park	Patrol the Park regularly	Council Rangers carry out patrols	О	Decrease in annual number of incidents reported	
		Remove graffiti as soon as practicable	Removing graffiti quickly sends the message that Council facilities are well-cared for	О	Speed of graffiti removal	Graffiti Management Policy 2018 NSC
		Repair vandalised park furniture and play equipment as soon as practicable or remove if damaged beyond repair	Park users are encouraged to alert Council to any vandalism as soon as it is noticed	О	Speed of repair work or removal	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Parks Department Depot	To ensure the Parks Depot operates effectively and with minimal impact on the rest of the Park	Limit Depot access to Council vehicles, delivery vehicles and vehicles associated with major fixtures at North Sydney Oval	Access to the Depot is via Fig Tree Lane, a private road that is also used by pedestrians and cyclists	О	No access to Depot by unauthorised vehicles or visitors	
		Close gates outside Depot operating hours to prevent unauthorised access	Gates are open between 6.30 am - 3.45 pm, Monday – Friday only	0	No access to Depot outside operating hours	
		Prohibit vehicles from parking in Fig Tree Lane	Lane may be used by delivery vehicles as a drop-off zone only, under Council supervision	О	Adherence by delivery vehicles with conditions of use of access road	
		Install controlled vehicle security access in Fig Tree Lane	This safety measure will be implemented as part of Priority 1 LMP works	1	Unauthorised access prevented	LMP
		Ensure that the appearance of all Depot structures is sympathetic to other Park and Oval structures		О	Depot structures have minimal visual impact on the rest of the Park	
		Maintain Depot in a clean, tidy and safe condition and ensure all hazardous materials are secured	Fuels, pesticides and fertilisers are locked up in a designated shed	О	Depot maintained in a tidy and safe condition	

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Stakeholder Involvement	To provide opportunities for public involvement in the development and implementation of this Plan of Management	Actively seek public input by widely publicising the draft PoM	In addition to general public notification, known stakeholder groups and individuals are notified directly and invited to comment when the draft Plan is on public exhibition	O	Public exhibition of draft Plan of Management widely publicised	
	C	Carry out consultation as required by the <i>NT Act</i>	Refer Issue 'Native Title Obligations'	0	Obligations under the <i>NT Act</i> fulfilled	Native Title (Cwlth) Act 1993
		Encourage stakeholders to provide written submissions to Council regarding the draft PoM	Council considers all submissions and appropriate information and comments are incorporated into the final PoM	O	Review of draft PoM appropriately considers all public submissions	
	f	Ensure that both the draft and the final PoM are easily accessible to the community and stakeholders	All adopted PoM's are available on Council's website	O	Plan of Management available on Council's website	
		Carry out future development in the Park as per the LMP	The LMP was prepared with a very high level of community and stakeholder involvement and was generally very well received and well supported. (Refer Section 3.8 of this Plan)	O	Future work is consistent with the LMP	LMP

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Drainage	To address drainage and flooding issues near the Music Shell	Undertake localised path reconstruction and other work as needed to mitigate drainage and flooding issues at the Music Shell	The entrance to the interior of the Music Shell is located at the Park's low-point	1	Music Shell flooding problems resolved	LMP

Issue	Objective	Action	Comments	Priority	Performance Indicators	References
Monitoring of	To review and	Review the PoM regularly, and		О	Plan objectives	
Management	update the St	comprehensively update the Plan			and actions	
Systems	Leonards Park Plan	after 5-10 years to conserve,			implemented.	
	of Management	maintain and enhance the values			Relevance of Plan	
	regularly	and character of the Park			of Management	

5.2 Implementing the Indicative Works Program

Council's Plans of Management identify clear objectives and directions for planning, resource management and maintenance of public open space. They inform the community about upcoming projects and works; clarifying what will be happening, explaining why, and assigning priorities.

However, the Plans of Management also need to be flexible enough to respond to new opportunities and to changed circumstances and community needs. While significant shifts in policy or planning issues during the life of a Plan of Management are unlikely, it is conceivable that specific works or the order in which works are undertaken may vary.

New Opportunities

Whenever possible, Council makes application for grant funding from relevant state and federal organisations. Grant funding generally targets specific types of work (for example upgrading a playground or restoring a historical monuments). If successful, Council may need to bring forward the project to comply with the grant conditions.

Changed Circumstances and Community Needs

There may also be instances where specific projects need to be rescheduled. The impending construction of the Western Harbour Tunnel & Northern Beaches Line will directly impact the eastern side of the Park and works planned for this area (such as construction of the commuter cycleway) will be delayed.

The process of reviewing and revising Council's Plans of Management is cyclical, and, in some instances, other significant studies completed 'between Plans' may identify actions and works that can appropriately jump ahead of existing Plan of Management actions. This is particularly so when the study in question (for example a Recreation Needs Study) has been prepared with a very high level of community engagement and involvement. If the fast-tracked action is consistent with the core values and stated management objectives expressed in the relevant Plan of Management, this is justified.

5.3 INDICATIVE WORKS PROGRAM

WORKS/ACTION	2020 – 2022	2022 - 2024	2024 onwards
	Priority 1	Priority 2	Priority 3
Upgrade paving and associated infrastructure on the Park's Miller St frontage, beside the North Sydney Oval complex			
Install controlled vehicle security access in Fig Tree Lane			
Plant a single line of figs from Fig Tree Lane to Tunks Fountain			
Crown lift figs along Tunks Avenue, and remove selected other trees			
Plant new palms along the southern portion of Tunks Avenue			
Widen Tunks Avenue, install accent lighting and edging			
Construct footpath along the Ridge Street park edge			
Undertake localised path reconstruction and other work to mitigate drainage and flooding issues at the Music Shell			
Install new park benches along primary park paths (eg Tunks Ave)			
Relocate Field Gun to the northern end of Tunks Ave and install accent lighting			
Expand and refurbish gardens along park edges (Falcon, Miller and Ridge Street frontages, under existing trees, and at other key locations specified in the LMP			
Upgrade the access road to the bowling club			
Realign the existing diagonal path south of Tunks Fountain, upgrade its surface and install path lighting			
Install gardens between the southern portion of the diagonal pathway and North Sydney Oval wall			
Install bench seats, picnic tables, chess tables etc in the new gardens between the Oval wall and the relocated diagonal path			
Improve the War Memorial surrounds to enhance the significance and improve the useability of the area			
Upgrade Tunks Fountain by constructing a new elevated base, planting new gardens, installing seats and accent lighting			
Upgrade Fig Tree Lane by installing new lighting, new road surfacing and raised thresholds at the entry points, removing bollards and replacing kerb and gutters			
Carry out minor regrading to create a flatter ground surface east of netball courts			
Carry out minor regrading to create flatter ground surface adjacent to junior practice wicket			
Install picnic tables in the Palm Grove and southern gardens (adjacent to the southern lawn)			

WORKS/ACTION	2020 – 2022	2022 - 2024	2024 onwards
	Priority 1	Priority 2	Priority 3
Plant additional shade tolerant species in the Palm Grove			
Install new drinking fountains on the diagonal path, near the netball courts and at the playground			
Install a fitness station in the Park			
Upgrade other park paths and relocate lamp posts away from the middle of remaining paths			
Design and construct new public amenities			
Expand the existing playground west, and cater for older children in a new nature-based garden play space			
Upgrade the existing walled play space (this area will cater to younger children)			
Provide new play equipment and ancillary facilities including seating, picnic tables, drinking fountains and BBQs in the new play space			
Install a separated, commuter cycleway on the eastern Park edge, linking Falcon Street to Ridge Street			
Expand freeway buffer gardens on the eastern Park edge (in conjunction with new cycleway)			
Investigate replacing the existing wire fence around Bowling Club			
Carry out a major review of this Plan of Management			

6.0 **Appendices and Supporting Material**

6.1 Appendix 1 - Zoning of St Leonards Park

Zone RE1 Public Recreation

1. Objectives of the Zone

The particular objectives of this zone are to:

- To enable land to be used for public open space or recreational purposes.
- To provide a range of recreational settings and activities and compatible land uses.
- To protect and enhance the natural environment for recreational purposes.
- To ensure sufficient public recreation areas are available for the benefit and use of residents of, and visitors to, North Sydney.

2. Permitted without consent

Environmental protection works.

3. Permitted with consent

Building identification signs; Business identification signs; Community facilities; Environmental facilities; Information and education facilities; Kiosks; Recreation areas; Recreation facilities (outdoor); Roads; Water recreation structures.

4. Prohibited

Any development not specified in item 2 or 3.

Source: North Sydney Local Environmental Plan, 2013.

6.2 **Appendix 2 - Definitions**

Organised Sport

Organised sport refers to sporting activities that require use of a marked playing surface provided and maintained by Council. Typically, organised sport is played by clubs, schools, commercial hirers and the like who have an organised administration and/or who are participating in formal competitions.

Fees are charged for exclusive use of marked playing surfaces by the groups listed above, and bookings must be made through Council's Customer Services Department.

No activities are to occur on sportsfields that are designated as 'closed'.

Informal Sport and Games

Informal sports and games are those that do not require use of a marked or designated playing surface. They may be played in Council's parks and reserves (including sportsgrounds when these are not being used for organised sport) free of charge, on a first come first served basis, as long as they comply with the requirements of the relevant Council Plan of Management, and do not significantly compromise the amenity of other park users.

In order to ensure the sportsfields continue to cater for needs of organised, fee-paying sporting groups, Council's sportsground maintenance activities take precedence over use of the sportsfields for informal sports and games. In addition, informal users must ensure they do not unduly damage the designated playing surfaces.

No activities are to occur on sportsfields that are designated as 'closed'.

Sportsground

Sportsgrounds are areas of land used primarily for active recreation involving organised sports. Sportsgrounds include the playing surface as well as related ancillary facilities including infrastructure (flood lights, goal posts, nets etc), amenities buildings (clubhouses, toilets, change rooms and storage areas), and other amenities (seating, shade, shelter, barbecues and picnic facilities).

Sportsfield/ Playing Field

The actual marked, or designated playing surface used for playing organised sport. While most playing surfaces in North Sydney are turf, other surfaces can include asphalt, acrylic/hard court and synthetic grass.

Sportsfields in North Sydney include ovals and other grassed playing fields, tennis courts, croquet courts, netball courts and the like.

6.3 **Appendix 3 - Maps**

Map 1 – Aerial Photograph of St Leonards Park, including cadastral information



DP 1149783 16-03-2010 Registered: Title System: CROWN LAND Purpose: CROWN LAND CONVERSION Reference Map: U1852-434 DP1117305 Last Plan: 316-3000 DP752067_UP109747_7321
PLAN OF CROWN LAND BEING CA152054 RESERVE 7321 Lengths are in metres. Reduction Ratio - NTS WARRINGAH EYO (1824 3000) Sheet 1 of 1 sheets NORTH SYDNEY LGA: 1108 NORTH SYDNEY LOCALITY: DP48839 WILLOUGHBY PARISH: COUNTY: CUMBERLAND THIS PLAN WAS PREPARED SOLELY TO IDENTIFY THE LAND ABOVE AND THE BOUNDARIES HAVE NOT BEEN INVESTIGATED BY THE REGISTRAR GENERAL THIS PLAN IS NOT A CURRENT PLAN IN TERMS OF S.7A CONVEYANCING ACT 1919 Drawn By: RV Signed Off: RW Office: NX

Map 2 – St Leonards Park - Lot and DP Information

DRAFT St Leonards Park Plan of Management

BON ANDREWS OVAL ST LEONARDS PARK NORTH-SYDNEY NORTH SYDNEY LEAGUES BOWLING CLUB DyWLING GREEN NORTH SYDNEY OVAL NETBALL COURTS Category Boundaries Extent of Reserve (Source: Crown Lands Portal Maps) Park Sportsground

Map 3 - Reserve no. 89592 (St Leonards Park) and Approved Category Boundaries

DRAFT St Leonards Park Plan of Management



Draft Plan of Management for St Leonards Park Native Title Manager Advice

The Crown Land Management Act 2016 – Section 8.7 (1) requires that Council as Crown Land Manager obtain written advice from its Native Title Manager prior to its approval or submission for approval of a Plan of Management for the land that authorises or permits any kinds of dealings referred to in Section 8.7.

Summary of Native Title Manager Advice

A. Proposed Act

Draft Plan of Management for St Leonards Park

B. Relevant Land

St Leonards Park R. 89592. Lots 1106-1107 DP 46990, Lot 7321 DP 1149783

C. Advice

In my opinion the proposed act will affect native title, however it complies with the applicable provisions of the *Native Title Act (Cwlth) 1993* being a valid future act under Subdivision J of the future acts regime. The proposed act meets the requirements for validity under Subdivision J that relate to areas that are subject to a reservation in accordance with Section 24JA of the *Native Title Act (Cwlth) 1993*.

As the draft Plan of Management for St Leonards Park authorises activities that could be considered to be future acts within the meaning of Section 233 of the *Native Title Act* (Cwlth) 1993, there are notification requirements.

Should native title be determined to exist, at some future date, Council may be liable for compensation under the provisions of the *Native Title Act (Cwlth) 1993* and the *Crown Land Management Act 2016*, for the impact on native title rights and interests by the proposed act.

Robert Emerson

Native Title Manager North Sydney Council

NORTH SYDNEY COUNCIL

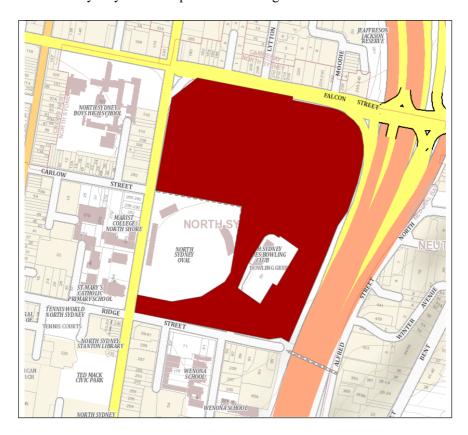
NATIVE TITLE MANAGER'S ADVICE

1. Description and Location of the Land

The land to which the draft Plan of Management for St Leonards Park applies is Crown Reserve 89592. It is contained in Lots 1106-1107 DP 46990, Lot 7321 DP 1149783. The land is known as St Leonards Park*. The Park occupies an area of 108,492 m2 in the suburb of North Sydney. The Crown is the owner of the land.

North Sydney Council is the Crown Land Manager of the land. The management and use of the land is subject to the provisions contained in the *Crown Land Management Act 2016*.

*The North Sydney Oval complex and Bowling Club are not included in Reserve 89592.



Crown Reserve 89592 (St Leonards Park) Council Crown Land Manager

2. Land Status

The status of Crown Reserve 89592 is as follows:

a) Not excluded land

A search of the National Native Title Register on 20/12/2019 determined that there are:

- no native title claims
- no ILUAs
- no native title determinations

NORTH SYDNEY COUNCIL

NATIVE TITLE MANAGER'S ADVICE

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b) Relevant land

Reserve Purpose: Public Recreation (gazette date 19/09/1975) Managed by North Sydney Council as crown land manager

c) Not Past Act

No current authority over the affected land was issued before 1994

The proposed act will affect native title and Native Title Manager advice is required.

3. Details of Activity on Crown Land

The draft Plan of Management for St Leonards Park has been prepared by North Sydney Council to provide for the effective management of this significant open space over the next 5-10 years. It examines the relevant issues in a comprehensive and holistic manner, identifies clear objectives for management, maintenance, and future development of the land It clarifies management policy and direction for Council staff and the general public.

The Plan of Management is required in accordance with Section 3.23 of the *Crown Land Management Act 2016* and Section 36 of the *Local Government Act 1993*.

The Plan of Management outlines the way in which St Leonards Park will be used and managed and provides the framework for Council to follow in relation to the leasing, licencing and permit processes for the land.

4. Is the Activity a Future Act?

The Plan of Management for St Leonards Park provides authorisation for the granting of leases, licences, permits and other estates over the land, and the granting of easements and further development of the land, buildings and infrastructure on the land. These activities could be considered to be 'future acts' within the meaning of Section 233 of the *Native Title Act (Cwlth)* 1993.

5. Validating Future Acts under the Native Title Act 1993

The Plan of Management provides strict guidelines such that each activity requires a Native Title Assessment and validation under the *Native Title Act (Cwlth) 1993* to be carried out by Council's Native Title Manager prior to the commencement or approval of that activity.

The Plan of Management is clear that the management of those activities that could be considered to be a future act must take into account the reserve purpose for the land.

The Plan of Management does not provide authorisation for any activity that is not in accordance with the reserve purpose of the land.

Accordingly, the activities authorised under the draft Plan of Management for St Leonards Park could be validated under Subdivision J of the *Native Title Act (Cwlth) 1993*.

NORTH SYDNEY COUNCIL

NATIVE TITLE MANAGER'S ADVICE

Subdivision J – Checklist

Requirements for validity of future acts under Subdivision J that relate to areas that are subject to a reservation

Requirement	Section 24JA	Requirement Satisfied
The reservation was created on or before 23 December 1996	(1)(a)	Yes
The reservation was valid	(1)(b)	Yes
The creation of the reservation was done by the Crown (the Commonwealth or State)	(1)(c)	Yes
The whole or part of any land or waters under the reservation was to be used for a particular purpose	(1)(d)	Yes
The future act (later act) is done in good faith under: (i) under or in accordance with the reservation (ii) in the area covered by the reservation, so long as the act's impact on native title is no greater than the impact that any act that could be done under or in accordance with the reservation of the land	(1)(e)	Yes

Note: Prior to validation under Subdivision J, Subdivisions B – JA were all considered but found not to be applicable.

6. Notification and Opportunity to Comment

The Plan of Management provides strict guidelines such that each activity requires a Native Title Assessment and validation under the *Native Title Act (Cwlth) 1993* to be carried out by Council's Native Title Manager prior to the commencement or approval of that activity.

Notification will be assessed on a case-by-case basis, and parties will be notified when an activity is assessed. The Native Title (Notices) Determination 2011 (No. 1) (Cwlth) prescribes how notification must occur.

Where Subdivision J validates the act, the following compliance actions are required to comply with the *Native Title Act (Cwlth)1993* where the future act is a 'public work' as defined in Section 253 of the *Native Title Act (Cwlth) 1993*.

Subdivision J		
Procedure	Requirements (for future acts that are public works)	
Notification of proposed act	Yes	
Provide opportunity to comment	Yes	
Consult with NT holders/ claimants	No	
Reporting	No	
Other	No	

As the draft Plan of Management for St Leonards Park authorises activities that could be considered to be future acts within the meaning of Section 233 of the *Native Title Act (Cwlth) 1993*, Council will notify NTS Corp and the Metropolitan Local Aboriginal Land Council prior to the adoption of the Plan of Management. A minimum of 30 days will be allowed to receive comments.

NORTH SYDNEY COUNCIL

NATIVE TITLE MANAGER'S ADVICE

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9. Notices of Motion NOTICE OF MOTION

9.1. Notice of Motion No. 5/20 - Cr Baker - Open Government and Public Participation Under COVID-19 Pandemic Arrangements

- **1. THAT** consistent with Council's adopted Code of Meeting Practice, the following arrangements be immediately facilitated for all Council meetings held remotely:
- A. all Council meetings to be live streamed/vodcast; and
- B. reinstate the public forum prior to all Council meetings with appropriate arrangements for remote participation/registration for residents to address Council to be put in place.

BACKGROUND

Nil.

General Manager's Comment

Clause 3.11 of the North Sydney Council Code of Meeting Practice (CoMP) states:

If the General Manager [read as GM or delegate] considers that a notice of motion submitted by a Councillor for consideration at an Ordinary Meeting has legal, strategic, financial or policy implications which should be taken into consideration by the meeting, the General Manager may prepare a report in relation to the notice of motion for inclusion with the business papers for the meeting at which the notice of motion is to be considered by Council.

(Part A) Councillors participating in the most recent Council meetings on 6 April and 27 April 2020 did so either by attendance in the Council Chamber or remote access using the Zoom audio visual platform.

While Ordinary Council meetings could be livestreamed, the picture quality and size of the Zoom presentation (showing the Minutes being updated and the Councillors participating remotely) is reduced to less than a quarter of the screen and will be difficult to follow. The size and picture quality of the recorded Zoom meeting that has been uploaded the day after the last two Council meetings enables the viewer to clearly see the Minutes and the Councillors participating remotely.

With respect to Committee Meetings, North Sydney Council's past practice has been to upload audio recordings the following day. This has recently been improved by changing the audio only recording to include video. Livestreaming of Committee meetings would require them to be relocated to the Council Chambers form the Supper Room. Duplication would also occur with set up and staffing as video Briefings, held prior to and post the Committee Meeting, will still need to be conducted in the Supper Room.

Council staff are continuing to improve video/telecom arrangements. The approach being taken by North Sydney Council has been of interest to our neighbouring Councils who have sought our assistance and advice in how to establish their own.

The Code of Meeting Practice also defines a webcast as "a video or audio broadcast of a meeting transmitted across the internet either concurrently with the meeting or at a later time." North Sydney Council's current arrangements fully comply.

(Part B) In March 2020, amendments were made to the Local Government Act 1993 to provide that the requirement under section 10 of the Act that members of the public be permitted to attend meetings is satisfied if the meeting is held remotely using audio-visual links. The amendment also provides that the requirement under section 10 for meetings to be open to members of the public is satisfied if a webcast (audio or visual, either livestreamed or recorded) of the meeting is made public.

To ensure that members of the public can continue to lobby to Councillors on Agenda items, submissions can be made by 4pm on the day of the meeting, which are sent to all Councillors.

Council could more firmly reinstate Public Forums by continuing past practice with the only exception being that the submissions are submitted in writing rather than verbally.

It is considered that allowing remote public participation in the meetings via video conference is not practicable for the following reasons:

- Not everyone has access to the equipment required to participate in an online meeting, which creates an inequality in the level of public access to Council meetings;
- There is no capacity to "pre-test" links or equipment from a large number of infrequent users. This will substantially add to the risk of technology failure;
- Zoom meetings have previously been subject to hacking, resulting in some agencies and
 organisations ceasing to use it. Security precautions have been put in place at North Sydney to
 mitigate this, these measures are not conducive to facilitating broader public participation;
- Audio and picture quality would deteriorate the more people are logged into the Zoom meeting;
- Delays as participants who are waiting to be admitted to the online meeting may not be ready or available when the item of interest is called.

During the period that the current arrangements for holding Council meetings has been conducted, no feedback has been received from members of the public requesting that the meetings be webcast live or that the Public Forums be reinstated.

A check of our neighbouring NSROC Councils - Mosman, Willoughby and Lane Cove - has revealed that they are taking same approach as Council with respect to Public Forums, suspending them for the duration in favour of written submissions.

10. Confidential Matters

In accordance with the Local Government Act 1993 and the Local Government (General) Regulation 2005, in the opinion of the General Manager, the following business is of a kind as referred to in section 10A(2) of the Act, and should be dealt with in a part of the meeting closed to the media and public.

Set out below is section 10A(2) of the Local Government Act 1993 in relation to matters which can be dealt with in the closed part of a meeting.

The matters and information are the following:

- (a) personnel matters concerning particular individuals (other than councillors).
- (b) the personal hardship of any resident or ratepayer.
- (c) information that would, if disclosed, confer a commercial advantage on a person with whom the Council is conducting (or proposes to conduct) business.
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- (e) information that would, if disclosed, prejudice the maintenance of law.
- (f) matters affecting the security of the Council, councillors, council staff or Council property.
- (g) advice concerning litigation, or advice that would otherwise be privileged from production in legal proceedings on the grounds of legal professional privilege.
- (h) information concerning the nature and location of a place or an item of Aboriginal significance on community land.

Councillors and staff are reminded of their statutory obligations in relation to the non-disclosure to any outside person of matters included in this section.

10.1. Ward Street Precinct Implementation Strategy - Civic Design Spaces Brief

AUTHOR: Tomas van der Meer, Senior Strategic Planner - Urban Design

ENDORSED BY: Marcelo Occhiuzzi, Acting Director City Strategy

ATTACHMENTS:

Nil

PURPOSE:

To seek Council's endorsement for the Civic Spaces Design Brief that is part of a Confidential Report to Council, which sets out the desired requirements for public benefits in the Ward Street Precinct, North Sydney CBD and their cost implications. This establishes the basis for confidential negotiations with private landowners/developers regarding the provision of public amenities and land.

EXECUTIVE SUMMARY:

In order to deliver much needed public domain and amenity improvements to the North Sydney CBD and identify commercial growth opportunities, Council has been undertaking the Ward Street Masterplan (WSMP) project.

On 24 June 2019, Council resolved to endorse the finalised Ward Street Masterplan and prepare an implementation strategy. A subsequent report was presented to Council at its meeting held 23 September 2019 on the Ward Street Precinct Implementation Strategy. The preparation of the Civic Spaces Design Brief is one of the actions under the strategy.

The purpose of the Civic Spaces Design Brief is to inform future negotiations and any voluntary planning agreement (VPA) relating to the development of 56-66 Berry Street and the Councilowned car park in Ward Street.

As the brief contains design proposals and cost reports that will inform future commercial negotiations, the detailed report it is kept as Commercial in Confidence.

The associated Confidential Report to this item addresses the details of the Civic Spaces Design Brief in further detail as the starting point of Council's Implementation Strategy and will be used by Council for further development negotiations. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(c) of the Local Government Act (LGA).

FINANCIAL IMPLICATIONS:

Up to \$150,000 has been identified in the LEP Acceleration Program grant funding to support the WSMP - Civic Design Spaces Brief in this financial year (2019/20). Of this budget, \$112,000 has been allocated and expended on consultancy fees including Urban Design, Landscape Architecture, Architecture, Quantity Surveying and Structural Advice associated with this work.

- **1. THAT** the meeting be closed to the public in accordance with Section 10A(2):
- (c) information that would, if disclosed, confer a commercial advantage on a person with whom the Council is conducting (or proposes to conduct) business.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 1. Our Living Environment
- 1.4 Public open space and recreation facilities and services meet community needs
- 2. Our Built Infrastructure
- 2.1 Infrastructure and assets meet community needs
- 2.2 Vibrant centres, public domain, villages and streetscapes
- 3. Our Future Planning
- 3.1 Prosperous and vibrant economy
- 3.2 North Sydney CBD is one of NSW's pre-eminent commercial centres
- 3.4 North Sydney is distinctive with a sense of place and quality design
- 4. Our Social Vitality
- 4.1 North Sydney is connected, inclusive, healthy and safe
- 4.2 North Sydney is creative and home to popular events
- 4.4 North Sydney's history is preserved and recognised
- 5. Our Civic Leadership
- 5.1 Council leads the strategic direction of North Sydney

CONSULTATION REQUIREMENTS

Community Engagement on the Ward Street Masterplan has previously been undertaken in accordance with Council's Community Engagement Protocol.

Community engagement is not required at this stage.

DETAIL

This report deals with design and associated cost implications which will inform Council's negotiations with a private party(ies) in pursuing the implementation of the Ward Street Masterplan. It is therefore prepared as a confidential report as it will deal with precursors to commercial negotiations associated with a future voluntary planning agreement including land negotiations.

10.2. Council Owned/Leased Properties - 3rd Quarterly Property Portfolio Report 2019/20 - January to March 2020

AUTHOR: Risha Joseph, Property Officer

ENDORSED BY: Duncan Mitchell, Director Engineering Services

ATTACHMENTS:

1. Waiving Charges During COVID-19 Pandemic Policy [10.2.1 - 4 pages]

EXECUTIVE SUMMARY:

The purpose of this report is to provide Council with the following information for the quarter ending 31 March 2020:

- Financial overview of the Property Portfolio;
- Vacancy Position of the Property Portfolio;
- Leasing Transactions for the quarter;
- Acquisitions and Disposals for the quarter; and
- Major Property Projects for the quarter.

FINANCIAL IMPLICATIONS:

The Year-to-Date income which is received on a <u>cash</u> basis by Colliers International is \$5,497,594 as at 31 March 2020, which is 72% of the annual forecast for this period. An overview of the income received is outlined in Section 1.

A detailed breakdown is outlined in the attached Confidential Report.

Covid-19 Pandemic Impacts:

The Property Portfolio income since the COVID-19 regulations were enforced by the Federal Government on 23 March 2020 has significantly impacted Council's income.

To provide support to Council's tenants and businesses who have leased premises or taken out licences under our Property Portfolio, Council implemented the "Waiving Charges during Novel Coronavirus (COVID-19) Policy" on 25 March 2020, refer to Attachment 1. The policy comprises of hardship assistance provisions that apply to the various tenants who have entered into leases or licences with Council.

Council's budgeted income from its entire Property Portfolio for 2019/20 financial year was forecast to be \$7,688,515. Due to the implementation of the "Waiving Charges during Novel Coronavirus (COVID-19) Policy", Council reviewed its revenue forecast which has now been amended down to \$6,327,499 at year end, which is approximately 18% lower than the original forecast. The full impact on the Property Portfolio revenue will be presented to Council in the Q4 Report.

RECOMMENDATION: 1. THAT the Quarterly Property Portfolio Report for the 3 rd Quarter of 2019/20 (January to March 2020) be received.

LINK TO COMMUNITY STRATEGIC PLAN

The relationship with the Community Strategic Plan is as follows:

- 2. Our Built Infrastructure
- 2.1 Infrastructure and assets meet community needs

BACKGROUND

This Quarterly Property Portfolio report is designed to give Council and the Community a detailed overview of the performance of Council Property Portfolio for the quarter ending 31 March 2020:

The report comprises both a Public and Confidential Report that provides detailed information on the following key business activities of the Property Portfolio:

- Consolidated summary of income received through the Property Portfolio;
- Property Portfolio Vacancy Rates;
- Leasing Transactions for the quarter;
- Major Property Projects (Maintenance and Capital works) for the quarter;
- Acquisitions and Disposals for the quarter; and
- Response to the Covid-19 Pandemic this matter is discussed in the Confidential Report.

As per the report to Council in August 2019, Council's General Manager has delegated authority, in accordance with Section 683 of the Local Government Act 1993, to enter into leases and will be the signoff authority for Council when entering into a lease. It is no longer a requirement for Council to have its Official Seal affixed to Leases pursuant to Section 400(4) of the Local Government (General) Regulation 2005.

To effectively manage Council's large and varied Property Portfolio, Council engages Property Consultants. The existing Property Management Services Contract was awarded to the current Management Consultants, Colliers International (NSW) Pty Ltd in 2019, with options to extend the contract based on performance on a 2 + 2-year basis up to a total of 6 years.

CONSULTATION REQUIREMENTS

Community engagement is not required.

DETAIL

Section 1: Overview of North Sydney Council's Property Portfolio's Financial Position –3rd Quarter -2019/2020

The following information is a consolidated summary of North Sydney Council's Property Portfolio financial performance for the 3rd Quarter – the period ending 1 January to 31 March 2020.

Total number of leases: 310 including 177 Outdoor Dining licences.

Total number of Outdoor Dining licences: 177

The total income received through Council's Property Portfolio as at 31 March 2020: \$5,497,594.

The total arrears for Council's Property Portfolio this quarter: Refer to Confidential Memorandum dated 20 April 2020.

Vacancy Position of the North Sydney Council Property Portfolio

The following Properties are currently listed as vacant in North Sydney Council's Property Portfolio. Council's Property Managing Agents – Colliers International manage all of Council's leasing transactions and are responsible for all aspects of lease negotiation and management of Council's Properties that are under lease.

As at 31 March 2020:

- Overall vacancy rate of Council's Property Portfolio: 8.59%
- Vacancy rate excluding short term leases: 3.94%

Section 2. Current Vacancies within the North Sydney Council Property Portfolio

Building	<u>Unit</u>	Area (m²)	Comments
Alfred St, 52 Milsons Point	Car Park Spaces	8	Colliers International current marketing car spaces for Lease
41 Alfred Street, Milsons Point	Community	286.2	Tenant vacated August 2019. Negotiations are underway to accommodate a community based tenant into this facility.
Hume St, 43 Crows Nest	Ground Floor	79.72	Vacated for proposed Hume Street Park Development – Building to be demolished
Hume St, 43 Crows Nest	Level 2	210	Vacated for proposed Hume Street Park Development – Building to be demolished

Building	<u>Unit</u>	Area (m²)	Comments
Hume Street, 36	Shop 1	83.8	Tenant vacated. Colliers
Crows Nest	1		marketing for a new tenant.
Hume St, 45-47	Ground	281	Vacated for proposed Hume
Crows Nest			Street Park Development –
			Building to be demolished
Hume St, 45-47	Parking	1	Vacated for proposed Hume
Crows Nest			Street Park Development –
			Building to be demolished.
Hume St, 49	Level 1	254.6	Vacated for proposed Hume
Crows Nest			Street Park Development –
			Building to be demolished
Hume St, 49	Level 2	198	Vacated for proposed Hume
Crows Nest			Street Park Development –
			Building to be demolished
James Place, 1	5.01	273.2	Tenant did not proceed. Colliers
North Sydney			International currently
			marketing property for lease.
Military Road, 190-192	3	21.7	Colliers International current
Neutral Bay			marketing for Lease
Munro Street, Shed	Shed	21.2	Colliers International current
McMahons Point			marketing Property for Lease
Pacific Highway, 80	Car Space	1	Colliers International current
North Sydney			marketing Property for Lease
Miller Street, 242 North	Shop	93	Tenant vacated 14th February
Sydney			2020. Colliers marketing for a
			replacement tenant.
Miller Street, 246	Shop &	63	An Architect has been engaged
North Sydney	Residential		to review major restoration
			options of the building as a
			result of the structural issues of
			this property.
283A Miller Street,	Shop 3	45	Colliers International marketing
North Sydney			for a new tenant.
Willoughby Rd, 90-92	Shop 1 & 2	188	Vacated for proposed Hume
Crows Nest			Street Park Development –
			Building to be demolished

Section 2(a): Summary of Vacancies as an Overall Area (m²) and % of the Total Property Portfolio

MONTH	January	February	March	Comments
Total Vacant Areas	2015.82	2054.42	2147.42	
(m^2)				
Portfolio NLA (m ²)	24,988	24,988	24,988	
Vacancy % of Portfolio				Vacancy rate high because this figure
NLA	8.07%	8.22%	8.59%	includes all Hume Street Park project

MONTH	January	February	March	Comments
				properties that can no longer be leased as they are earmarked for demolition as part of the Hume Street Park redevelopment.
Total Short-Term Lease				
Areas	1211.32	1211.32	1211.32	
Vacancy % of Portfolio NLA less Short-Term Lease Areas	3.38%	3.55%	3.94%	The vacancy rate excludes Hume Street Park properties - pending demolition for the Hume Street Park Development

Section 2(b): Parking Spaces for Lease in the Property Portfolio

MONTH	January	February	March	Comments
Total Vacant				Colliers International current marketing
Parking Spaces	10	10	10	Property for Lease
Total Parking				Most parking spaces are at 52 Alfred
Spaces under	12.00	12.00	12.00	Street.
Portfolio				

Note: The rental income information is submitted in a separate memorandum to Councillors.

Leasing Transactions for the Quarter

The following information is provided to Council in relation to the leasing transactions for Council's owned and leased properties for the period ending 31 March 2020.

Note: As per the report to Council in August 2019, Council's General Manager has the delegated Authority to enter into leases and will be the signoff authority for Council when entering into a lease. It is no longer a requirement for Council to have its Official Seal affixed to Leases pursuant to Section 400(4) of the Local Government (General) Regulation 2005, whereby:

- (a) the use of the Council seal only in circumstances where the Council has resolved that the seal is to be affixed.
- (b) the discontinuing of the use of the Council Seal relating to the following documents:
 - (i) The sale or purchase of land;
 - (ii) The entering into leases relating to Council property; and
 - (iii) The entering into of Voluntary Planning Agreements.

Section 3. Council's Leasing Transactions for the Period Ending 31 March 2020

Proposed Transaction	Subject Property & Lessee/ Tenant	Term	Permitted Use	Lettable Area	Terms of Lease / Remarks
Lease Renewal	Address: 3/43 Hume Street, Crows Nest	Period: In the quarter commencing May 2020 to the period ending June 2020	Residential	110m ²	Renewal of residential lease to coincide with Development plans for the site.

Proposed Transaction	Subject Property & Lessee/ Tenant	Term	Permitted Use	Lettable Area	Terms of Lease / Remarks
Assignment of Lease	Address: Roof of Parking Station 35-41 Ridge Street, North Sydney	01/05/2020 – 01/07/2020 Term: 9 weeks Option: Nil Period: In the quarter commencing March 2020 to the period ending December 2029 01/03/2020 – 31/12/2029 Term:	Recreational tennis Courts	N/A	The Lessee must pay the Lessor's reasonable costs in relation to the preparation of the Lease. Lessee is responsible for the cost of the registration of the lease.
New Lease	Address: Shop 2, O'Reilly Stand, 283A Miller Street, North Sydney	9 years Option: Nil Period: In the quarter commencing April 2020 to the period ending March 2022 01/04/2020— 31/03/2022 Term: 2 years Option: 2 years	Retail suite	45m ²	The Lessee must pay the Lessor's reasonable costs in relation to the preparation of the Lease. The lessee is responsible for the costs of the registration of the lease.
Licence Agreement	Address: 41 Alfred Street South, Milsons Point - Bradfield Park	Period: In the quarter commencing July 2020 to the period ending June 2025 01/07/2020— 30/06/2025 Term: 5 years Option: Nil	Telecommunications site	31.2	The Lessee must pay the Lessor's reasonable costs in relation to the preparation of the Lease. The lessee is responsible for the costs of the registration of the lease.
Licence Agreement	Address: 52 Alfred Street South, Milsons Point	Period: In the quarter commencing April 2020 to the period ending October 2020	Car park	N/A	Colliers International utilising standard licence agreement. No legal costs involved.

Proposed Transaction	Subject Property & Lessee/ Tenant	Term	Permitted Use	Lettable Area	Terms of Lease / Remarks
		21/04/2020– 20/10/2020 Term: 6 months Option: Nil			
New Lease	Address: 244 Miller Street, North Sydney	Period: In the quarter commencing June 2020 to the period ending May 2025 1/06/2020 to 31/05/2025 Term: 5 years Option: 5 years	Cafe	40m²	The Lessee must pay the Lessor's reasonable costs in relation to the preparation of the Lease. Lessee is responsible for the cost of the registration of the lease.
Assignment of Lease	Address: Shop 5, 34-48 Alexander Street, Crows Nest	Period: In the quarter commencing July 2020 the period ending May 2021 01/07/2020 – 24/05/2021 Term: 11 months Option: 2 X 5 years	Retail Liquor Store	127m²	Previous Assignment was from Jelax Pty Ltd to Woolworths Group Ltd. New Assignment of Lease is due to demerger of Woolworths Group Limited resulting from a Court order. The Lessee must pay the Lessor's reasonable costs in relation to the preparation of the Assignment. Lessee is responsible for the cost of the registration of the lease.

Proposed Transaction	Subject Property & Lessee/ Tenant	Term	Permitted Use	Lettable Area	Terms of Lease / Remarks
New Lease	Address: Shop 3, 190- 192 Military Rd, Neutral Bay	Period: In the quarter commencing August 2020 the period ending July 2023 01/08/2020 – 31/07/2023 Term: 3 years Option: 3 years	Barber Shop	30m ²	The lessor will pay for the initial preparation of the lease. Each party will be responsible for their own costs incurred to review the lease. The Lessee is responsible for the registration fees.

Refer to the Attachment dated 20 April 2020 (Confidential Report) for details relating to the rental income Council will receive through these leasing transactions.

Acquisitions and Disposal for the Quarter

Section 4. Acquisitions and Disposals within the North Sydney Council Property Portfolio

	Address	Reason	Date	Remarks
Acquisition	Nil			

	Address	Reason	Date	Remarks
Disposal	Nil			

Section 5: List of Property Projects for the Quarter (Major Periodic Maintenance and Upgrade)

Apart from routine maintenance for Council properties, the Property Asset Management Plan has identified major periodic maintenance and capital works upgrades for properties throughout Council's Property portfolio. These essential works need to be carried out in accordance with the program. These works are funded from the annual Property Capital Works program which is essential to avoid asset deterioration that would result in operational and functional degradation of the buildings.

Major Maintenance and Capital Works to Council Properties for the Last Quarter

Property	Project Description	Commencement Date	Completion Date
Alexander Street Car Park	New line marking and toilet signage of additional toilets on Level one	9/03/2020	27/03/2020
Central Depot	New roof and structure for storage	9/03/2020	10/04/2020
North Sydney Oval Grandstand	O'Reilly Stand repainting	10/02/2020	6/03/2020
North Sydney Oval Function Centre	Replace Fire Indicator Panel and fire system upgrade	16/03/2020	24/04/2020
Bradfield Park Community Centre	Internal repainting	16/03/2020	10/04/2020
Stanton Library	Raise balustrades height to meet current National Construction Code NCC2019	25/03/2020	15/04/2020

Alexander Street Car Park L1 Amenities-New Line Marking and Toilet Signage



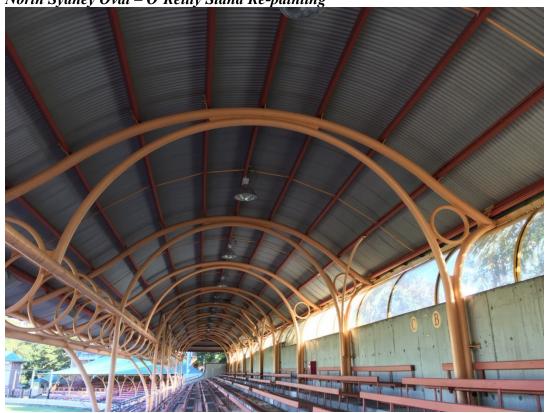


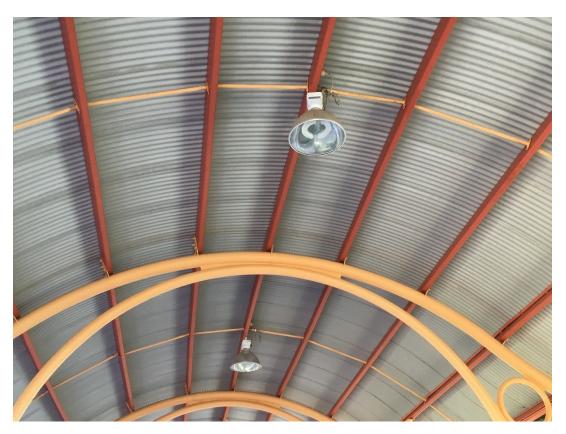






North Sydney Oval – O'Reilly Stand Re-painting

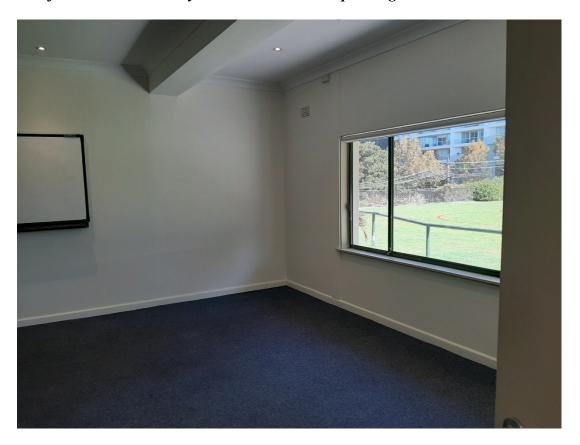




North Sydney Oval Function Centre – New Fire Indicator Panel



Bradfield Park Community Centre – Internal Re-painting





Stanton Library – Raise Balustrades Height to Current NCC Requirement







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D5-49

Policy Owner: General Manager

Category: 5. Our Civic Leadership

1. STATEMENT OF INTENT

- 1.1 On 11 March 2020, the World Health Organisation characterised novel coronavirus (COVID19) as a pandemic. As a result, significant measures have been put in place by the Federal and State Governments to manage the spread of COVID-19. These measures are having a significant impact on our community and local businesses who are suffering financial hardship as a result.
- 1.2 This Policy identifies support and/or waivers of lease fees or charges that Council will provide to community groups and businesses impacted by the Government response to COVID-19. Particularly those impacted by enforced closures.
- 1.3 This Policy is not exclusive. Further support and/or discretionary waivers may be applied as a result of another policy or application of discretionary authority in the application of charges.

2. ELIGIBILITY

2.1 Eligibility is identified within each provision. The support offered within this policy is discretionary and the General Manager (or delegate) has discretion to determine eligibility in accordance with the intent of this Policy.

3. **DEFINITIONS**

- 3.1 Lessee is a person or entity who rents land or property from a lessor (North Sydney Council). The Lessee is also known as the "tenant" and must uphold specific obligations as defined in the lease agreement that is executed between the Lessee and the Lessor by law.
- 3.2 Licensee is a person or entity that holds a current approved Outdoor Dining License agreement with North Sydney Council to conduct business on Council footpaths.
- 3.3 Small Business for the purposes of this Policy, is "an individual, partnership, company or trust that is carrying on a business and the business has an aggregated turnover of less than \$2 million per annum".

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- 3.4 Enforced Closure Period is the mandatory closure period in accordance with Public Health Orders concerning prohibited activities and venues. The enforced closure for cafes and restaurants commenced on 20 March 2020. The enforced closure period for community and recreational facilities commenced on 25 March 2020.
- 3.5 Demonstrating financial hardship for the purpose of this policy is defined as those Lessees (Tenants) and Licensees who are having severe difficulty in paying the rent/lease due on their Leases and Licences that they have with North Sydney Council as a result of the Government mandated closures to stop the spread of COVID-19 that became effective from 20 March 2020.

4. PROVISIONS

- 4.1 Outdoor Dining and Goods on Footpath Licences with Council:
 - a) Where a Licensee has had to temporarily close their business or change their trading practices as a result of Government regulation to manage the spread of COVID-19, Council will waive all Outdoor Dining and Goods on Footpath Licence fees for the duration of the enforced closure period, plus two (2) months to facilitate re-establishment from when the closure period is officially relaxed.
 - b) Where a Licensee has already paid in advance at the commencement of the enforced closure period, the prepaid term will be credited to the Licensee rather than refunded.
 - c) Council will waive interest charges on any existing arrears a Licensee currently has with Council.
- 4.2 Rental /Lease Fees for Community (Not for Profit) Groups Occupying Council Owned Community, Recreational and Sporting facilities under lease:
 - a) Council will waive all rental/lease fees associated with the tenancy agreement for the duration of the enforced closure period to manage the spread of COVID-19, plus two (2) months for re-establishment for when the closure period is officially relaxed.
 - b) Council will also waive any existing arrears a Lessee in this provision category had with Council at the commencement of the enforced closure.
- 4.3 Rental/Lease Fees for Cafés/Restaurants Occupying Council Owned Premises under lease:
 - a) Where a Lessee has had to temporarily close their business Council will waive all rental/lease fees for the duration of the enforced closure period, plus two (2) months for re-establishment from when the closure period is officially relaxed.

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- b) The rent/lease waiver also applies to those Lessee's providing take-away only services for the period of the enforced closure, however the additional two (2) months for re-establishment from when the closure period is officially relaxed does not apply.
- c) Council will also waive interest charges on any existing arrears a Lessee in this provision category had with Council for the period of the enforced closure plus two (2) months.
- d) Lessee's (Tenants) must ensure the leased premises remain in a clean and safe condition throughout the enforced closure period, and all other conditions of the Lease/Tenancy Agreement are met.

4.4 Cafés/Restaurants generally:

- a) Council will establish a prioritised temporary contact arrangement for Cafés/Restaurants seeking parking access arrangements to facilitate takeaway deliveries.
- b) These will be considered and approved on a case by case basis subject to the specific location and situation of the business.
- c) The service and associated approved temporary parking/access arrangement will be free of charge for the duration of the enforced closure period of Cafés/Restaurants.
- d) All requests for temporary parking access arrangements to facilitate takeaway deliveries must be made in writing via email to <u>takeawayaccess</u> <u>requests@northsydney.nsw.gov.au</u> and may take up to 72 hours to assess. Use email subject "Food Takeaway Parking Request".
- 4.5 Other Small Businesses which currently have an executed lease with North Sydney Council, the following will apply:
 - a) Where a Lessee can demonstrate that they are experiencing financial hardship and cannot make rental payments to Council as a result of Government regulation to manage the spread of COVID-19, the Lessee may make a request in writing to Council for rental waivers or abatement and show evidence with supporting documentation that they are experiencing financial hardship.
 - c) Where a Lessee has had to temporarily close their business as a result of Government regulation to manage the spread of COVID-19 and is suffering financial hardship, Council may waive all rental/lease fees associated with the tenancy agreement for the duration of the government enforced closure period, plus two (2) months for re-establishment from when the closure period is officially relaxed.
 - d) Where a Lessee is still operating and the business remains open, Council may provide a rent rebate of 50%.
 - e) Council may also waive the interest due on existing arrears a Lessee in this provision category currently has with Council.
 - b) Lessee's in this provision category will be considered on a case by case basis by the General Manager (or their delegate).

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5. RESPONSIBILITY/ACCOUNTABILITY

- 5.1 The General Manager (or delegate), may exercise discretion where considered reasonable and appropriate with regards to the application of all commercial and cost recovery fees, including any fees associated with an approval issued under Council's Local Laws during the National declared emergency related to the COVID-19 pandemic.
- 5.2 The Director Engineering and Property Services will be responsible for the implementation of this Policy.

6. RELATED POLICIES/DOCUMENTS/LEGISLATION

The Policy should be read in conjunction with the following Council policies and documents:

- Debt Management Policy
- Outdoor Dining and Goods on Footpath Policy
- Outdoor Dining Licence Agreement

The Policy should be read in conjunction with the following documents/legislation:

- Local Government Act 1993
- Retail Leases Act 1994

Version	Date Approved	Approved by	Resolution No.	Review Date
1	27 March 2020	Mayor/General	n/a	As required
		Manager		

10.3. North Sydney Olympic Pool - Licence Arrangements

AUTHOR: Rob Emerson, Director Open Space and Environmental Services

ENDORSED BY: Ken Gouldthorp, General Manager

ATTACHMENTS:

Nil

PURPOSE:

The report addresses the licence arrangements at North Sydney Olympic Pool.

EXECUTIVE SUMMARY:

The associated Confidential Report to this item addresses commercial information of a confidential nature. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(d) of the Local Government Act (LGA).

- **1. THAT** the meeting be closed to the public in accordance with Section 10A(2):
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

10.4. Tender No. 23/2020 Legal Services Panel

AUTHOR: Craig Winn, Solicitor

ENDORSED BY: Ken Gouldthorp, General Manager

ATTACHMENTS:

Nil

PURPOSE:

To appoint the Legal Services Panel.

EXECUTIVE SUMMARY:

Tenders were called and were received until 4pm, 11 February 2020 for the submission of tenders to undertake Legal Services.

The associated Confidential Report to this item addresses commercial information of a confidential nature. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(d) of the Local Government Act (LGA).

- **1. THAT** the meeting be closed to the public in accordance with Section 10A(2):
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

10.5. Tender No. 28/2020 Sawmillers Reserve & Hayes Street Wharf Reconstruction

AUTHOR: Robert Esdaile, Engineering Project Manager

ENDORSED BY: Duncan Mitchell, Director Engineering Services

ATTACHMENTS:

Nil

PURPOSE:

Tenders were called for the reconstruction of the existing Sawmillers Reserve and Hayes Street Wharves as a significant number of the timber components within the wharf structures have reached the end of their asset lives and need replacement. The works will comprise the removal and replacement of damaged timber piles, capwhales, girders and decking and other associated works.

EXECUTIVE SUMMARY:

Tenders were called and were received until 4pm, 6 May 2020 for the submission of tenders to undertake the Sawmillers Reserve & Hayes Street Wharf Reconstruction - Tender No. 28/2020.

The associated Confidential Report to this item addresses commercial information of a confidential nature. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(d) of the Local Government Act (LGA).

FINANCIAL IMPLICATIONS:

Funding for the project is appropriate.

- 1. THAT the meeting be closed to the public in accordance with Section 10A(2):
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

SAWMILLERS RESERVE WHARF – EXISTING CONDITION





HAYES STREET WHARF – EXISTING CONDITION



10.6. Tender No. 29/2020 Anderson Park Landscape Upgrade - Stage One

AUTHOR: Alicja Batorowicz, Landscape Architect/Project Co-ordinator

ENDORSED BY: Rob Emerson, Director Open Space and Environmental Services

ATTACHMENTS:

Nil

PURPOSE:

The purpose of this report is to provide Council with an analysis of the tender process for Tender No. 29/2020 Anderson Park Landscape Upgrade – Stage One.

EXECUTIVE SUMMARY:

Following the adoption of 2019 Anderson Park Plan of Management, which provides an overall framework for the management of this significant park and identifies strategies for improvement, money has been allocated in 2019/20 budget to implement Stage One of the identified works.

Stage One of the implementation focuses on the south-eastern area of the park located along the foreshore of Neutral Bay and intersected by a pedestrian access link between Kurraba Road and Clark Road.

In line with the objectives identified in 2019 Anderson Park POM, Stage One proposal aims to address the following recommendations:

Foreshore path upgrade – upgrade of East-West access path surface and edging.

Memorial – improvement of accessibility through an elimination of raised paving area, installation of low planting at the base of the memorial fence and seating, repair to the heritage fence.

Park furniture – upgrade park furniture to a coordinated suite of elements in accordance with Council's Public Domain Style Manual and Design Code 2018. Refine locations and placement to be most effective and to serve park use. Minimise visual clutter through relocation of a bubbler, a light pole and seats.

Lawn – reduction to the existing slope gradient of the foreshore grassed space adjoining seawall to increase the usability.

On 19 March 2020 Council informed 211 registered stakeholders about the progress of the Stage One implementation works.

The associated Confidential Report to this item addresses commercial information of a confidential nature. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(d) of the Local Government Act (LGA).

- **1. THAT** the meeting be closed to the public in accordance with Section 10A(2):
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

10.7. Tender No. 30/2020 Blues Point Reserve Amenities Refurbishment

AUTHOR: Alicja Batorowicz, Landscape Architect/Project Co-ordinator

ENDORSED BY: Rob Emerson, Director Open Space and Environmental Services

ATTACHMENTS:

Nil

PURPOSE:

The purpose of this report is to provide Council with an analysis of the tender process for Tender No. 30/2020 Blues Point Reserve Amenities Refurbishment.

EXECUTIVE SUMMARY:

The proposal is for a complete refurbishment of the existing amenities building located in Blues Point Reserve at the end of Blues Point Road in McMahons Point.

The upgrade aims to address the poor condition of the existing toilet amenities and includes the following: replacement of existing roof timber base panels, metal sheeting and skylights, tile re-grouting and replacement, painting, replacement of all fixtures and lighting, concreting and upgrade to meet current AS1428 standards for Accessibility. Within the scope of works there was also a provision made for the capacity investigation of the existing sewer line for future temporary installation of WC amenities for major events e.g. New Year's Eve and coordination of work with Sydney Water.

The associated Confidential Report to this item addresses commercial information of a confidential nature. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(d) of the Local Government Act (LGA).

- **1. THAT** the meeting be closed to the public in accordance with Section 10A(2):
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

10.8. Tender No. 3/2021 - GPT Cleaning, Pipe Cleaning and CCTV Camera Inspections

AUTHOR: Jim Moore, Engineering Project Manager

ENDORSED BY: Duncan Mitchell, Director Engineering Services

ATTACHMENTS:

Nil

PURPOSE:

This report is for Tender No. 3/2021 - GPT Cleaning, Pipe Cleaning and CCTV Inspections.

EXECUTIVE SUMMARY:

Tenders were called and were received until 4pm, 28 April 2020 for the submission of tenders to undertake GPT Cleaning, Pipe Cleaning and CCTV Inspections.

The associated Confidential Report to this item addresses commercial information of a confidential nature. If the Council wishes to discuss the report, the meeting should be closed to the public to do so in accordance with s10A(2)(d) of the Local Government Act (LGA).

RECOMMENDATION:

- 1. THAT the meeting be closed to the public in accordance with Section 10A(2):
- (d) commercial information of a confidential nature that would, if disclosed;
 - (i) prejudice the commercial position of the person who supplied it, or
 - (ii) confer a commercial advantage on a competitor of the Council, or
 - (iii) reveal a trade secret.
- **2. THAT** the report be treated as confidential and remain confidential until Council determines otherwise.

11. Closure